

CALIFORNIA COASTAL COMMISSION

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DSL/5/23

**Th8a****RECORD PACKET COPY**

May 23, 2002

TO: Commissioners and Interested Persons

FROM: Charles Lester, Acting Deputy Director
Susan Craig, Coastal Planner

SUBJECT: **CITY OF SANTA CRUZ: LOCAL COASTAL PROGRAM MAJOR AMENDMENT NO. 1-01 (Part B).** For public hearing and Commission action at its meeting of June 13, 2002, to be held in Long Beach at the Queen Mary, 1126 Queens Highway, Long Beach, CA 90802.

SYNOPSIS

The City of Santa Cruz is proposing to replace the policies of the Beach Area Plan with new policies in the *Beach and South of Laurel (BSOL) Area Comprehensive Area Plan*, amend the land use designation map to re-designate five sites in the BSOL area, amend the Design Guidelines for the Beach area of the City (except for the Wharf Design Guidelines), and establish Design Guidelines for the South of Laurel area of the City. The City is also amending its Zoning Ordinance (implementation plan) to amend six existing zoning districts in the BSOL area, to add three new zoning districts (one residential and two commercial) in the BSOL area, to rezone seven areas within the BSOL area (zoning map amendment), and to amend the zoning map to apply two new overlay districts within the BSOL area. The primary purposes of the proposed LCP changes are to provide a revitalization program for the BSOL area of the City, to improve the visual and aesthetic qualities of the BSOL area through the application of design guidelines, to reduce reliance upon automobiles and encourage public transit, and to ensure neighborhood preservation and revitalization and provide for stringent controls on large scale economic/residential development. Specifically, the City is requesting the following changes to its certified Local Coastal Program:

1. Amend the zoning ordinance to include a new residential zone: RH (Multiple Residence/High Density District. (Implementation Plan Amendment).
2. Amend the zoning ordinance to include two new commercial zones: RTC/PER (Beach Commercial/Performance Overlay) and RTB/PER (Motel Residential/Performance Overlay) (Implementation Plan Amendment).
3. Amend the zoning ordinance to revise six existing zones: 1) RTD (Beach Residential)(reduce allowable residential density; require conformance with Design Guidelines; limit allowable commercial uses); 2) RTA (reduce allowable residential density; require conformance with Design Guidelines; delete inappropriate uses); 3) RTB (Motel/Residential) (reduce allowable residential density; require conformance with Design Guidelines; delete inappropriate uses); 4) RTC (Tourist Residential/Beach Commercial) (reduce allowable residential density; require

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conformance with Design Guidelines; establish stringent development controls; 5) CBD/E (Lower Pacific Avenue Subdistrict) (delete quasi-industrial uses; require conformance with Design Guidelines; encourage live/work environment; and 6) CB (Beach Commercial) (require conformance with Design Guidelines; prohibit ferris wheels and roller coasters north and west of Beach Street).

4. Rezone the following six areas within the Beach/South of Laurel areas as follows:
 - A. Rezone the South of Laurel Depot Site and adjacent properties fronting Washington Street from RTC (Tourist Residential/Beach Commercial) to PF/PK (Public Facility/Parks);
 - B. Rezone the South of Laurel area between Center Street, west of Pacific Avenue and south of Sycamore Street from CT (Thoroughfare Commercial) to RM (Multiple Residence Medium Density), with an MU (Mixed Use) overlay on the western portion;
 - C. Rezone the South of Laurel area bordering Front Street and the San Lorenzo River from RM/MU (Multiple Residence Medium Density/Mixed Use) to RH/MU (Multiple Residence High Density/Mixed Use);
 - D. Rezone the northeast corner of Second Street and Front Street (in the Beach Hill area) from RTA (High Density Residential) to RTB (Motel Residential);
 - E. Rezone an area west of Cliff Street between First Street and Second Street from RTB (Motel Residential) to RTC (Tourist Residential/Beach Commercial);
 - F. Rezone a portion of the Beach Hill area bordered by Cliff Street, Wave Lane, Holden Avenue (east of Riverside Avenue), and Second Street from RTB (Motel Residential) to RTC (Tourist Residential/Beach Commercial);
5. In addition to the zoning map changes to the underlying zoning districts described in #4 above, amend the zoning map to apply two overlay zone districts above as follows:
 - A. Apply the CON overlay district (Neighborhood Conservation Overlay District) to the following four neighborhoods:
 - Myrtle Street, south of Laurel Street
 - A portion of Washington Street, south of Laurel Street
 - Cedar Street, between Pacific Avenue and Center street south of Laurel Street
 - Pacific Avenue, between Front Street and Second Street;
 - B. Apply the PER overlay district (Performance Overlay District) to the following two areas:
 - The parking lot adjacent to the West Coast Santa Cruz Hotel (former Dream Inn)
 - The Main Beach/Boardwalk parking lot



6. Replace all the certified policies of the existing *Beach Area Plan* with new policies in the *Beach and South of Laurel Comprehensive Area Plan* (Land Use Plan Amendment).
7. Amend the certified Design Guidelines (Land Use Plan Amendment), except for the Wharf Design Criteria, which will be amended upon completion of a proposed wharf study.
8. Amend the land use designation map as follows:
 - A. Re-designate the Depot Site and adjacent properties that front Washington Street from RVC (Regional Visitor Serving Commercial) to PK/CF (Parks/Community Facility);
 - B. Re-designate the South of Laurel area between Center Street and Pacific Avenue from CM (Community Commercial) to M (Medium Density Residential);
 - C. Re-designate the South of Laurel area bordering Front Street and the San Lorenzo River from M (Medium Density Residential) to H (High Density Residential);
 - D. Re-designate the Beach Commercial area west of Cliff Street and south of Second Street from M (Medium Density Residential) to RVC (Regional Visitor Serving Commercial);
 - E. Re-designate the Beach Hill area bordered by Cliff Street, Wave Lane, Leibrandt Avenue, and Second Street from M (Medium Density Residential) to RVC (Regional Visitor Serving Commercial).

This amendment, which constitutes the majority of a larger amendment submittal, was filed on October 10, 2001. The Planning Commission and the City Council held noticed public hearings. The Commission adopted Part A of the amendment, regarding rezoning and re-designation of a portion of the Beach Flats area, at the meeting of December 13, 2001. Also on that date, the Commission voted to extend the 90-day time limit to act on Part B of the amendment for a period not to exceed one year.

SUMMARY OF STAFF RECOMMENDATION

Staff has reviewed the proposed amendment to the Land Use Plan for conformance with the Coastal Act and the Implementation Plan amendment for consistency with the proposed amended Land Use Plan. Issues raised by the proposed amendment include community character, density, visitor-serving and recreational uses, and public access. As discussed in detail below, Staff recommends **approval** of the City of Santa Cruz Local Coastal Program proposed Land Use Plan Major Amendment No. 1-01 (Part B) if it is modified to add and/or modify a number of policies regarding community design, land use, community facilities, and circulation. Staff recommends **approval** of Implementation Zoning Ordinance Major Amendment No. 1-01 (Part B) if it is modified to further protect historic community character.

ANALYSIS CRITERIA

The Commission certified the City of Santa Cruz's Land Use Plan in July 1981. The Implementation Plan was certified in April 1985 and the City assumed coastal development permit authority that year. The City has organized and submitted this LCP amendment request in accordance with the standards for



amendments to certified LCPs (Coastal Act Section 30514, California Code of Regulations 13551 through 13553).

The proposed amendment affects the IP and LUP components of the City of Santa Cruz LCP. The standard of review for land use plan amendments is that they must be consistent with the Chapter 3 policies of the Coastal Act. The standard of review for implementation amendments is that they must be consistent with and adequate to carry out the policies of the certified coastal land use plan.

ADDITIONAL INFORMATION

Further information on the submittal may be obtained from Susan Craig at the Central Coast District Office of the Coastal Commission at 725 Front Street, Suite 300, Santa Cruz, CA 95060, (831) 427-4863.

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I. STAFF RECOMMENDATION: MOTIONS AND RESOLUTIONS

Staff recommends adoption of the following resolutions:

Resolution I. (Resolution to approve City of Santa Cruz Land Use Plan Major Amendment No. 1-01 (Part B) as submitted)

Staff recommends a **NO** vote on the motion below. Failure of this motion will result in denial of the land use plan amendment component as submitted and adoption of the following resolution and findings. The motion passes only upon an affirmative vote of a majority of the appointed Commissioners.

Motion #1: I move that the Commission certify Major Amendment No. 1-01 (Part B) to the City of Santa Cruz Land Use Plan as submitted by the City.

Resolution to Deny: The Commission hereby denies certification of Major Amendment No. 1-01 (Part B) to the land use plan of the City of Santa Cruz as submitted and adopts the findings set forth below on the grounds that the amendment component, as submitted, does not conform with the policies of Chapter 3 of the Coastal Act.



Resolution II. (Resolution to approve City of Santa Cruz Land Use Plan Major Amendment No. 1-01 (Part B), if modified)

Staff recommends a **YES** vote on the motion below. Passage of this motion will result in certification of the amendment component with the suggested modifications and adoption of the following resolution and findings. The motion to certify with the suggested modifications passes only upon an affirmative vote of a majority of the appointed Commissioners.

***Motion #2:** I move that the Commission certify Major Amendment No. 1-01 (Part B) to the City of Santa Cruz Land Use Plan as submitted by the City, if modified as suggested by modifications #1-5 in this staff report.*

***Resolution to approve:** The Commission hereby approves Major Amendment No. 1-01 (Part B) to the land use plan of the City of Santa Cruz if modified according to the suggested modifications and adopts the findings set forth below on grounds that the land use plan amendment with the suggested modifications will meet the requirements of and be in conformity with the policies of Chapter 3 of the Coastal Act. Certification of the land use plan amendment if modified as suggested complies with the California Environmental Quality Act because either 1) feasible mitigation measures and/or alternatives have been incorporated to substantially lessen any significant adverse effects of the plan on the environment, or 2) there are no further feasible alternatives and mitigation measures that would substantially lessen any significant adverse impacts which the land use plan amendment may have on the environment.*

Resolution III. (Resolution to approve City of Santa Cruz Implementation Plan Major Amendment No. 1-01 (Part B) as submitted)

Staff recommends a **YES** vote on the motion below. Passage of this motion will result in rejection of the Implementation Plan amendment and the adoption of the following resolution and findings. The motion passes only by an affirmative vote of a majority of the Commissioners present.

***Motion.** I move that the Commission reject Major Amendment #1-01 (Part B) to the City of Santa Cruz Local Coastal Program Implementation Plan as submitted.*

***Resolution to reject.** The Commission hereby rejects certification of Major Amendment #1-01 (Part B) to the Implementation Plan of the City of Santa Cruz Local Coastal Program, as submitted, and adopts the findings set forth below on grounds that the Implementation Plan, as submitted, is not in conformity with the certified Land Use Plan. Certification of the Implementation Plan amendment would not meet the requirements of the California Environmental Quality Act as there are feasible alternatives and mitigation measures that would substantially lessen the significant adverse impacts on the environment that will result from certification of the Implementation Plan amendment as submitted.*



Resolution IV. (Resolution to approve City of Santa Cruz Implementation Plan Major Amendment No. 1-01 (Part B), if modified)

Staff recommends a YES vote on the motion below. Passage of this motion will result in rejection of the Implementation Plan amendment and the adoption of the following resolution and findings. The motion passes only by an affirmative vote of a majority of the Commissioners present.

***Motion.** I move that the Commission certify Major Amendment #1-01(Part B) to the City of Santa Cruz Local Coastal Program Implementation Plan if modified as suggested by modification 6 in this staff report.*

***Resolution to certify.** The Commission hereby certifies Major Amendment No. 1-01 to the Implementation Plan of the City of Santa Cruz Local Coastal Program, as modified by suggested modification 6, and adopts the findings set forth below on grounds that the Implementation Plan amendment with the suggested modification will be in conformity with and adequate to carry out the certified land use plan. Certification of the Implementation Plan amendment, if modified as suggested, complies with the California Environmental Quality Act, because either 1) feasible mitigation measures and/or alternatives have been incorporated to substantially lessen any significant adverse effects of the Implementation Plan amendment on the environment, or 2) there are no further feasible alternatives and mitigation measures that would substantially lessen any significant adverse impacts on the environment.*

II. SUGGESTED MODIFICATIONS

The Commission hereby suggests the following changes to the proposed Local Coastal Program amendments, which are necessary to make the requisite findings. If the local government accepts the suggested modifications within six months of Commission action, by formal resolution of the City Council, the corresponding amendment portion will become effective upon Commission concurrence with the Executive Director finding that this has been properly accomplished.

***Note:** The Commission-suggested modifications are shown by deleting existing text with ~~strikethrough~~ and adding text with underline.*

Modification #1

Apply “wave” symbols to the following General Plan Community Design, Land Use, and Community Facilities policies to effectively make these policies part of the City of Santa Cruz Local Coastal Program:

- a. General Plan Community Design Policies 1.2, 1.3, 1.4, 1.5, 1.6, 1.9, 1.10, 1.13, 1.15, 1.16, 1.17, 1.18, and 1.19; General Plan Land Use Policies 2.2, 2.5, 2.9, 2.10, and 2.15;
- b. General Plan Land Use Policy 2.12
- c. General Plan Community Facilities Policy 6.1. Also amend this policy to state: Develop the depot site as a focal point of community activity by providing uses including, but not limited to, parking, a small scale multi-modal transit facility, and recreational open space. (P. 148)



Modification #2

Amend LCP Land Use policy 2.8 to add the following language:

If future proposed development for the Main Beach parking lot site is incompatible with retention of the historical cottages that occupy the eastern portion of the parking lot, the cottages shall be preserved and relocated to another appropriate site or sites within the City or County of Santa Cruz, with preference given to relocating the cottages within the Beach and South of Laurel Area.

Modification #3

Add new Land Use Policy 2.16 to the Land Use section of BSOL Plan:

The La Bahia shall be redeveloped as a visitor accommodation use available to the general public. If the La Bahia is converted to visitor-serving condominiums in order to fund the renovation project, restrict use of the condominiums by individual owners to no more than 45 days per year.

Modification #4

Add new Land Use Policy 2.17 to the Land Use section of BSOL Plan:

Future development of the West Coast Santa Cruz Hotel overflow parking lot may consist of medium density residential use, bed and breakfast use, or ancillary hotel/motel uses. Hotel/motel development is not allowed.

Modification #5

Add new Circulation Policy 3.34 to the Circulation section of the BSOL Plan:

Maintain a minimum of 3,690 public parking spaces in the overall Beach and South of Laurel Area and approve no projects that would reduce this number below the floor figure of 3,690.

Modification #6

Amend Section 24.10.625.5 of the proposed Zoning Ordinance (District Regulations for RTC/PER Subdistrict) as follows:

4. If new development on the Main Beach parking lot is incompatible with retention of the historic cottages located on the eastern portion of the site, relocate the cottages to another appropriate site or sites within the City or the County of Santa Cruz, with preference given to relocating the cottages within the Beach and South of Laurel area.



III. FINDINGS AND DECLARATIONS

The Commission finds and declares as follows:

A. Amendment Description

The City of Santa Cruz (Exhibit 1) is proposing to amend the existing *Beach Area Plan* section of its certified Local Coastal Program with a new *Beach and South of Laurel (BSOL) Comprehensive Area Plan* (see Exhibit 2 for map of BSOL area). For the following proposed amendments, please see Exhibit 3 for the proposed land use map amendments, Exhibit 4 for the proposed new policies, Exhibit 5 for the proposed new Design Guidelines, Exhibit 6 for the proposed zoning map amendments, Exhibit 7 for the proposed zoning ordinance amendments, and Exhibit 8 for a zoning summary that details the effects of each zoning change.

The specific changes include:

1. Amend the zoning ordinance to include one new residential zone: the RH (Multiple Residence/High Density District) (Implementation Plan Amendment). The purpose of the RH district is to preserve the General Plan's desire to provide high density residential housing (up to 55 dwelling units per acre) in the South of Laurel area (in this case, along the San Lorenzo River). The RH zone will have an MU (Mixed Use) overlay which will encourage the construction of street level retail in conjunction with residential uses above.
2. Amend the zoning ordinance to include two new commercial zones: RTC/PER (Tourist Residential/Beach Commercial/Performance Overlay Zone) and RTB/PER (Motel Residential/Performance) (Implementation Plan Amendment). The purpose of the RTC/PER zone is to identify areas of critical public interest in the Beach Area (i.e., the Main Beach parking lot) and to provide regulations and standards necessary to achieve planning goals for development and/or redevelopment in a manner that protects neighborhood integrity while stimulating appropriate economic development. The RTB/PER zone will be applied to the overflow parking lot across from the West Coast Santa Cruz Hotel. The purpose of the RTB/PER zone is to ensure development that protects neighborhood integrity while supporting appropriate uses, including residential use and ancillary hotel support facilities.
3. Amend the zoning ordinance to revise six existing zones: 1) RTD (Beach Residential)(reduce allowable residential density; require conformance with Design Guidelines; limit allowable commercial uses); 2) RTA (reduce allowable residential density; require conformance with Design Guidelines; delete inappropriate uses); 3) RTB (Motel/Residential) (reduce allowable residential density; require conformance with Design Guidelines; delete inappropriate uses); 4) RTC (Tourist Residential/Beach Commercial) (reduce allowable residential density; require conformance with Design Guidelines; establish stringent development controls; 5) CBD/E (Lower Pacific Avenue Subdistrict) (delete quasi-industrial uses; require conformance with Design Guidelines; encourage live/work environment; and 6) CB (Beach Commercial) (require conformance with Design Guidelines; prohibit ferris wheels and roller coasters north and west of Beach Street).



4. Rezone the following six areas within the Beach/South of Laurel areas as follows:
 - A. Rezone the South of Laurel Depot site and adjacent properties fronting Washington Street from RTC (Tourist Residential/Beach Commercial) to PF/PK (Public Facility/Parks). The purpose of the revised zoning is to accommodate a planned park, multi-modal transportation station, museum site, and parking;
 - B. Rezone the South of Laurel area between Center Street, west of Pacific Avenue, and south of Sycamore Street from CT (Thoroughfare Commercial) to RM (Multiple Residence Medium Density), with an MU (Mixed Use) overlay on the western portion. The purpose of the revised zoning is to form a stronger residential/commercial separation between the visitor serving commercial area along Pacific Avenue and the South of Laurel residential area;
 - C. Rezone the South of Laurel area bordering Front Street and the San Lorenzo River from RM/MU (Multiple Residence Medium Density/Mixed Use) to RH/MU (Multiple Residence High Density/Mixed Use). The purpose of the revised zoning is to promote a more active river levee and facilitate local housing needs. The purpose of the MU overlay is to encourage street level commercial development along Front Street, with residential use above;
 - D. Rezone the Northeast Corner of Second Street and Front Street (in the Beach Hill area) from RTA (High Density Residential) to RTB (Motel Residential). The site currently has an existing non-conforming 22-room motel. The rezoning will encourage upgrades consistent with the historic nature of Beach Hill neighborhood;
 - E. Rezone an area west of Cliff Street between First Street and Second Street from RTB (Motel Residential) to RTC (Tourist Residential/Beach Commercial). The purpose of this rezoning is to promote reinvestment in the area, to create a strong protective edge for the residential area, and to allow supporting commercial services for existing lodging in the area;
 - F. Rezone a portion of the Beach Hill area bordered by Cliff Street, Wave Lane, Holden Avenue (east of Riverside Avenue), and Second Street from RTB (Motel Residential) to RTC (Tourist Residential/Beach Commercial). The purpose of this rezoning is to promote reinvestment in the area, to create a strong protective edge for the residential area, and to allow supporting commercial services for existing lodging in the area.
5. In addition to the zoning map changes to the underlying zoning districts described in #4 above, amend the zoning map to apply two overlay zone districts as follows:
 - A. Apply the CON (Neighborhood Conservation Overlay District) to the following four neighborhoods:
 - Myrtle Street, south of Laurel Street
 - A portion of Washington Street, south of Laurel Street
 - Cedar Street, between Pacific Avenue and Center street south of Laurel Street
 - Pacific Avenue, between Front Street and Second Street



The purpose of the CON overlay is to identify neighborhoods that are in need of special conservation and preservation strategies.

B. Apply the PER zone (Performance Overlay Zone) to the following two areas:

- The parking lot adjacent to the West Coast Santa Cruz Hotel (formerly the Dream Inn)
- The Main Beach/Boardwalk parking lot

The purpose of the PER overlay for the West Coast Santa Cruz Hotel parking lot is to establish and control uses to ensure development that protects neighborhood integrity while supporting appropriate uses. The purpose of the PER overlay for the Main Beach/Boardwalk parking lot is to identify this area as a critical area of public interest in the Tourist Residential/Beach Commercial area and to provide regulations and standards necessary to achieve planning goals for development in a manner that protects neighborhood integrity while stimulating appropriate economic development.

6. Replace all the certified policies of the existing *Beach Area Plan* with new policies in the proposed *Beach and South of Laurel Area Comprehensive Area Plan* (Land Use Plan Amendment). The purpose of this portion of the amendment is to provide the policy base for the proposed zone district text and map changes, and to provide a policy base for the amended Design Guidelines (see #7 below).
7. Amend the certified Design Guidelines (Land Use Plan Amendment), except for the Wharf Design Criteria, which will be amended upon completion of a proposed wharf study. The purpose of amending the Design Guidelines is to provide more specificity regarding the design for new development in the Beach and South of Laurel areas of the City.
8. Amend the land use designation map as follows:
 - A. Re-designate the Depot Site and adjacent properties that front Washington Street from RVC (Regional Visitor Serving Commercial) to PK/CF (Parks/Community Facility). The revised designation can accommodate a proposed future multi-modal transportation station, park, museum site, playing fields, and parking;
 - B. Re-designate the South of Laurel area between Center Street and Pacific Avenue from CM (Community Commercial) to M (Medium Density Residential). The purpose of this change is to form a stronger residential/commercial separation between the visitor serving commercial areas long Pacific Avenue and the South of Laurel residential area;
 - C. Re-designate the South of Laurel area bordering Front Street and the San Lorenzo River from M (Medium Density Residential) to H (High Density Residential) to promote a more active river levee and to facilitate the City's housing needs;
 - D. Re-designate the area west of Cliff Street and south of Second Street from M (Medium Density Residential) to RVC (Regional Visitor Serving Commercial). The purpose of this re-



designation is to promote reinvestment in the area, to create a strong protective edge for the adjacent residential area, and to allow supporting commercial services for existing lodging facilities in the area;

- E. Re-designate the Beach Hill area bordered by Cliff Street, Wave Lane, Leibrandt Avenue, and Second Street from M (Medium Density Residential) to RVC (Regional Visitor Serving Commercial). As above, the purpose of this re-designation is to promote reinvestment in the area, to create a strong protective edge for the adjacent residential area, and to allow supporting commercial services for existing lodging facilities in the area.

Part A of this amendment, which amended land use policies, the land use map, Design Guidelines, the zoning map, and the zoning ordinance for a portion of the Beach Flats area (which is in the BSOL area), was approved at the Commission hearing of December 13, 2001.

B. Analysis of Land Use Plan Amendments

1. Design Guidelines/Community Character

Coastal Act Section 30253(5) addresses community character issues and states:

New development shall: (5) Where appropriate, protect special communities and neighborhoods which, because of their unique characteristics, are popular visitor destination points for recreational uses.

Coastal Act Section 30253(5) requires that new development protect special communities and neighborhoods that are popular visitor destination points for recreational uses. The Santa Cruz Main Beach, the Municipal Wharf, and the Boardwalk are popular visitor destination points in the City of Santa Cruz that draw millions of visitors each year. The distinctive and diverse Beach Hill, Beach Flats, and South of Laurel areas/neighborhoods co-exist with these major recreational attractions. Regarding community aesthetics, the challenge for the City is to conserve the historic attributes of both the residential and commercial communities through the application of design guidelines and new development controls in zoning and permitting.

The amendment proposes to incorporate new Design Guidelines for the Beach and South of Laurel (BSOL) area, as well as amend all of the existing community design land use plan policies for the BSOL area (see Exhibit 5 for proposed Design Guidelines; see Exhibit 4 for proposed LUP policies). The goals of the proposed Design Guidelines include improving the quality of life in the BSOL area, retaining neighborhood character, promoting safe and secure streets, developing linkages with the downtown area, enhancing the seaside resort character, and promoting tourist commercial uses. The proposed Design Guidelines include general Design Guidelines to be applied throughout the BSOL area for streetscapes and entries, site planning, architecture, screening, lighting and security, signage, and conservation areas. In addition to these general Design Guidelines that apply to the entire BSOL area, the proposed Design Guidelines also include specific plan area guidelines for the Beach Flats, Beach Hill, Beach Commercial, and South of Laurel areas/neighborhoods.



The proposed Design Guidelines provide much more specificity and direction regarding design review than do the existing Design Guidelines, which is necessary to attain the City's goals of achieving a high quality of aesthetic and functional design in the BSOL area. For example, the existing Design Guidelines for Beach Hill do not discuss Victorian residences, which are Beach Hill's most notable characteristic. The proposed Design Guidelines for the Beach Hill area discuss Victorian architecture in detail and require specific architectural elements for new development, such as hipped, gabled, and other roof forms indicative of Victorian architecture and historic Victorian color schemes. The proposed Design Guidelines also address landscaping, scale of development, color, etc., for each of the four BSOL areas, including the South of Laurel area, which currently has no specific design guidelines.

The proposed Community Design and Land Use policies (see Exhibit 4 pp. 1-4) reference the Design Guidelines and require development that respects the physical and environmental characteristics of the BSOL area. However, only policies with a "wave" symbol are proposed LCP policies. Those policies without the "wave" symbol are General Plan (GP) policies. There are 13 GP Community Design policies that either reference the Design Guidelines (which will be certified as part of this amendment) or contain language that provides objectives for enhancing community design and ultimately the character of the Beach and South of Laurel Area, which is entirely in the coastal zone. For example, GP Community Design policy 1.5 states "Strengthen the residential quality and character of neighborhoods by improving the appearance of architecture and landscaping (P. 11 Design Guidelines)." Given that these policies contain the objectives that describe actions that need to be taken to meet the goals and purposes of the BSOL plan, these policies should be made part of the LCP. In addition, there are five GP Land Use Policies that provide direction for future development in the South of Laurel area and also for protection of community character in the Beach Flats, Beach Hill, and South of Laurel neighborhoods. For example, in the South of Laurel area, GP Land Use Policy 2.10 states "Encourage mixed use development in the residential sections of the South of Laurel area on major arterials..." Also, GP Land Use Policy 2.9 states "Protect and enhance the charming small scale residential neighborhoods in the South of Laurel..." In total there are 18 General Plan Community Design and Land Use policies that should be LCP policies in order to be consistent with Coastal Act Section 30253(5), which requires protection of special communities and neighborhoods in popular visitor destination points. Thus, **Modification #1a** is required. This modification will require that these 18 "unwaved" General Plan Community Design and Land Use policies have "waves" applied to them, thus becoming Local Coastal Program policies.

Proposed Land Use Plan policy 2.8 details the community-based planning process that will be undertaken prior to any development of the Main Beach parking lot, which is located directly across from the Boardwalk. Possible future development of the site includes, but is not limited to, a parking structure on part of the site and commercial uses on the remainder of the site. There are a number of small cottages located on the eastern portion of the site. These cottages are designated historic in the City's Historic Building Survey. The retention of these historic cottages may not be compatible with future development on this site. Thus, **Modification #2** is necessary. This modification adds language to Land Use Policy 2.8 to require that if future proposed development on the Main Beach/Boardwalk Parking Lot is incompatible with retention of the historical cottages on the site, the cottages shall be preserved and relocated to another appropriate site or sites within the City or County of Santa Cruz.



The proposed Community Design and Land Use policies and the proposed Design Guidelines provide the basis for improving the visual and aesthetic qualities of both the residential and commercial communities of the BSOL area by providing objectives and more specificity regarding the design for new development. The suggested modifications add policy language to the LCP that will further protect the community character of the BSOL area. Thus, the proposed amendment, as modified, is consistent with Coastal Act Section 30253(5) regarding protection of special communities and neighborhoods.

2. Density

Coastal Act Section 30250(a) addresses the siting of development and states:

New residential, commercial, or industrial development, except as otherwise provided in this division, shall be located within, contiguous with, or in close proximity to, existing developed areas able to accommodate it or, where such areas are not able to accommodate it, in other areas with adequate public services and where it will not have significant adverse effects, either individually or cumulatively, on coastal resources...

The South of Laurel area consists of approximately 80 acres and is home to approximately 900 residents. The Beach Area is approximately 125 acres in size and is home to nearly 2,700 residents. Within the boundaries of the Beach Area there are three distinct subareas: Beach Flats, Beach Hill, and Beach Commercial (see Exhibit 2 for map of area).

The Beach Flats neighborhood is bordered by the San Lorenzo River, the Boardwalk, and the tourist motel uses that line Riverside Avenue. The housing stock in Beach Flats primarily consists of converted small summer cottages and motels, with approximately 34% in substandard condition in 1998. Part A of the amendment, approved in December 2001, amended the zoning ordinance, the zoning maps, one land use plan policy, and added two new land use plan policies for a portion of the Beach Flats area in order to facilitate approval of an affordable housing project. Thus the current amendment applies to only those areas of Beach Flats outside the above-mentioned areas.

Beach Hill is a distinct promontory within the Beach Area. The Beach Hill area covers approximately 40 acres and contains primarily residential uses, with hotel use located at the fringes of the area. There is an extensive mixture of single-family dwellings, apartments, condominiums, and motels in the Beach Hill subarea. Third Street retains a number of majestic Victorians interspersed with motels and multiple-family dwellings.

In addition to commercial uses, the Beach Commercial area contains a mix of residential housing types including seasonal apartment units, rooming houses and apartments, and a mobile home park, equaling approximately 186 permanent residential units and 79 mobile homes.

The South of Laurel area is highly developed and consists primarily of charming, small-scale residential neighborhoods intermixed with large, land intensive light industrial uses. In general, the residential neighborhoods are clustered along the southern edge of Laurel Street, while the commercial and light industrial uses follow the transportation arterials of Pacific Avenue and Washington Street.



Coastal Act Policy 30250(a) requires that new residential development be located in existing developed areas able to accommodate it. As described in Section III(A)(8) above, the City is proposing five land use designation map amendments. For those areas re-designated from CM (Community Commercial) to M (Medium Density Residential) and from M (Medium Density Residential) to RVC (Regional Visitor Commercial), the maximum allowable residential density will remain the same (30 dwelling units per acre). The re-designation of the Depot Site from RVC (Regional Visitor Commercial) to PK/CF (Parks/Community Facilities) will disallow residential use that site. Only the re-designation of the South of Laurel area bordering Front Street and the San Lorenzo River (the River Bend site) involves an increase in residential density, from M (Medium Density Residential) to H (High Density Residential) (see Exhibit 3 for land use map). Development in this area consists primarily of light industrial use. The existing medium density designation on the River Bend site allows 20.1 to 30 dwelling units per acre. The high-density designation would allow 30.1 to 55 dwelling units per acre. The City envisions a high-density residential area at this site to take advantage of the amenity of the San Lorenzo River and the recently enhanced river levee and to facilitate expected housing needs. The existing developed South of Laurel area has all the public services, e.g. water and sewer, necessary to accommodate the proposed increase in density. Therefore, the proposed increase in density from medium to high would not cause any significant impacts to coastal resources.

General Plan (GP) Land Use Policy 2.12 establishes the above-mentioned area River Bend site as a high-density mixed-use area (see Exhibit 4 pg. 4). This policy provides the textual basis for the above-discussed land use designation map amendment, and will provide the policy basis for the proposed zoning change for this area. The City, however, did not submit this policy as a Local Coastal Program policy. Thus, **Modification #1b** is necessary. This modification requires that a “wave” symbol be applied to General Plan Land Use Policy 2.12 to transform it into an LCP policy. With this modification the proposed LUP amendment is consistent with Coastal Act Section 30250(a) regarding density.

3. Visitor Serving /Recreation

Coastal Act Sections 30213, 30220, and 30221 address visitor serving and recreational facilities and state:

Section 30213 (in part): Lower cost visitor and recreational facilities shall be protected, encouraged, and, where feasible, provided. Developments providing public recreational opportunities are preferred.

Section 30220: Coastal areas suited for water-oriented recreational activities that cannot readily be provided at inland water areas shall be protected for such uses.

Section 30221: Oceanfront land suitable for recreational uses shall be protected for recreational use and development unless present and foreseeable future demand for public or commercial recreational activities that could be accommodated on the property is already adequately provided for in the area.

Over two million visitors visit the Santa Cruz Beach Area yearly. Therefore, recreational and visitor-serving uses largely define how the Beach Area looks and functions. The Main Beach, the Boardwalk,



and the Wharf largely define the activities and the entire tempo of life for the Beach Area. They form the basis of the Beach Area economy and the health and function of this sector is crucial to the health of the visitor serving industry as well as the health of the surrounding neighborhoods.

Visitor accommodations are an important use in the BSOL area. There are a total of 24 hotel/motels in the area: ten in the Beach Commercial area, nine in the Beach Hill area, three in Beach Flats, and two in the South of Laurel area along Pacific Avenue (there are also bed and breakfast facilities in the Beach Hill and Beach Commercial areas). These hotels and motels are often interspersed with residential use. Approximately 80% of the lodging facilities are budget class or below and many would benefit from a thorough renovation.

As described in Section III(A)(8), the City is proposing five land use designation map amendments. Three of these amendments involve changes that affect the RVC (Regional Visitor Serving Commercial) designation. In the Beach Area, this designation primarily emphasizes the development of visitor-serving uses such as hotels, motels, restaurants, amusements, and also allows for the development of mixed-use and residential areas. In the South of Laurel area, the RVC designation emphasizes the development of mixed-use and residential areas, neighborhood commercial areas, and visitor serving uses that help link the Beach Area to the Downtown.

The amendment proposes to re-designate the 6.05 acre Depot Site and adjacent properties from RVC (Regional Visitor Serving Commercial) to PK/CF (Parks/Community Facility) (see Exhibit 3 for land use map). The PK designation includes neighborhood, community, and regional parklands used for passive and/or active recreational uses by residents and visitors. The CF designation identifies existing and potential community facilities to acknowledge their location and to ensure that suitable area will be set aside to accommodate the need for these facilities. The Depot Site currently consists of weedy vegetation interspersed with a variety of trees. Existing structures on the site include a former railway building, a former liquor store building, and a feed and grain building. There are a total of 129 parking spaces on the site. The proposed re-designation will potentially accommodate a future multi-modal transportation station, pedestrian and bicycle trails, a multi-use sports field, a public plaza, a natural history museum, and approximately 214 parking spaces. The existing skateboard park located at the "Fun Spot" (originally considered a temporary location for the skatepark) is included in the proposed Depot Site Master Plan as a permanent facility at the current location. The re-designation from RVC to PK/CF is appropriate to accommodate these future planned uses, which will broaden the recreational activities available to visitors and residents alike. General Plan Community Facilities Policy 6.1 regarding the Depot Site, however, lacks a "wave" symbol and thus is not an LCP policy (see Exhibit 4 pg. 8). Given that this policy outlines future redevelopment of the Depot Site, which is entirely in the Coastal Zone, **Modification #1c** is required. This modification will require that a "wave" symbol be applied to GP Community Facilities Policy 6.1, effectively making this an LCP policy. This modification will also add language to demonstrate that the proposed uses detailed in the policy are not the only uses that will be allowed on the Depot Site.

The Boardwalk, the Municipal Wharf, and the Main Beach are primary visitor attractions in the Beach Area. The Main Beach will continue to be designated CR (Coastal Recreation). The Municipal Wharf



and the Boardwalk will continue to be designated RVC. The Seaside Company (owner and operator of the Boardwalk) is using a portion of this RVC-designated area for Boardwalk parking. This site is located adjacent to the Boardwalk and the San Lorenzo River. There is an ongoing dispute regarding whether the underlying owner of this property is State Lands or the Seaside Company. However, neither the land use designation nor the zoning for this site is being amended in this plan.

Two of the proposed land use map amendments re-designate two groups of parcels in the Beach Area from M (Medium Density Residential) to RVC (Regional Visitor Serving Commercial) (see Exhibit 3). These parcels are located within several blocks of the Boardwalk, the Main Beach, and the Municipal Wharf, and currently contain a mix of older motels and residential use. As stated above, the RVC designation primarily emphasizes the development of visitor-serving uses such as hotels, motels, restaurants, amusements, and also allows for the development of mixed-use and residential areas. The change of emphasis from a residential designation to a visitor-serving designation that allows residential use is appropriate in this visitor-serving area. These re-designations emphasize the reality of current uses on these parcels, which should facilitate more effective management of the City's visitor-serving resources.

The visitor-serving policies in the existing *Beach Area Plan* recognize the Beach Area as a regional tourist attraction and encourage commercial uses in the area that have year-round as well as seasonal potential. Several of these policies also address future development on the Municipal Wharf. The proposed visitor-serving policies recognize the visitor-serving components of the Beach Area in terms of retail, entertainment, lodging and support uses. Several policies address lodging in the BSOL area and one specifically calls for the upgrading of existing Beach Area lodging (see Exhibit 4 Land Use Policies 2.6 and 2.8, Economic Development Policies 5.7, 5.8, and 5.12). In addition, proposed Land Use Policy 2.8 requires that a community-based planning process be undertaken to determine future development parameters for the Main Beach parking lot site. Furthermore, proposed Land Use Policy 2.7 calls for a comprehensive analysis of the wharf to more fully address recreation and marine-related issues. The proposed policies provide a more current framework from which to develop plans for this important area of the City, consistent with the visitor-serving and recreational policies of the Coastal Act. The City, however, has not included a specific policy to provide guidance on the potential visitor-serving use of an important site on Beach Street, the La Bahia.

The La Bahia is located on Beach Street in close proximity to the recreational uses of the Boardwalk, the Main Beach, and the Municipal Wharf. Currently the La Bahia is used for student and seasonal Boardwalk employee housing. The architecture of the La Bahia is Spanish Colonial, with white stucco walls and a tile roof. Revitalization of the La Bahia would help to transform Beach Street into a "signature street," which the City hopes will help provide a linkage between the Pacific Avenue downtown area and the Beach Area. The City's adopted BSOL plan calls for developing the La Bahia site as a major conference facility, to serve as the main catalyst for future beach improvements. It is currently questionable whether a conference facility will be developed on this site. However, there is a current proposal to redevelop the La Bahia for visitor-serving lodging. This proposal includes developing the La Bahia as condominium units, because in the current economy it is difficult to obtain funds for hotel development. To maintain the visitor-serving use of the condominiums, the owners of the individual



condominium units would be limited in the number of days per year they would be allowed to stay in the units. Similar restrictions on condominium usage by owners are in place at the Seascape Resort in Aptos and the Beach Hotel in Half Moon Bay. As submitted, however, the amendment provides no policy guidance for future development of the La Bahia. Given the importance of the La Bahia in relation to redevelopment of the Beach Area, **Modification #3** is required. This modification adds a policy to the Land Use Plan that states that the La Bahia shall be redeveloped as a visitor accommodation site.

Another area in the City that may be appropriate for visitor-serving development is the overflow parking lot across from the West Coast Santa Cruz Hotel (formerly the Dream Inn) (see Exhibit 3 for land use map). The parking lot is located in a transitional area between visitor-serving uses, such as the West Coast Santa Cruz Hotel, and residential use along Bay Street and West Cliff Drive. The existing RVC designation allows for visitor-serving uses such as hotels and motels, and also allows residential use. The designation for this site is not changing under the proposed amendment, but a special overlay zone (RTB/PER) is being applied to this site in the zoning (see Exhibit 6 for proposed zoning map and Exhibit 8 pg. 6 for zoning summary). The RTB/PER overlay zone would allow residential development up to 30 dwelling units per acre, as well as bed and breakfast development and ancillary hotel/motel development, but would preclude hotel/motel development because the intensity of such development would be incompatible with the surrounding neighborhood. The Commission concurs that the proposed amended uses are appropriate for this site, and that either residential development or bed and breakfast development would be appropriate given the existence of the surrounding neighborhood. However, given that the proposed amendment applies a specific zoning district/overlay zone and a specific zoning ordinance to this site, it is appropriate that the Land Use Plan have a specific policy regarding future development of this site. Thus **Modification #4** is necessary. This modification will add a specific policy to the Land Use Plan that outlines future uses on this site. With this modification and the other modifications discussed above, the proposed LUP amendment is consistent with the Coastal Act policies that protect visitor-serving development and recreational uses.

4. Public Access

Coastal Act Section 30214(a) states in part:

The public access policies of this article shall be implemented in a manner that takes into account the need to regulate the time, place, and manner of public access depending on the facts and circumstances in each case including, but not limited to, the following: (3) The appropriateness of limiting public access to the right to pass and repass depending on such factors as the fragility of the natural resources in the area and the proximity of the access area to adjacent residential uses. (4) The need to provide for the management of access areas so as to protect the privacy of adjacent property owners and to protect the aesthetic values of the area by providing for the collection of litter.



Coastal Act Section 30212.5 states:

Wherever appropriate and feasible, public facilities, including parking areas or facilities, shall be distributed throughout an area so as to mitigate against the impacts, social and otherwise, of overcrowding or overuse by the public of any single area.

Coastal Act Section 30252 states:

The location and amount of new development should maintain and enhance public access to the coast by (1) facilitating the provision or extension of transit service, (2) providing commercial facilities within or adjoining residential development or in other areas that will minimize the use of coastal access roads, (3) providing nonautomobile circulation within the development, (4) providing adequate parking facilities or providing substitute means of serving the development with public transportation, (5) assuring the potential for public transit for high intensity uses such as high-rise office buildings, and by (6) assuring that the recreational needs of new residents will not overload nearby coastal recreation areas by correlating the amount of development with local park acquisition and development plans with the provision of onsite recreational facilities to serve the new development.

The primary public attractions in the Beach Area include the Boardwalk, the Municipal Wharf, and the Main Beach. Visitors to these areas generally arrive by automobile. Thus parking availability in the BSOL area has a direct effect on public access to these visitor-serving areas. In general, the Beach Area has its greatest visitation between Memorial Day and Labor Day, with especially high use on weekends and holidays during that period. Parking occupancy rates are high at these times. According to the *BSOL Comprehensive Area Plan DEIR*, during the peak design day (the peak hour between 2:00 and 4:00 p.m. on the 20th highest activity day in the Beach/Boardwalk area), parking occupancy in the Beach Area is close to capacity, ranging from 91% to 97% occupied. These occupancy rates demonstrate that, during peak periods, there is at present little or no additional parking capacity in the area to accommodate new growth and any increase in visitor demand (even without new growth) may result in a significant spillover into adjacent neighborhoods. There are, however, large portions of the year when parking is widely available in the Beach Area. Thus it is not realistic or desirable to require an extensive increase in paved parking areas in the BSOL area to accommodate the peak periods. To do so would create large paved areas or parking structures that would have very low occupancy rates for much of the year.

Coastal Act Sections 30214(a), 30215.5, and 30252 address public access as it relates to adjacent residential uses and parking impacts. In particular, Coastal Act policy 30252 provides for adequate parking facilities or substitute means of serving new development, and also encourages public transit. The proposed LUP Circulation policies (see Exhibit 4 pp.4-6) contain language to promote creation of a transportation system that encourages alternative forms of transportation (e.g., shuttles, an integrated mass transit system, bikeway systems, etc.) as well as alternative routes to the Beach Area. A number of these policies also address the need for pedestrian enhancements in the BSOL area. In addition, Circulation Policy 3.18 provides for the creation of a parking assessment district to provide new resources to construct additional parking. Circulation Policies 3.21 and 3.22 address development of employee



ride-sharing and employee off-site parking programs. The proposed LUP, however, does not retain an existing LUP policy that requires that a minimum number of parking spaces (3,350) be maintained in the overall Beach Area and also states that no projects may be implemented to reduce the number of available parking spaces below this baseline. Thus, while the City's proposed LUP policies adequately promote alternative transportation methods but do not adequately provide for protecting the existing parking supply.

The total number of estimated public parking spaces in the BSOL area is 4,098. This number is greater than the 3,480 spaces estimated in the 1980 *Beach Area Plan* for two reasons: 1) A major earthquake in 1989 destroyed a number of buildings on parcels in the Beach Area; several of these parcels have been converted to parking, and; 2) The proposed BSOL plan includes the South of Laurel area, which was not a part of the original *Beach Area Plan*; the additional available parking spaces in the South of Laurel area contribute to the higher current total of approximately 4,098 public parking spaces in the BSOL area. Given that at 90% or greater occupancy of parking spaces at any given time is essentially equivalent to capacity, staff calculated 90% of 4,098, which is equivalent to approximately 3,690 spaces. Thus **Modification #5** requires that a minimum of 3,690 public parking spaces be maintained in the overall BSOL area. With this modification the proposed LUP amendment is consistent with the Coastal Act policies that protect public access to the coast.

C. Analysis of Implementation Plan (Zoning Ordinance) Amendments

1. Design Guidelines/Community Character

As stated in Sections III (A)(1-5), the City proposes to add three new zoning districts (one residential and two commercial), amend the zoning ordinance for six zoning districts, amend the zoning map for six areas within the BSOL area, and amend the zoning map to apply two new overlay zone districts (CON and PER) in the BSOL area (see Exhibit 6 for proposed zoning map amendments, Exhibit 7 for proposed zoning ordinance amendments, and Exhibit 8 for a zoning summary). The proposed zoning ordinances for each of these districts/overlay areas require that all new development conform to the BSOL Design Guidelines and the Community Design and Land Use LCP Policies. Also, each of the new and amended zoning districts has specific design requirements in the zoning ordinances that implement the Design Guidelines. For example, the architectural standards in the Design Guidelines require that new development in the revised RTC (Tourist Residential/Beach Commercial) zone be based on "Spanish Colonial Revival" architecture. The revised RTC ordinance incorporates language from the Design Guidelines regarding allowable architectural elements, allowable building height, pedestrian amenities such as plazas and courtyards, and landscaping requirements (see Exhibit 7 pp. 18-24 for RTC zoning ordinance language). The existing RTC zone (Tourist/Residential Beach Commercial) has no design standards, no requirement for building articulation or architectural quality or style, and no landscaping standards.

The three new zoning districts (RH, RTB/PER, and RTC/PER) also require design and development standards consistent with the Design Guidelines and the amended Community Design and Land Use LCP Policies. For example, the new RH zone (Multiple Residence/High Density) establishes clear design and development standards. Specific findings are required for this zone. These include requiring that new



development substantially enhance the site, that the bulk, massing, height, and rooflines be consistent with the Design Guidelines and add architectural quality to the neighborhood, and that the siting, landscaping, access, and design of proposed development demonstrate a sensitive relationship to the San Lorenzo River and maximize the natural attributes of the riverside location (see Exhibit 7 pp. 14-18 for RH Zoning Ordinance language).

Two overlay zones (CON and PER) will be applied to various neighborhoods of the BSOL area. The CON overlay will be applied to three neighborhoods in the South of Laurel area and one neighborhood in the Beach Area (see Exhibit 6). The City has identified these neighborhoods as at risk due to a variety of factors, including deteriorated housing stock and the need to preserve character and uniqueness. The addition of the CON overlay zone to these neighborhoods will provide for concentrated housing and zoning code enforcement and demolition control. The CON overlay will also provide for controlled transition between neighborhoods by providing specific standards for all development, redevelopment, and building expansion on sites about neighborhoods with a CON overlay. The CON overlay ordinance also contains provisions regarding demolition control. These provisions require that, prior to issuance of a demolition permit, the structure be evaluated by an appropriate professional regarding its contribution to the neighborhood historical and/or architectural context. This evaluation must be submitted to the Zoning Administrator, who shall make the determination whether the demolition is appropriate. Of course, for those demolitions requiring a coastal permit, the Zoning Administrator's decision may be appealed to the City Council.

The PER overlay will be applied to the West Coast Santa Cruz Hotel parking lot (RTB/PER zone) and the Main Beach/Boardwalk parking lot (RTC/PER zone) (see Exhibit 6). The RTB/PER overlay zone contains specific design requirements that implement the Design Guidelines and conform to the amended Community Design and Land Use LCP Policies. The RTC/PER overlay zone also requires conformance with Design Guidelines and encourages design that will produce a desirable relationship between development on the site and adjacent residential and commercial areas (see Exhibit 7 pp. 24-33 for proposed zoning ordinances). As discussed in the land use section above, there are historical cottages on the Main Beach/Boardwalk parking lot site. The proposed zoning ordinance language for the RTC/PER does not address the effect of potential development of the parking lot on the cottages. Therefore, **Modification #6** is necessary. This modification requires that if proposed future development on the parking lot site is incompatible with retaining the historical cottages on site, the cottages shall be moved to another area or areas of the City or County of Santa Cruz. With this modification, the new residential and commercial zones, the amended existing zones, and the applied overlay zones will be in conformance with the Design Guidelines and will adequately implement the LUP policies regarding the preservation of community character. Thus the Commission finds that the proposed Implementation Plan amendments, as modified, are consistent with the Land Use Plan as amended and are adequate to carry out the LUP's provisions regarding community character.

2. Density

The following tables detail the existing maximum density and proposed maximum density (in dwelling units per acre) for the three new zones, the six revised zones, and the five rezoned areas:



NEW ZONES	EXISTING DENSITY (DU/A)	PROPOSED DENSITY (DU/A)
RH (Multiple Residence/High Density)	30 (RM Zone)	55
RTC/PER (Tourist Res. Beach Commercial/Performance Overlay)	43 (RTC Zone)	27
RTB/PER (Motel Residential/Performance Overlay)	55 (RTB Zone)	30
REVISED ZONES	EXISTING DENSITY (DU/A)	PROPOSED DENSITY (DU/A)
RTD (Beach Residential)	55	27
RTA (High Density Residential → Medium Density Residential)	55	30
RTB (Motel Residential)	55	30
RTC (Tourist Residential/Beach Commercial)	43	27
CBD/E (Lower Pacific Avenue Subdistrict)	30	30
CB (Beach Commercial)	30	30

REZONED AREAS	EXISTING DENSITY (DU/A)	PROPOSED DENSITY (DU/A)
A. Depot Site/Adjacent Properties (RTC → PF/PK)	43	0
B. Center Street/Pacific Avenue (CT → RM)	30	30
C. Front Street/San Lorenzo River (RM/MU → RH/MU)	30	55
D. Second Street/Front Street (RTA → RTB)	55	30
E. Cliff Street/First Street/Second Street (RTB → RTC)	55	27
F. Cliff Street/Wave Lane/Leibrandt Avenue/Second Street (RTB → RTC)	55	30

The purpose of the new RH (Multiple Residence/High Density District) zone is to provide high-density residential housing adjacent to the San Lorenzo River (see Exhibit 6 for proposed zoning map and Exhibit 7 pp. 14-18 for proposed zoning ordinance). This will accomplish a stated goal in the City's General Plan and will also assist in creating a viable live/work environment adjacent to the Lower Pacific CBD/E (Lower Pacific Avenue Subdistrict) zone. The intensity of development allowed under the proposed RH zoning district is 55 dwelling units/acre, which is consistent with and would implement that allowed under the proposed High Density Residential land use designation.

As shown in the tables above, the allowable maximum density for the RTA and RTB zones will be reduced from 55 DU/acre to 30 DU/acre. For the RTC zone the allowable maximum density will be



reduced from 43 DU/acre to 27 DU/acre; for the RTD zone the allowable maximum density will be reduced from 55 DU/acre to 27 DU/acre. The existing land use designation for these zones is M (Medium Density Residential), which allows up to 20.1 to 30 DU/acre. Thus the density allowed under the existing zoning is inconsistent with that allowed under the existing land use designation. Therefore, the proposed amendment will bring the allowable densities for these zones into conformance with the density allowed under the existing Medium Density Residential designation.

The existing allowable residential density at the Depot Site is 43 DU/acre (see Exhibit 6 for proposed zoning map). Under the proposed amendment, the Depot Site will be rezoned from RTC (Tourist Residential/Beach Commercial) to PF/PK (Public Facilities/Parks) and residential use will not be allowed. This is consistent with the proposed land use designation for this site (PK/CF – Parks/Community Facility), which does not provide for residential use.

In summary, the proposed residential zoning districts are appropriate to implement the residential land use designations. Therefore, the Commission finds that regarding residential density, the proposed Implementation Plan amendment, as submitted, is consistent with the Land Use Plan as amended and is adequate to carry out its provisions.

3. Visitor Serving/Recreation

As described in Section III (A)(3), the City is proposing to amend the zoning ordinance to revise six existing zones. The proposed amendment does not change the types of visitor-serving accommodations allowed in each of these zones. For example, under the proposed amendment, bed and breakfast facilities will continue to be allowed in the RTD and RTA zones, but hotels and motels will continue to be disallowed uses in these zones. The existing RTC zone allows motels, hotels, and bed and breakfast facilities and will continue to do so under the proposed amendment. The amended zones will continue to be consistent with and adequate to implement the underlying land use designations and the policies of the amended LUP.

The proposed amendment also adds two new commercial zones (RTC/PER and RTB/PER) to the zoning ordinance, RTB/PER (Motel Residential/Performance Overlay) and RTC/PER (each Commercial/Performance Overlay) (see Exhibit 6 for proposed zoning map and Exhibit 7 pp. 24-30 for proposed zoning ordinance amendments). As discussed above, the proposed RTB/PER zone allows bed and breakfast facilities, ancillary hotel uses, and residential use, but does not allow motel/hotel development. This new zone is consistent with the RVC (Regional Visitor Serving) land use designation, which allows for visitor-serving uses and residential use, and is also consistent with the amended LUP policies. The primary purpose of the new RTC/PER zone is to allow for development of a variety of land uses and public activities to meet the needs of visitors and residents alike. Allowable uses include retail use, eating establishments, lodging, and residential development if it is located above the first floor of commercial uses. This new zone is also consistent with and adequate to implement the underlying RVC land use designation as well as the amended LUP policies.

As stated above in Section III (A)(4), the City proposes to rezone six areas in the BSOL area (see Exhibit 6 for proposed zoning map). Four of these amendments involve potential changes to visitor serving



and/or recreational uses. Currently the Depot Site is zoned RTC (Tourist Residential/Beach Commercial), which allows lodging among other uses. The rezoning of the Depot Site to PF/PK (Public Facilities/Parks) will eliminate lodging as an allowable use. However, the new zoning will allow public parks, recreational facilities, and museum use. This is consistent with and adequate to implement the proposed PK/CF (Parks/Community Facilities) designation, as well as the amended land use policies.

The northeast corner of Second Street and Front Street in the Beach Hill area will be rezoned from RTA (High Density Residential) to RTB (Motel Residential). This site contains an existing motel. The existing RTA zoning does not allow for motel use. Thus, the rezoning to RTB will be consistent with the existing use on the site. The underlying land use designation for this site is M (Medium Density Residential) designation. RTB is an allowable zoning district under the M land use designation.

Two of the proposed zoning district amendments rezone two groups of parcels in the Beach Area from RTB (Motel Residential) to RTC (Tourist Residential/Beach Commercial). While both zones allow hotel/motel uses, the RTC zoning district allows a wider range of commercial uses. The amended underlying land use designation for both groups of parcels is RVC (Regional Visitor Commercial). RTC is an allowable zoning district under the RVC designation, while RTB is not. Thus the proposed amendment will provide consistency with the amended land use designation.

In conclusion, the proposed amendments regarding visitor-serving and recreational uses are appropriate to implement the underlying land use designations and are consistent with the amended land use policies. Therefore, the Commission finds that regarding visitor-serving/recreational uses, the proposed Implementation Plan amendment, as submitted, is consistent with the Land Use Plan as amended and is adequate to carry out its provisions.

4. Public Access

As discussed above, the proposed LUP contains numerous policies regarding alternative methods of transportation, pedestrian enhancements, and additional parking supply. The proposed zoning ordinances, however, do not include language requiring the City to implement the numerous circulation policies by funding shuttles, construction, rail transit, etc. PRC Section 30516, however, states, "Approval of a local coastal program shall not be withheld because of the inability of the local government to financially support or implement any policy or policies..." Thus, the Commission may not require the implementation of these circulation policies, many of which will require significant public funds. The ordinances are therefore adequate to carry out the LUP policies and it is the City's intention to implement these programs as funds become available.

The proposed zoning ordinances for the primary visitor-serving zones in the BSOL plan (RTC, RTC/PER, and CB) do contain language sufficient to implement portions of the proposed LUP circulation policies (see Exhibit 7 pp. 23, 28, 39). Specifically, the amended zoning ordinances for the above zones requires that all development proposals shall discourage employee automobile use by instituting one or more of the following: carpooling requirements, transit subsidies, employee shuttle service, and/or provide a contribution and/or cost sharing for shuttle and/or parking. Thus, for these visitor-serving zones, the amended zoning ordinances are appropriate to implement the LUP circulation policies. Therefore, regarding public access, the Commission finds that the proposed Implementation

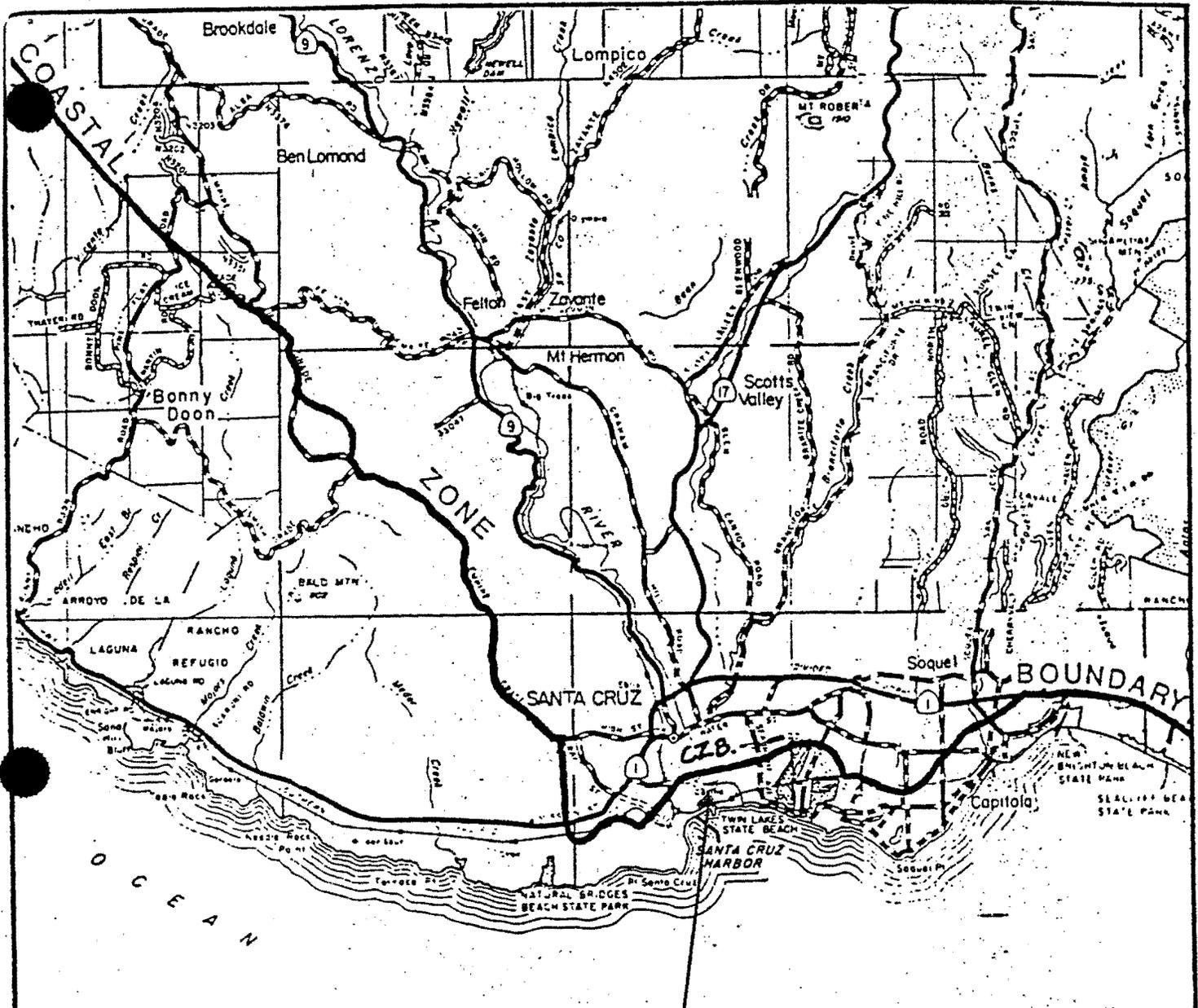


Plan amendment, as submitted, is consistent with the Land Use Plan as amended and is adequate to carry out its provisions.

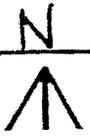
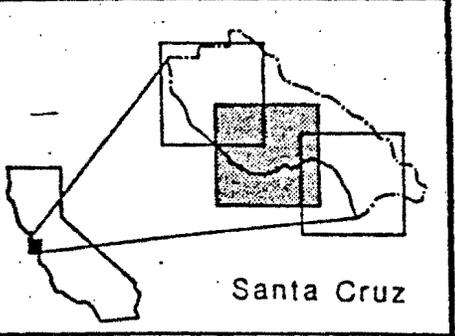
IV. CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

The Coastal Commission's review and development process for Local Coastal Programs and amendments to them has been certified by the Secretary of Resources as being the functional equivalent of the environmental review required by CEQA. Therefore, local governments are not required to undertake environmental analysis on LCP amendments, although the Commission can and does utilize any environmental information that the local government has developed. In this case, the City approved an EIR for the amendment finding that it did not generate any significant environmental impacts. The findings in this report are consistent with the City's environmental analysis. Approval of the amendment, as modified, will not have significant environmental effects for which feasible mitigation measures have not been employed consistent with the California Environmental Quality Act.



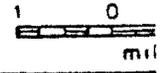


AREA OF CITY
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PROPOSED AMENDMENT



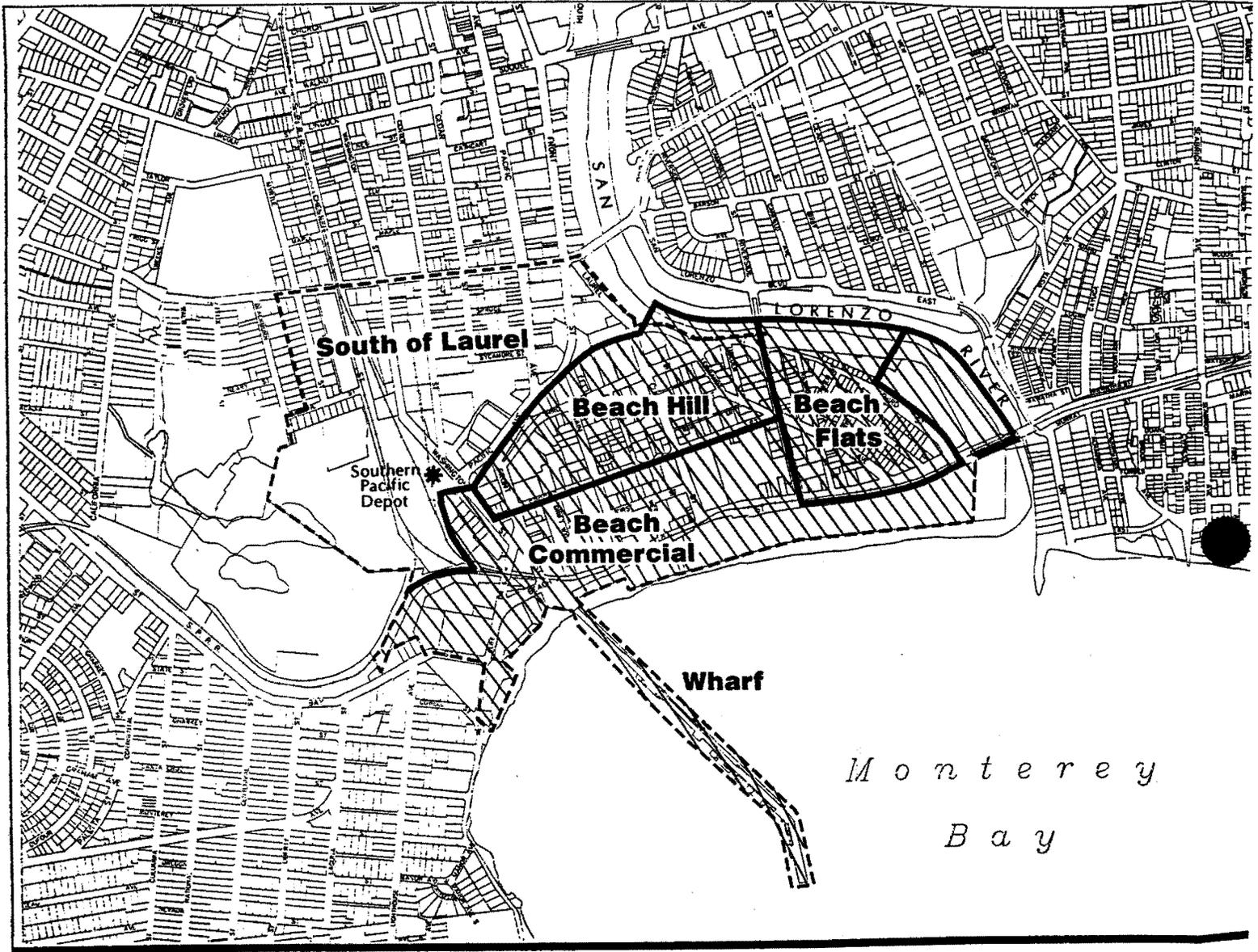
 California Coastal Commission

LOCATION MAP

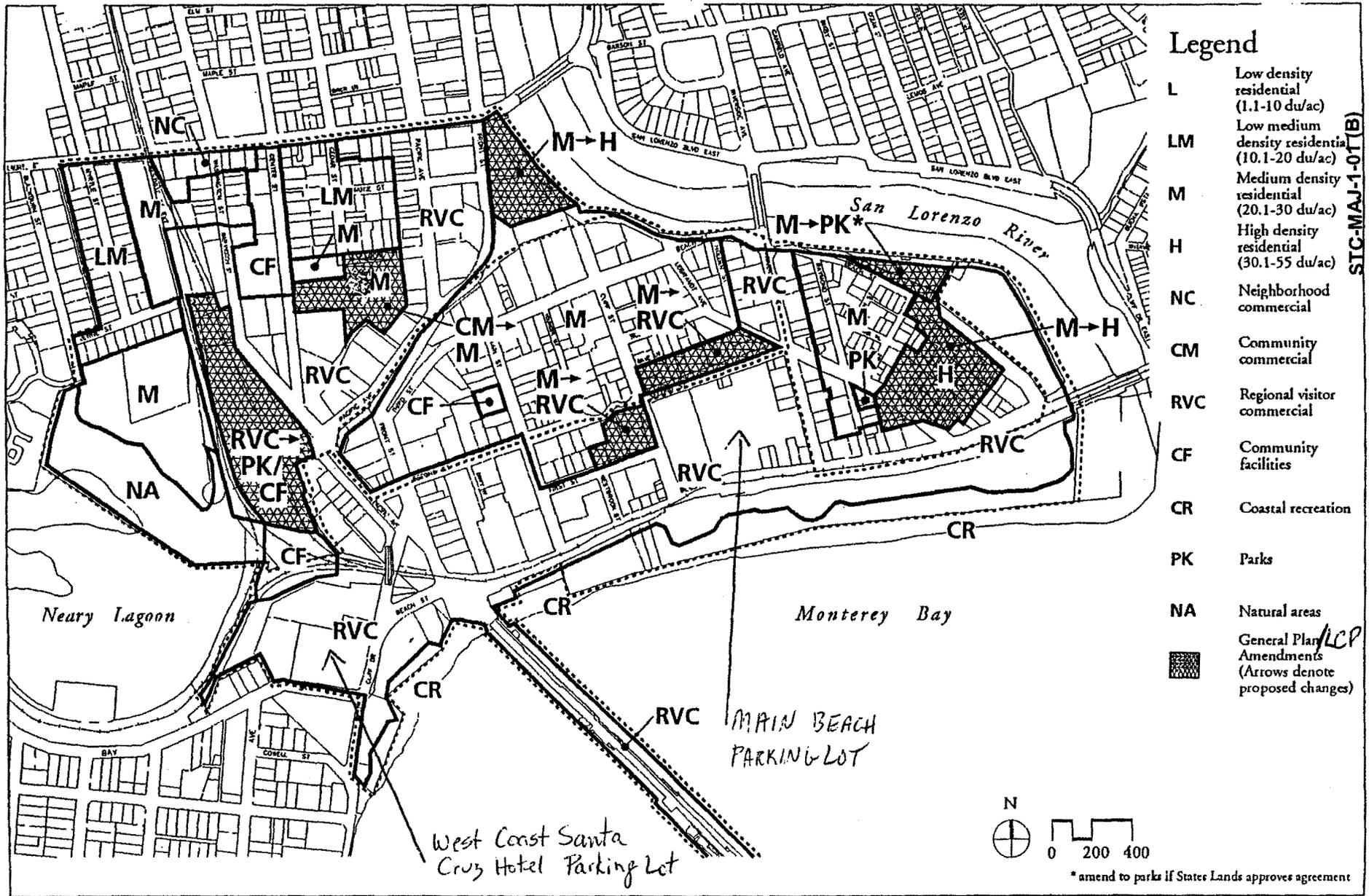


County of Santa Cruz

EXHIBIT NO.	1
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BEACH AREA/SOUTH OF LAUREL SUB AREAS



The Phipps Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Figure 71

LCP Land Use Amendments

Policies and Programs

COMMUNITY DESIGN

- 1.1 Maintain General Design Guidelines to promote development that respects the physical and environmental characteristics of the community and the site, reflecting functional and attractive site planning and high quality design. (P. 3 Design Guidelines)
- 1.2 Create a sequence of experiences and views in the landscape that lead to destination points. (P. 11 Design Guidelines)
- 1.3 Enhance entryways to neighborhoods at definable intersections. (P. 11 Design Guidelines)
- 1.4 Encourage distinctive architectural treatment at major intersections to accentuate community gateways. (P. 11 Design Guidelines)
- 1.5 Strengthen the residential quality and character of neighborhoods by improving the appearance of architecture and landscaping. (P. 11 Design Guidelines)
- 1.6 Protect and enhance the unique historic and architectural qualities that are present by establishing/maintaining historic conservation districts. (P. 11 Design Guidelines)
- 1.7 Maintain the heritage tree preservation program and use the existing heritage trees as a basis for streetscape design. (P. 11 Design Guidelines)
- 1.8 Protect historic resources such as rock walls and stairs. Use existing historic resources as a basis for streetscape design. (P. 11 Design Guidelines)
- 1.9 Expand the economic vitality of Downtown by transforming Beach Street into a "signature street" that will complement and complete the Pacific Avenue "signature" shopping district. Link these commercial areas. (P. 11 Design Guidelines)
- 1.10 Implement traffic calming measures, such as textured paving and chokers, in Conservation Areas. (P. 11 Design Guidelines)
- 1.11 Implement hierarchical streetscape design that promotes identification of streets as thoroughfares and residential streets. Use a consistent landscape design theme to create "imagable paths" through the community, such as palm tree lined streets that lead to the beach. (P. 11 Design Guidelines)
- 1.12 Implement a street tree program to give scale and definition to the public walkways and promote a visually attractive, comfortable street with reduced heat and glare. (P. 12 Design Guidelines)

- 1.13 Balance the needs of automobiles, pedestrians and bicyclists with wide sidewalks for promenading and pedestrian-oriented activities; narrow roadways to slow traffic; continuous bicycle routes; ample, accessible, at-grade pedestrian crossings or ramps with appropriately textured surfaces; and curbside parking, bike lanes or landscaped parkways to separate pedestrians and moving cars. (P. 12 Design Guidelines)
- 1.14 Provide unobstructed access to all citizens, in compliance with Uniform Federal Accessibility Standards and State Title 24 Accessibility requirements. (P. 12 Design Guidelines)
- 1.15 Provide public amenities such as benches, planters, lighting, street name and traffic sign posts, trash receptacles, public restrooms, bicycle racks, public telephones and information kiosks that are functionally and aesthetically integrated into the streetscape for pedestrian comfort, convenience and safety. (P. 12 Design Guidelines)
- 1.16 Improve the pedestrian connection to Downtown by upgrading and maintaining the two existing pedestrian stairways. (P. 12 Design Guidelines)
- 1.17 Provide public art for aesthetics and contemplation. (P. 12 Design Guidelines)
- 1.18 Underground utilities to eliminate visual clutter and provide a more aesthetic streetscape. (P. 12 Design Guidelines)
- 1.19 Implement an area-wide sign program to eliminate visual clutter of oversized and inappropriate signs. (P. 12 Design Guidelines)
- 1.20 Maintain the "Wharf Design Criteria" until the results of the proposed wharf study are approved. (P. 111 B/SOLA Plan)

LAND USE

- 2.1 Stabilize and strengthen the existing neighborhood at the interior of the Beach Flats by establishing a conservation area with a maximum density of 27 units an acre. Develop "New Development Area" with a range of densities from 30 to 36 units an acre to provide better managed multifamily housing opportunities on the perimeter. (P. 28)
- 2.2 Maintain the RTD zone and a Neighborhood Conservation Overlay in the Conservation Area, which requires conformance with specific design guidelines. (P. 32)
- 2.3 Maintain the RTE zone - Beach Residential Development - for the "New Development Area" a ~~medium density~~ *modified under STC-MAJ-1-01 (Part A)* residential zone in which the density may range from 30 to 36 units an acre, with reduced parking requirements, flexible setbacks, no specified lot size for planned developments (PDs), and subject to specific design guidelines. (P.32)

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- 2.4 Establish a stronger definition of where the Beach Hill neighborhood begins and ends to protect it from tourist related penetration as well as increasing local traffic along Third Street. (P. 69)
- 2.5 Formally designate the Beach Hill area as an Historic Preservation District. Modify Cultural Resources Map CR-3 to implement the district. (P. 70, 72)
- 2.6 Provide significant opportunities for Beach Commercial area redevelopment of underutilized land that will provide attractive retail, entertainment, lodging and support uses that will appropriately extend the operational activities of the Beach. (P. 103)
- 2.7 Complete a comprehensive analysis of the wharf to include study of its two fundamental and interrelated aspects: its maritime aspect and its retail aspect. Elements of this study should include, but not be limited to:
- the physical inventory,
 - access, circulation and parking,
 - additional maritime potential,
 - marine sanctuary potential,
 - design and architectural character,
 - signature physical features or programs,
 - retail mix and performance,
 - market niche, and
 - a cost/benefit analysis of recommendations stemming from analysis.
- The study should examine the feasibility of: expanding maritime activity, expanding visitor amenities, and expanding local resident marketing. (P. 110, 111)
- 2.8 Complete a community-based planning process to determine the ultimate development parameters for the main beach parking lot site. Any proposal for development of the site will require preparation of initial studies or similar environmental documents to determine whether the certified B/SOLA Final EIR remains legally adequate for purposes of action on the proposal or if a new EIR is required. (P. 114)
- 2.9 Protect and enhance the charming small scale residential neighborhoods in the South of Laurel while encouraging the significant development opportunities presented by vacant and underutilized parcels.
- 2.10 Encourage mixed use development in the residential sections of the South of Laurel area on major arterials by overlaying the mixed use zoning district on appropriate areas. (P. 144)
- 2.11 Establish the southern end of Pacific Avenue below Laurel Street as an extension of the Downtown Recovery Plan policies for Pacific Avenue with an emphasis on mixed residential development above ground floor retail and other uses. Maintain a CBD zone that uses the same village design and use policies as the Cedar Street area of the Downtown Recovery Plan. (P. 144)

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- 2.12 Establish the area South of Laurel adjacent to the river as a high-density mixed-use area that will provide new market rate housing. (P. 144)
- 2.13 Extend the RTC Beach Commercial zone to the lower portion of the South of Laurel to encourage further visitor serving uses such as motels and restaurants and other visitor support uses. (P. 144)
- 2.14 Create an expanded neighborhood commercial use area at the corner of Chestnut and Laurel Streets, to better serve the new residential intensity established in the plan. (P. 150)
- 2.15 Encourage mixed use projects along Front Street. (P. 150)

CIRCULATION

- 3.1 Revise configuration of Leibrandt Street to provide aggregated residential parking for Beach Flats residents. (P. 29)
- 3.2 Limit impacts of traffic circulation and parking on residents of Beach Hill by maintaining the permit parking program to limit visitor parking, developing an early warning system to monitor changing traffic, parking and circulation impacts on Beach Hill as the Beach Commercial area develops, and implementing the proposed neighborhood gateways to help define entries to the residential area and calm traffic. (P. 76)
- 3.3 Improve lighting for pedestrian stairways at Cliff and Front Streets for linking the heights of Beach Hill with the South of Laurel area. (P. 76)
- 3.4 Allow projects to off-set on-site parking requirements through payment of annual deficiency fee and/or through funding of shuttle service or other options provided in the comprehensive transportation strategy. (P. 144)
- 3.5 Consider land swaps, Federal Grants for promoting transit, and other outside resources in combination with City funds already identified as methods to acquire and improve all of the Union Pacific property for Public Use. (P. 144)
- 3.6 Develop an ordinance to indicate the parking waiver procedure and the mechanism for allocating funds to South of Laurel parking projects. This approach is necessary because it is unlikely that the South of Laurel could support a separate parking district as most of the sites provide on-site parking. (P. 149)
- 3.7 Emphasize Front Street as a major new vehicular access route to the Beach in order to divert some traffic from Ocean Street and provide Downtown more exposure. (P. 150)

- 3.8 Develop the most appropriate combination of circulation improvements, transit/rail, parking, pricing and pedestrian/bicycle strategies which will:
 - support new development and expand the operational season,
 - protect existing neighborhoods, and
 - encourage transit, rail, pedestrian and bicycle access. (P. 159)
- 3.9 Develop a comprehensive, multi-modal circulation planning program that takes as its highest priority reduction of automobile trips by the creation of viable alternative transportation modes, effective transportation systems management programs, and integration of land-use and circulation planning. (P. 165)
- 3.10 Develop and promote pedestrian travel as a viable transportation mode by developing and maintaining a safe, comprehensive, convenient, accessible and aesthetically pleasing pedestrian system. (P. 165)
- 3.11 Develop a safe, convenient and effective bikeway system that promotes bicycle travel as a viable transportation mode and connects work, shopping, schools, residential and recreational areas. (P. 165)
- 3.12 Aggressively pursue development of a comprehensive and integrated mass transit system including buses, paratransit and rail transit/fixed guideways to provide increased mobility in the community. (P. 165)
- 3.13 Maximize the efficiency and safety of the existing road system while ensuring that it accommodates all modes of travel, operates at an acceptable level of service, and is not expanded unnecessarily. (P. 165)
- 3.14 Create a transportation system which encourages alternative forms of transportation and alternative routes to the Beach, prevents visitor intrusion into neighborhoods, links the Beach Area with the Downtown, and provides alternative transit modes and parking to accommodate anticipated recreational and commercial improvements. (P.30-37)
- 3.15 Establish signage to promote alternate beach access routes to limit increased traffic impacts on lower Ocean Street neighborhood. (P. __)
- 3.16 Promote rail access to the Beach. (P. __)
- 3.17 Establish a formal funding mechanism to promote transit. (P. __)
- 3.18 Evaluate creating a parking assessment district in the Beach and South of Laurel to provide new resources to construct additional parking. (P. __)
- 3.19 Implement transportation management measures prior to Boardwalk expansion or commercial expansion on the Main Beach parking lot. (P. 167)
- 3.20 Pursue outside funding sources for alternative transportation projects. (P. 167)

- 3.21 Develop employee off-site parking programs to encourage employee ridesharing and levy parking transfer fees on employers. (P. 191)
- 3.22 Develop and implement a comprehensive, peak season employee off-site parking program which would address the needs of both large and small employers in the Beach area. (P. 192)
- 3.23 Implement concurrently with the meter program, the traffic calming and urban design recommendations for neighborhood entryways to Beach Hill and Beach Flats. (P. 198)
- 3.24 Install a two-way year-round east-west bikeway on the south side of Beach Street between Pacific Avenue and the San Lorenzo River. (P. 219)
- 3.25 Pursue pedestrian and bicycle access across the San Lorenzo trestle to the east, and connect the Beach Street bikeway to West Cliff Drive at Bay Street. (P. 219)
- 3.26 Enhance bicycle and linkage access along the San Lorenzo River between the Beach area and the downtown through signage, curb lanes, and/or installing bike lanes. (P. 219)
- 3.27 Provide pedestrian enhancements along the Front Street and Pacific Avenue corridors, including appropriate urban design elements as opportunity sites develop, with lighting, sidewalk furniture and plantings. (P. 219)
- 3.28 Improve Beach Street as a pedestrian-oriented corridor by implementing compatible features from the Urban Design Recommendations. (P. 219)
- 3.29 Design and implement a pedestrian walkway from the Depot parking lot to the Wharf intersection. (P. 219)
- 3.30 Improve the Cliff Street and Pacific Avenue stairways and pedestrian pathways in the South of Laurel area.
- 3.31 Improve east-west pedestrian connections in South of Laurel by identifying new pedestrian and bikeways. (P. 221)
- 3.32 Abandon lower Washington Street at the Center Street intersection. (P. 278)
- 3.33 Incorporate transportation hub concept into planning of Depot site to provide transit facilities to local residents and visitors. Pursue "Livable Communities" and ISTE A funding to implement concept. (P. 278)

HOUSING

- 4.1 Maintain the current number of units in the Beach Flats and provide residents opportunities to live in better quality units and better managed living conditions. (P. 28)

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- 4.2 Promote more family-oriented development by requiring at least 50% of all new units to be two or more bedrooms. (P. 29)
- 4.3 Promote a diversity of new housing opportunities by amending the demolition conversion permit requirement from 100% replacement of affordable bedrooms to 50% replacement, in the Beach Area to be consistent with the rest of the City, as long as there is no net loss of affordable units in the greater Beach and SOLA planning areas. Additionally allow dilapidated units to be demolished, so long as replacement units have been identified. (P. 29)
- 4.4 Maintain an aggressive housing code enforcement program to insure that existing units meet health and safety codes. (P. 29)
- 4.5 The City will make a good faith effort to provide affordable housing opportunities for current residents displaced by changes resulting from the Plan and who are eligible for affordable housing in accordance with appropriate relocations laws. (P. 29)
- 4.6 The City of Santa Cruz shall continue to allocate Community Development Block Grant funds to the pro-active Target Area Code Enforcement and Nuisance Abatement Program. (P. 29)
- 4.7 The City of Santa Cruz shall continue to direct in-lieu housing trust fund and fees and HOME fund money to the Target Area Housing and Relocation Assistance Fund, to improve or replace substandard housing in the Target Area and Beach Flats. (P. 29)
- 4.8 When tax increment housing set-aside money become available, consideration should be given to establishing new construction as one of the high priorities of the fund. Additionally rehabilitation of housing within Beach Flats and other portions of the Target Area will be promoted. (P. 29)
- 4.9 Conserve and rehabilitate existing housing stock by improving the effectiveness of the City's Code Enforcement program and by offering inducements to current property owners that encourage their direct investment and voluntary compliance. (P. 47)

ECONOMIC DEVELOPMENT

- 5.1 Establish an active partnership with property owners, informing them of proposed changes and how they can benefit from them.
- 5.2 Work with the lending community to direct lending commitments to the Beach Commercial Area and the Beach Flats and formally designate the area a "Revitalization District" which requires Community Reinvestment Act lending participation by local banks. Establish a loan pool for commercial and residential projects. (P. 113)
- 5.3 Identify appropriate funding mechanisms such as Business Improvement

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Districts, Parking Assessment Districts, or other funding approaches to facilitate implementation of improvements. (P. 113)

- 5.4 Establish an active partnership with lodging industry representatives, the Lodging Association and the Conference and Visitors Council with the goal of repositioning Santa Cruz within the hospitality market. (P. 236)
- 5.5 Examine the market potential of time sharing and quality extended stay facilities in Santa Cruz. If found to be feasible, consider developing a marketing campaign targeted to take advantage of the high-tech growth corridor and the university. (P. 236)
- 5.6 Establish an outreach program to lodging facility owners/managers to inform them of Beach proposals including Ocean Resort ambiance, Conference facility status, proposed transportation improvements, changes to the zoning code, design and landscaping requirements, and proposed licensing requirements. (P. 237)
- 5.7 Examine the potential to provide a broad array of lodging experiences to an expanding visitor base, and encourage Bed and Breakfasts and small inns. (P. 237)
- 5.8 Develop comprehensive program to ensure general upgrading of entire Beach area lodging inventory. (P. 237)
- 5.9 Evaluate developing minimal lodging industry standards as a basis for applying for the business license. (P. 239)
- 5.10 Develop Heritage Tourism marketing strategy, emphasizing historic assets of the Wharf, the Boardwalk, surrounding neighborhoods - particularly Beach Hill and Downtown neighborhoods. (P. __)
- 5.11 Develop Heritage Tourism funding strategy including U.S. Congressional/Senatorial officials. Examine feasibility of becoming a formal "Heritage Area". Invite HUD, DOI, and DOC officials to series of walking tours and briefings. Solicit support from State representatives. Examine feasibility of Maritime Heritage Funding in conjunction with National Marine Sanctuary designation. (P. 242)
- 5.12 Examine the potential of joint marketing/programming between the Boardwalk, Beach area businesses and the Wharf targeting local Santa Cruzians for both mid-week and off-season activities. (P. 243)

COMMUNITY FACILITIES

- 6.1 Develop the depot site as a focal point of community activity, by providing parking, a small scale multi-model transit facility, and recreational open space. (P. 148)

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- 6.2 Consider abandonment of lower portion of Leibrandt Avenue at Beach Street to provide the location of new temporary community center. Incorporate permanent community center in New Development Area. (P. 276)
- 6.3 Continue support and funding for the following Beach Flats programs:
 - after-school recreation programs and summer Kids Club programs;
 - the Beach Flats community center programs including the community liaison;
 - centralized municipal services and outreach at the community center, such as an emergency services substation, recreation programs, neighborhood improvement program;
 - use of the community center to act as a neighborhood focus to direct various community recreation programs and provide access to City personnel including, housing code enforcement, parks and re-creation and police; and
 - support of the existing social service programs that serve residents in the area. (P. 276 and 277)
- 6.4 Prepare a water line deficiency report to identify water lines that are deficient for potential replacement or upgrade as development in the Beach and South of Laurel progresses. (P. 282)
- 6.5 Prepare a sewer line deficiency report to include the results of a comprehensive survey of sewer laterals in the Beach Area neighborhoods and identify sewer facility improvements needed, along with a timeline and funding source(s) to accomplish such improvements. (P. 282)
- 6.6 Ensure the stability of residential neighborhoods in the Beach Area by continuing to require proper private connections to public facilities and services. (P. 282)
- 6.7 Construct a drainage system to serve the intersections of Kaye, Uhden and Third Street. (P. 283)
- 6.8 Support resident-organized Neighborhood Clean-up Days by offering City assistance up to twice a year to allow residents to discard larger items, beautify structures and landscaping, and promote neighborhood pride and involvement. (P. 287)
- 6.9 Improve street maintenance services in order to maintain a clean and orderly setting for the Beach Flats neighborhood. (P. 287)
- 6.10 Consider expanding existing program allowing individual wheeled refuse containers for residents that can be stored in a central location on site. Investigate other program improvements to reduce garbage problems. (P. 287)

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PARKS & RECREATION

- 7.1 Establish a new active park space South of Laurel to meet the needs of the anticipated population increase. (P. 144)
- 7.2 Maintain and improve existing Raymond/Uhden Park and Beach Flats Park. (P. 276)
- 7.3 Consider a permanent community garden to be relocated on an expanded Raymond/Uhden Park site. If required, one additional parcel adjacent to Raymond/Uhden Park, fronting on Park and Uhden (Parcel # 5-644-01) might be purchased to expand this Park. (P. 276)
- 7.4 Masterplan six-acre Depot site to accommodate 2+ acres of active recreational space and 300 parking spaces. (P. 278)
- 7.5 Evaluate cost-benefit analysis of incorporating Santa Cruz Feed lot site as part of park aggregation. (P. 278)
- 7.6 Improve the San Lorenzo River levee to provide an additional community "linear park" resource which serves as improved access to Downtown in accordance with river design concept plans. (P. 278)

SAFETY

- 8.1 Assign additional police officers to the Beach Beat as funding permits. (P. 31)
- 8.2 Consider creating a demonstration project of enhanced security in combination with community service officers working with locally based security patrols. The basic program element of such a demonstration project includes working with the Police Department and Beach Flats residents to identify:
 - the time of day/night when Police officers could best use assistance, and any seasonal variation;
 - the most appropriate composition of a roaming security patrol; and
 - the safest and most effective way for residents to participate in the program.(P. 31)
- 8.3 Continue efforts by the Santa Cruz Police Department to use a community-oriented policing approach in Beach Flats, emphasizing crime prevention and a high degree of interaction and communication between police officers and neighborhood residents. (P. 284)
- 8.4 Improve police presence to respond to the unusual pressures and problems in Beach Flats neighborhood resulting from an influx of tourists during summer months and the presence of criminal activity associated with increased drug traffic. (P. 285)
- 8.5 Implement a two-officer per day, year-round police foot patrol and a Community

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Services Officer for Beach Flats. (P. 285)

- 8.6 Continue to seek the assistance and cooperation of other law enforcement agencies and security resources, such as CNET and Seaside Company private security personnel, in providing for public safety in Beach Flats. (P. 285)
- 8.7 Improve fire protection capability by increasing fire flow in Beach Flats to respond to the unusual pressures and problems resulting from an influx of visitors during summer months. (P. 286)
- 8.8 Create an emergency access route in the Beach Area by promoting a priority shuttle lane on Beach and Third Streets during peak summer days. (P. 286)

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I. INTRODUCTION

A. DESIGN GUIDELINES

The Design Guidelines is both an appendix to the Beach and South of Laurel Area Plan (BA/SOLA Plan) and the General Design section of the General Plan/Local Coastal Plan. The intent of the document is to assist the city and the development community to achieve a high quality of aesthetic and functional design. Design review is intended to achieve orderly and attractive development, to protect and enhance the city's unique character and assets, and to ensure the stability and growth of public and private investment in land and improvements.

The purpose of the document is to promote development which respects the physical and environmental characteristics of the community and the site, and which reflects functional and attractive site planning and high quality design. The contents of the document are organized into the following topics:

- Goals and Objectives
- General Design Guidelines
- Planning Area Design Guidelines

The Goals and Objectives section states the comprehensive aims or purposes as identified by the General Plan and BA/SOLA Plan that relate to design. The Objectives describe actions to be taken to meet the goals. The General Design Guidelines describe methods to be used to attain the goals and objectives. General Design Guidelines apply to all types of development throughout the plan areas. Plan Area Design Guidelines provide direction for development within each Plan Area to be used in addition to the General Design Guidelines. Projects will be evaluated on their ability to meet the goals and objectives as stated herein.

B. DEVELOPMENT REVIEW PROCESS

Note: The following text describes proposed changes to the existing review process. Upon approval of these draft guidelines, the text will be revised to describe the adopted review process.

Chapter 24.08 of the City Zoning Ordinance specifies the various land use permits and findings that are required for all development in the City. However, the current regulations do not provide sufficient review oversight for development in the Beach and South of Laurel Area.

For example, the current zoning regulations do not provide for design review of duplexes or single-family residences on standard lots. In addition, all commercial additions totaling less

than 25% of the total floor area, or having less than \$50,000 valuation of improvements, do not require a Design Permit.

To ensure that the objectives of the Beach Area/SOLA Plan are met, it is necessary to exercise a higher degree of design review oversight than currently provided in the zoning district regulations.

The existing Design Permit regulations (Sec. 24.08.410) should be modified to require a Design Permit in the Beach/SOLA Planning Area for all residential, commercial, industrial or mixed-use building additions exceeding 15% of the existing floor area; any addition to the front of a building that is visible from a public right-of-way; and any new residential unit(s). It is generally anticipated that new single-family units and additions will only require a Design Permit with no public hearing. Most other development projects will require a Design Permit with Zoning Administrator review.

Following is a summary of the principal land use applications that will be applicable to the planning area.

Land Use Applications

1. Conceptual Plan Review

The City recently instituted a conceptual plan review process, whereby a proposed development project can be submitted for inter-departmental review, and comments can be prepared by designated department staff persons. There is currently no application fee for this process, and it is an invaluable method of receiving early comments and recommendations, before committing financial resources to a particular project. It is recommended that all major projects receive a Conceptual Plan Review before a Design Permit application is filed. Those participating in this process should allow approximately 3-5 weeks for comments to be received.

2. Design Permit

The primary land use permit required for most development projects in the Beach/SOLA will be a Design Permit. Zoning Ordinance Section 24.08.410, lists the types of development projects that require a Design Permit, and should be amended to include the type of projects referenced in subsection 'B' above. A Design Permit may be required for a principally-permitted, or conditional use, and can be processed concurrently with other land use applications which may be required (i.e. Variance, Use Permit, etc.).

The objective of a Design Permit is to promote the public health, safety and general welfare through the review of architectural and site development proposals. To approve a Design Permit, the Zoning Ordinance requires findings of consistency with the objectives of this document, as well as the General Plan and applicable zoning regulations. The general findings that are required are listed in Section 24.08.430 of the Zoning Ordinance. Development on sub-standard residential lots also requires additional findings listed in Section 24.08.440. Depending upon the complexity of the project and the other related applications to be processed for a development proposal, a Design Permit typically can take 6-8 weeks to process.

3. Coastal Permit

The entire Beach Area/SOLA planning area is located within the Coastal Zone. The type of development projects which will require a Coastal Permit are specified in Part 3 of the Zoning Ordinance. Applications for Coastal Permits can be processed concurrently with other applicable land use permits or building permit applications.

4. Other Permit Types

In addition to the land use application-types described above, a development proposal may also require other land use permits before a use or structure can be established. Other land use permits established by the Zoning Ordinance are listed below:

- Administrative Use Permit
- Variance
- Conditional Fence Permit
- Historic Alteration
- Residential Demolition/Authorization
- Special Use Permit
- Historic Demolition
- Reconstruction Permit

C. SUBMITTAL REQUIREMENTS

In order to properly evaluate a development proposal in the Beach Area/SOLA, it is important to provide a complete application packet, including site plan, floor plan, and architectural elevations which precisely detail the intended scope of the project. The following checklist shows the minimum amount of information that must be shown on the plans. (** Items marked with an asterisk are the minimum needed to submit for Conceptual Plan Review*)

General Information Required on all Applications:

- Photographs of surrounding, adjacent properties
- Six sets of complete plans (if no public hearing required)
- Twelve sets of complete plans (if public hearing required)
- One set of 8 1/2" x 11" reduction of all plans (if public hearing required)

1. Site Plan

- Appropriate scale: 1/8"=1' or engineer scale not less than 1"=40';
- North arrow;
- Lot dimensions;
- Existing and proposed building locations in relation to lot lines;
- Location of structures on abutting lots in relation to lot lines;
- Location of all trees if greater than 16 inches in diameter, measured two-feet above grade;
- Location of all significant land forms and physical constraints;
- For slopes exceeding 20%, slope contours certified by a licensed surveyor or civil engineer
- Note: The contour interval shall be five-feet

I. Introduction

- Preliminary drainage flow and ultimate disposition of water;
 - Parking layout, including stall size, backup areas and drives and curb cuts;
 - Location of existing and proposed landscaping (except for single-family), including total area and percentage of lot area;
 - Dimensions and nature of all easements;
 - Loading zones and outside storage areas (if applicable);
 - Location of refuse areas and enclosure description (if applicable);
 - Location, height and composition of screening fences, walls, etc. (if applicable).
- 2. Drainage and erosion control plan (if required by Code Section 24.14.050 and .060)**
- Disposition of runoff;
 - Details of drainage control facilities, including size and location.
- 3. Preliminary Grading Plan (required for Planned Developments)**
- Appropriate scale: Engineer scale not less than 1"=40';
 - Prepared by a licensed civil engineer or other qualified professional;
 - Five-foot slope contours;
 - Cut and Fill locations and total cubic feet;
 - Site section showing existing slopes and, and with crosshatching, the new slopes;
 - Erosion control measures as specified in Section 24.14.050 of the Zoning Code.
- 4. Floor Plan**
- Appropriate scale: Architect scale not less than 1/8"=1';
 - Interior layout and dimensions of all levels;
 - Finished floor elevations of ground floors (if in flood plain);
 - Total square footage for each level.
- 5. Elevations**
- Appropriate scale: 1/8"=1' or engineer scale not less than 1"=20';
 - All sides of each structure, to scale and dimensioned, including exterior stairs, ramps, elevators, down spout, and roof equipment;
 - Type of materials and color, labeled on elevations;
 - Screening treatment for HVAC equipment (include a cross-section of screening treatments)
- 6. Cross section**
- Appropriate scale: 1/8"=1' or engineer scale not less than 1"=40';
 - Primary street facade(s) of each proposed structure and their adjacent structures (or relevant portions) on neighboring properties; to scale and dimensioned, including topography and building and roof outlines;
 - Location of property lines and dimensioned setbacks, building separations and building heights;

7. Landscape Plan (except for single-family)

- Appropriate scale: 1/8"=1' or engineer scale not less than 1"=20';
- Size, variety, number and location of proposed plants and type of irrigation system;
- Existing plants to be removed and those to remain;
- Existing and proposed street trees (if applicable).

8. Signage Plan (if applicable)

- Elevations of all existing and proposed signs, location and arrangement;
- Material, size, colors and lettering;
- Method of lighting.

II. GOALS AND OBJECTIVES

II. GOALS AND OBJECTIVES

This section states the goals or comprehensive aims or purposes as identified by the General Plan/Local Coastal Program and BA/SOLA Plan. The objectives describe actions to be taken to meet the goals. Additional goals and objectives specific to individual plan areas are provided in the Plan Area Design Guidelines.

A. GOALS

- Improve the quality of life in the Beach and South of Laurel Plan Areas.
- Retain neighborhood character.
- Promote safe and secure streets.
- Develop linkages with Downtown.
- Enhance the seaside resort character.
- Promote tourist commercial uses.

B. OBJECTIVES

- Create a strong sense of place, with edges that have a beginning and an end, and with gateways and intimate residential streets clearly defined.
- Create a sequence of experiences and views in the landscape that lead to destination points.
- Enhance entryways to neighborhoods at definable intersections.
- Encourage distinctive architectural treatment at major intersections to accentuate community gateways.
- Strengthen the residential quality and character of neighborhoods by improving the appearance of architecture and landscaping.
- Protect and enhance the unique historic and architectural qualities which are present by establishing historic conservation districts.
- Maintain the heritage tree preservation program and use the existing heritage trees as a basis for streetscape design.
- Protect historic resources such as rock walls and stairs. Use existing historic resources as a basis for streetscape design.
- Expand the economic vitality of Downtown by transforming Beach Street into a "signature street" which will complement and complete the revitalized Pacific Avenue "signature" shopping district. Link these commercial areas.
- Implement traffic calming measures, such as textured paving and chokers, in Conservation Areas.
- Implement hierarchical streetscape design that promotes identification of streets as thoroughfares and residential streets. Use a consistent landscape design theme to create "imagable paths" through the community, such as palm tree lined streets which lead to the beach.

- Implement a street tree program to give scale and definition to the public walkways and promote a visually attractive, comfortable street with reduced heat and glare.
- Balance the needs of automobiles, pedestrians and bicyclists with wide sidewalks for promenading and pedestrian-oriented activities; narrow roadways to slow traffic; continuous bicycle routes; ample, accessible, at-grade pedestrian crossings or ramps with appropriately textured surfaces; and curbside parking, bike lanes or landscaped parkways to separate pedestrians and moving cars.
- Provide unobstructed access to all citizens, in compliance with Uniform Federal Accessibility Standards and State Title 24 Accessibility requirements.
- Provide public amenities such as benches, planters, lighting, street name and traffic sign posts, trash receptacles, public rest rooms, bicycle racks, public telephones and information kiosks that are functionally and aesthetically integrated into the streetscape for pedestrian comfort, convenience and safety.
- Improve the pedestrian connection to Downtown by upgrading and maintaining the two existing pedestrian stairways.
- Provide public art for aesthetics and contemplation.
- Underground utilities to eliminate visual clutter and provide a more aesthetic streetscape.
- Implement an area-wide sign program to eliminate visual clutter of oversized and inappropriate signs.

III. GENERAL DESIGN GUIDELINES

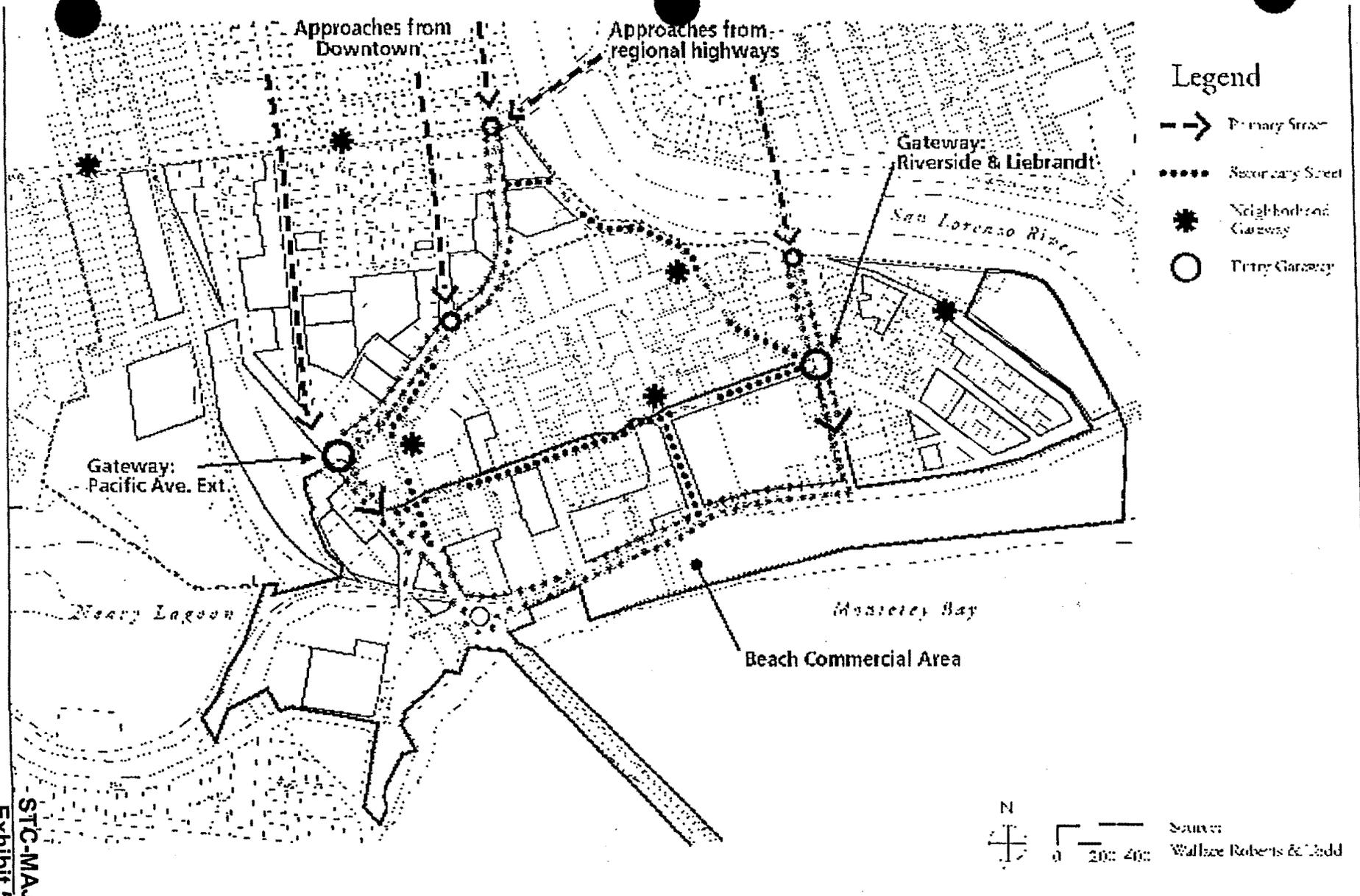
III. GENERAL DESIGN GUIDELINES

This section provides design guidelines that will be applied throughout the Beach and SOLA Plan areas. General guidelines are provided for:

- *Streetscapes and Entries* to be implemented in partnership between private developers and the city;
- *Site planning* to ensure neighborhood compatibility and appropriate design for building placement, circulation and parking;
- *Architecture* to ensure compatible and appropriate building design;
- *Landscaping* to ensure appropriate and adequate landscape design for building exteriors and open spaces;
- *Screening, Lighting and Security* to ensure appropriate and aesthetic walls, fences, screening, lighting, and noise attenuation;
- *Signage* to ensure adequate, aesthetic and well integrated sign design;
- *Conservation Areas* to ensure compatibility of new and remodeled developments within traditional neighborhoods.

A. STREETSAPES AND ENTRIES

Streetscape improvements are implemented in partnership between the city and private development. In order to meet the Beach and SOLA Plan Area goals and objectives, new development is required to contribute streetscape improvements adjacent to and within the public right-of-way. Some of these improvements will be implemented through a City Street Improvement Plan and portions may be implemented specific to development projects.



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 Exhibit 5
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 The Phipps Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Urban Design : Approaches & Gateways

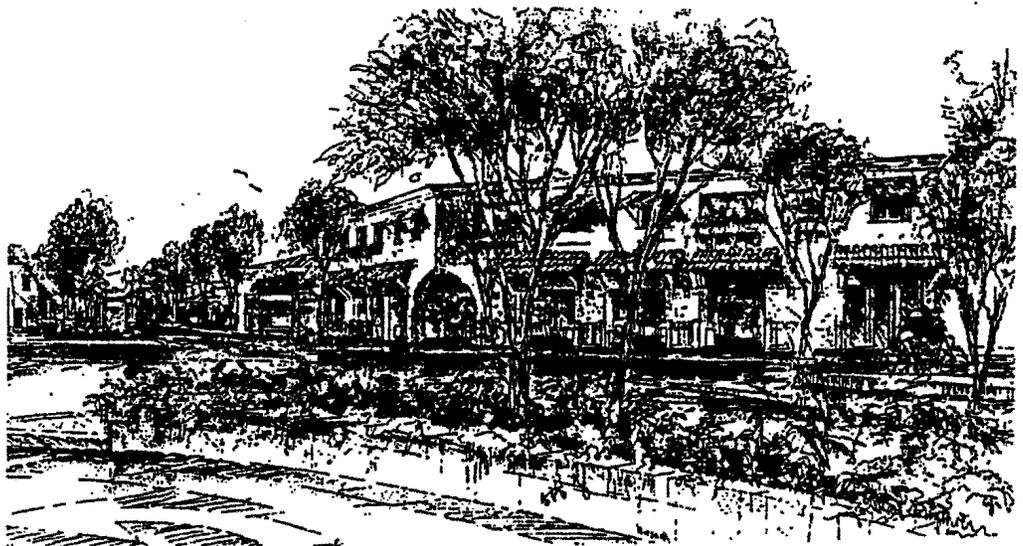
Figure 1

The following design guidelines describe improvements for specific streets and neighborhood entries as well as general landscaping, amenity and lighting improvements.

1. Specific Streets and Entries

Primary Circulation Streets

- Emphasize the importance of primary circulation streets with large scale trees regularly spaced in a formal pattern, a single style of decorative street lights and street furnishings used throughout, decorative brick paving at crosswalks, a consistent sign program and storefront landscaping.
- Maintain a strong sense of visual and pedestrian continuity along the entire length of the primary circulation streets.
- Introduce continuous curbside street trees along the north and south side of Laurel Street.
- Implement the San Lorenzo River Enhancement Plan along the Laurel Street extension and continue to the ocean waterfront.
- Introduce *Washingtonia robusta* palm trees as the street trees that identify Front Street as the primary connection to the Beach Area.
- Extend the Downtown streetscape design concept for Pacific Avenue south of Laurel Street to Front Street but use *Washingtonia robusta* palms as street trees to identify the area as the Beach Community.



Thacher & Thompson Architecture

- Use Pacific Avenue streetscape design elements, including Washingtonia robusta palms, on Riverside Avenue and Third Street to identify them as the primary circulation streets in the Beach Area.

Secondary Circulation Streets

- Distinguish secondary streets with large and medium sized canopy shade trees.



Thacher & Thompson Architecture

- Implement streetscapes appropriate to the style of redevelopment projects.
- Implement street furnishings that are compatible with the architectural style and community character of the adjacent neighborhoods.

Neighborhood Streets

- Create distinctive streetscapes that identify individual neighborhoods.
- Select medium sized trees which provide canopies, shade and distinctive foliage or flowers as neighborhood theme trees.



Thacher & Thompson Architecture

- Maintain and expand to the maximum extent practical, sidewalks along all streets and decorative paving at crosswalks.
- Implement brick sidewalks and stone walls in the Beach Hill Conservation Area.

Community Entries

- Identify community entries by maintaining them as large open spaces within the streetscapes.
- Encourage distinctive architectural and landscape features at intersections to enliven the space.
- Locate specimen focal point trees with distinctive form, foliage and or flowers to accent the entries.
- Utilize colorful flowering shrubs, groundcovers and annual flower plantings to accent the entries.
- Locate monuments such as stone or masonry pillars, gates, signs naming the community and public art at entries.
- Locate community name and directional signs at entries and design such signs to be compatible with the architectural themes and character of the community. Signs shall reflect permanence, elegance and community pride.



Thacher & Thompson Architecture

- Utilize textured paving such as interlocking brick within the street, crosswalks and sidewalks to distinguish entries.
- Frame views from the entry into the community with tall, vertical elements such as tree-lined streets, gateways and flag poles.

Neighborhood Entries

- Design entries to identify residential neighborhoods as separate from commercial areas. Contrast streetscape features to identify residential cross streets as different from circulation streets.
- Provide monuments such as pillars, gates and signs to identify entries into residential areas.



Thacher & Thompson Architecture

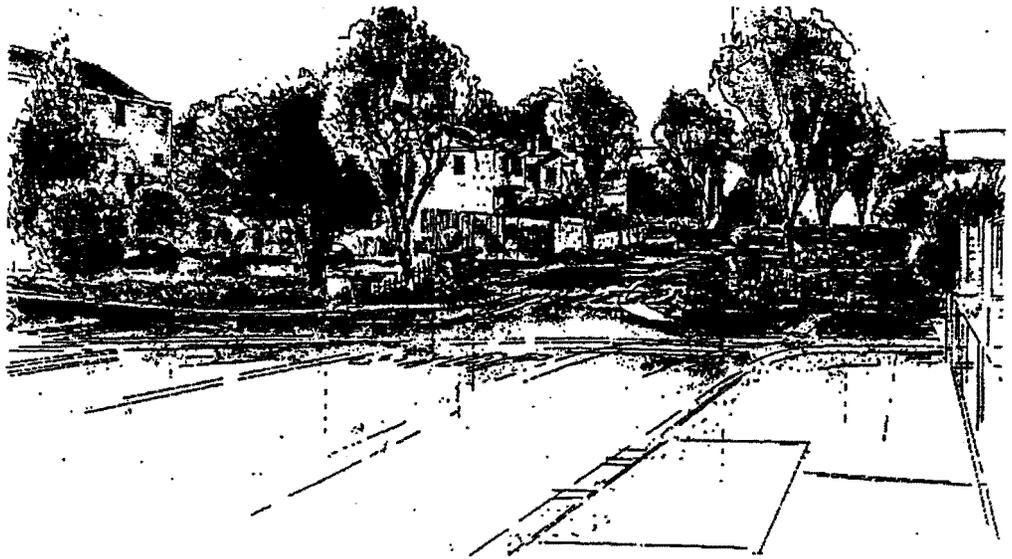
- Utilize closely spaced, medium and small scale canopy street trees to reinforce the sense of quiet and serenity associated with residential areas.
- Utilize "neckdowns" to narrow entries to residential streets, textured paving and similar traffic calming measures.

Community Focal Points

Pacific Avenue Circle at the Depot Site

Pacific Avenue Circle at the Wharf

- Implement traffic circles to improve traffic flow and create elegant entries and focal points.
- Maintain the open character of the traffic circle to visually contrast with narrow, tree-lined streets and accentuate views to the ocean.
- Emphasize the circular design form by planting street accent trees in a circular pattern around the traffic circle.
- Emphasize the circular design and prominence of the traffic circle with colored and textured pavement patterns. Utilize pavement patterns to distinguish crosswalks and bicycle lanes.
- Emphasize the circular design form with low walls, seating and other features that encompass the traffic circle.

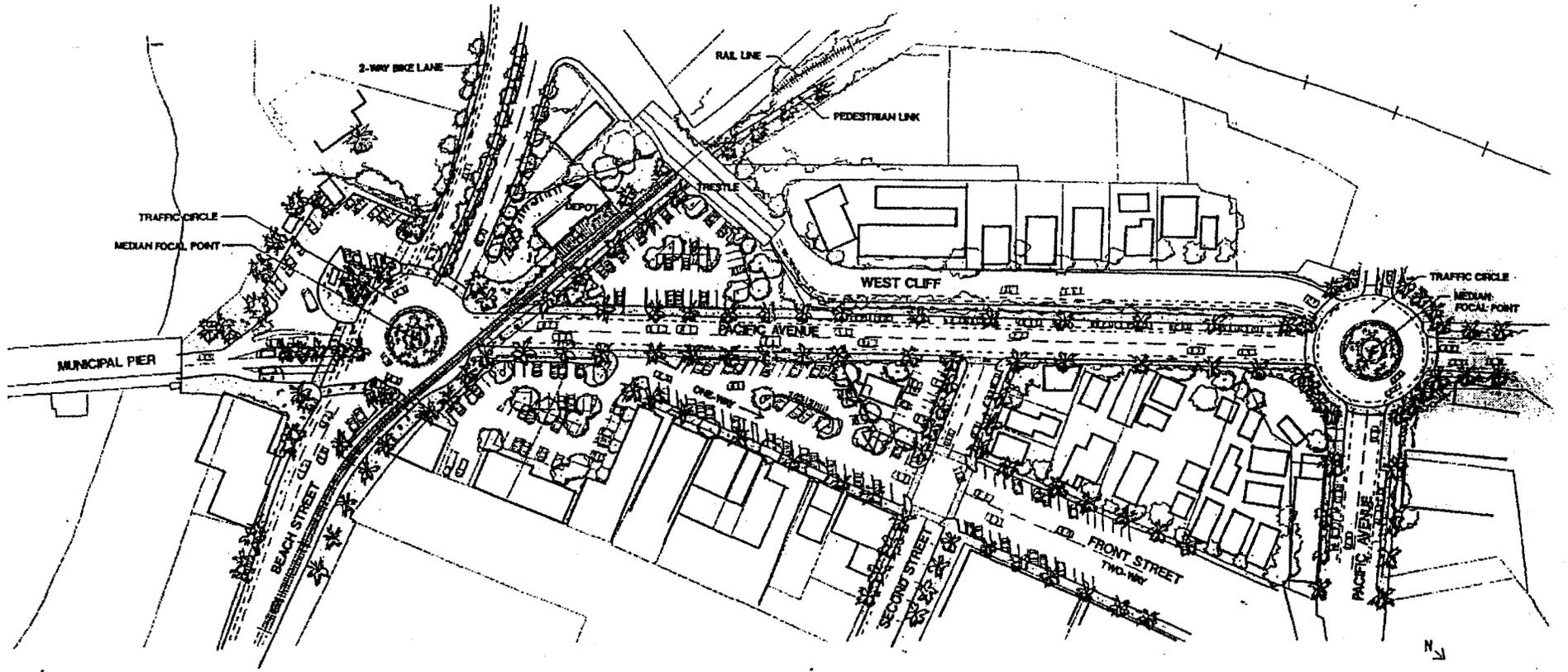


Thacher & Thompson Architecture

- Utilize the center open space as the area focal point. The focal point should be landscaped with bright colored flowers and plants and accented with public art or a fountain.
- Enhance the entry to the Wharf as a distinctive maritime gateway with architectural and landscape features.

2. Landscaping

- Landscaping shall be used as a unifying design element and a cost-effective, long-term aesthetic and environmental enhancement to the community.
- Landscaping can be in the form of planting beds, raised planters, berms, containers or window boxes.
- Landscaping shall generally use a three tier system of 1) groundcovers, 2) shrubs and 3) trees.
- Specimen trees shall be used as accents and focal points in locations such as community and neighborhood entries.
- Trees shall be used to create sheltering canopies and shade to reduce heat and glare over sidewalks, parking and streets.
- Plantings and walls shall be used to screen outdoor areas from wind and unsightly views.
- Vines on trellises or walls shall be used to soften the appearance of bare walls, deter graffiti and provide greenery in narrow spaces.
- In all commercial areas, container plants with colorful foliage and flowers are required adjacent to storefronts along walkways.
- Landscaping shall be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks or curbs, and decorative tree grates.
- All trees in paved areas shall be provided with "deep root" barriers and shall be planted in tree wells of sufficient size to promote healthy growth.
- Trees and large shrubs shall be located so they will not interfere with lighting, pedestrian and vehicular sight lines, and emergency access
- Tree spacing along streets shall relate to species size. Where view preservation or site constraints limit regular spacing, trees may be placed close together in groups.
- Implement water conservation techniques which include use of drought tolerant native species, limited use of turf, state of the art irrigation systems, minimization of run-off, application of surface mulch and soil improvements for water penetration and retention, and use of low water consumptive plant species.



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City of Santa Cruz and Associates, Inc.

City of Santa Cruz - Beach / South of Laurel Area Plan

Beach Gateway Concept Plan

- Maintenance shall consist of regular watering, pruning, fertilizing, clearing of weeds and debris, removal and replacement of dead plants, and repair and replacement of irrigation systems and integrated architectural features.

3. Amenities and Lighting

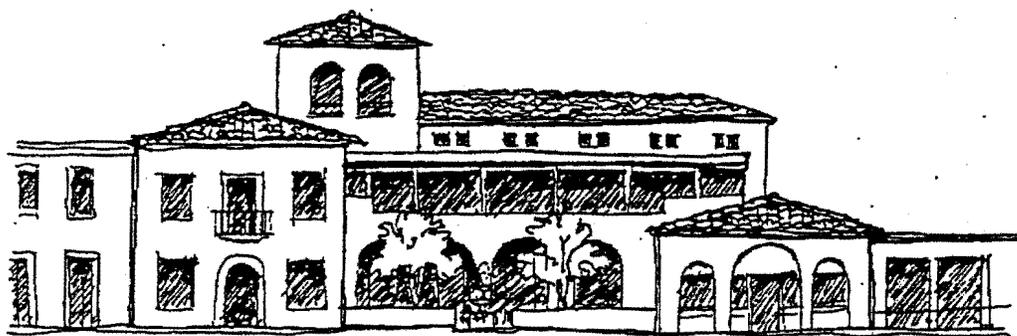
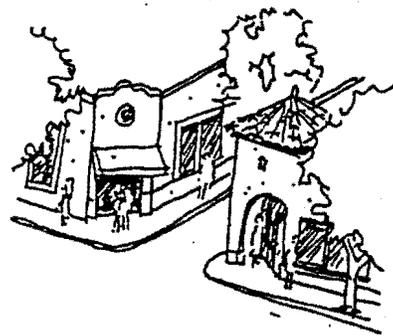
Select street furnishings that are compatible with community character and architectural styles. Furnishings shall be compatible in scale and form to the overall streetscape and facade plan. Furnishings shall not be heavy or massive to ensure that they do not visually dominate the street.

- All furnishings shall be consistent in design, materials and colors. Furnishings shall be durable, low maintenance, high quality and suitable for outdoor use.
- Seating shall be located at intervals along the street to provide resting spaces.
- Newspaper stands shall be consolidated into a single unit designed stand as provided in the Downtown.
- Bicycle racks shall be provided at convenient locations.
- Bus stop shelters, seating and signage shall be designed for comfort, convenience and integration into the overall streetscape design.
- A consistent sign program shall be created to provide important information and direction while adding an enlivening and aesthetic design element to the community.
- Artwork shall be located to enhance the streetscape and create focal points. Artwork shall be appropriate in scale and character to the surrounding streetscape and architecture.
- Decorative textured paving, such as interlocking bricks, shall be used for crosswalks at major intersections.
- A distinct sidewalk paving design of textured and colored concrete, bricks or stone may be used within an area to identify it as a special neighborhood or shopping district.
- Lighting shall be provided for safety, security and orientation.
- Lighting shall reinforce the streetscape design after dark and enhance the image and best features of the city during daylight hours.
- Lighting shall be directed to use areas to avoid waste, over lighting and glare.
- A single-head, ornamental, pedestrian scale light standard shall be selected and located along streets at an appropriate interval spacing. Locate similar designed double headed pedestrian/street lights at intersections.

B. SITE PLANNING

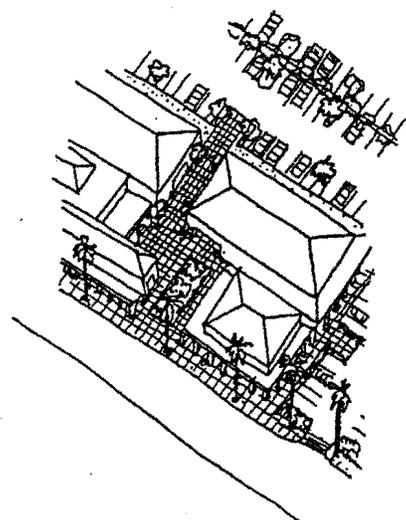
1. Compatibility And Building Placement

- The arrangement of structures, parking, and circulation areas, and open spaces shall recognize the particular characteristics of the site and shall relate to the surrounding built environment in pattern, function, scale, character and materials.
- The siting and design of structures and landscaping shall ensure that the development blends into rather than dominating the neighborhood.
- Where adjacent to single story developments, multiple story developments shall minimize scale through upper story setbacks, modular building units and other similar design techniques.
- Structures shall be sited in a manner that will complement adjacent land uses and circulation patterns.
- Building setbacks shall be proportionate to the scale of the structures and considerate of existing development. Larger structures require more setback area for balance of scale and so as not to impose on neighboring uses.



Building modules and setbacks minimize scale.

- Buildings located on corner lots shall integrate design features that create focal points at intersections such as angled corners and towers.
- The design and orientation of buildings and open space shall take advantage of available sunlight and, where possible, be sheltered from the noise of traffic or other incompatible uses.
- Multiple building developments shall be visually linked with arcades trellises or similar structures.
- Commercial development shall locate store fronts in proximity to streets and sidewalks and parking lots at the rear of structures.



2. Open Space

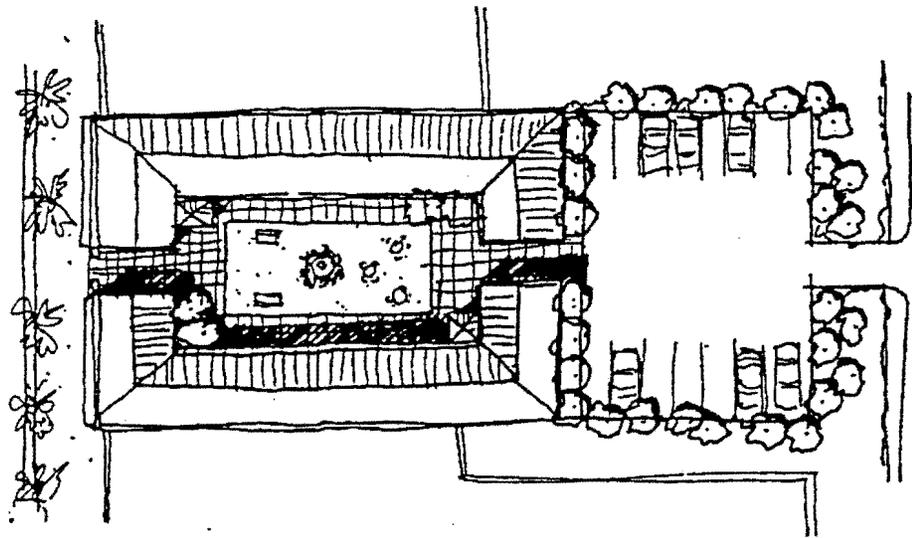
- Courtyard style developments are encouraged and shall have main entries oriented to the street.
- Multiple building developments shall be clustered to increase open space, enhance spaciousness.
- Standards shall be developed to address this issue.

Commercial Development

- Commercial development shall be designed to create plazas and pedestrian spaces for amenities such as shade and benches.

Hotels and Motels

- Recreational open spaces shall be centrally and conveniently located for guests.
- Private open spaces shall be directly accessible from guest rooms and shall be defined by railings, trellises, canopies or other architectural features designed as an integral part of the building.



Hotel open space is provided in a central courtyard - parking is located at the rear of the building.

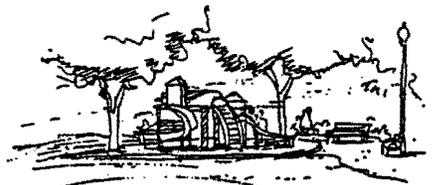
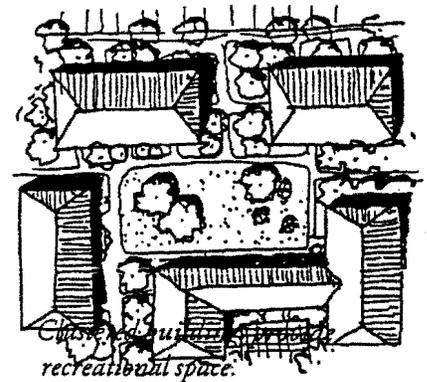
Multiple Family Residential

- All developments shall provide a common open space such as a landscaped entry or courtyard. Larger developments shall provide common open space, recreational open space, tot lots and private open space.
- Small scale developments shall provide a patio courtyard or similar passive open space.
- Family-oriented developments shall provide active recreational areas such as play lawns, paved play courts and swimming pools.
- Recreational open spaces shall be centrally and conveniently located for all residents.
- Private open space shall be directly accessible from the residential unit and shall be defined by railings, trellises, canopies or other architectural features designed as an integral part of the building.

3. Vehicular Circulation and Parking

Driveways and Entries

- The visual impact of driveways shall be minimized through appropriate location and landscaping.
- Site access and internal circulation shall promote safety, efficiency and convenience. Avoid conflicts between vehicles and pedestrians, and provide adequate areas for maneuvering and accommodating service and emergency vehicles.
- Visual clearance shall be provided at street intersections and driveway areas.
- Vehicular access from an alley is preferred. The number of site access points shall be minimized and located adequate distances from intersections.
- The use of common or shared driveways which provide access to more than one site is encouraged.
- Broad expanses of concrete driveways should be avoided. Textured concrete, decorative pavers, paved strips divided by lawn or groundcovers and turf block paving are suggested methods of improving driveway appearance.
- Entries to commercial, hotel and motel and large scale multiple family residential developments shall be enhanced with landscaping and may include low profile signage.



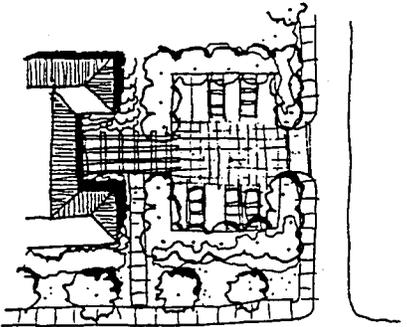
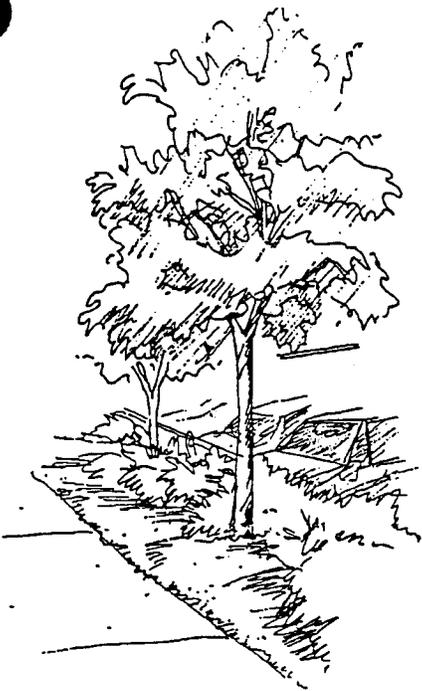
Tot lot



Pool and Plaza



Parking

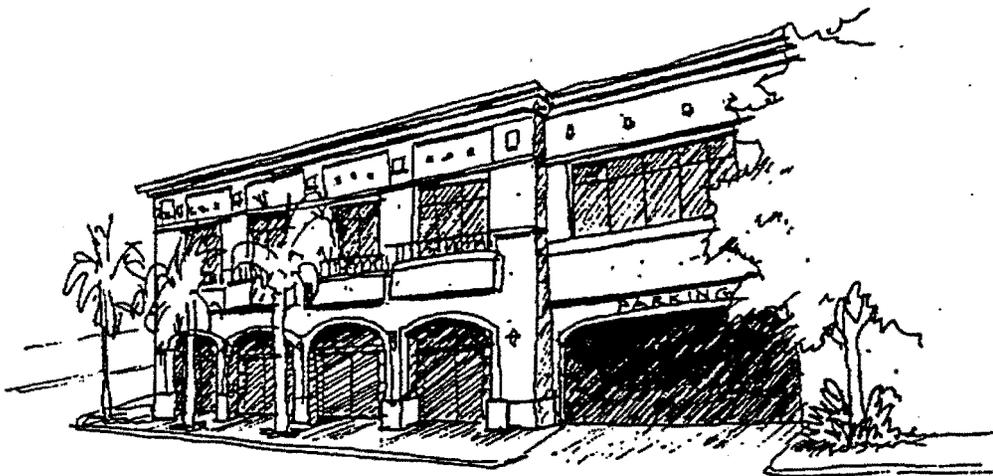


Landscaped parking court accessed from side street.

- Locate parking at the rear of buildings wherever possible or to be as unobtrusive as possible.
- Parking shall not be the dominant visual element of the site. Landscaping and varied setback widths are effective in reducing the visual impact of large, open parking areas.
- Parking which is visible from the street or other areas exposed to public view must be screened and softened by landscaping, low screen walls or a combination of elements.
- Views of parked cars and headlights must be screened from the public right of way and from adjacent structures with walls and or landscaping.
- Visually interrupt broad, bleak expanses of paving with landscaping.
- Provide trees in parking lots to shade cars and pavement to reduce heat and glare to enhance the comfort of pedestrians.
- Provide perimeter landscaping, including trees, shrubs and groundcovers to enhance adjacent streetscapes and buffer adjacent uses.
- Parking facilities shall be designed in a manner which links the structures to the street sidewalk system as an extension of the pedestrian environment. This can be accomplished by using enhanced paving, trellis structures and other special landscape treatments.
- Design parking so that pedestrians walk parallel to moving cars and reduce the need for pedestrians to cross parking aisles and landscape areas.
- Locate detached garages at the rear of properties and utilize alley access to garages wherever possible.
- Where garages must be located facing streets, they shall be minimized by placement at a lower elevation than the street or recessed within the building facade. Continuous rows of street-facing garages are not allowed.
- Where lot area constraints require cars to be parked adjacent to residences, a pergola or similar architectural feature shall be provided to minimize the visual impact of the parked car.
- Carports may not be located adjacent to streets or other highly exposed areas.
- Parking courts shall be treated as landscaped plazas with enhanced paving surfaces, shade trees, landscape planters and pedestrian walkways.

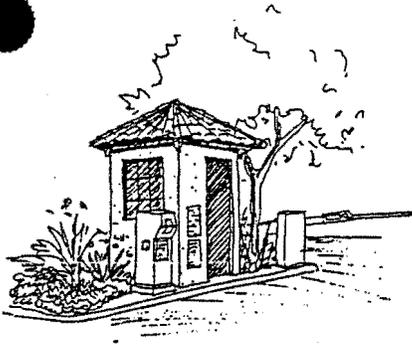
Parking Structures

- Parking structures shall be located at the rear of commercial or mixed uses. Where parking structures occupy an entire block, the structure shall be faced with commercial or mixed uses on all commercially active streets.
- Parking structures exteriors shall maintain the same high-quality architectural design and construction standards as all other commercial buildings.
- Parking structure architecture shall demonstrate compatibility with Conservation District and Plan Area community character. Compatibility can be demonstrated through architectural style, building scale, mass and form, and building materials and colors.
- Parking structure design shall be compatible with the neighborhood, block and adjacent structures.
- The large scale and mass of parking structures shall be alleviated through wall offsets, pilasters, arched openings and other distinctive design elements.
- Cars parked within structures and on open top levels shall be screened as much as possible with architectural elements, trellises and landscaping.
- Decorative elements such as cornices, balustrades, finish materials, colors and lighting shall be used to add interest and integrate the structures within the community.



Parking structure is located behind the commercial building and accessed from a side street. The same architectural details are used fro the entire development.

Parking Accessory Structures and Signage

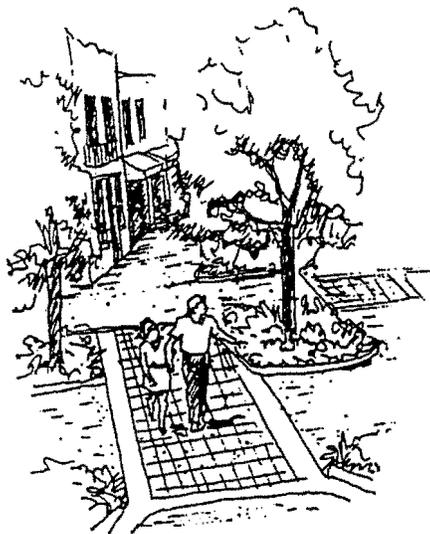


Architectural details and landscaping can enhance a parking lot ticket booth and gate.

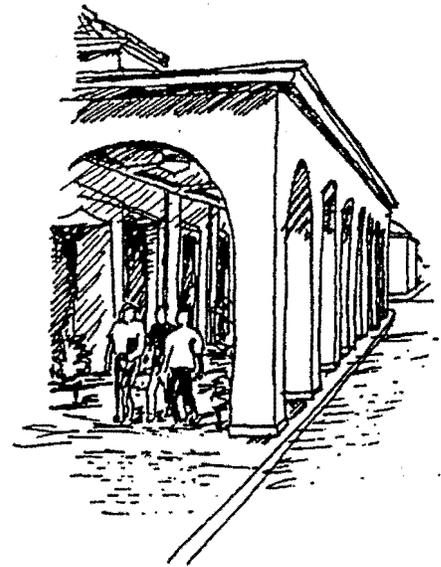
- Ensure that parking lot structures, such as ticket booths, gates and fencing are designed to be aesthetically pleasing and appropriate to the surrounding community architectural styles.
- Gates and fencing shall be designed to be aesthetically pleasing and compatible with the community architectural styles. Decorative wrought iron or a combination of masonry pillars and wrought iron is preferred.
- Ensure that all signage is functional, aesthetically pleasing and compatible with surrounding architectural and community character.

4. Pedestrian Circulation

- Sidewalks shall be accessible, with relatively smooth, non-slip walking surfaces. In private development, the use of brick, interlocking pavers, enhanced concrete or similar surfaces compatible with the development architectural style is encouraged. At a minimum, decorative paving shall be used to delineate pedestrian crossings in vehicular use areas.
- In commercial areas, sidewalks shall be adequate to provide pedestrian amenities and uses such as sidewalk cafes.
- Separate vehicular and pedestrian circulation wherever possible and utilize raised pathways, landscape plantings, bollards and other elements to create separation.
- In multiple family residential complexes and hotel/motel developments, pedestrian walkways shall be provided to link dwelling units with common open space areas, recreational and support facilities, parking areas and the street. Appropriate paving shall be used where pedestrians are likely to cross landscaped areas.



Enhanced paving and landscaping create an aesthetic and safer street crossing.



Pedestrian arcade in commercial district.

ARCHITECTURE

Compatibility

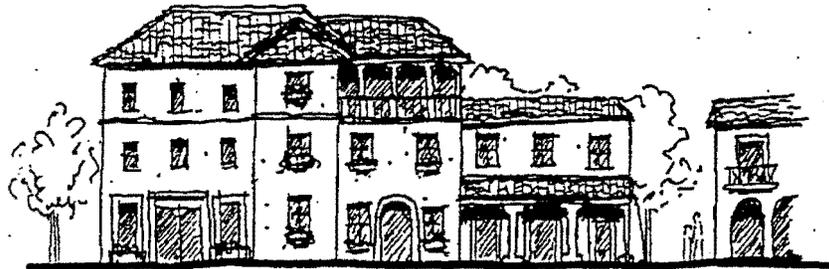
- Building design shall demonstrate compatibility with the community as a whole, the streetscape or block, and adjacent structures. New buildings shall emulate the finest architecture of the community.
- New and renovated developments shall exhibit high quality design and construction that will enhance the community.
- Building design shall demonstrate compatibility with community character. Compatibility shall be demonstrated through architectural style, building scale, mass and form, and building materials and colors.
- All structures within a development, including accessory structures such as carports, garages and storage facilities shall share a common architectural theme and design characteristics for overall development unity.
- The chosen architectural style shall be employed on all building elevations. The designer is expected to vary building form, details and siting to create visual interest.
- Additions or remodeling to buildings that exhibit a distinct historic architectural style shall adhere to the guidelines for Conservation Districts in Section III G.
- New buildings located adjacent to buildings that exhibit a distinct historic architectural style shall adhere to the guidelines for new construction in Conservation Districts in Section III G.
- Remodeling of existing buildings that do not exhibit a distinct historic style shall be in accordance with the design guidelines and shall incorporate elements of the design theme identified for the Plan Area.
- New developments shall incorporate elements of the design theme identified for the Plan Area.
- Standardized "corporate" architectural styles associated with chain-type facilities shall be modified to be compatible with the Plan Area design themes.



Single story buildings with minimal facade interest are incompatible with the larger building that exhibits a distinctive architecture style.

2. Proportions

- Building proportions (relationship of height, width and depth of a structure) shall be compatible with the prevailing proportional relationships along a street or block except in areas that have been identified to be developed with larger scale buildings.
- The height of new development shall "transition" from the height of adjacent development. This can be achieved by varying the height of the building so it appears to be divided into distinct elements and/or articulating the building facade by horizontal and vertical offsets in wall planes.
- Building articulation shall be used to create interest and reduce building scale. This can be achieved through varying heights and setbacks within the same building, offsetting wall planes and adding architectural interest with roof overhangs, awnings, trellises, windows, moldings and other elements.
- Second stories shall be differentiated from the lower floor by wall offsets, stepbacks, balconies or other features. Standards shall be prepared to address this issue.



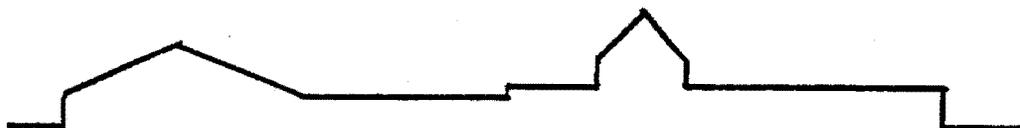
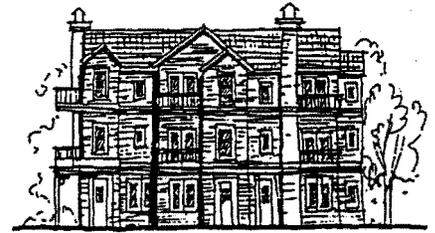
Building mass is divided into smaller parts and third story is stepped back to transition to neighboring two story building.

3. Building Envelope

- Building envelope design shall adhere to the allowable height, size, projections, setbacks, etc. defined by the city zoning ordinance and also incorporate creative design application of volume, mass and roof line.
- Community compatibility can be achieved through replicating the strong volume and mass exhibited in historic structures. For example, recessed doors and windows can be used to suggest thick masonry reminiscent of Spanish Colonial Revival architecture. Bay windows and porches can be used to provide the sculptural, three dimensional qualities of Bungalow architecture.
- Building volume and mass are partially defined by facade treatment. Facades shall be designed to be proportional to the overall building and reflect the architectural style. Building volumes shall be reduced through wall offsets or projections. Such variations shall be designed so that there is no more than 25 linear feet of unvaried facade.



- For multiple family residential developments, dwelling units shall be individually recognizable. This can be accomplished through varied roof lines, setbacks, projections, balconies, color and other features that set units apart from one another.
- Building roof lines make a profile against the sky and the design of profile helps define community character. Roof line design shall be consistent with existing block or neighborhood patterns where appropriate.
- Roof design shall be an integral part of the overall building design. It shall be of a scale and proportion so as not to appear as an afterthought or appendage.
- Hipped or gabled roofs are encouraged with roof slopes comparable to surrounding structures.
- Spanish Colonial Revival and related Mediterranean style buildings may have flat roofs completely surrounded by a parapet. Parapets shall incorporate moldings, curvilinear decorative shapes or other finish features. Flat roofed buildings must incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance.
- Roof materials shall be appropriate to the building' architectural style. Composition and wood shingle are appropriate to the Bungalow and Victorian styles and red clay tile is appropriate to the Spanish Colonial Revival style. Materials with highly reflective surfaces are not allowed.



Undesirable roofline is flat and monotonous. Sporadic gable roofs have unrelated slope forms.



Desirable roofline repeats steep pitched gable roof forms of Victorian architecture.



Desirable roofline repeats low sloped gable roof forms of Spanish Colonial Revival architecture. The domed tower creates an accent in the skyline. The curved form is repeated in a parapet surrounding a flat roof.

- Rooftop outdoor living spaces such as decks and gardens, shall be integrated into the building architecture. Step backs, parapets and other features shall be used to provide privacy and screening of utilitarian areas and equipment.

4. Texture and Detail

- Facade details that reflect the building architectural style shall be used on all building elevations, not just the street facing or entry facade.
- Monotonous, uninterrupted expanses of wall shall be avoided. Recesses, projections, columns, openings, ornamentation, materials and colors shall be used to add texture and detail. Blank walls shall not normally exceed 10 feet of lineal frontage along a street.
- Materials and colors shall be compatible with the community character, streetscape or block and adjacent structures.
- Light colors shall be used for major buildings. Colors are accentuated on large wall expanses so it is recommended that the next lighter value of the selected paint chip sample be used on such expanses.
- Spanish Colonial Revival and related Mediterranean styles shall have white, off-white, light or medium value, warm-toned, colored walls. Stained wood trim is preferred, however, trim may also be painted a darker value color than the main building color. Tile roof colors shall be natural terra-cotta tones ranging from pale peach to red.
- California Bungalow cottages and related vernacular styles shall be medium to dark stained wood or painted white, off-white, or light to medium value colors. Contrasting colors shall be used to accentuate door and window frames, porch railings and other distinctive features. Roof colors shall be subtle and compatible with the selected building colors.
- Victorian and related styles shall be painted colors appropriate to the architectural style. Contrasting colors shall be used to accentuate door and window frames, porch railings and other distinctive features. Roof colors shall be subtle and compatible with the selected building colors. Historic color schemes are provided on page III-43.
- Where multiple color combinations are used, they shall be subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features and heighten interest.
- The selected material and color palette shall be used on all exposed sides of a building.
- Contrasting materials, textures and colors shall be used with subtlety and only as accents.
- Materials shall be high quality and durable.

- Exterior wall materials shall be stucco, wood shiplap siding, or other high-quality materials appropriate to the architectural style. Plywood, plastic, aluminum siding and similar materials are not permitted.
- Large expanses of materials with reflective surfaces and intense vibrant color compositions are not allowed.
- Corporate color striping, wall logos, murals or other paint and color combinations used to transform buildings or building elements into a sign or advertising are not permitted.
- City Design Permits are required for murals, pictorial mosaics and other wall art intended for public view.

5. Openings

- Building entries, windows and other openings shall be compatible in location, scale and pattern to other structures on the street or block.
- Individual residence and business entries shall be readily identifiable. Entries can be articulated with recesses, projections, porches and other distinctive elements.
- Long monotonous access balconies and corridors which provide access to five or more residential, hotel/motel, or office units are not allowed. Access points shall be clustered in groups of four or less. Separate entries for each unit are preferred.



Desirable



Undesirable

- Stairways must be integrated into and complement the overall architectural form and style. Simple, functional design is encouraged. Flimsy, open metal, prefabricated stairs are not allowed. Uncovered stairs shall be screened from view with wing walls or landscaping.
- Balconies, porches, patios and other attached open spaces shall be integrated into the overall architecture.
- Entries and windows shall contribute to the volume, mass, proportion and texture of the building. They shall be designed as an integral part of the overall building design and shall reflect the building's architectural style.

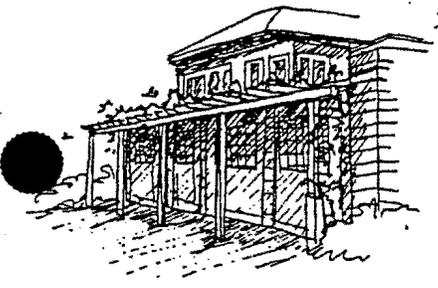




Street-Facing Garage: Scale is minimized and style is compatible with architecture.

Residential

- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.
- In residential developments, entry design shall promote security and privacy. Entries shall be clearly visible from nearby street frontages and privacy can be enhanced by the use of patios or courtyards at individual entries.
- Private open spaces shall be distinguished through the use of balustrades, trellises, screening walls and other architectural features.
- Private open spaces facing streets or common areas are encouraged to be designed with openings for "eyes on the street" views.
- Where garages must be located facing streets, they shall be minimized by placement at a lower elevation than the street or recessed within the building facade and framed by balconies, porches, trellises or other architectural elements.



Trellis frames street facing garage

Commercial Development

- The facades of commercial buildings shall provide appropriate proportions of transparent glass in the entry street facing wall to promote visibility between the street and building interiors.
- Street fronting windows shall be no more than 30 inches above the sidewalk level.
- Bay windows may project into the public right-of-way provided there is adequate area for pedestrian circulation. Bay windows shall maintain glazing on all projecting faces and may not exceed 15 feet in width. For ground floor windows, there shall be a clearance of at least 12 inches between the bottom of the projecting bay and the sidewalk.
- Building entry porticos may project into the public right-of-way provided there is adequate area for pedestrian circulation.
- Colorful landscaping shall be provided to frame doorways or accent windows. Landscaping may be sidewalk planters, containers or window boxes.



First level carports are accessed from alley.

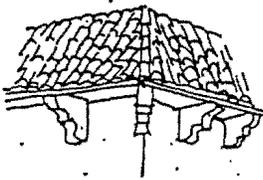


6. Spanish Colonial Revival

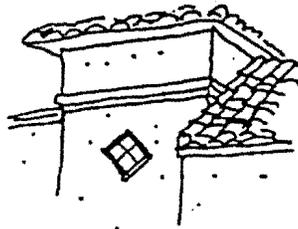
- "Spanish Resort" architectural styles shall be used to establish the design theme for the new development in the Beach Commercial and Beach Flats Plan Areas. While Spanish Colonial Revival is considered dominant, the architectural styles of the Beach Area includes a mix of characteristics of Mission Revival and Mediterranean architecture resulting in a general "Spanish Resort" style. Architecture shall adhere to the following design guidelines:
- Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, decorative iron and tile details or other features typical of the Spanish Colonial Revival style.
- Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.
- Building walls shall be stucco and colored white, off-white or very light to medium value, warm-toned hues.
- Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.
- Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet.
- Parapets shall incorporate curvilinear decorative shapes and molding.
- Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance. Wall surfaces have decorative texture or ornament concentrated around doors and windows.
- Tile roof colors shall be natural clay terra cotta tones.
- Stained wood is preferred, however, trim may also be painted a darker value color than the main building color.
- Wood sash or casement windows shall be used to maintain compatible architectural character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established architectural style.



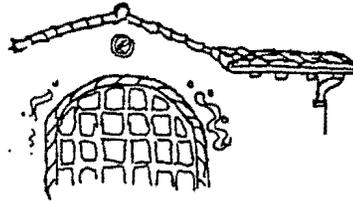
Spanish Colonial Architecture



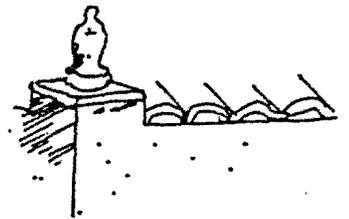
Mansard Tile Roof



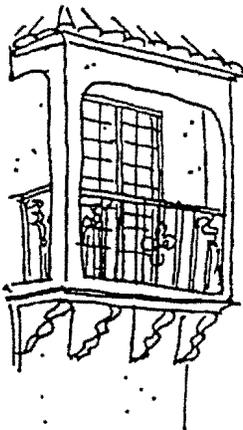
Banding and Tile Detail



Plaster Detail



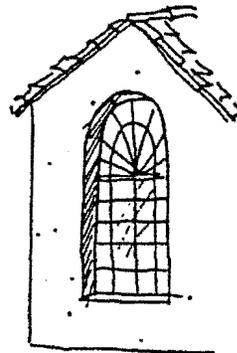
Parapet Detail



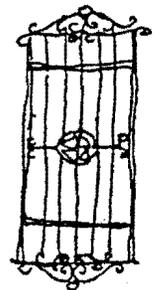
Balcony



Wood Panel Door



Arched, Multi-Paneled Window



Decorative Metal Window Grille

7. Victorian

- Victorian historic architectural styles shall be used to establish the Beach Hill design theme for new development and in Beach Hill and South of Laurel Conservation Districts. Building design shall be derived from one of the following styles and shall adhere to the traditional elements inherent in that style. "Victorian" includes the following as described in the Santa Cruz Historic Building Survey:

Eastlake (1880's) is probably the most common 19th century style in Santa Cruz, found in very simple cottages and large elaborate houses, often in combination with elements of the Stick Style. Principally a style of ornament derived from the English farmhouse designer, Charles Eastlake, and easily worked on machinery that was developed and made widely available in the 1880's. Irregular in plan and outline with gabled roofs, sometimes with flaring eaves and square bays. Frequently having profuse application of ornamental detail including spindles, curved brackets, incised carving, grooved moldings, sunbursts, and flash glass.

Stick-Eastlake (1880's) is rarely found in its pure form in Santa Cruz, but is frequently used in combination with Eastlake. In addition to the Eastlake ornament there is an overlay of plain or molded stick work that resembles half timbering and suggest the balloon frame construction of the time, often by extending window frames beyond the windows. Curved brackets are usually replaced by diagonal brackets.

Queen Anne (1885-1900) is less formal than earlier Victorian styles. It sought to be picturesque with an asymmetrical plan, complex roof line, corner towers, and gables. Frequently displaying a variety of textures and colors in bands of different siding materials including brick, clapboard, and shingles. Having bay windows of various shapes, porches, and balconies and a variety of predominantly classical ornamental details. In Santa Cruz the Queen Anne, Colonial Revival, Shingle, and Craftsman styles all blend together easily.

- Buildings shall be wood frame with traditional wood siding such as shiplap or shingle. Foundations and first floors may be brick or stone if appropriate to the selected architectural style.
- Hipped, gabled and other roof forms indicative of Victorian architecture are preferred.
- Roof materials shall be appropriate to the architectural style. Composition or wood shingle roofs are most appropriate for Victorian styles.
- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.



Eastlake



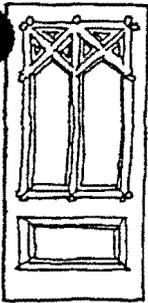
Queen Anne



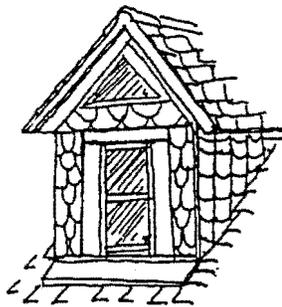
This Vernacular structure incorporates Victorian elements including a bay window, porch and steeped pitched roofs.

- Recommended historic Victorian color schemes relate to three style transitions that occurred from the 1870's to the turn of the century:
 - Pre-1884 buildings were painted pale colors, usually tans or white with a deeper toned trim of the same color. Window sashes were black or white and wood roof shingles were stained green, red or black.
 - From 1884 to 1895 darker colors were used and there was a trend to use two exterior materials, such as shingles on the second story and wood siding on the first story. Each material and its trim was painted a different color, resulting in four colors. Second stories were usually lighter colors. Trim sashes were usually black, deep red and sometimes white. Wood shingle roofs were stained green, red or black.
 - About 1885 colors were affected by the classical influence. Main body colors were lighter colors such as grays, yellows and tans. The trim was often white and the window sashes were generally black. Wood shingle roofs were stained green, red or black.

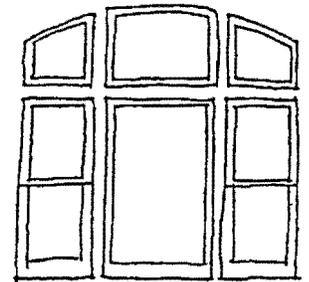
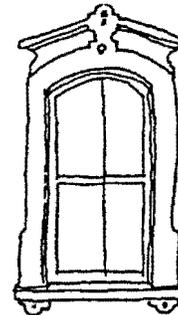
Typical Victorian design elements are shown below.



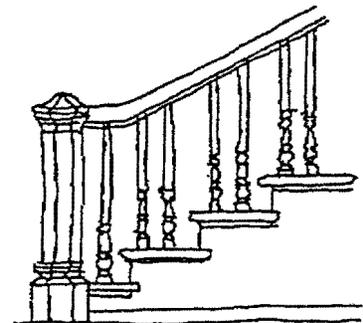
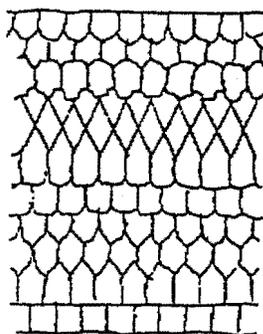
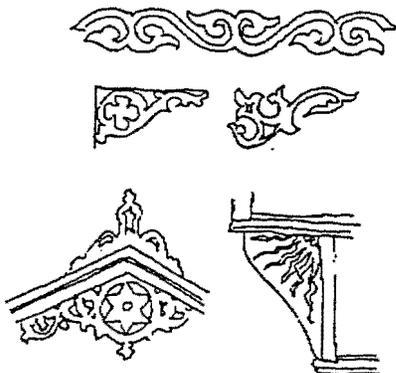
Doors



Gables



Windows



Decorative Wood Details

D. LANDSCAPING

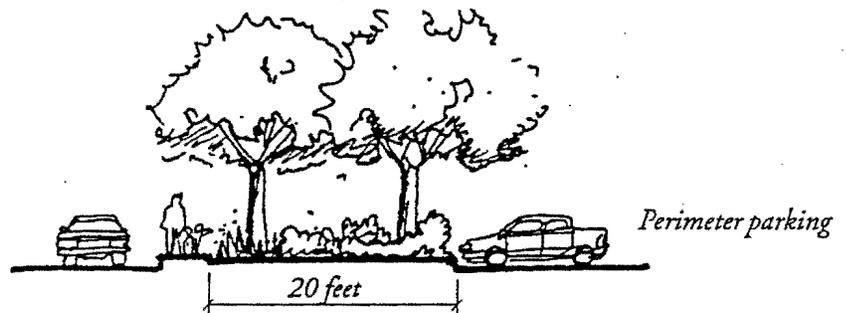
1. General Guidelines

- Landscaping shall be used as a unifying design element and a cost-effective, long-term aesthetic and environmental enhancement to the community.
- Interior courtyards and passages are encouraged and shall be planted with colorful, perennial and annual plant species. A combination of trees, shrubs and groundcovers shall be used to frame, soften and embellish the quality of the development, to screen undesirable views and to define development boundaries
- Landscaping shall be in scale with adjacent structures and be of appropriate size at maturity to accomplish its intended purpose. Landscaping shall be used around the base of buildings, walls and fences to soften the edge between pavement and structures.
- All areas of a property not covered by structures or vehicular, pedestrian, recreational use areas shall be landscaped with plants. Lawn areas should be minimized.
- Lush, colorful, scented, eclectic and dramatic-form plants are encouraged to enhance the character of the neighborhood and to promote individual development identity.
- Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.
- Plant materials shall be selected for compatibility with the neighborhood. For example, the popularity of exotic flowering plants and specimen trees in Victorian gardens are evident in the Beach Hill neighborhood. Native drought tolerant species should be encouraged.
- Landscaping shall be designed to enhance the development architectural style. For example, bougainvillea, palms and other Mediterranean and tropical plants are associated with the "Spanish Resort" style.
- Landscaping shall be planned and maintained to avoid creating "hiding places" for possible criminal activity.
- Development entries shall be accented with trees, shrubs and or flowering plants. Flowering or multi-trunked trees are encouraged.
- All planter areas shall be adequate in size to achieve the design purpose and to promote healthy plant growth.

- Plant materials shall be maintained in a healthy growing state and all diseased or dying plants shall be promptly replaced.

2. Parking Facilities and Screening

- Parking areas shall be planted with trees to reduce heat and glare and to provide visual relief from broad expanses of paving. Shade tree shall be planted around the perimeter and within the lot. The selected species, size and location of the trees should provide adequate shade coverage within five years of installation. Selected tree species shall be tolerant of reflected heat and glare and minimal areas of exposed soil surface.
- Landscape perimeter planting shall enclose parking areas on all sides exposed to public view. Recommended planter width is 20 feet on major thoroughfares. Planter widths may be narrowed to 15 feet if screening walls are also provided or if it can be determined that adequate screening is provided.
- Perimeter planting shall provide dense shrubbery or a combination of shrubs and solid masonry wall adequate to screen the expanse of parking, car headlights and reflective car surfaces. The screening walls and shrubs shall be three to four feet in height.



- Landscaping in and around entry drives shall be designed to maintain sight distances.
- Landscaping shall be protected from vehicles and pedestrians. Protection devices include raised planting surfaces, depressed walks, curbs, wheel stops and tree guards. Stepping stones or sidewalks shall be provided in places where pedestrians are likely to cross landscaped areas. Flexibility may be allowed for temporary recreation and entertainment uses.
- All utility and maintenance areas shall be screened with trees and shrubs or a combination of plant materials and walls or fencing.
- Parking structures shall be densely screened with plants on all sides of the structure exposed to public view except where ground level retail uses occur. Recommended planter width is 15 feet minimum.



Parking Structure

3. Commercial Building Facades

- Permanent containers for flowering plants, such as window boxes and planters, are required for building facades.

- Trellis structures are encouraged, particularly on upper portions of buildings that are stepped back from the street. Trellises above 8 feet in height may project a minimal distance into the public right-of-way.
- Flower boxes may project into the public right-of-way up to 12 inches. Flower boxes may project from windows and balconies by 12 inches.
- Flower boxes shall be designed with high quality, durable materials that are compatible and integral with the building architecture.
- Flower boxes shall utilize watering conserving irrigation methods and drip trays or drains to minimize draining onto sidewalks.
- A mix of perennial and annual flowering plants are encouraged to provide continual color and emphasize seasonal changes.

4. Hotel and Motel Open Spaces

- Project perimeter open spaces, including street setbacks, spaces between buildings and parking area perimeters, shall provide a combination of plant materials such as shrubs, groundcovers and shade trees to soften expanses of walls and pavement and contribute to the community streetscape.
- All developments shall provide a combination of walkways and areas landscaped with trees, shrubs and groundcovers.
- Smaller scale developments shall provide a landscaped entry as the minimum requirement, and, where space is available, a central patio courtyard.
- Recreational open spaces shall provide a combination of plant materials such as lawn, shrubs, groundcovers and shade trees and "hardscape" materials such as paved play surfaces, patios, decks, and shade structures.
- Family-oriented developments are encouraged to provide active recreational areas such as play lawns, paved play courts and swimming pools.
- Private open space shall provide a relatively smooth, non-skid hard surface suitable for outdoor living amenities such as tables and chairs.
- Ground floor private open spaces shall be planted with shrubs, vines or other suitable plant materials to soften paving, walls and fences.



5. Multiple Family Open Spaces

Project Perimeter Open Space

- Project perimeter open spaces, including street setbacks, spaces between buildings and parking area perimeters, shall provide a combination of plant materials such as shrubs, groundcovers and shade trees to soften expanses of walls and pavement and contribute to the community streetscape.

Common Open Space

- All developments shall provide a combination of walkways and areas landscaped with trees, shrubs and groundcovers.
- Smaller scale developments shall provide a landscaped entry as the minimum requirement, and, where space is available, a central patio courtyard. Larger scale projects shall provide similar common open spaces and common open spaces may include recreational open spaces and tot lots.

Recreational Open Space

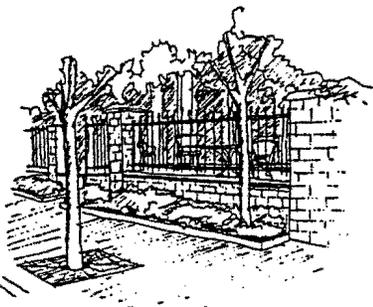
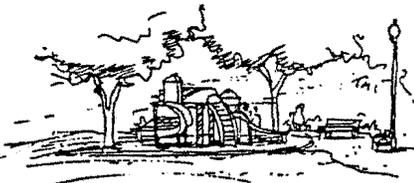
- Recreational open spaces shall provide a combination of plant materials such as lawn, shrubs, groundcovers and shade trees and "hardscape" materials such as paved play surfaces, patios, decks, and shade structures.
- Family-oriented developments are encouraged to provide active recreational areas such as play lawns, paved play courts and swimming pools.

Tot Lots

- Landscaping around tot lots shall provide shade in the form of trees or overhead structures, lawn play areas and patio seating areas.
- Landscaping around tot lots shall include safety and security features such as fencing and barrier planting that protects and buffers the area from traffic and noise. Landscaping shall preserve views to the play areas.

Private Open Space

- Private open space shall provide a relatively smooth, non-skid hard surface suitable for outdoor living amenities such as tables and chairs.
- Ground floor private open spaces shall be planted with shrubs, vines or other suitable plant materials to soften paving, walls and fences.
- Where appropriate, trees, trellises or other plants and structures shall be provided to shade private open spaces.

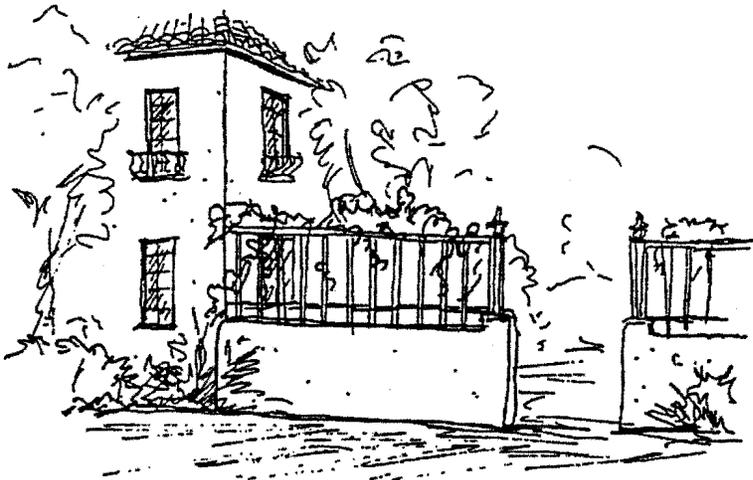


E. SCREENING, LIGHTING AND SECURITY

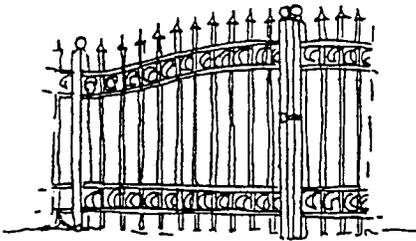
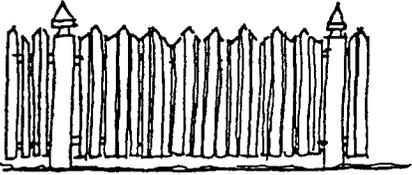
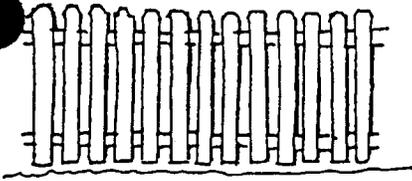
1. Screening

Walls and Fences

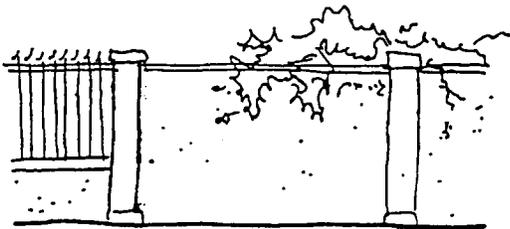
- The design, placement and orientation of buildings, yards, fencing, landscaping, floor plans, balconies and windows is expected to promote privacy with the structures to the extent feasible.
- Walls, fences and landscaping shall be used to define development entries and boundaries, provide security, privacy, noise attenuation and screen unsightly views of parking, storage and equipment areas.
- Low walls or fences located at the edge of sidewalks are encouraged to define residential front yards.
- Wall and fence materials, style and color shall complement the development architecture. Decorative iron fencing, wood picket, brick and stone are the most appropriate for California Bungalow and Victorian design themes. Decorative stucco and wrought iron are most appropriate for Spanish Colonial Revival design themes.
- Trees, shrubs and vines shall be used to soften the appearance of fences and walls. Plants can be used to deter graffiti and barrier plantings can be used for passive security.
- The use of dense, landscape hedges and vine-covered fencing is encouraged as an efficient way to provide more landscaping within the limited available land area of the community.



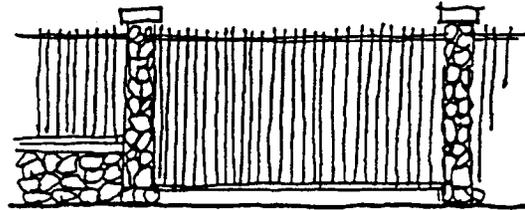
Stucco and decorative metal fencing create privacy while allowing views to the street for security.



- Rear yards may utilize wood or decorative metal fencing or masonry walls. Chain link fencing is acceptable provided it is not visible from the public right-of-way. The utilitarian appearance of chain link and stockade fencing should be softened with hedges or climbing vines.
- Where visible from the public right-of-way, the top edge of fencing shall be treated as a design element. Suggested edge treatments include:
 - shaped boards, horizontal board caps or trellises for wood fences
 - stucco finish and rounded or decorative caps for masonry walls
 - decorative finials or metal framing integral to the design of metal fences
- Stone or decorative masonry walls are preferred for areas exposed to noise, such as streets or incompatible adjacent uses.
- Long expanses of blank wall surfaces shall be avoided. Pilasters, offsets, combinations of solid and open decorative metal, vines and other landscape treatment will create visual interest and reduce graffiti vandalism.
- Wrought iron or combinations of wrought iron and masonry are encouraged for use where views are desired. View fencing is encouraged for "eyes on the street" passive security.
- Where walls are used at property frontages, in parking areas and to conceal storage and equipment areas, they shall be designed to blend with the site's architecture. Both sides of walls shall be architecturally treated.
- Security fencing may be solid masonry walls or decorative metal fencing. Thorny, barrier plants may be used as a decorative and security measure. Barbed or razor wire is unacceptable.



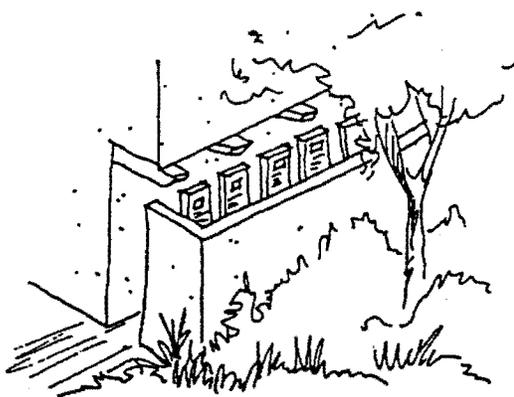
Stucco Wall



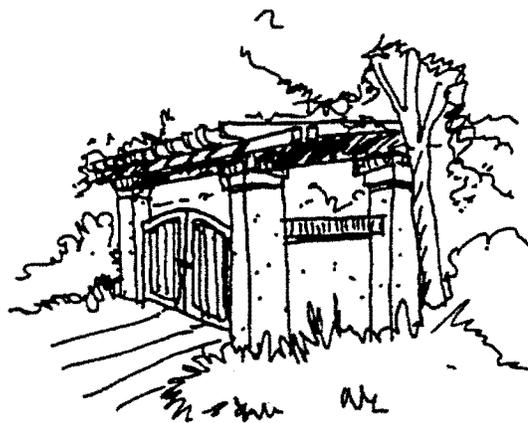
Stone Wall with Metal fencing

Loading and Storage Areas

- Outdoor equipment and storage areas shall be located away from public views where possible. Such utilitarian areas shall be screened with materials compatible with the development architecture.
- Loading and service areas shall be located and designed to minimize visibility, circulation conflicts and potential adverse noise impacts.
- Loading areas shall be screened with portions of the building, architectural wing walls, freestanding walls and landscaping. Ensure adequate noise attenuation for adjacent incompatible land uses.
- Trash and recycling storage must be fully enclosed and designed to be adequate in size. Roof, trellis or similar structures shall be provided if enclosures are visible from upper building levels. Enclosures shall be designed to complement the site architecture and to be unobtrusive.
- Trash and recycling storage shall be located away from public view and where noise and odors will not intrude on neighbors. Storage areas shall be easily accessed by loading vehicles.



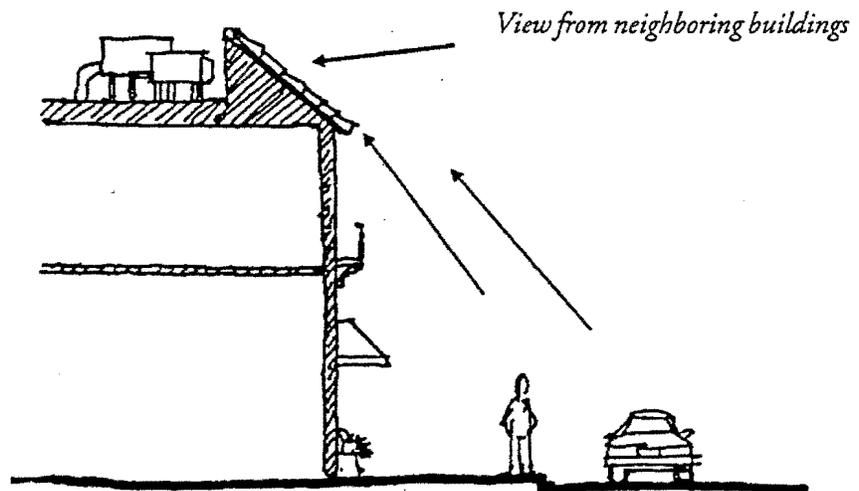
Multiple family residential utility meters are clustered and screened with wall and landscaping.



Trash enclosure is completely screened with walls, gates, overhead trellis and landscaping.

Mechanical and Utility Equipment

- All mechanical and utility equipment shall be placed in locations out of public view or where they can be screened.
- All screening devices shall be compatible with the architecture, material and color of the adjacent structures.
- Roof mounted equipment shall be incorporated into the design of the roof. Full parapets, sloping roofs and low parapets with supplemental screens may be used provided that the screen walls are fully integrated with the building architecture. Fence type screening is unacceptable.
- Mechanical equipment is expected to be below the roof line. Equipment visible from the upper floors of adjacent buildings or other elevated locations shall be grouped, colored to match the roof surface, and screened to the extent possible.
- Satellite dishes shall be located to avoid visual intrusion on adjacent uses and the neighborhood.
- Solar panels shall be integrated into the roof design and located to be as visually unobtrusive as possible. Panels on sloped roofs shall be parallel and resting on the roof slope. Panel frames shall match the roof color.
- The design and location of gutters and down spouts shall be compatible with the building architecture.
- Roof vents, flashing and sheet metal shall be located and colored to be as visually unobtrusive as possible.
- Gas and electric meters, backflow prevention devices and similar utilitarian elements shall be screened from view.



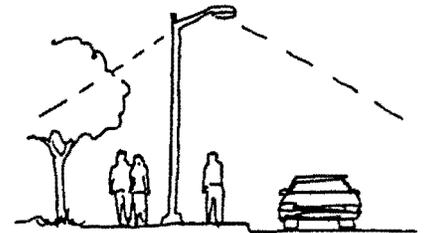
Rooftop equipment shall be screened from public view and from neighboring buildings.

Noise Attenuation

- Loading, mechanical equipment and similar noise-generating uses shall be located away from adjacent residential or other sensitive use areas.
- Structures and noise attenuation walls shall be located to enclose or buffer noise generation areas.
- Living spaces, and specifically, bedrooms within buildings shall be located to buffer exterior noise sources.
- Buildings shall utilize state-of-the-art noise attenuating materials such as insulation and double paned windows where exterior noise is excessive and unavoidable.
- Masonry walls are recommended for yard enclosures where noise attenuation is required.

2. Lighting and Security

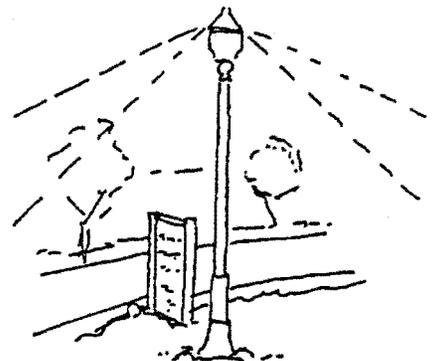
- Adequate lighting shall be used to provide illumination for the security and safety of on-site areas including entries, parking, loading, shipping and receiving, pedestrian areas and work areas.
- Building illumination and architectural lighting shall be indirect and concealed from view. Decorative building and landscape lighting shall be subtle.
- All exterior lighting shall be selective and shielded to confine light within the site and prevent glare onto adjacent properties or streets.
- Light shall be directed to use areas to avoid waste, over lighting and glare.
- Lights shall be located away from signs, trees and other elements that would block illumination.
- Lighting shall be compatible with the development and its architectural style.
- Decorative lighting shall be used to enhance festive night life in appropriate commercial areas..
- Alley, garage or other utility area security lighting should be motion activated or on timers and not overly bright.
- Porches and primary residence entries should be lighted to promote neighborhood security.
- Decorative building and landscape lighting shall be subtle and may be used with discretion to accentuate design features.



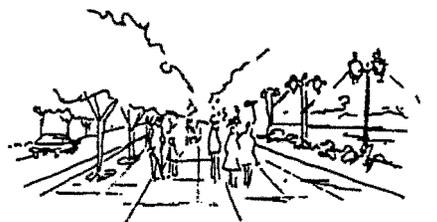
Safety



Security

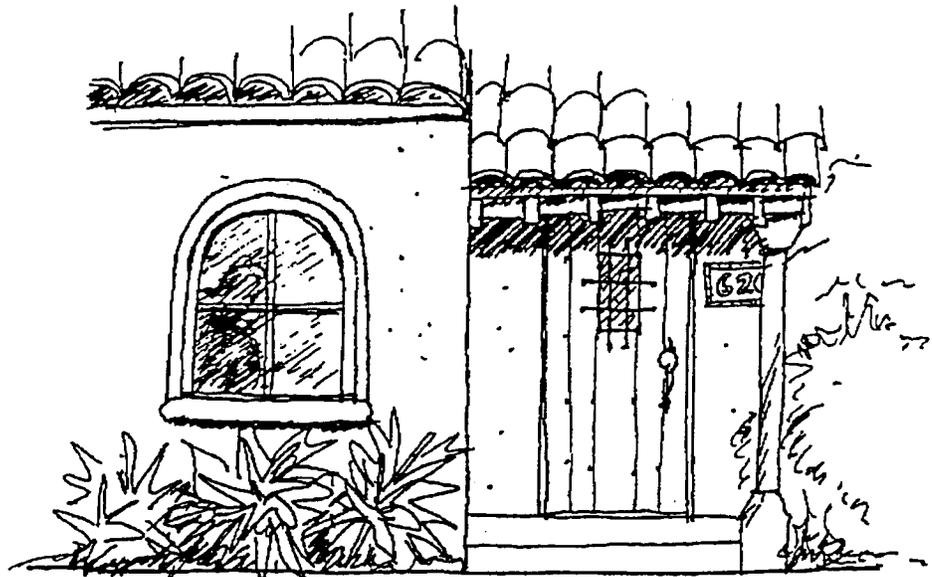


Orientation



Aesthetics

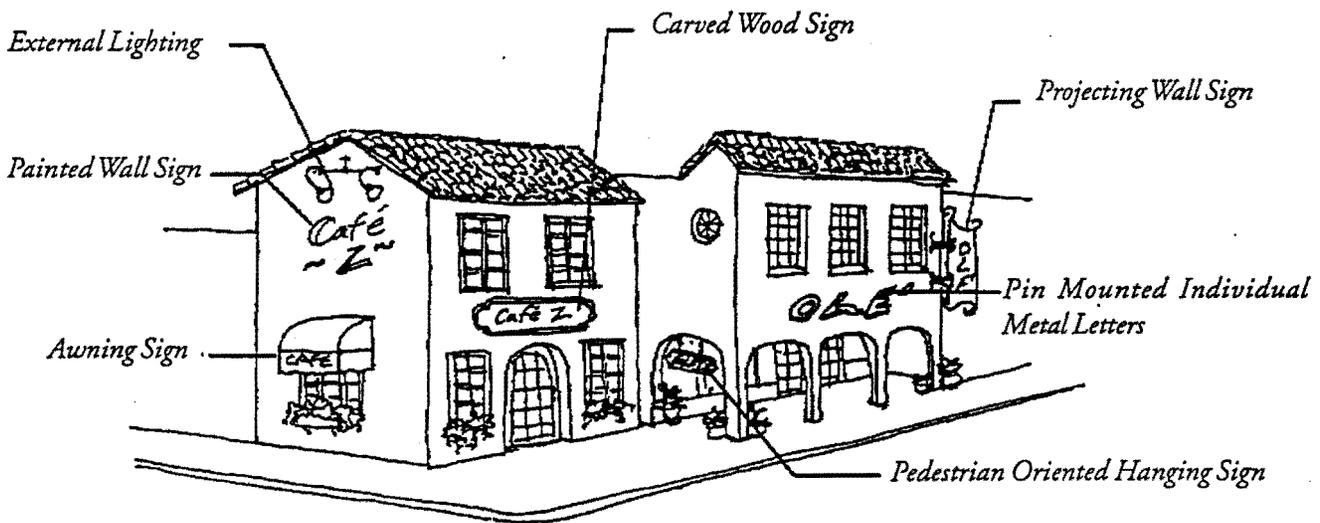
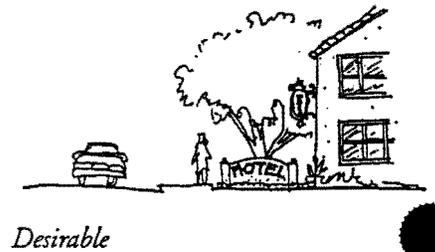
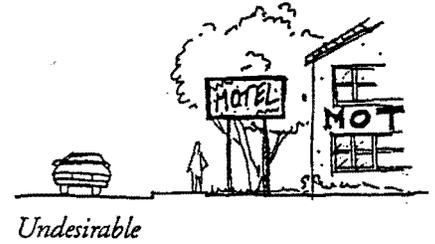
- Ground level and other landscape lighting can be used to illuminate walkways and steps promoting safety and security.
- Addresses shall be clearly visible and readily identifiable in order to facilitate emergency response.
- Security is enhanced when neighborhood residents are present and able to recognize one another. Developments are encouraged to provide areas such as front porches in proximity to the street, neighborhood mail stations and tot lots to encourage residents to interact with one another.
- Security is enhanced through visibility. Clear views shall be maintained along pedestrian paths between structures and parking areas. Site design shall minimize blind corners and creation of potential hiding places.
- Use of metal window and door security grilles should be minimized. If grilles are used, they must be decorative and in a style and scale that is compatible with the building architecture. Grilles must have interior fire safety release features. Night security grilles shall be designed to be completely retractable during daylight hours.
- Passive security, such as dense growing or thorny plant barriers and noisy, crunchy gravel used under windows as a passive alarm should be used to deter entry into private spaces.



Passive security includes street facing front porch and windows, barrier planting under window, decorative grille over front door window and clear address numbers.

F. Signage

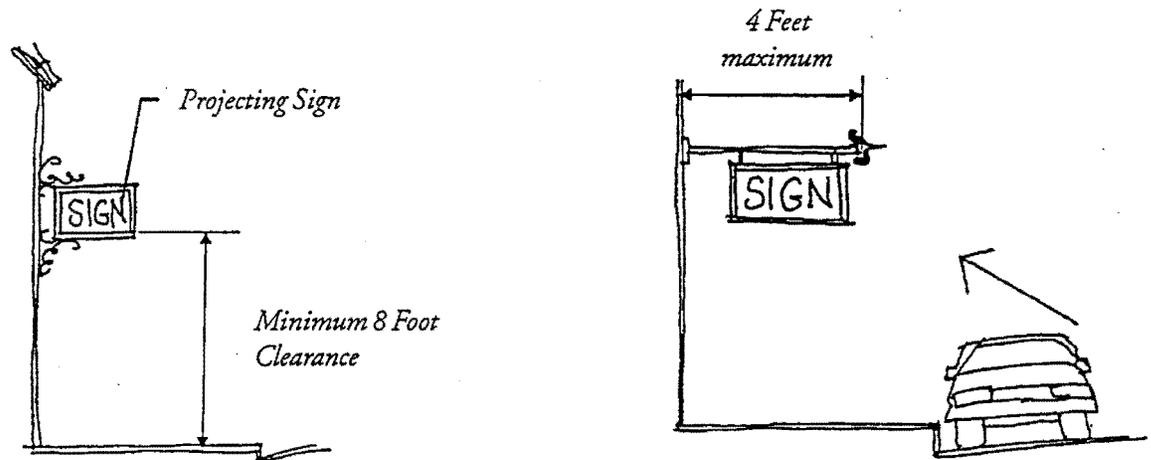
- Project signs shall be compatible with the surrounding community, street, block and adjacent structures. Signage plans shall strive to create harmony and avoid ostentation, competition and obstruction with other businesses.
- Every structure or commercial complex shall incorporate sign design into the overall project design plan. Sign design, materials and illumination shall complement the building scale, proportions, architectural style, materials and colors.
- Where more than one sign is allowed, all signs shall be designed using the same construction methods and materials, colors, lettering and logo styles.
- The location and size of signs shall clearly relate to the architecture of the particular structure. Signs shall not cover windows, roof shapes or dominate trim. Roof and wall signs that extend above the roof line are prohibited.
- Wall and fascia signs shall be located to indicate building entries without dominating the facade.
- Sign colors and materials shall be selected to contribute to legibility and design quality. High contrast colors promote legibility and the use of too many colors distracts from legibility.
- Low profile monument signs are preferred. Freestanding pole and pylon signs are prohibited. Freestanding signs shall be placed so as not to obstruct vision or create a traffic hazard.



Signs integrated with Architecture



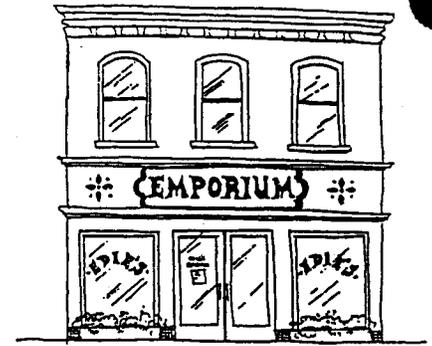
- A single freestanding sign may incorporate the addresses and business names for multiple tenants of a building or complex. All tenant signs shall be of a uniform size, color and style when combined in a single sign format.
- Signs should be located directly above the business entrance or to the sides of the entrance at eye level, so that they are visible below the foliage of street trees for the amenity and convenience of the pedestrian.
- The size of signs shall be appropriate to viewing distance. For example, pedestrian oriented signs shall be smaller than vehicle-oriented signs.
- Projecting signs are allowed on storefronts up to a height of 14 feet, with maximum projection of 4 feet from the face of the building. Clearance of at least 8 feet must be maintained to the sidewalk. The overall size of the sign shall not exceed 10 square feet.



Projecting signs shall consider safety, encroachment into the public right-of-way and appropriate size.

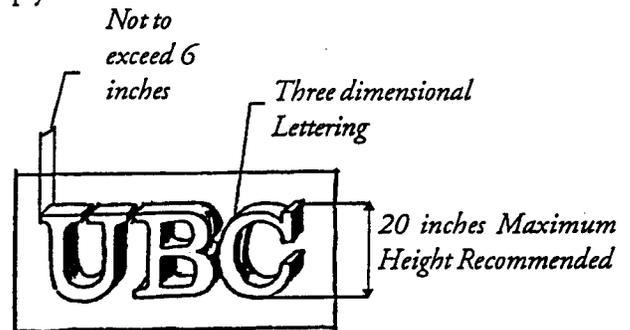
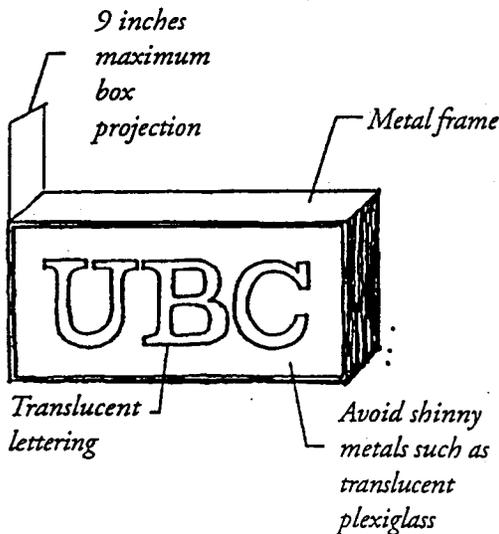
- The maximum number of permanent signs per business shall be two different types of signs, such as a wall sign, a projecting sign or a permanent banner.
- The total area of all signs on a building should be limited to a size equal to one square foot per frontage foot of building face, with a minimum allowance of 25 square feet regardless of front footage. When the ground floor of a building is occupied by more than one tenant, each shall be entitled to a pro-rata share footage based on the frontage used by that tenant with each tenant being entitled to a minimum of 25 square feet regardless of each tenant's front footage.

- Additional or larger signs may be allowed for large scale projects provided that the function and design is justified through a comprehensive sign plan for the project. Justification for additional signs shall be based on functional needs, such as multiple entrances or uses within a development. Larger signs shall be justified by specific site conditions or appropriate proportional scale to the overall building.
- Internally illuminated plastic signs are discouraged. Internally illuminated signs shall be limited to four square feet (per side for projecting signs). Individually illuminated sign letters within an allowable wall sign area may be used, provided the entire sign and background are not internally illuminated.
- Additional signs are allowed for businesses that provide pedestrian access to sales and service areas through the rear of buildings. For such businesses, rear access signage shall be equal to allowable signage for front facades.
- Window graphics shall be applied so that they do not obscure visibility into a shop. No more than 25% of an area of any one window shall be used for a sign. Such signage shall be designed to complement the other signs and the design of the storefront.
- Window graphics are recommended to be painted or applied vinyl letters on glass or a clear acrylic panel behind the windows. These types of window graphics are permanent signs.
- Banners and cloth signs shall be used primarily for special, festive locations. If such signs are used on a permanent basis, a condition of their initial approval is agreement to replace them with they begin to show wear. A banner shall be considered a permanent sign if it displays content or identification of any kind which relates to the business or service establishment of the building to which the banner is attached.
- Standardized corporate signs are permitted if they comply with these guidelines.



Sign scale, style and placement are appropriate to the architecture.

Sign area is the maximum allowed per frontage.



6 feet minimum from street level

G. CONSERVATION DISTRICTS

The City of Santa Cruz Historic Building Survey identifies historic structures within the Beach and SOLA Plan areas and the Historic Preservation Ordinance regulates alterations to such structures. Because historic architectural styles help establish neighborhood design themes in the plan areas, Conservation Districts have been established to enhance the setting of historic structures and to preserve neighborhood character. The City is pursuing establishing Mills Act Funding allowances for the potential historic district in Beach Hill.

1. Vernacular Structure Remodeling

The following guidelines address new development and remodeling of "Vernacular" style structures. "Vernacular" in this document refers to buildings that exhibit historic or traditional architectural styles but are not listed in the City Historic Building Survey. Additionally, these guidelines shall be used for development that occurs in proximity to historic structures.

Goal: Retain neighborhood character.

Objectives:

- Strengthen the residential quality and character of neighborhoods by improving the appearance of architecture and landscaping.
- Protect and enhance the unique historic and architectural qualities which are present by establishing Historic Conservation Districts.

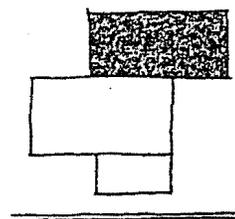
Design Guidelines:

- Additions or alterations shall not change the street view of the building's distinguishing architectural features or overall character.
- Building additions shall be located to the rear of existing structures whenever possible and/or desirable.



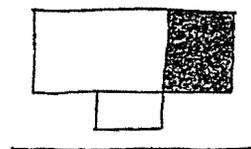
DESIRABLE

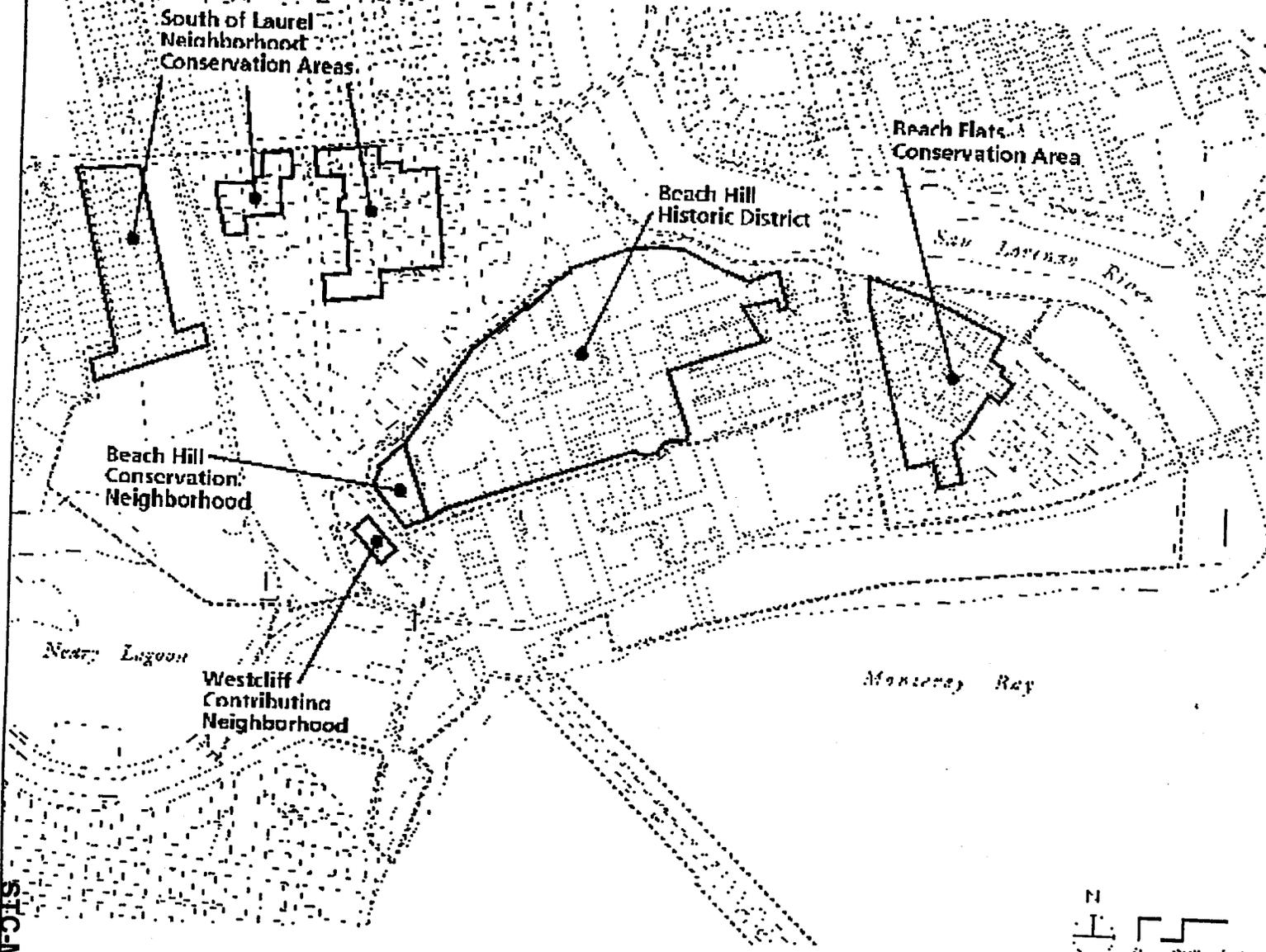
Roofline, windows and architectural details are integrated into the design of the existing building.



UNDESIRABLE

Flat roof, vertical window and lack of detail contrast with the existing building.





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The Phipps Group

City of Santa Cruz Beach / South of Laurel Area Plan

Conservation Areas

Figure 3



UNDESIRABLE

Stucco siding, enclosed porch, tile roof, horizontal aluminum windows, solid, plain front door and location of stair are inappropriate.



DESIRABLE

Second story addition replicates building and roof form, wood siding and casement windows. Stairs are located to the rear of the house and the original porch and front door are retained.

- Where additions are located to the side of an existing building, the addition shall be set back behind the line of the front facade. If the addition is larger than the original structure it may be appropriate to provide visual separation such as a breezeway or hall between the existing and new buildings.
- Additions to existing buildings shall employ the same scale, proportions, openings (porches, entries, windows, etc.) and materials as the original. It is not necessary to make the addition look identical, especially if original workmanship or details cannot be matched.
- Original architectural elements such as porches, steps, railings, and decorative detail shall not be removed. Necessary replacements shall be similar in design and materials to the original
- Maintain the proportions of existing door and window openings and the pattern of existing window sash in replacements or additions.
- Original siding material shall not be replaced, covered over or clad with another material such as stucco, wood or composition shingles, aluminum siding and the like.
- Where inappropriate or later materials have been removed, they shall be replaced with the original material.
- Replacement roofing shall maintain the original form and utilize original or similar materials.
- Integrally colored materials such as brick, stone and stained wood shall not be painted over. Removal of inappropriate paint shall follow approved preservation methods to avoid damage to original materials.
- Replacement or repaired chimneys shall utilize original materials such as stone or brick.
- Utilize original materials wherever possible and select new materials that replicate the original as close as possible.
- Wood window sash is preferred, however, vinyl clad wood, baked enamel aluminum frame or other windows may be acceptable provided the original design is duplicated.
- Paint colors and color schemes shall be appropriate to the style and design of the building.
- Elements of Victorian and Spanish Colonial Revival architectural styles are evident in the Vernacular structures and shall be retained or referenced in remodeling. Exemplary styles include the following:

Eastlake (1880's) is probably the most common 19th century style in Santa Cruz, found in very simple cottages and large elaborate houses, often in combination with elements of the Stick Style. Principally a style of ornament derived from the English farmhouse designer, Charles Eastlake, and easily worked on machinery that was developed and made widely available in the 1880's. Irregular in plan and outline with gabled roofs, sometimes with flaring eaves and square bays. Frequently having profuse application of ornamental detail including spindles, curved brackets, incised carving, grooved moldings, sunbursts, and flash glass.



Eastlake

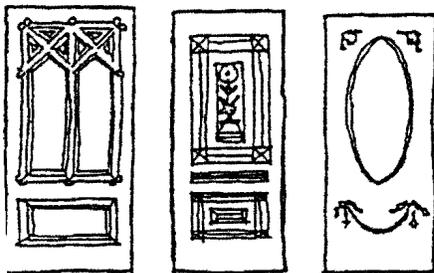
Queen Anne (1885-1900) is less formal than earlier Victorian styles. It sought to be picturesque with an asymmetrical plan, complex roof line, corner towers, and gables. Frequently displaying a variety of textures and colors in bands of different siding materials including brick, clapboard, sand shingles. Having bay windows of various shapes, porches, and balconies and a variety of predominantly classical ornamental details. In Santa Cruz the Queen Anne, Colonial Revival, Shingle, and Craftsman styles all blend together easily.



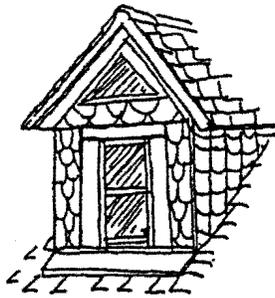
Queen Anne



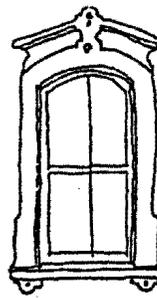
This Vernacular structure incorporates Victorian elements including a bay window, porch and steep pitched roofs. Typical Victorian design elements are:



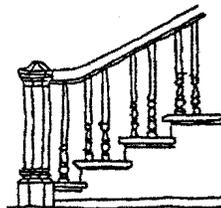
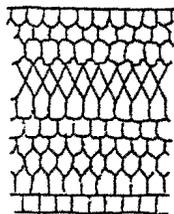
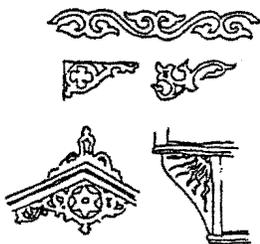
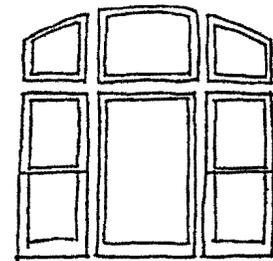
Doors



Gables

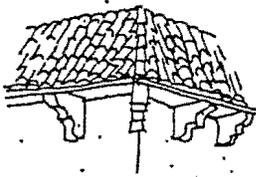


Windows

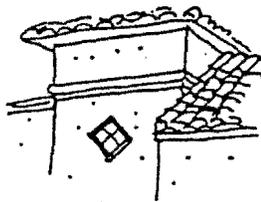


Decorative Wood Details

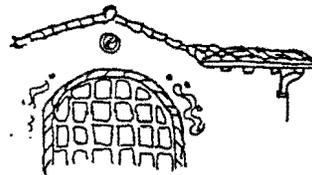
Spanish Colonial Revival (1920's) is characterized by red tile roofs, white or light colored stucco exteriors with occasional tile insets and facade decoration, arched openings and arcades, decorative iron or carved wooden grillwork, and projecting balconies. Plain facades facing the street are sometimes modified by intricate ornamental plaster decoration around doors or windows. Features of plans are the patio or courtyard, balconies, galleries, or loggias and asymmetrically arranged rooms.



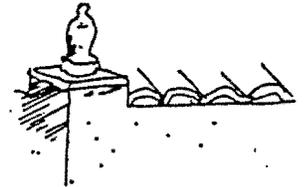
Mansard Tile Roof



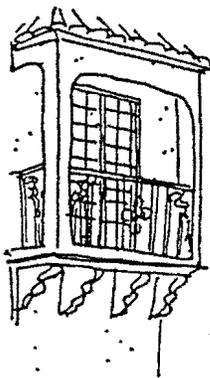
Banding and Tile Detail



Plaster Detail



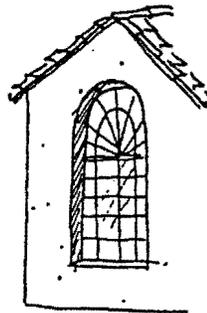
Parapet Detail



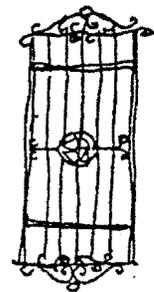
Balcony



Wood Panel Door



Arched Multi-Panel Window



Decorative Metal Window Grille

2. New Development

Goal: To ensure compatibility of new structures with historic structures.

Objectives:

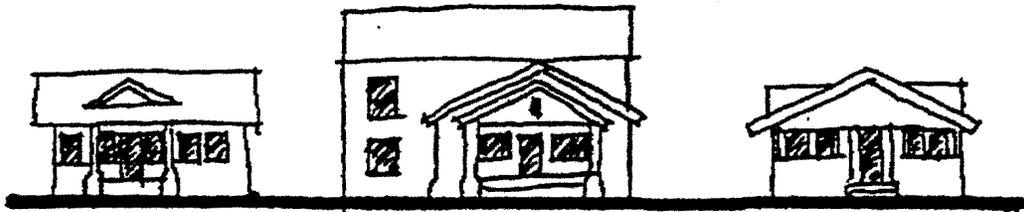
- Maintain existing patterns of building setbacks, spacing and siting.
- Encourage building forms which are similar in scale, form and massing to neighboring historic structures.
- Discourage designs which call undue attention to themselves in favor of those which reflect the architectural qualities that tie the buildings of the community together.
- Encourage the uses of materials commonly used for construction and finishing of historic buildings in the district and colors which complement those materials and styles.

Design Guidelines:

- New buildings shall have similar front, rear and side yard setbacks as adjacent structures. Demonstration of neighborhood compatibility may allow for a variation from city zoning and development standards.
- Model new construction mass, form, balance, proportions, and openings on neighboring historic structures.



Existing

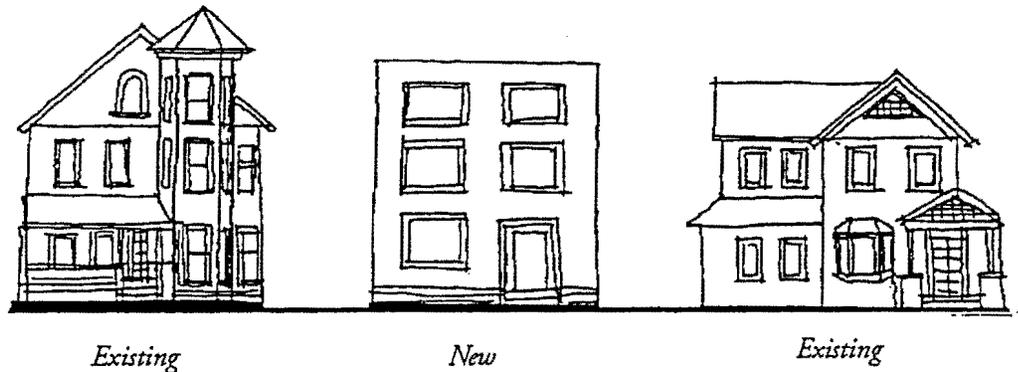


Incompatible Character



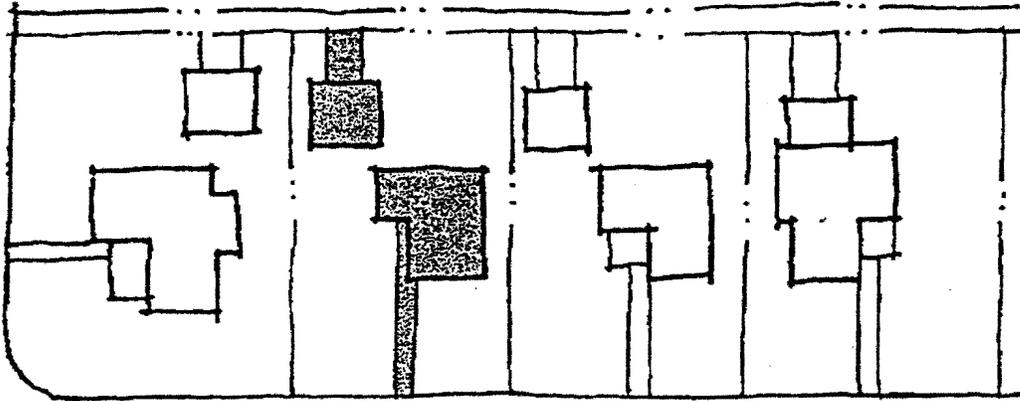
Compatible Character

- Use Smaller building modules to reduce the scale of larger buildings to blend with adjacent smaller scale buildings.
- Use projecting bays, porches, individual balconies, upper floor setbacks, bay windows and other elements to provide variation in the building mass and form.
- Minimize the height and bulk of buildings through the use of setbacks, dormers, gables and the like on upper floors.
- Roof forms and pitches shall be similar and compatible to neighboring historic structures.
- Where a single architectural style is predominant in a given area or along a street, that style shall be used for new construction.
- Avoid the use of historic architectural styles or stylistic elements which are not represented in the community.

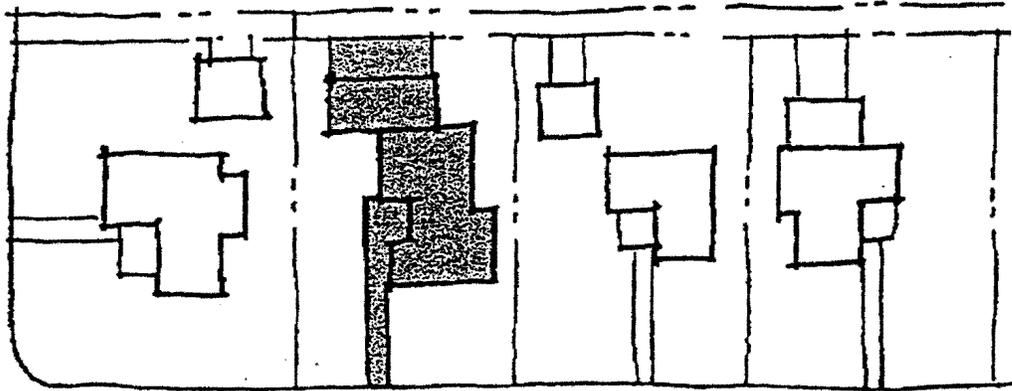


The new building's box form, flat roof, plain facade and large horizontal windows are incompatible with the existing architectural styles.

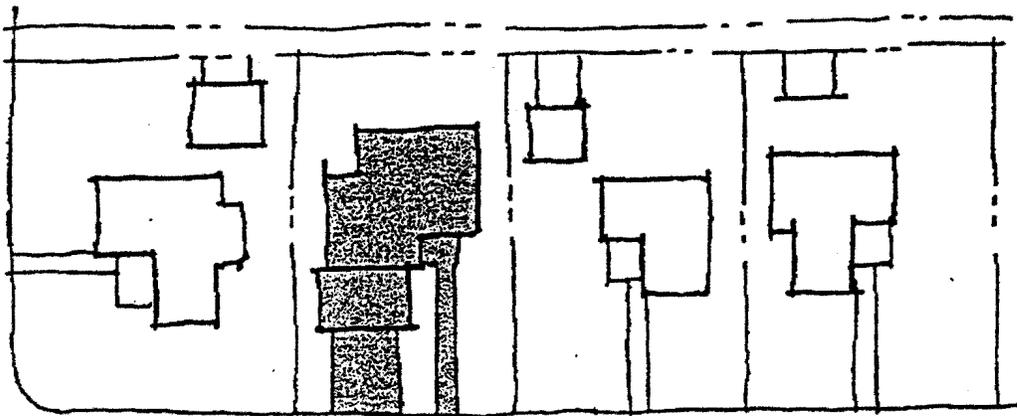
- Encourage alley-accessed, detached garages for residences.



Desirable new infill development respects existing building relationships and setbacks and locates alley accessed, detached garage at the rear of the property.



Acceptable new infill development respects building relationships and setbacks and locates the garage behind the main building.



Undesirable new infill development does not respect existing building relationships and setbacks and the garage dominates the building front.



Historic
landmark



New
Building

The simple design and lower height of the new cupola references, but does not diminish the unique historic landmark.

- Avoid incompatible building scale and architecture that distracts from or diminishes the unique features of landmark buildings.
- Unique features of landmark buildings should not be replicated, but may be used as models for new design. New buildings may “reference” landmark buildings with subtle, appropriate replication of landmark building features such as roof forms, balconies, openings, materials and colors.
- Materials shall be selected that are similar to historic structures. Materials such as plywood, aluminum, panel siding products and composition shingles on exterior walls are inappropriate.
- Traditional window styles such as double-hung sash and casement are preferred.
- Set in or “nail-on” aluminum windows with thin frames set close to the exterior wall surface and reflective metallic window finishes such as silver, polished bronze, or aluminum are not appropriate.
- Colors which are neon bright, shiny, metallic iridescent or otherwise attention grabbing are inappropriate for paint or other finish.

3. Site Improvements

Goal: Retain neighborhood character

Objectives:

- Strengthen the residential quality and character of neighborhoods by improving the appearance of architecture and landscaping.
- Protect and enhance the unique historic and architectural qualities which are present by establishing Historic Conservation Districts
- Maintain the heritage tree preservation program and use the existing heritage trees as a basis for streetscape design.
- Protect historic resources such as rock walls and stairs and use existing resources as a basis for streetscape design.

Design Guidelines:

- Preserve historic landscape elements including walls, fences, light fixtures, trees and landscaping.
- Encourage the use of traditional paving materials such as brick and stone where compatible with the architectural style.
- Modern landscape materials such as artificial turf or outdoor carpeting are not appropriate.



Spanish Colonial Revival site features include stucco walls, arch forms, wood trellises, decorative wood and metal gates and fencing, decorative metal light fixtures, terra cotta tile, brick or stone paving, colorful glazed accent tiles and Mediterranean plants such as palms and bougainvillea.



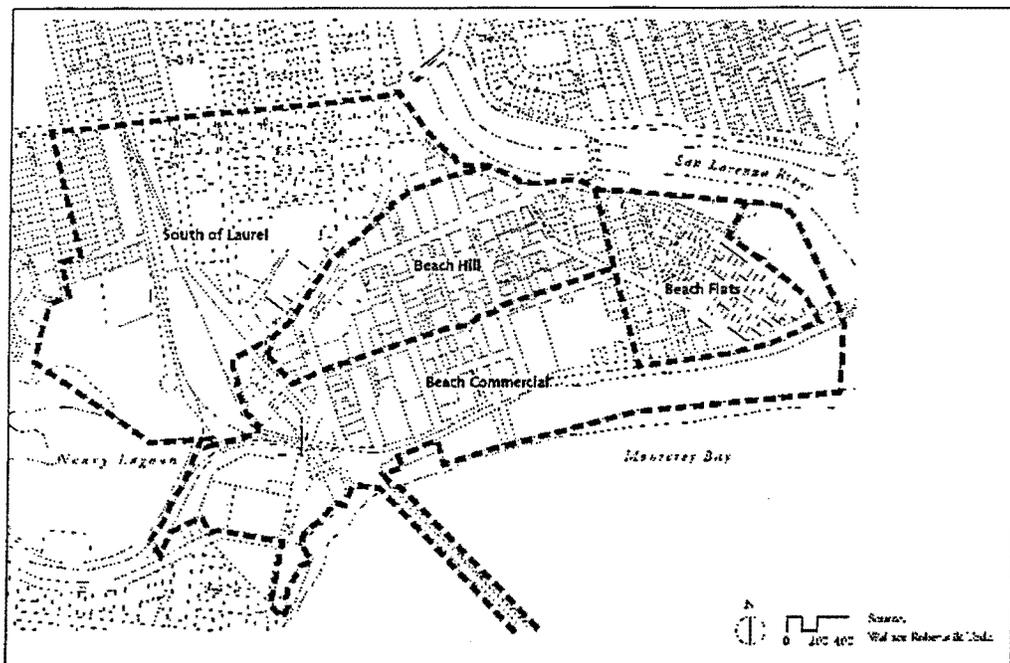
Victorian site features include decorative metal light fixtures and fencing with ornamental detail, brick and stone paving and walls, flowering plants and large scale trees that complement architectural forms.

IV. PLAN AREA DESIGN GUIDELINES

The Beach and SOLA Plan Areas are divided into four Sub-Area Plans: Beach Flats, Beach Hill, Beach Commercial and South of Laurel. Each Sub-Area has distinct land use and community characteristics that can best be addressed by specific guidelines.

The following sections provide descriptions of the natural and the built environment that the Plan proposes to retain, enhance or replace. Each section also includes the Beach and SOLA Plan Goals and Objectives that address design issues. Specific Design Guidelines are provided to direct development to create distinctive Sub-Area communities. The Guidelines reflect the desire to retain and build on the best of existing qualities. Each section also reiterates General Design Guidelines that are most relevant to specific design issues within Sub-Areas.

For those transitional areas between Beach Commercial and Beach Hill, additional design guidelines are being prepared that allow a flexible vernacular style of architecture.



The Philip Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Figure 4

Beach and South of Laurel Planning Subareas

A. BEACH FLATS

1. Community Character

Beach Flats is a distinct land area within the Beach Community. The fairly low, flat topography of the area is bordered by the San Lorenzo River levee on the north and east, the large structures of the amusement park to the south, and Riverside Avenue, the major

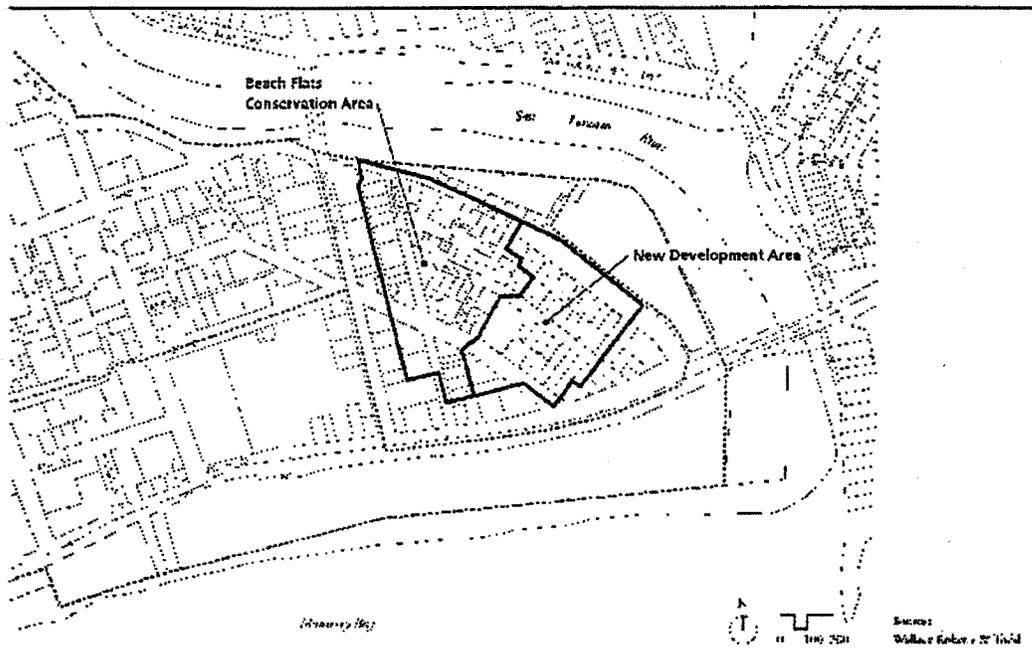


Beach Flats

entry street to the beach, on the west. Diagonal street patterns, open spaces and small scale buildings offer views through the area that contribute to understanding its size and physical boundaries.

The character of the area is established primarily by the development pattern that began when summer tourist tent cities were subdivided for small summer beach cottages and modest hotels and motels in the early part of this century. In the early days, streets were narrow and garages, parking spaces and driveways were not needed. The area gradually changed to year-round residential and the hotels and motels were converted to multiple family residences. As a result, random additions to buildings and automobile accommodations have created an eclectic design mix.

Although the community is densely developed, it has a "human scale" due to the small building scale and small lot size. A large percentage of buildings are single story and only a few buildings exceed two stories. Building coverage leaves very little space between neighboring buildings and building setbacks to the street are shallow. Most buildings have street-facing entries and traditional features such as front porches. This proximity of buildings to the street provides an opportunity for interaction between residents, which promotes neighborliness and security.



The Phelps Group

City of Santa Cruz - Beach & South of Laurel Area Plan

Figure 5

Beach Flats New Development and Conservation Areas

Conservation Area

The lower density residential area occurs primarily north of Kaye Street and is designated by the Beach Area Plan as a "Conservation Area." This designation is intended to maintain the existing character, building scale, and architectural styles, primarily through rehabilitation of existing structures or replacement with similar development.



Eastlake Style Residence

Multiple family residences in the area also contribute to its neighborhood character. Many of the buildings were originally hotels and motels, which were made up of smaller units, similar in scale to the cottages. Modest building scale and composition, combined with similarities in architectural styles and features such as courtyards, make many of these developments compatible with the lower scale residences. Other multiple family developments occupy larger lots and some are three stories high. Generally, the location of these larger scale developments on the periphery of the Conservation Area helps to define the neighborhood and transition between the Conservation Area and surrounding commercial development.

New Development Area

The area south of Kaye Street is designated as a "New Development Area." New multiple family developments are expected to occur in vacant sites and as replacement housing. New multiple family residential buildings may be three stories in height, however, the site and building design must reflect the small scale character of Beach Flats. New developments are expected to utilize design methods that minimize building scale and enhance the image of the existing neighborhood.



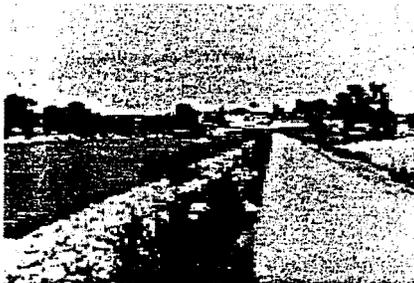
New Development Area

Architectural styles in Beach Flats are eclectic, however, characteristics of two architectural styles are dominant: "Vernacular" (most closely related to California Bungalow and Victorian) and Spanish Colonial Revival. Vernacular characteristics are dominant in the Conservation Area and set the design theme for that area. However, Spanish Colonial Revival and other historic styles also occur in the Conservation Area and may be appropriate depending on the neighborhood context.

In the New Development Area, residential development shall use Spanish Colonial Revival architecture as a basis for design. This style has been selected to enhance the historic character of the area and create a more cohesive community design theme. Fine examples of this style of architecture are evident in the nearby La Bahia Apartments and the Casa Blanca Hotel.

The architectural guidelines section describes how characteristics of historic styles shall be used in Beach Flats. Additional guidelines provided in the General Design Guidelines section for Conservation Districts and the Santa Cruz Historic Building Survey provide useful information on design within the context of the neighborhood.

Open spaces in Beach Flats have both positive and negative influences on the neighborhood character. The surrounding San Lorenzo River is an important regional open space that also defines the boundaries of Beach Flats. With the implementation of the landscape and pedestrian improvements defined by the San Lorenzo River Design Concept Plan, it will be a significant community amenity. Smaller open spaces interspersed through the area provide visual and physical relief from the urban environment and provide recreational opportunities. Parking lots and vacant properties contribute to the overall sense of openness and provide opportunities for views in and around Beach Flats, but their bleak appearance has an overriding negative influence. Parking areas also diminish the perception of the residential neighborhood boundaries by bringing in more traffic and visitors. Street trees and landscaping for beautification and shade are sorely lacking in the area.



Lorenzo River Open Space

Implementation of the San Lorenzo River Design Concept Plan and landscape development standards and design guidelines applied to rehabilitation and new developments will enhance

streetscapes, parking lots and residential open spaces. The Plan also provides for existing city park land to be maintained at the corner of Raymond Street and Liebrandt Avenue and additional park space to be developed along the river and in the interior of the neighborhood.

2. Goal and Objectives

Goal:

- Revitalize Beach Flats and acknowledge the primacy of two issues as fundamental to neighborhood health: (1) safe and secure streets and (2) vested, resident inhabitants.

Objectives:

- Achieve a successful dynamic between diverse, quality residential neighborhoods serving all income groups at urban densities and pedestrian scale.
- Encourage the provision of family-oriented development by requiring at least 50% of all new units to be two or more bedrooms.
- Increase percentage of owner-occupied properties which will result in improved property maintenance, increased public and personal safety, increased property values and a healthier neighborhood and living environment.
- Accommodate flexibility in design to allow development of small lots that are predominant in the Beach Flats area, including the elimination of the lot size requirement of P.D. developments.
- Create a viable, more diverse residential neighborhood with appropriate levels of connected amenities and clear boundaries that are separate from intensive commercial activities.
- Identify the area north of Kaye Street toward Riverside Avenue as a "Conservation Area" in which rehabilitation, interior and exterior maintenance, and increased home ownership will be encouraged.
- Maintain the existing RTD zone base density of 27 units/acre in the Conservation Area.
- Identify the area south of Kaye Street as a "New Development Area" in which new construction and higher density of 36 units/acre is encouraged.
- Strengthen the boundaries of the neighborhood through new multiple family development along the perimeter of Third Street which will provide significant new housing opportunity sites around the perimeter of the Beach Flats.
- Establish a strong sense of place, with edges which have a beginning and an end, and with gateways and intimate residential streets clearly defined.

- Limit access and egress from the residential neighborhood to discourage tourist commercial traffic.
- Enhance the entries to the neighborhood at gateway intersections (Riverside/Liebrandt and Liebrandt/Beach) to strengthen the residential quality of the neighborhood and improve the appearance of the area.

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3. Design Guidelines

Conservation Area



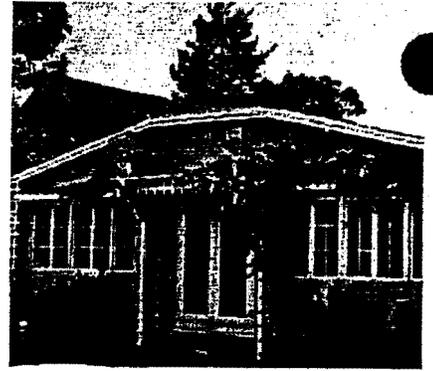
Vernacular Style Residence



Vernacular Style Residence

- Development shall adhere to the guidelines listed in the General Design Guidelines and the following guidelines.
- Driveways may be narrower than city standards in order to maximize land use efficiency.
- Locate detached garages at the rear of buildings wherever possible.
- Front and side yard fencing shall be decorative wood picket fencing in traditional designs compatible with neighborhood character.
- Security is enhanced when residents are present and able to recognize one another. Design features such as front porches in proximity to the street encourage residents to interact with one another.
- Use of metal window and door security grilles should be minimized. If grilles are used, they must be decorative and in a style and scale that is compatible with the building architecture.
- Passive security, such as dense growing or thorny plant barriers and noisy, crunchy gravel used under windows as a passive alarm should be used to deter entry into private spaces.
- The dominant Vernacular architectural styles related to California Bungalow and Victorian shall be used to establish the Beach Flats design theme. Architecture shall adhere to the following design guidelines:
 - Buildings shall be similar in scale and form to existing structures and shall incorporate Vernacular characteristics, such as pitched gabled roofs, proportionally large overhangs, exposed roof beams and rafter tails, vertically oriented multi-paned windows, and front porches
 - Buildings shall be wood frame construction with horizontal wood siding.
 - Roof forms shall be typical of Beach Flats with steeper pitches more similar to Victorian than California Bungalow style.
 - Roof materials shall be composition or wood shingle.

- Buildings shall be painted white, off-white, or pastel colors. Contrasting colors shall be used to accentuate door and window frames, porch railings and other distinctive features. Roof colors shall be subtle and compatible with the selected building colors.
- Where multiple color combinations are used, they shall be subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features and heighten interest.
- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.
- Colorful flowering annual and perennial plants are encouraged to enhance the character of the neighborhood and to promote individual residence identity
- Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas.
- The use of dense, landscape hedges and vine-covered fencing is encouraged as an efficient way to provide more landscaping within the limited available land area of the neighborhood.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.



Vernacular Style Residence

modified in STC-MAJ-1-01 (Part A)

New Development Areas

- Development shall adhere to the guidelines listed in the General Design Guidelines and the following guidelines.
- The siting and design of structures and landscaping, particularly when adjacent structures are single story, shall ensure that the development blends into rather than dominating the neighborhood.
- Three story buildings are encouraged in the perimeter of the Area. Multi-story buildings shall step down to transition to the smaller scale Conservation Area.
- Multiple story developments shall minimize scale through upper story setbacks, modular building units and other similar design techniques.
- Multiple family units shall be clustered to increase open space. Large developments shall be made up of groups of structures.
- Courtyard style developments are encouraged for usable open space. Open spaces shall be consolidated to create usable open space.



Spanish Colonial Revival Style Multiple Family Residence

Confirmed
in
STC-MAJ-1-01
(Part A)

- Parking shall be located within or at the rear of structures and parking lots shall not be visible from the public right-of-way.
- Large parking lots are not allowed. Parking shall be incorporated within residential structures and small parking courts wherever possible.
- Parking courts shall be treated as landscape plazas with attention to paving surfaces, shade trees, landscape planters and pedestrian walkways.
- "Spanish Resort" architectural styles shall be used to establish the design theme for the New Development Area of Beach. While Spanish Colonial Revival is considered dominant, the architectural styles of the Beach Area includes a mix of characteristics of Mission Revival and Mediterranean architecture resulting in a general "Spanish Resort" style. Architecture shall adhere to the following design guidelines:
 - Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, decorative iron and tile details or other features typical of the Spanish Colonial Revival style.
 - Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.



Spanish Colonial Revival Style Multiple Family Residence

- Building walls shall be stucco and colored white, off-white or very light to medium value, warm-toned hues.

- Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.
- Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet.
- Parapets shall incorporate curvilinear decorative shapes and molding.
- Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance. Wall surfaces have decorative texture or ornament concentrated around doors and windows.
- Tile roof colors shall be natural clay terra cotta tones.
- Stained wood is preferred, however, trim may also be painted a darker value color than the main building color.
- Wood sash or casement windows shall be used to maintain compatible architectural character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established architectural style.
- Landscaping shall be designed to enhance the development architectural style. Bougainvillea, palms and other Mediterranean and tropical plants are associated with the "Spanish Resort" style found in Beach Commercial.
- Permanent containers for flowering plants are encouraged for use in limited space areas, at entries and in courtyards and plazas. Containers may also serve as seating.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.
- Wall and fence materials, style and color shall complement the development architecture. Decorative stucco and wrought iron are most appropriate for Spanish Colonial Revival design themes.
- View fencing is encouraged for "eyes on the street" passive security.
- Lighting type shall be suitable to the project architectural style, such as wrought iron for the Spanish Colonial Revival style.

*Corrected
in STC-MAJ-1-01
(Part A)*

B. BEACH HILL

1. Community Character

Beach Hill is a distinct promontory within the Beach Area. Steep cliffs define the west and northern edges along Pacific Street and San Lorenzo River. Mature trees and Victorian mansions on the hilltop are visual landmarks from northern portions of the city. Some of



Beach Hill

STC-MAJ-1-01 (B)

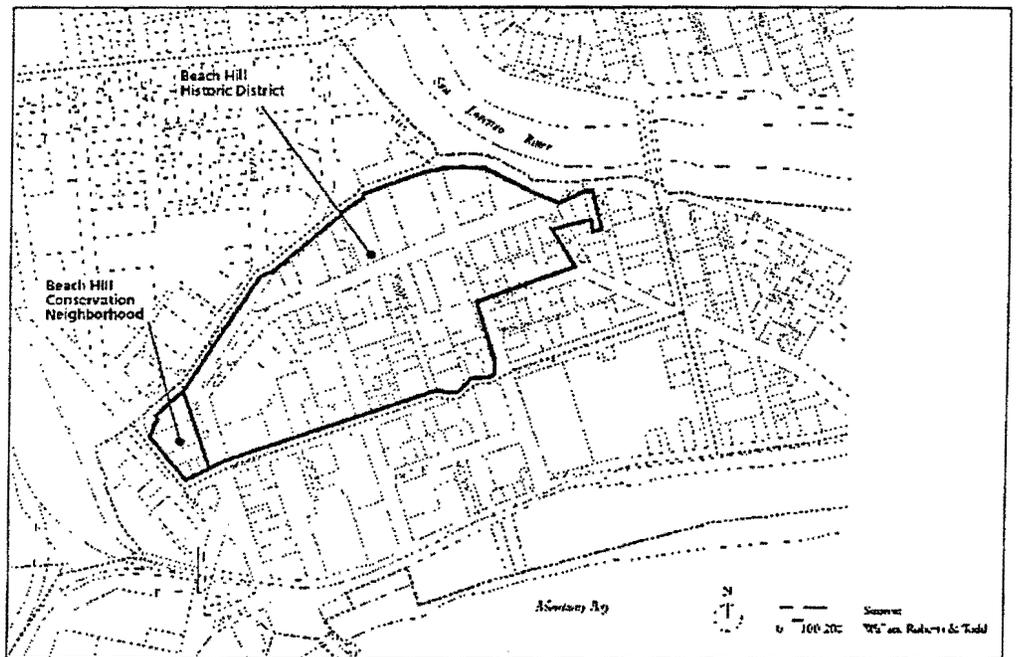
Exhibit 5

pg. 72 of 100

the most attractive and picturesque views into the Beach Area are from the entry bridges towards Beach Hill with the river in the foreground. On the south side of Beach Hill, the topography slopes more gradually towards the ocean and the physical boundaries are less distinct.

Like other neighborhoods in the Beach Area, the overall character is "dense" due to small lot sizes and building separations and fairly large scale buildings in relation to lot sizes. The mix of single and multiple family residences and hotels and motels also contributes to the dense character.

The most notable characteristic of Beach Hill are the large scale Victorian residences. Many of these are large mansions that give an overriding elegance to the eclectic neighborhood mix. The City of Santa Cruz Historic Building Survey identifies numerous historic structures within Beach Hill and the Historic Preservation Ordinance regulates alterations to such structures. The quality of the neighborhood has warranted its designation as an Historic Preservation District. Until the Historic Preservation District is formed, the design guidelines for Conservation Districts shall be applied to enhance the setting of historic structures and to preserve neighborhood character.



De Filippo Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Figure 6

Proposed Beach Hill Historic District

Beach Hill is almost entirely developed, so new developments will most likely be remodels or replacement of existing structures. Renovations to historic buildings shall follow established guidelines for historic preservation. In the Conservation Area (future Historic District), when non-historic buildings are remodeled or replaced, the Victorian architectural theme is required. Developments on the perimeter of the Conservation Area are recommended to use Victorian, Spanish Colonial Revival or other traditional architectural themes and may be required to be a particular style to ensure compatibility with nearby buildings that



Victorian Style

exhibit distinct historical styles. "Victorian" is a term that encompasses a number of architectural styles that were popular from the 1860's to the early 1900s. Descriptions of these styles and how their characteristics shall be used are provided in the architectural guidelines section. Additional guidelines are provided in the General Design Guidelines section and the Santa Cruz Historic Building Survey provides useful information on design within the historic context of the neighborhood.

Another notable characteristic of Beach Hill are hotels and motels. These uses are located primarily on the perimeters of the neighborhood, including the west side of Riverside Avenue and the east side of Front Street. Additionally several smaller hotels are located along the bluff off of Third Street. Most of the hotels and motels were built in the post war years and do not exhibit distinctive architectural styles. The parking lots, signs and limited landscaping associated with these uses contrast with the residential character of Beach Hill. In order to improve the design cohesiveness of Beach Hill, new and remodeled hotel and motel developments in the Conservation Areas are required to adhere to the Victorian design theme. Developments outside the Conservation Area are required to contribute to the overall design improvements for the area. Victorian, Spanish Colonial Revival and other traditional architectural themes are recommended and may be required depending on the project's proximity to nearby buildings that exhibit distinct architectural styles. Additional design guidelines for commercial and hotel and motel developments on Riverside Avenue are included in the Beach Commercial section.

Public open spaces within Beach Hill are limited to streetscapes, the Cliff and Front Street stairways and the Carmelita Cottages garden. Heritage trees, such as the deodar cedar at the Carmelita Cottages, make a significant contribution to the historic character of the neighborhood. Preservation of historic landscapes is an important goal for Beach Hill and guidelines are provided in the General Design Guidelines Historic section. The stairways also have historic importance and provide functional pedestrian access. The maintenance and improvement of the stairways and all pedestrian spaces in Beach Hill will contribute to area identity and enhancement. There is an opportunity for streetscape improvements that



Appropriate new development incorporates Victorian Architecture in contrast to the less compatible 1950's horizontal form and flat roof



include a street tree plan and implementation program, street lights, and neighborhood signage.

2. Goal and Objectives

Goal:

- Preserve the historic quality of the Beach Hill subdistrict, enhance its historic residential quality and maintain its prominence within the built environment.

Objectives:

- Protect and enhance the unique historic and architectural qualities which are present by establishing an Historic District.
- Enhance the entries to the neighborhood at definable intersections to strengthen the residential quality of the neighborhood and improve the appearance of the area.
- Enhance pedestrian and bicyclist safety and comfort with improved streetscapes including bike paths, sidewalks, street trees, landscaping, and other amenities.
- Allow development at lower ranges of General Plan density designations.
- Improve the pedestrian connection to Downtown by upgrading and maintaining the two existing pedestrian stairways.
- Implement traffic calming measures where appropriate to reduce traffic speed and visitor entry into the neighborhood.
- Promote rehabilitation of existing historic structures by pursuing Mills Act funding options for residents.

3. Design Guidelines

- Development shall adhere to the guidelines listed in the General Design Guidelines and the following guidelines.
- The siting and design of structures and landscaping shall ensure that the development blends into rather than dominating the neighborhood and shall not detract from landmark buildings.
- Victorian historic architectural styles shall be used to establish the Beach Hill design theme. Building design shall be derived from one of the following styles and shall adhere to the traditional elements inherent in that style. "Victorian" includes the following as described in the Santa Cruz Historic Building Survey:



Rock - Eastlake Style

Eastlake (1880's) is probably the most common 19th century style in Santa Cruz, found in very simple cottages and large elaborate houses, often in combination with elements of the Stick Style. Principally a style of ornament derived from the English farmhouse designer, Charles Eastlake, and easily worked on machinery that was developed and made widely available in the 1880's. Irregular in plan and outline with gabled roofs, sometimes with flaring eaves and square bays. Frequently having profuse application of ornamental detail including spindles, curved brackets, incised carving, grooved moldings, sunbursts, and flash glass.

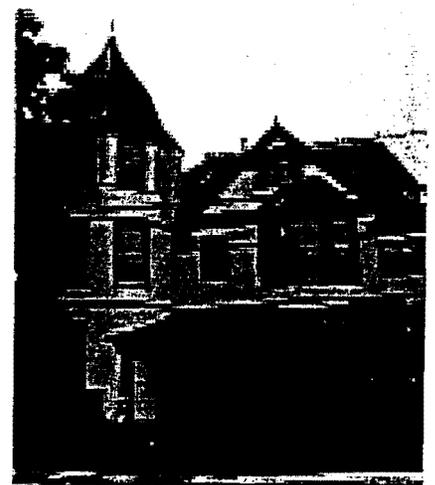
Stick-Eastlake (1880's) is rarely found in its pure form in Santa Cruz, but is frequently used in combination with Eastlake. In addition to the Eastlake ornament there is an overlay of plain or molded stick work that resembles half timbering and suggest the balloon frame construction of the time, often by extending window frames beyond the windows. Curved brackets are usually replaced by diagonal brackets.

Queen Anne (1885-1900) is less formal than earlier Victorian styles. It sought to be picturesque with an asymmetrical plan, complex roof line, corner towers, and gables. Frequently displaying a variety of textures and colors in bands of different siding materials including brick, clapboard, and shingles. Having bay windows of various shapes, porches, and balconies and a variety of predominantly classical ornamental details. In Santa Cruz the Queen Anne, Colonial Revival, Shingle, and Craftsman styles all blend together easily.

- Buildings shall be wood frame with traditional wood siding such as shiplap or shingle. Foundations and first floors may be brick or stone if appropriate to the architectural style.
- Hipped, gabled and other roof forms indicative of Victorian architecture are preferred.
- Roof materials shall be appropriate to the architectural style. Composition or wood shingle roofs are most appropriate for Victorian styles.
- Community compatibility can be achieved through replicating the strong volume and mass exhibited in historic structures. For example, bay windows and porches can be used to provide the sculptural, three dimensional qualities of Victorian architecture.
- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.
- Recommended historic Victorian color schemes relate to three style transitions that occurred from the 1870's to the turn of the century:
- Pre-1884 buildings were painted pale colors, usually tans or white with a deeper toned trim of the same color. Window sashes were black or white and wood roof shingles were stained green, red or black.



Queen Anne Style



Chateau Eastlake



Exotic Victorian Landscape

- From 1884 to 1895 darker colors were used and there was a trend to use two exterior materials, such as shingles on the second story and wood siding on the first story. Each material and its trim was painted a different color, resulting in four colors. Second stories were usually lighter colors. Trim sashes were usually black, deep red and sometimes white. Wood shingle roofs were stained green, red or black.
- About 1885 colors were affected by the classical influence. Main body colors were lighter colors such as grays, yellows and tans. The trim was often white and the window sashes were generally black. Wood shingle roofs were stained green, red or black.
- Plant materials shall be selected for compatibility with the neighborhood. For example, the popularity of exotic flowering plants and specimen trees in Victorian gardens are evident in the Beach Hill neighborhood.
- Light fixtures shall be consistent in design and style of the architecture.
- Wall and fence materials, style and color shall complement the development architecture. Decorative iron fencing, wood picket, brick and stone are the most appropriate for Victorian design themes
- Where visible from the public right-of-way, the top edge of fencing shall be treated as a design element. Suggested fencing treatments for the Victorian design theme include:
 - Shaped boards, horizontal board caps or trellises for wood fences.
 - Decorative finials or metal framing integral to the design of metal fences.
 - Utilize traditional paving materials such as brick and stone.

C. BEACH COMMERCIAL

1. Community Character

Beach Commercial is primarily the waterfront area south of Second Street between Pacific and Riverside Avenues and Bay Street. Commercial uses on Riverside Avenue and the east end of Beach Street are also included as part of this community. The topography terraces down towards the ocean from high points on the west at Bay Street and West Cliff Drive and the north at Second Street between Front and Main Streets.

Regional open spaces within the Beach Commercial area are of primary importance, since this is a recreational use area. The Boardwalk, Wharf, Beach Promenade and Beach Street are open spaces that create the opportunity for specialized, pedestrian-oriented commercial uses. Less attractive open spaces are the large parking lots, which dominate the center of the plan area. The lack of landscaping in the lots creates a barren, bleak appearance. Typically, existing motels dating from the 1940's and 50's are also designed with unattractive parking lots located between the buildings and the street. Improved landscaping, signage,



lighting and attractive building facades will enhance pedestrian activities and create a more vital commercial community.

The character of Beach Commercial is largely formed through the contrasts between various development types and open spaces. The most expansive development in this community is the Santa Cruz Beach Boardwalk. The amusement park buildings and rides are the largest scale structures in the area and they occupy most of the waterfront. Historically, this large scale was matched by the Casa del Rey Resort Hotel located on the north side of Beach Street and the former grand Sea Beach Hotel. Remnants of the past seaside resort grandeur are still evident in the core of the area, between Second Street and the amusement park, where historic Spanish Colonial Revival style buildings closely mingled with simple block-style commercial buildings and motels. Other remnants of the past grandeur are large scale Victorian style seaside residences and resorts still evident in the surrounding West Cliff and Beach Hill areas.

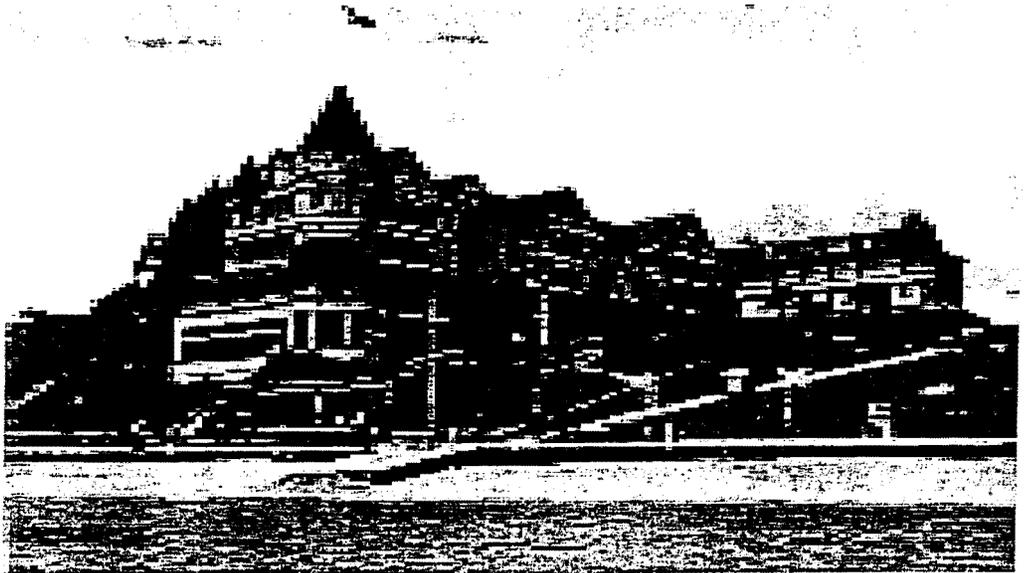
Development opportunities in the Beach Commercial area include new hotel and conference facilities and improved visitor serving commercial development. These contemporary opportunities shall emulate the past era when Santa Cruz was a prominent seaside resort. Hotels, such as the Sea Beach, incorporated significant scale with unique architecture that complemented the Victorian villas of Beach Hill. New development west and north of the wharf towards Beach Hill may utilize Victorian architecture, as was exemplified by the Sea Beach Hotel. New development in proximity to the La Bahia Apartments and Casa Blanca Hotel shall be derived from Spanish Colonial Revival style architecture. New developments are encouraged to recreate the historic scale, architectural quality and distinction of the

Beach Area at the turn of the century. The architectural guidelines section that follows describes how characteristics of these styles shall be used in the Beach Commercial Area.



Casa Del Rey Resort Hotel

Additional guidelines provided in the General Design Guidelines section on Conservation Districts and the Santa Cruz Historic Building Survey provide useful information on design within the historic context of the area. These design guidelines are intended to be used in conjunction with specific Development Standards for the Beach Commercial area. In addition, all projects within the Beach Commercial area will be subject to specific review through the Design Permit process, except the existing Boardwalk facilities on the south side of Beach Street.

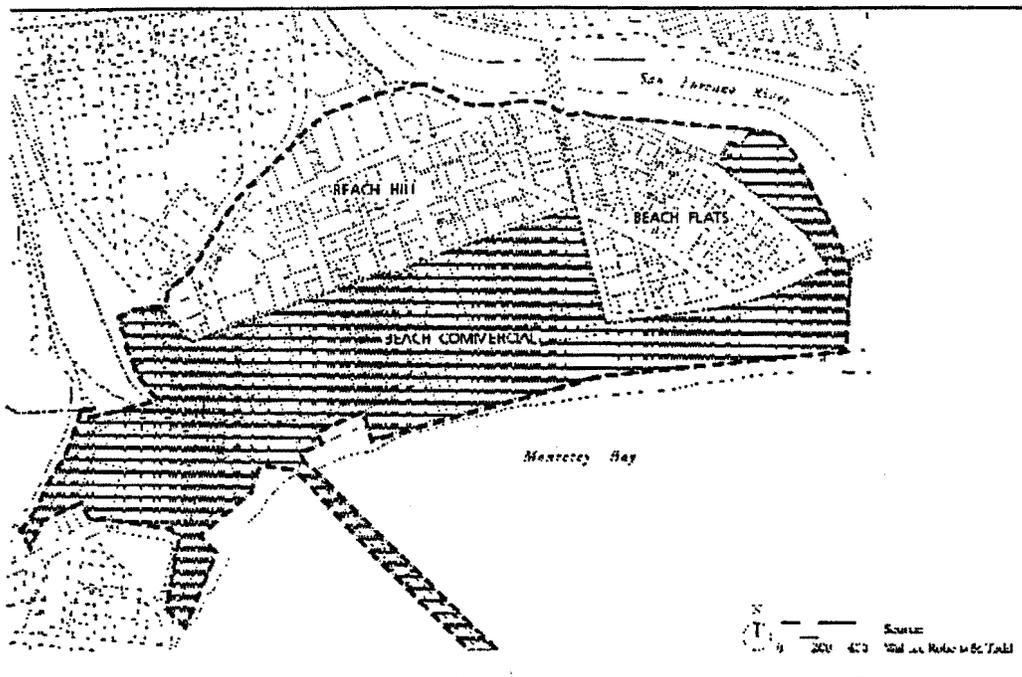


Sea Beach Hotel

2. Goal and Objectives

Goal:

- Recapture the former Ocean Resort grandeur, character and scale through intensification of tourist oriented development that promotes year round activities in the Beach area.



The Philips Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Figure 7

Beach Commercial Area

Objectives:

- Recapture the Ocean Resort through intensification of uses.
- Support maintenance and improvements to the amusement park and boardwalk.
- Encourage improvements to lodging facilities including development of a quality, full service, hotel conference facility and redevelopment of older facilities.
- Develop zoning and development regulations which ensures design flexibility and encourages recreation of the Ocean Resort style.
- Encourage Bed and Breakfast lodging throughout the area.
- Add and improve parking facilities.
- Enhance the wharf with landscaping, signage and other design improvements.
- Preserve the character of the area as demonstrated by historic buildings.
- Establish a strong sense of place, with edges in the Beach Commercial area which have a beginning and an end, and with gateways and intimate residential streets clearly defined.
- Develop physical design and transportation linkages with Downtown.

- Enhance the entries to the area at definable intersections to strengthen and improve the appearance of the area.
- Locate parking and utilitarian areas within or behind main structures and not on major streets.
- Enhance pedestrian and bicyclist safety and comfort with improved streetscapes including bike paths, sidewalks, street trees, landscaping, and other amenities.

3. Design Guidelines

- Development shall adhere to the guidelines listed in the General Design Guidelines section and the following guidelines.
- Development shall utilize the natural topography to create stepped and terraced multi-level buildings and open spaces and optimize view potential.
- Multi-level development is encouraged to enhance the grand resort character of the area. No new buildings shall be less than two stories.
- Building articulation shall be used to create interest and reduce the visual impact of large buildings. This can be achieved through varying heights and setbacks within the same building, offsetting wall planes and adding architectural interest with roof overhangs, awnings, trellises, windows, moldings and other elements.
- Roof design shall consider the natural topography and potential views of roofs from higher elevations as well as characteristic area architecture. Care shall be taken to ensure sensitive placement, pitch, style, materials and colors of roof designs. Views of flat roofs and roof top mechanical equipment shall be screened.
- Flat roof forms that create a monotonous skyline are not allowed. Gable roof forms are encouraged to create an attractive skyline. Towers, turrets, spires, cupolas and other architectural forms are encouraged to create interest in the skyline.
- Whenever possible, new structures shall be clustered to create plazas and pedestrian spaces. Such spaces shall provide amenities such as shade, benches, and fountains. When multiple-structure projects cannot be clustered, they shall be visually linked with arcades, trellises or similar structures.
- Balconies, terraces, courtyards and similar outdoor spaces shall be provided along building street facades to take advantage of views, create street vitality and enhance the resort atmosphere of the area.
- Standardized "corporate" architectural styles associated with chain-type commercial and hotel and motel development shall be modified to be compatible with the Plan Area design themes.

- Corporate color striping, wall logos, murals or other paint and color combinations used to transform buildings or building elements into a sign or advertising are not permitted.
- The number of vehicular site access points on Beach Street shall be limited. Site access and internal circulation shall promote safety, efficiency and convenience. Avoid conflicts between vehicles and pedestrians and provide adequate areas for maneuvering, stacking, truck staging and loading and accommodating emergency vehicles on side streets and alleys.
- Spanish Colonial Revival architecture shall be used to establish the Beach Commercial design character in proximity to the La Bahia Apartments and Casa Blanca Hotel. While Spanish Colonial Revival is considered dominant, the architectural styles of the Beach Area includes a mix of characteristics of Mission Revival and Mediterranean architecture resulting in a general "Spanish Resort" style. Architecture shall adhere to the following design guidelines for Spanish Colonial Revival:
 - Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, decorative iron and tile details or other features typical of the Spanish Colonial Revival style.
 - Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.
 - Building walls shall be stucco and colored white, off-white or very light value, warm-toned hues.
 - Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.
 - Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet.
 - Parapets shall incorporate curvilinear decorative shapes and molding.
 - Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance. Wall surfaces have decorative texture or ornament concentrated around doors and windows.
 - Tile roof colors shall be natural clay terra cotta tones.
 - Stained wood is preferred, however, trim may also be painted a darker value color than the main building color.
 - Wood sash or casement windows shall be used to maintain neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.



The Casa Del Rey is an excellent example of an elegant Spanish Colonial Revival Style building designed to fit the site topography.

- Victorian historic architectural styles are encouraged to recreate the seaside resort theme west of the Municipal Wharf and north towards Beach Hill. Building design shall be derived from one of the following styles and shall adhere to the traditional elements inherent in that style. "Victorian" includes the following as described in the Santa Cruz Historic Building Survey:

Eastlake (1880's) is probably the most common 19th century style in Santa Cruz, found in very simple cottages and large elaborate houses, often in combination with elements of the Stick Style. Principally a style of ornament derived from the English farmhouse designer, Charles Eastlake, and easily worked on machinery that was developed and made widely available in the 1880's. Irregular in plan and outline with gabled roofs, sometimes with flaring eaves and square bays. Frequently having profuse application of ornamental detail including spindles, curved brackets, incised carving, grooved moldings, sunbursts, and flash glass.

Stick-Eastlake (1880's) is rarely found in its pure form in Santa Cruz, but is frequently used in combination with Eastlake. In addition to the Eastlake ornament there is an overlying of plain or molded stick work that resembles half timbering and suggest the balloon frame construction of the time, often by extending window frames beyond the windows. Curved brackets are usually replaced by diagonal brackets.

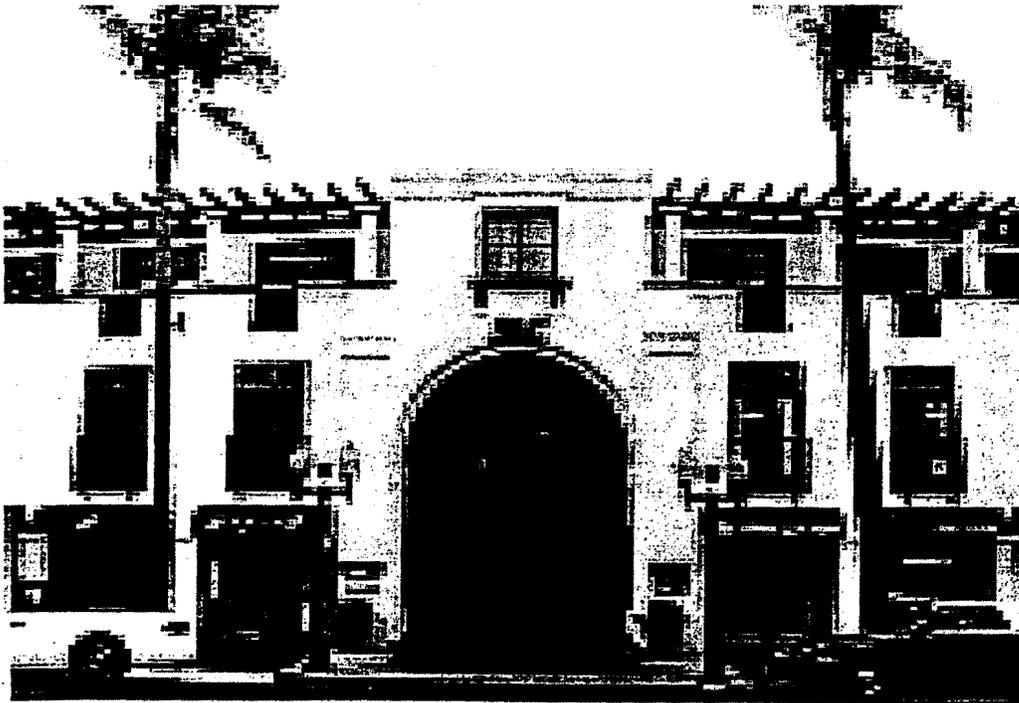
Queen Anne (1885-1900) is less formal than earlier Victorian styles. It sought to be picturesque with an asymmetrical plan, complex roof line, corner towers, and gables. Frequently displaying a variety of textures and colors in bands of different siding materials including brick, clapboard, sand shingles. Having bay windows of various shapes, porches, and balconies and a variety of predominantly classical ornamental details. In Santa Cruz the Queen Anne, Colonial Revival, Shingle, and Craftsman styles all blend together easily.

- Buildings may be metal or wood frame with traditional wood siding such as shiplap or shingle. Foundations and first floors may be brick or stone if appropriate to the selected architectural style.
- Complex roof forms including hipped and gabled forms indicative of Victorian architecture are required.
- Roof materials shall be appropriate to the architectural style. Composition or wood shingle and slate are most appropriate for Victorian styles. Fire code-approved synthetic or treated roof materials shall replicate traditional materials.
- Compatibility can be achieved through replicating the strong volume and mass exhibited in historic structures. For example, bay windows and porches can be used to provide the sculptural, three dimensional qualities of Victorian architecture.

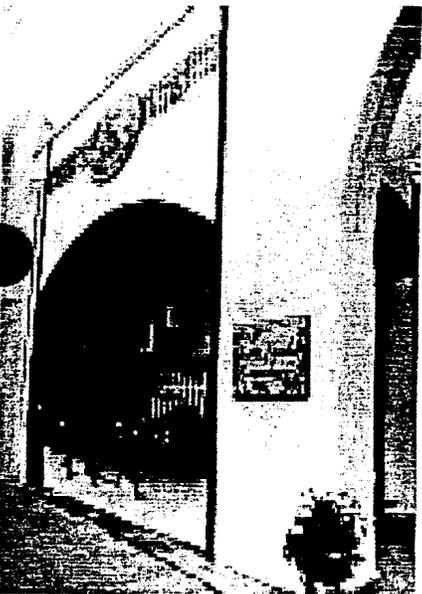
- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.
- Historic Victorian color schemes shall be used, however, strong, multi-colored, "painted ladies" styles that use more than four colors would be overpowering in large scale developments. Large scale development shall use light to medium value colors with a limited palette to minimize scale and mass.

Commercial

- Store fronts shall be located along streets and sidewalks. Parking lots and garages shall be located within or at the rear of structures or off site.
- Commercial development shall be designed to create plazas and pedestrian spaces featuring amenities such as shade, benches, outdoor dining, fountains, gardens and performance spaces.
- Building facades shall be articulated with wall offsets, recesses, openings, ornamentation, and appropriate colors and materials to add texture and detail to the streetscape.
- Street level commercial spaces shall have 12-foot minimum floor-to-ceiling height to encourage retail activity.



Contemporary mixed use development references Spanish Colonial Revival Architecture.



Street level landscaping in the Commercial District.

- The facades of commercial buildings shall provide transparent glass in the entry street facing wall to promote visibility between the street and building interiors.
- Street fronting windows shall be no more than 30 inches above the sidewalk level.
- Based on the Downtown model, building facades are encouraged to introduce architectural variation at the pedestrian level in order to create a diverse building edge between the public and private realms. Bay windows, entry porticos, marquis, canopies, awnings, transparency, retractable storefronts and similar elements are encouraged.
- Building entry porticos may project into the public right-of-way provided there is adequate area for pedestrian circulation.
- Colorful landscaping shall be provided to frame doorways or accent windows. Landscaping may be sidewalk planters, containers or window boxes.

Hotel and Motel

- New and remodeled developments shall utilize Spanish Colonial Revival or Victorian architecture to recreate the Ocean Resort style.
- Buildings three stories or higher shall step back from adjacent streets after the second story. Standards will be developed for this guideline.
- When located on sloping topography, multiple level buildings shall be stepped or terraced for site compatibility and to provide solar access.
- The design and orientation of buildings and open space shall take advantage of available sunlight and, where possible, be sheltered from the noise of traffic or other incompatible uses with walls or other screening.
- Developments on primary thoroughfares shall orient common use facilities such as the entry lobby, shops and services at street level to contribute to the commercial resort mix of uses.
- Openings, including courtyard entries, gates, doors, windows and balconies shall be located along street facades at closely spaced intervals to avoid expanses of blank walls and to contribute to the pedestrian-oriented commercial resort character of the area.
- Upper level terraces are encouraged for common open space areas and outdoor restaurants.
- Courtyard style developments are preferred. Courtyards shall be used as hotel entries, outdoor café or common open space. Multiple building unit developments shall be clustered to consolidate open space.

- Encourage development that provides a variety of unit layout, orientation and entries.
- Long monotonous access balconies and corridors which provide access to five or more hotel/motel units are not allowed. Access points shall be clustered in small groups.
- Guest rooms shall be located to avoid noise generators such as mechanical equipment, trash enclosures and adjacent incompatible uses. Architectural measures such as masonry sound walls and noise attenuating building materials shall be used where exterior noise is excessive and unavoidable.
- Security is enhanced through visibility. Clear views shall be maintained to parking areas and through pedestrian paths and landscape areas. Windows shall be located so that guests have views into common areas.
- Use of metal window and door security grilles should be minimized. If grilles are used, they must be decorative and in a style and scale that is compatible with the building architecture. Night security grilles shall be designed to be completely retractable during daylight hours.
- Pedestrian walkways shall be provided to link guest units with common open space areas, recreational and support facilities, parking areas and the street. Appropriate paving shall be used where pedestrians are likely to cross landscaped area.
- Recreational open spaces shall be centrally and conveniently located for guests.
- Private open spaces shall be directly accessible from guest rooms and shall be defined by railings, trellises, canopies or other architectural features designed as an integral part of the building.
- Landscaping shall be lush, eclectic, colorful, and exotic to reflect the mild climate and resort character. The use of flowering and scented shrubs and trees, species with sculptural, bold forms and the use of a variety of palm species is encouraged.



Spanish Style Hotel

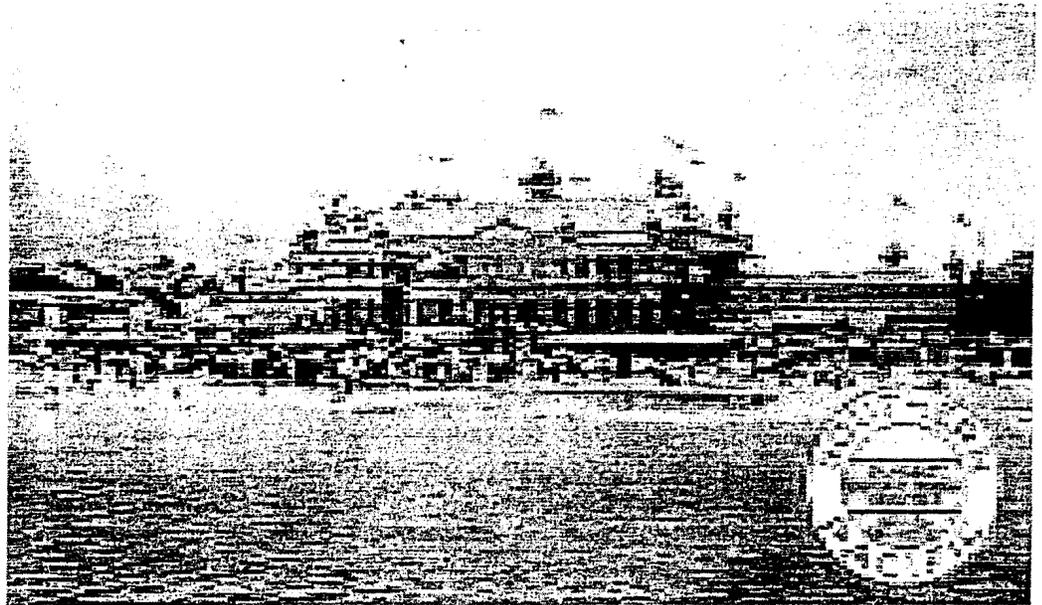


Victorian Style Hotel

Commercial Recreation and Entertainment

- The design and orientation of buildings and open space shall maintain and encourage safe, convenient and aesthetic pedestrian-oriented development.
- New buildings and entertainment facilities, such as amusement park rides, shall be designed to minimize blockage of ocean views.
- A unified character including architecture, colors, materials, lighting, signage and landscaping shall be identified and implemented for all structures and facilities within a development.
- Gateways and entries shall be designed as focal points with exceptional architecture and landscaping.

- "Fantasy Victorian" is encouraged as the preferred architectural style for recreational and entertainment development. This style is eclectic and derives from Victorian fascination and interpretation of the exotic. Moorish architecture themes such as peaked arches and domes were used historically in the Beach Area.
- New entries, signage, lighting, seating and other amenities shall be designed to be consistent with the architectural style.



Parking Facilities

- Parking shall not be the dominant visual element of the site. Parking which is visible from the street or other areas exposed to public view must be screened and softened by landscaping, low screen wall or a combination of elements.
- Parking lots shall be planted with trees to reduce heat and glare and to provide visual relief from broad expanses of paving. Shade tree shall be planted around the perimeter and within the lot. The selected species, size and location of the trees should provide adequate shade coverage within five years of installation. Selected tree species shall be tolerant of drought, salt, reflected heat and glare and minimal areas of exposed soil surface.
- Landscape perimeter planting shall enclose parking areas on all sides exposed to public view. Recommended planter width is 20 feet on major thoroughfares. Planter widths may be narrowed if screening walls are also provided.
- Perimeter planting shall provide dense shrubbery or a combination of shrubs and solid masonry wall adequate to screen the expanse of parking,

car headlights and reflective car surfaces. The screening shall be three to four feet in height. Berming may also be appropriate.

- Parking structure exteriors shall maintain the same high-quality architectural design and construction standards as all other commercial buildings and shall either incorporate uses, such as retail shops, in the building facade or lush landscaping.
- Parking garage entries shall be located on side streets wherever possible. The number and size of garage entries shall be minimized. Such entries shall be integrated with the development architecture and incorporate decorative elements and discreet signage.
- Parking structure architecture shall demonstrate compatibility with Conservation District and Plan Area community character. Compatibility can be demonstrated through architectural style, building scale, mass and form, and building materials and colors.
- The large scale and mass of parking structures shall be alleviated through wall offsets, pilasters, arched openings and other distinctive design elements.
- Cars parked within structures and on open top levels shall be screened as much as possible with architectural elements, trellises and landscaping.
- Decorative elements such as cornices, balustrades, finish materials, colors and lighting shall be used to add interest and integrate the structures within the design character of the area.
- Parking structures shall be densely screened with plants on all sides of the structure exposed to public view except where uses such as retail shops occupy the ground floor. Recommended planter width is 15 feet minimum.



Large shade trees provide shade and screen the parking lot from neighboring multi-story buildings.



Elegantly detailed walls and lush landscaping enclose the parking

D. SOUTH OF LAUREL

1. Community Character

South of Laurel is a fairly flat land area between Downtown Santa Cruz and the Beach Commercial community. The area is bordered by Downtown and Laurel Street on the north, the San Lorenzo River on the east, Beach Hill on the southeast, the Municipal Pier entry area to the south, Neary Lagoon to the southwest and Walii Street and the Eastside hills to the west. The river, lagoon, hills and waterfront are natural topographic features that define the area boundaries. Another significant land feature is the railroad open space that bisects the area.

Development areas are fairly distinct in the community. Traditional design, and small scale residential neighborhoods make up most of the northern part of the community. There are a number of historic homes in those neighborhoods which warrant the formation of neighborhood Conservation Areas. Multiple family residences are located near the railroad open space area, in the area south of Sycamore Street and near major circulation streets. Commercial uses are located primarily along Pacific Avenue, in the northeast area near the river, and sporadically, along Laurel Street. Commercial uses transition from Downtown commercial in the northern areas to visitor commercial in the Beach Area.

Natural open spaces, such as Neary Lagoon and the San Lorenzo River contribute a sense of spaciousness to the area and provide an important community amenity. Access to the open spaces and through the community will be maintained and enhanced with streetscape improvements including pedestrian and bicycle paths, trees, and other amenities. Vacant properties also contribute to spaciousness of the area, however, their bleak appearance is not a positive characteristic of the community. South of Laurel has the most vacant land area and development opportunity sites within the Beach and South of Laurel Plan Area. Multiple family residential, commercial, mixed-use, hotels and motels and neighborhood park uses are proposed developments within the vacant lands.



San Lorenzo River Open Space

Residential Development

Residential development is expected to occur in different areas throughout the planning area, both in areas designated residential, as well as areas mostly designated to be commercial. Depending on the setting, the residential character will vary. The Plan envisions three different residential densities: low medium, medium and high density. The Plan also allows mixed use development including a percentage of development in residential areas dedicated to commercial uses, or work-live arrangements. Residential development may occur in areas expected to develop primarily for commercial purposes. This includes Pacific Avenue, where the plan requires two story development, similar to development regulations in the Downtown Recovery Plan.

New residential development shall exhibit the high quality design and detail of traditional architecture in the community. Developments within or in close proximity to the Conservation Areas shall be compatible and not overshadow historic buildings that create the character of Santa Cruz. The character of new multiple family development shall be derived from the adjacent neighborhoods which preserve primarily Victorian buildings.



Vernacular Architecture

Commercial Development

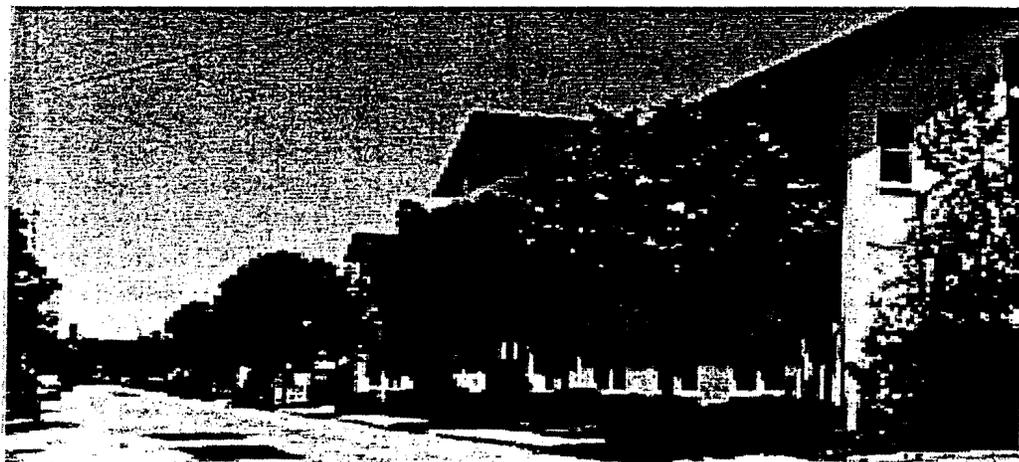
Another emphasis for the SOLA Plan is a connection between Downtown and the Beach. This connection is manifested in the commercial development pattern and physical design of the corridors between the two areas. Because of their particular function, different commercial areas will take on distinct and different characters. Pacific Avenue below Laurel Street is a natural extension of the Downtown restaurant and entertainment district with pedestrian focus and outdoor activity. The visitor serving area is concentrated near the Washington/Pacific intersection. Neighborhood serving commercial uses may occur throughout the area and an existing neighborhood commercial concentration at Laurel/

Chestnut may see some modest expansion. Front Street and adjacent land uses along the San Lorenzo River will be mixed use residential.

Pacific Avenue and adjacent commercial and mixed use developments are planned as one of the important linkages between Downtown and the Beach. Pacific Avenue as it has developed north of Laurel Street is the model. Not only will a similar treatment provide continuity, it will bring with it a similar emphasis on pedestrian amenities.

Land use intensification should produce development ranging from two to three stories in height. Ground floor uses are most appropriately those which cater to pedestrians and provide pedestrian interest and amenities, although complete residential projects would also be appropriate to provide support for downtown businesses. Buildings with strong architectural definition at major street intersections will mark the entrances to the beginning of Pacific Avenue. Minimizing access driveways and locating parking areas to the rear or within buildings will allow placement of buildings close to or at the property line of the street and add to a pedestrian friendly environment. Reduction in parking requirements in the area will allow greater intensification of land uses by not requiring standard on-site parking requirements.

The Pacific Avenue streetscape will continue the strong pedestrian character of Pacific north of Laurel. Similar outdoor eating areas and other outdoor activities can enliven the street. Landscape and pedestrian amenities similar to the Downtown are proposed to extend to the intersection of Front Street. Sidewalk width should be planned to allow the same promenading widths as exist to the north. The streetscape design elements, such as lighting and street trees, should also continue to the Beach.



Community Redevelopment

Hotel and Motel

Visitor serving uses are anticipated in the lower end of the planning area in the vicinity of the intersections of Pacific Avenue, Center and Washington Streets. Several motels are already existing in this area. Support services such as restaurants or entertainment and additional lodging are also appropriate. This area also has served as a support area for marine related uses. Such uses may continue in the future and, consistent with the mixed use theme, residential development may also occur in this area.



Contemporary Motel



Revitalized Vernacular Motel



Development Opportunity Site

Development Opportunity Sites

One vacant land parcel provides an opportunity for larger scale development: the "triangle" bordered by Laurel and Front Streets. In order to maintain community character, these larger scale developments will be designed to be sensitive to the smaller scale of surrounding development. Buildings will be located close to the street with parking at the rear, underground, or screened from the street. Large buildings will avoid a "bare box" appearance through design that reduces scale and mass.

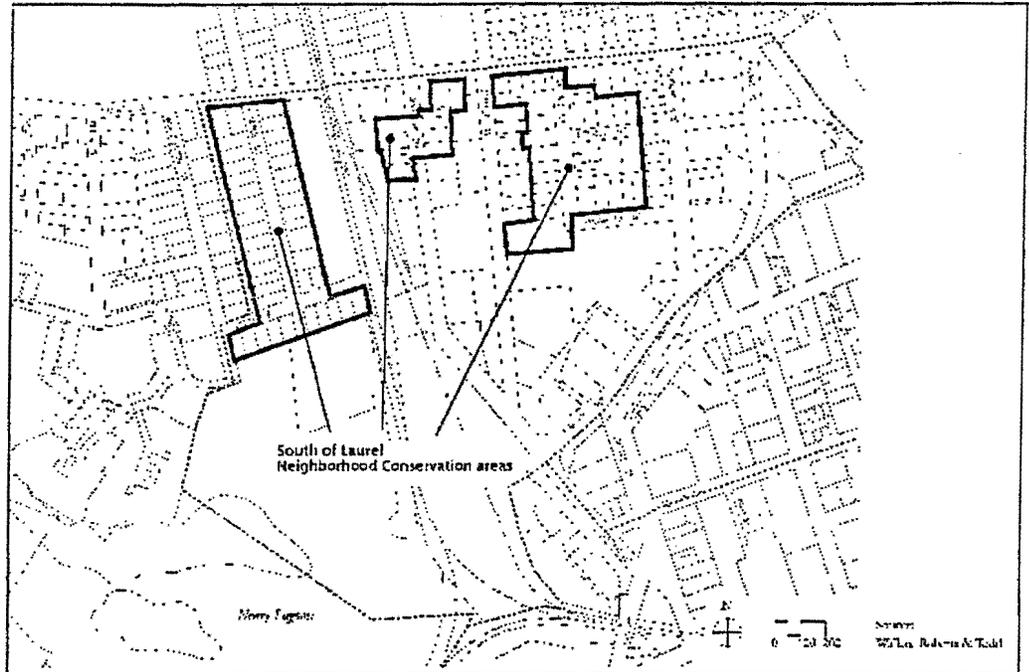
The site at Laurel and Front Streets is encouraged to be a high density urban-style residential/mixed use development. This type of development could incorporate structured or enclosed parking, multi-level residential buildings that optimize views of the river and mountains, and open spaces that connect to the river levee and surrounding streets to encourage pedestrian connections to the Downtown and Beach Area. The site is a focal point within the city and should be designed as a "signature" development that will generate pride within the community.

The Southern Pacific Depot site is uniquely located between Downtown and the Beach Area where it can become a primary destination point. As a focal point of community activity, the site is envisioned as a premium public use facility. The site has the potential to provide community serving uses such as a park, transit station and Beach Area parking facility along with the potential for a museum. A master plan for the site will be created to ensure that the site is planned and designed as another signature development in the community.



Southern Pacific Depot Redevelopment Site





The Pligge Group

City of Santa Cruz - Beach / South of Laurel Area Plan

Figure 8

South of Laurel Conservation Areas

2. Goal and Objectives

Goal:

- Improve the quality of life for the existing residents and businesses in the area through integrating appropriate visitor serving and commercial uses that serve to connect the Downtown and Beach Areas.

Objectives:

- Establish the southern end of Pacific Avenue below Laurel Street as an extension of the Downtown Recovery Plan policies for Pacific Avenue with an emphasis on mixed residential development above ground floor retail and other uses. Create a CBD zone that uses the same village design and use policies as the Cedar Street area of the Downtown Recovery Plan.
- Establish a two story minimum for commercial development along Pacific Avenue.
- Establish the area adjacent to the river as a high density mixed use area that will provide new market rate housing.
- Identify the Spruce/Sycamore, Washington Street and Myrtle Street neighborhoods as "Conservation Areas" in which historic homes will be preserved and new and remodeled homes will adhere to historic compatibility guidelines.
- Encourage mixed-use developments that will serve as a transition between the residential neighborhoods and commercial areas.

- Establish a strong sense of place, with edges which have a beginning and an end, and with gateways and intimate residential streets clearly defined.
- Enhance the entries to the neighborhood at definable intersections to strengthen the residential quality of the neighborhood and improve the appearance of the area.
- Maintain commercial buildings facing and in close proximity to the street and locate parking and utilitarian areas behind main structures.
- Enhance pedestrian and bicyclist safety and comfort with improved streetscapes including bike paths, sidewalks, street trees, landscaping, and other amenities.

3. Design Guidelines

Conservation Areas

- Where new development is located adjacent to Conservation Areas, structures within 30 feet of the Conservation Area shall reflect the same massing and scale as that of the adjacent Conservation Neighborhood.
- Development shall adhere to the guidelines listed in the General Design Guidelines, the Conservation Area Design Guidelines and the following guidelines.
- Locate detached garages at the rear of buildings wherever possible.
- Front and side yard fencing shall be decorative wood picket or metal fencing in traditional designs compatible with neighborhood character.
- Security is enhanced when residents are present and able to recognize one another. Design features such as front porches in proximity to the street encourage residents to interact with one another.
- Use of metal window and door security grilles should be minimized. If grilles are used, they must be decorative and in a style and scale that is compatible with the building architecture.
- Passive security, such as dense growing or thorny plant barriers and noisy, crunchy gravel used under windows as a passive alarm should be used to deter entry into private spaces.
- The dominant Vernacular architectural styles related to California Bungalow and Victorian shall be used to establish the South of Laurel Conservation Area design theme. Architecture shall adhere to the following design guidelines:
- Buildings shall be similar in scale and form to existing structures and shall incorporate Vernacular characteristics, such as pitched gabled roofs, proportionally large overhangs, exposed roof beams and rafter tails, vertically oriented multi-paned windows, and front porches.



Hexagon House

- Buildings shall be wood frame construction with horizontal wood siding.
- Roof forms shall be typical of Vernacular styles and should be compatible with surrounding buildings.
- Roof materials shall be composition or wood shingle.
- Buildings shall be painted colors appropriate to the architectural style. Contrasting colors shall be used to accentuate door and window frames, porch railings and other distinctive features. Roof colors shall be subtle and compatible with the selected building colors.
- Where multiple color combinations are used, they shall be subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features and heighten interest.
- Wood sash or casement windows shall be used to maintain historic integrity and neighborhood character. Vinyl clad wood or baked enamel aluminum frame windows are acceptable if they duplicate the established neighborhood style.
- Colorful flowering annual and perennial plants are encouraged to enhance the character of the neighborhood and to promote individual residence identity
- Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.

New Development Areas

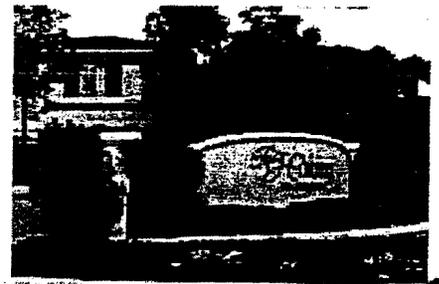
Multiple Family Residential

- Development shall adhere to the guidelines listed in the General Design Guidelines and the following guidelines.
- The siting and design of structures and landscaping, particularly when adjacent structures are single story, shall ensure that the development blends into rather than dominating the neighborhood.
- Three story buildings are encouraged. Multi-story buildings shall step down to transition to the smaller scale residential neighborhoods.
- Multiple story developments shall minimize scale through upper story setbacks, modular building units and other similar design techniques.
- Multiple family units shall be clustered to increase open space. Large developments shall be made up of groups of structures.

- Architecture shall enhance the community with similar high quality design, detail and materials as exhibited in the City's historic architecture.
- Courtyard style developments are encouraged for usable open space. Open spaces shall be consolidated to create usable open space.
- Parking shall be located within or at the rear of structures and parking lots shall not be visible from the public right-of-way.
- Large parking lots are not allowed. Parking shall be incorporated within residential structures and small parking courts wherever possible.
- Parking courts shall be treated as landscape plazas with attention to paving surfaces, shade trees, landscape planters and pedestrian walkways.
- Landscaping shall be designed to enhance the development architectural style.
- Permanent containers for flowering plants are encouraged for use in limited space areas, at entries and in courtyards and plazas. Containers may also serve as seating.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.
- Wall and fence materials, style and color shall complement the development architecture.
- View fencing is encouraged for "eyes on the street" passive security.
- Lighting type shall be suitable to the project architectural style.



Multiple family residential over ground floor commercial. Architecture incorporates building modules, offsets, bays, porches and balconies to minimize scale.



Multiple family residential development entry feature sign and lush landscape

Commercial

- Multi-level, large scale development is encouraged to optimize use of opportunity sites in the area.
- The number of site access points on primary thoroughfare streets shall be limited. Site access and internal circulation shall promote safety, efficiency and convenience. Avoid conflicts between vehicles and pedestrians and provide adequate areas for maneuvering, stacking, truck staging and loading and accommodating emergency vehicles on side streets and alleys.
- Parking lots shall be located at the rear of structures. Wherever possible, parking shall be located underground or within structures.
- Building articulation shall be used to create interest and reduce the visual impact of large building scale. This can be achieved through varying heights and setbacks within the same building, offsetting wall planes and adding architectural interest with roof overhangs, awnings, trellises, windows, moldings and other elements.



New Commercial development utilizes traditional architectural elements including cornices, balconies, arches and metal railings, and wall decoration. Pedestrian amenities include awnings, benches, landscaping, and textured paving

- Roof design shall be appropriate to the architectural style of the building. All unsightly views of flat roofs and roof top mechanical equipment shall be screened.
- Balconies, terraces, courtyards and similar outdoor spaces shall be provided along building street facades to take advantage of views and create street vitality.
- The facades of commercial buildings shall provide appropriate proportions of transparent glass in the entry street facing wall to promote visibility between the street and building interiors.
- Street fronting windows shall be no more than 30 inches above the sidewalk level.
- Bay windows may project into the public right-of-way provided there is adequate area for pedestrian circulation. Bay windows shall maintain glazing on all projecting faces and may not exceed 15 feet in width. For ground floor windows, there shall be a clearance of at least 12 inches between the bottom of the projecting bay and the sidewalk.
- Building entry porticos may project into the public right-of-way provided there is adequate area for pedestrian circulation.
- Standardized "corporate" architectural styles associated with chain-type commercial and hotel and motel development shall be modified to be compatible with the Plan Area design themes.
- Corporate color striping, wall logos, murals or other paint and color combinations used to transform buildings or building elements into a sign or advertising are not permitted.
- Every structure or commercial complex shall incorporate sign design into the overall project design plan. Signs shall complement the building scale, proportions, architectural style, materials and colors. Where more than one sign is allowed, all signs shall be designed using the same construction methods and materials, colors, lettering and logo styles.
- Sign design, materials and illumination shall complement the project architectural style. Internally illuminated plastic signs are inappropriate for traditional architectural styles in the Plan Areas.
- Commercial development on Pacific Avenue shall follow the Downtown Recovery Plan design guidelines for the village street corridor.
- Whenever possible, new structures shall be clustered to create plazas and pedestrian spaces. Such spaces shall provide amenities such as shade, benches, and fountains. When multiple-structure projects cannot be clustered, they shall be visually linked with arcades, trellises or similar structures.

- Colorful landscaping shall be provided to frame doorways or accent windows. Landscaping may be sidewalk planters, containers or window boxes.
- Permanent containers for flowering plants are encouraged for use in limited space areas, at entries and in courtyards and plazas. Containers may also serve as seating.
- Vines and climbing plants integrated with building design and used on walls and trellises are encouraged to enhance structures, soften blank walls, provide passive security barriers and deter graffiti.
- Wall and fence materials, style and color shall complement the development architecture.
- Lighting type shall be suitable to the project architectural style.
- Traditional paving materials such as brick, stone, tile and decorative concrete shall be used to accent entries or other significant pedestrian paths.



Downtown Street Scene



Downtown Street Scene

Hotel and Motel

- Standardized "corporate" architectural styles associated with chain-type commercial and hotel and motel development shall be modified to be compatible with the Plan Area design themes.
- Corporate color striping, wall logos, murals or other paint and color combinations used to transform buildings or building elements into a sign or advertising are not permitted.

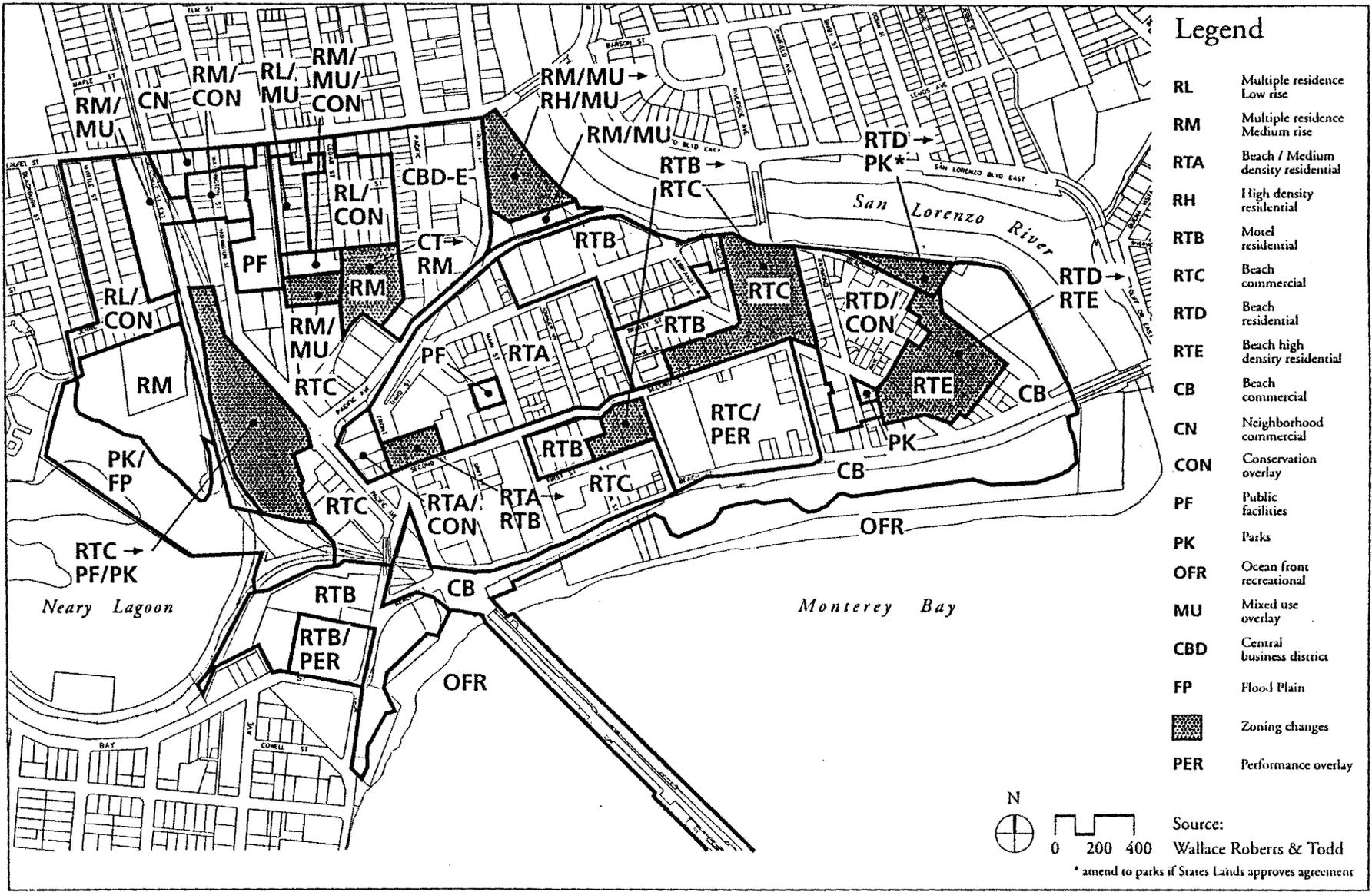


- The design and orientation of buildings and open space shall take advantage of available sunlight and, where possible, be sheltered from the noise of traffic or other incompatible uses.
- Developments on primary thoroughfares shall orient common use facilities such as the entry lobby, restaurants, shops and services at street level to contribute to the commercial resort mix of uses.
- Openings, including courtyard entries, gates, doors, windows and balconies shall be located along street facades at closely spaced intervals to avoid expanses of blank walls and to contribute to the pedestrian-oriented commercial resort character of the area.
- Upper level terraces with balcony street edges are encouraged for common open space areas and outdoor restaurants.
- Courtyard style developments are preferred. Courtyards shall be used as hotel entries, outdoor café or common open space. Multiple building unit developments shall be clustered to consolidate open space.
- Long monotonous access balconies and corridors which provide access to five or more hotel/motel units are not allowed. Access points shall be clustered in groups of four or less.
- Guest rooms shall be located to avoid noise generators such as mechanical equipment, trash enclosures and adjacent incompatible uses. Architectural measures such as masonry sound walls and noise attenuating building materials shall be used where exterior noise is excessive and unavoidable.

- Security is enhanced through visibility. Clear views shall be maintained to parking areas and through pedestrian paths and landscape areas. Site design shall minimize blind corners and creation of potential "hiding places." Windows shall be located so that guests have views into common areas.
- Use of metal window and door security grilles should be minimized. If grilles are used, they must be decorative and in a style and scale that is compatible with the building architecture.
- Pedestrian walkways shall be provided to link guest units with common open space areas, recreational and support facilities, parking areas and the street. Appropriate paving shall be used where pedestrians are likely to cross landscaped area.
- Recreational open spaces shall be centrally and conveniently located for guests.
- Private open spaces shall be directly accessible from guest rooms and shall be defined by railings, trellises, canopies or other architectural features designed as an integral part of the building.



*Remodeled 1950's Hotel
incorporates Spanish Colonial
Revival architectural elements and
extensive landscaping.*



- Legend**
- RL Multiple residence Low rise
 - RM Multiple residence Medium rise
 - RTA Beach / Medium density residential
 - RH High density residential
 - RTB Motel residential
 - RTC Beach commercial
 - RTD Beach residential
 - RTE Beach high density residential
 - CB Beach commercial
 - CN Neighborhood commercial
 - CON Conservation overlay
 - PF Public facilities
 - PK Parks
 - OFR Ocean front recreational
 - MU Mixed use overlay
 - CBD Central business district
 - FP Flood Plain
 - [Shaded Box] Zoning changes
 - PER Performance overlay

City of Santa Cruz - Beach / South of Laurel Area Plan

(RTD → RTE and RTD/CON were certified under STC-MAJ-1-01 (Part A))

Zoning Changes

ATTACHMENT 1

2. Adoption of the Zoning Ordinance text and map amendments contained herein serve and further the public necessity, the general community welfare, and good zoning practice. (Municipal Code, Section 24.06.040).
3. The Zoning Ordinance text and map amendments contained herein are in general conformance and internally consistent with the principles, policies and land-use designations set forth in the General Plan, the Local Coastal Plan, and the B/SOLA Area Plan. (Municipal Code, Section 24.06.040; Government Code Sections, 65300.5 and 65359.
4. Adoption of the Zoning Ordinance Map and Text Amendments implementing the B/SOLA Area Plan is consistent with the goals and policies of the Coastal Act. (Public Resources Code, Sections 30000 et seq.)
5. The public hearing process has been conducted as required by law.

BE IT ORDAINED by the City of Santa Cruz as follows:

Section 1. Chapter 24.10, Part 7D of the Santa Cruz Municipal Code is hereby amended as follows [deletions are ~~struck through~~; new language is underlined]:

Part 7D: R-T(D) SUBDISTRICT D - BEACH RESIDENTIAL

24.10.626 PURPOSE.

The purpose of Subdistrict D is to conserve, protect and enhance ~~establish standards for beach residential uses which promote and protect the beach~~ residential characteristics of the subdistrict and provide a suitable environment for residents. To preserve the small scale and enhance the historic beach cottage character of this subdistrict, and to ensure that new residential land uses are both compatible, permanent and of a high quality, all new development will be reviewed in compliance with the Beach Flats Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan and the Conservation Neighborhood Overlay requirements. (Ord. 85-05 § 1 (part), 1985).

24.10.627 PRINCIPAL PERMITTED USES.

1. The following uses are permitted, subject to a Design Permit, Conservation Overlay District Section 24.10.4000 and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Single Family and Duplexes; (800, 810)
- b. Storage and equipment structures, if ancillary to principal residential use;
- c. Small family day care facility in single-family home or duplex (510a);
- d. Accessory Uses. Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, accessory buildings;

(Ord. 85-05 § 1 (part), 1985).

24.10.628 USE PERMIT REQUIREMENT.

1. The following uses are subject to approval of an Administrative Use Permit and a Design Permit and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes.

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Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.):

- a. ~~Boardinghouses, lodging and rooming houses; residence halls.~~
- b. ~~Family animal farm.~~
- c. ~~Multiple dwellings, townhouses, townhouse and dwelling groups and condominiums, nine units or fewer.~~
- d. ~~Single family and duplex dwellings.~~
- e.a. Small community care residential facilities.
- f. ~~Storage and equipment structures.~~
- g.b. Temporary structures and uses.
- h. ~~The provision of board and room for not more than two paying guests per dwelling unit, when located within principal building.~~
- i.c. Accessory buildings containing plumbing fixtures subject to the provisions of Section 24.12.140.

2. The following uses are subject to approval of a Special Use Permit and a Design Permit and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Bed-and-breakfast inns, subject to the requirements contained in Part 9, Chapter 24.12.(300c)
- b. Community care facilities. (850e)
- c. Large family daycare facilities. (510a)
- e.d. Multiple family dwellings, townhouses, ~~townhouse and dwelling groups~~, and condominiums, three ten units or more (840)
- d. ~~Public and private commercial parking.(930,940)~~
- e. Public and private noncommercial recreation areas, buildings and facilities such as parks, ~~country clubs, golf courses, and riding, swimming and tennis clubs.(710)~~
- f. Public and quasi-public buildings and uses including administrative, recreational, educational, religious, cultural, public utility or public service uses; but not including yards, storage or repair yards, and warehouses. (500,510,530,540,570)
- g. Retirement homes or centers. (850b)

(Ord. 96-39 § 11, 1996; Ord. 88-60 § 13, 1988; Ord. 88-25 § 8, 1988; Ord. 85-66 § 11, 1985; Ord. 85-05 § 1 (part), 1985).

24.10.630 USE DETERMINATION.

Any other use or service establishment determined by the zoning administrator to be of the same general character as the foregoing uses, and which will not impair the present or potential use of adjacent properties, may be permitted. A use permit shall be required and processed pursuant to Part 1, Chapter 24.08, Use Permits, of this title.
(Ord. 85-05 § 1 (part), 1985).

24.10.632 DISTRICT REGULATIONS.

1. General.

Provision	Dwelling Unit Type				
	One-family Detached	Duplex	Triplex	4 or more units	Other Uses
a. Height of Buildings (Maximum) Stories & Feet			22		
• Principal (stories and feet)	2 & 22	2 & 22	2½ & 30	2½ & 30	2½ & 30
• Accessory (stories and feet)	1 & 15	1 & 15	1 & 15	1 & 15	1 & 15
b. Minimum lot area (net) (square feet)	3,000	3,600	7,200 4,800	8,000	8,000
c. Minimum lot area (net) per dwelling unit (sq. ft.)	--	1,800	1,600	1,600	--
d. Minimum lot width (feet)	40	40	40 80	50 80	50 80
e. Usable open space per dwelling unit (sq. ft.)	--	150 400	150 400	400	--
Setbacks	Dwelling Units				
	First One Story	Second Two Story	Other Uses		
f. Front yard (feet)	5*	10*	10*		
g. Rear yard (feet)	10	15	15		
h. Side yard each side (feet)	4	4	4		
or: one side (feet)	0	0	0		
total both sides (feet)	10	10	10		
i. Exterior side yard (feet)	5*	5*	5*		

* For any attached or detached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line.

2. Minimum Distance Between Buildings on the Same Lot. Between main buildings, including accessory dwelling units, ten feet or one foot of setback for each two feet of height of the tallest building, or portions thereof, whichever is greater; between main buildings and one-story accessory buildings, six feet; between accessory buildings, six feet.

2. 3. Other Requirements/Standards:

a. Design: All development is subject to a Design Permit and must be in compliance with adopted Design Guidelines. Other Regulations which may be applicable to site design in this zone are set forth in General Site Design Standards Part 2, Chapter 24.12. and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

(Ord. 99-04 § 6, 1999; Ord. 88-41 § 6, 1988; Ord. 88-24 § 8, 1988; Ord. 85-05 § 1 (part), 1985).

a.1. New buildings shall employ California Bungalow or Victorian architectural style as a basis for design.

a.2. Buildings shall be similar in scale and form to existing structures and shall incorporate Vernacular characteristics, such as pitched gabled roofs, proportionally large overhangs, exposed roof beams and rafter tails, vertically oriented multi-paned windows and front porches.

a.3. Buildings shall be wood frame construction with horizontal wood siding.

a.4. Roof forms shall be typical of the Beach Flats with appropriate steeper pitches for Victorians and lesser pitches for California Bungalow style.

a.5. Roof materials shall be composition or wood shingle.

STC-MAJ-1-01 (B)

**Exhibit 7
pg. 3 of 40**

b. Parking: All parking shall be located within the rear or at the rear of main structures, if possible. Private multi-residential parking lots shall be screened from the public right of way, and meet the requirements of Section 24.12.240, in addition to the following requirements:

b.1. All garages and entrances to parking areas shall be set back at least 5 feet from the adjacent front building setback.

b.2. On lots of 40 feet or less in width of street frontage, parking access is limited to a maximum of 12 feet of width. On lots of 40 feet to 65 feet in width, parking access is limited to a maximum of 16 feet of width; and on lots with greater than 65 feet in street frontage, parking access is limited to a maximum of 20 feet.

b.3. Driveways shall be minimized in order to maximize land use efficiency and the provision of landscaping and open space.

b.4. City parking standard requirements may be reduced in the following manner: one (1) parking space for a one bedroom unit; for two (2) or more bedrooms, the parking requirement may be reduced 50% if the following provisions are met:

- At least 50% of new units are two bedrooms or more;
- For units which meet the City's definition of "affordable"; and
- If development is deemed compatible with surrounding neighborhood.

c. Siting: All development shall be sited to create a harmonious streetedge, and to blend into rather than dominate the street.

c.1. Entries to individual units and grouping of units shall be located on the ground floor facing the street. These entries shall incorporate architectural and landscaping elements such as porches and arbors that visually reinforce the presence of entries.

c.2. Architectural elements, such as towers, balconies, stairs, decorative elements, etc., may be allowed to project up to 50% of the front yard setback requirement.

d. Height: Multiple story developments shall minimize scale through upper story setbacks, individual building elements, and other similar design techniques.

d.1. The height of buildings shall be minimized at the street, in the following manner:

- one story elements of buildings (including porches) must be set back 5 feet,
- second story elements of buildings must be set back 10 feet,

e. Landscaping, in compliance with the Design Standards, is required. Landscaping shall be maintained in an attractive condition.

e.1. Landscaping shall be designed to enhance the architectural style. All front, rear and side yards shall be fully landscaped except for areas devoted to driveways, patios, walkways or porches.

e.2. Permanent containers for flowering plants are encouraged for use in limited space areas, at entries and in courtyards and plazas.

e.3. Vines and climbing plants integrated with building design and used on walls and trellises are encouraged.

e.4. Opaque garden walls are not permitted within the frontyards setback to maintain the landscape continuity along the street. Fences limited to 3 feet in height are permitted as long as the fence is at least 60% open.

4. All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district.

24.10.633 CERTIFICATE OF OCCUPANCY REQUIRED.

In order to ensure safe and sanitary housing and rehabilitation of structures within the RT(D) District, a valid Certificate of Occupancy shall be required for each transfer of the property within the District. Certificates will not be issued for properties with a recorded Notice of Violation. Certificates will be issued when units comply with applicable codes.

Section 2. Chapter 24.10, Part 7 of the Santa Cruz Municipal Code is hereby amended to add a new Section, Part 7E, as follows:

Part 7E: R-T(E) SUBDISTRICT E - BEACH MEDIUM/HIGH DENSITY RESIDENTIAL

24.10.635 PURPOSE.

The purpose of Subdistrict E is to encourage quality medium and/or high density multifamily residential uses in a manner which promotes excellence in building design, provides for family-oriented development, ensures compatibility with the adjacent conservation overlay zone, and limits the need for parking by encouraging use of alternative means of transportation, including the multi-modal center proposed for the Depot site. All new development will be reviewed in compliance with the Beach Flats Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

certified
in STC-MAJ-1-01
(Part A)

24.10.636 PRINCIPAL PERMITTED USES.

1. The following uses are permitted subject to a design permit and other requirements of the Municipal Code:
- a. Duplex dwellings.
 - b. Small family day-care.
 - c. Accessory Uses. Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, accessory buildings.

24.10.637 USE PERMIT REQUIREMENT.

1. The following uses are subject to approval of an Administrative Use Permit, a Design Permit, and other requirements of the Municipal Code:
- a. Educational and cultural institutions.
 - b. Community care facilities.
 - c. Multiple dwellings, townhouses and condominiums, six units or fewer.
 - d. Single family dwellings on substandard lots.
2. The following uses are subject to approval of a Special Use Permit, a Design Permit and other requirements of the Municipal Code.
- a. Multiple dwellings, townhouses and condominiums, seven units or more, subject to the approval of the City Council upon recommendation of the Zoning Board.
 - b. Large family day care facilities.
 - c. Recreational buildings and community centers.
 - d. Public and private noncommercial recreation areas, buildings and facilities such as parks, playgrounds and basketball courts.
 - e. Public and private commercial parking, subject to landscaping and design standards. Non-conforming parking lots must be brought into compliance within 5 years of adoption of this title.

24.10.641 FINDINGS REQUIRED.

Prior to approval of any Design Permit for development within this District, the following additional findings must be made. The application:

1. Can be coordinated with existing and proposed development of the surrounding areas, and, if appropriate, particularly addressing the issue of transition to an adjacent Neighborhood Conservation Overlay District; and
2. Shall meet the requirements listed above for the high amenity level of the development, the quality of architecture, and the landscaping.

Section 3. Chapter 24.10, of the Santa Cruz Municipal Code is hereby amended to add a new Section, Part 31, as follows:

Part 31: CON - NEIGHBORHOOD CONSERVATION OVERLAY DISTRICT

*in
STC-MAJ-1-01
(Part A)*

24.10.4000 PURPOSE.

The purpose of the Neighborhood Conservation Overlay District is to conserve and enhance the residential quality of life within designated neighborhoods; to stimulate maintenance and reinvestment in structures consistent with design guidelines; to facilitate homeownership; and to ensure compatible development along district boundaries.

24.10.4010 APPLICABILITY.

Application of this district to specific neighborhoods may be initiated by the City to ensure that a logical district is formed and that appropriate boundaries are drawn to accomplish the stated goals.

24.10.4020 GENERAL PROVISIONS.

The underlying zoning district shall establish the basic parameters of the building coverage including setbacks, density, building height and floor area ratio within the Overlay district. All new construction or renovations are required to demonstrate compatibility with existing residential uses and will be subject to the architectural and landscaping requirements of the design guidelines.

1. Permitted Uses:
 - a. Residential Uses. All residential uses permitted in the underlying zoning district are permitted, with the same conditions, within the Neighborhood Conservation Overlay District.
 2. Other Uses.
 - a. All multi-family uses and all other uses permitted in the underlying zoning district are subject to a Special Use Permit and a Design Permit.
 - b. In addition to the requirements of the underlying district, other uses seeking to locate in a Neighborhood Conservation District will be required to demonstrate compatibility with existing residential use and provide appropriate buffering between the uses.
 3. Siting: All development shall be sited to construct a harmonious street edge. No building front yard setback, within a residential block, shall vary more than 5 feet from its adjacent neighbors.

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24.10.4030 PARKING.

Off street parking requirements must be fulfilled according to the requirements of the underlying zone. In addition, parking shall be located within or at the rear of structures, and parking lots shall be screened from the public right of way. In residential developments of three units or less, driveways shall not exceed the minimum city standards in order to maximize land use efficiency. In residential developments of three units or more, or in the development of other uses identified in the underlying zone, parking lots shall be treated as landscape plazas with attention to paving surfaces, shade trees, landscape planters and pedestrian walkways. For these projects, a landscaping plan must be submitted for approval by the Planning Director, and must be maintained in an attractive condition.

24.10.4040 EXTERIOR MAINTENANCE: DEMOLITION CONTROL: ORDERLY CHANGE.

The purpose of these provisions is to provide an administrative technique to ensure preservation and maintenance of contributing residential housing stock within the Conservation Overlay District in a manner which accomplishes public objectives, and provides for orderly change within existing housing stock. A Status Report shall be prepared by the Planning Department on the condition of the housing stock in the Conservation Districts within 24 months of Conservation Overlay designation.

1. Targeted Code Enforcement Area. Application of the Neighborhood Conservation Overlay District to a specific neighborhood will concurrently designate the neighborhood as a "targeted code enforcement area."

a. Concentrated housing and zoning code enforcement is to be carried out within the District.

b. An inventory of all vacant, boarded-up, and deteriorated exterior structures is to be submitted to the Zoning Board as part of the Status Report. Notices shall be sent to all owners requiring compliance with applicable codes. Notices requiring removal shall be sent to property owners where storage or accumulation of household items, or large household appliances or abandoned or inoperable cars are visible from a public street, alley or sidewalk.

c. The City shall have the right of first refusal to purchase all residential properties located within a Neighborhood Conservation District which are offered at tax delinquency sales. The City may make these properties available for purchase by qualified lower income first time homebuyers.

2. Demolition Control. In order to ensure preservation of the neighborhood character and ensure orderly change and rehabilitation, where appropriate, of existing structures, the following demolition control procedures are required in the Neighborhood Conservation Overlay District.

a. No demolition permit shall be issued in a Neighborhood Conservation District, until the structure has been evaluated for its contribution to the neighborhood historical and/or architectural context by a appropriate professionals, either architects, code enforcement/building inspectors and/or preservation staff as determined by the Planning Department. This evaluation shall be submitted to the Zoning Administrator who shall make the final determination whether or not a demolition is appropriate.

24.10.4050 HOMEOWNERSHIP.

To ensure residential stability within designated Neighborhood Conservation Overlay Districts, each neighborhood so designated shall be eligible for participation in the City's HOME fund program for lower income first-time home buyers, and other homeownership programs.

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24.10.4060 NEW CONSTRUCTION ON SITES ABUTTING OVERLAY DISTRICT BOUNDARIES.

The purpose of the following provisions is to ensure that new development which occurs on the boundaries of a designated Neighborhood Overlay District is compatible and supportive of the public policy goals established for these districts. These provisions apply to all sites abutting a Neighborhood Conservation Overlay District.

In addition to the regulations of the underlying zoning districts, all development, redevelopment and building expansions on sites abutting Neighborhood Conservation Overlay Districts shall comply with the following:

1. Siting: All development shall be designed in a manner that is compatible, to the extent possible, with the existing residential structures in the abutting Neighborhood Conservation Overlay District.
2. Design: All development shall be subject to a Design Permit and must be in compliance with adopted Design Guidelines.
3. Height: While the regulations of the underlying zoning district will control height, all development, redevelopment and building expansions on sites abutting the Neighborhood Conservation District and within 30 feet of existing adjacent Conservation District residential structures, shall transition in height and bulk to create a visually compatible relationship with existing structures. Distinctive architectural features may be allowed additional height if permitted by the underlying district, and if compatible with the neighboring district.
4. Parking: All parking must be screened from view from the first floor of existing residential structures in the Conservation District. This screening may be accomplished by walls, trellises, fencing, and/or landscaping. All development must submit a parking/landscaping plan.
5. All lighting must be designed so that the light source is not visible from the adjacent residential properties nor are the light rays directed or reflected into or on adjacent residential properties.

24.10.4065 CERTIFICATE OF OCCUPANCY REQUIRED.

In order to ensure safe and sanitary housing and rehabilitation of structures within the Neighborhood Conservation Overlay District, a valid Certificate of Occupancy shall be required for each transfer of the property within the District. Certificates will not be issued for properties with a recorded Notice of Violation. Certificates will be issued when units comply with applicable codes.

Section 4. Chapter 24.10, Parts 7 and 7A of the Santa Cruz Municipal Code are hereby amended as follows:

Part 7: R-T TOURIST RESIDENTIAL DISTRICT

24.10.600 PURPOSE.

The purpose of the R-T Tourist Residential District is to establish a zoning district to accommodate a mix of residential, motel, and commercial land uses and to preserve historical buildings and trees within the district. Recognizing this variety of land uses and the desire to utilize existing land uses, the R-T District is divided into ~~four~~ five subdistricts: Medium High-Density Residential, Motel Residential, Beach Commercial, and Beach Residential, and High/Medium Density Residential. Each subdistrict is designed to ensure compatibility of uses.

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upgrade the area by ensuring a high quality of new developments, and promote a suitable environment for residential and tourist-oriented uses. This section of the Zoning Ordinance is also part of the Local Coastal Implementation Plan.
(Ord. 94-33 § 32, 1994; Ord. 85-05 § 1 (part), 1985).

24.10.601 — ~~SPECIAL DENSITY ALLOWANCE.~~

~~The special density allowance is applicable as set forth below to Subdistricts A, B and C of the R-T District. One dwelling unit per one thousand square feet of land may be permitted on lots eight thousand square feet or larger, as set forth below:~~

- ~~1. A special use permit and design permit shall be required, and approved by the city council upon a recommendation of the zoning board.~~
- ~~2. All units shall be affordable by persons or families of low or moderate income, as established by city council resolution.~~

~~(Ord. 88-41 § 1, 1988; Ord. 85-05 § 1 (part), 1985).~~

Part 7A: R-T(A) SUBDISTRICT A - MEDIUM HIGH-DENSITY RESIDENTIAL

24.10.602 PURPOSE.

The purpose of Subdistrict A is to establish standards for medium high-density residential uses which promote and protect the residential characteristics of the subdistrict and provide a suitable environment for its residents. To preserve the architectural and historic character of this subdistrict, all new development will be reviewed to ensure high-quality design compatible with surrounding residential uses, in compliance with the Beach Hill Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

(Ord. 85-05 § 1 (part), 1985).

24.10.603 PRINCIPAL PERMITTED USES.

1. The following uses are subject to approval of a Design Permit and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Duplexes; (810)
- b. Small family day care facility in single-family home or duplex (510a);
- c. Accessory Uses. Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, accessory buildings;

24.10.604 USE PERMIT REQUIREMENT.

1. The following uses are subject to approval of an Administrative Use Permit and a Design Permit and other requirements of the Municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.):

- a. Boardinghouses, lodging and rooming houses; residence halls; (850a)
- b. Family animal farm;
- c. Multiple dwellings, townhouses, townhouse and dwelling groups, and condominiums, nine units or fewer;
- d. a. Single-family and duplex dwellings; (810)

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- e. ~~Storage and equipment structures;~~
- f. ~~Temporary structures and uses;~~
- g. ~~The providing of board and room for not more than two paying guests per dwelling unit, when located within principal building;~~
- j. b. Accessory buildings containing plumbing fixtures subject to the provisions of Section 24.12.140.

c. Multiple dwellings, townhouses, and condominiums (4-9 units); (830)

2. The following uses are subject to approval of a Special Use Permit and a Design Permit and other requirements of the Municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.):

- a. Bed-and-breakfast inns, subject to the requirements contained in Part 9, Chapter 24.12; (300c)
- b. Large community care facilities; (850e)
- c. Large family daycare facilities; (510a)
- e.d. Group care homes; (850e)
- d.e. Multiple dwellings, townhouses, townhouse and dwelling groups, and condominiums, ten units or more; (840)
- e.f. Public and private commercial parking;
- f.g. Public and private noncommercial recreation areas, buildings and facilities such as parks, country clubs, golf courses, and riding, swimming and tennis clubs; (710)
- g.h. Public and quasi-public buildings and uses including recreational, educational, religious, cultural or public utility or service nature; but not including corporation yards, storage or repair yards, and warehouses; (500,510,530,540,570)
- h.i. Retirement homes or centers. (850b)

(Ord. 96-39 § 7, 1996; Ord. 88-60 § 10, 1988; Ord. 88-25 § 5, 1988; Ord. 85-66 § 9, 1985; Ord. 85-05 § 1 (part), 1985).

24.10.606 USE DETERMINATION.

Any other use or service establishment determined by the Zoning Administrator to be of the same general character as the foregoing uses, and which will not impair the present or potential use of adjacent properties, may be permitted. A Use Permit shall be required and processed pursuant to Part 1, Chapter 24.08, Use Permits, of this title. (Ord. 85-05 § 1 (part), 1985).

24.10.608 DISTRICT REGULATIONS.

1. General.

Provision	Dwelling Unit Type			
	One-Family Detached	Duplex	3 or more units	Other Uses
a. Maximum Height of Buildings				
• Number of Stories	2	2	5 <u>3</u>	5 <u>3</u>
• Principal buildings (feet)	35 <u>30</u>	35 <u>30</u>	55 <u>36</u>	55 <u>36</u>
• Accessory buildings (feet)	25 <u>15</u>	25 <u>15</u>	25 <u>15</u>	25 <u>15</u>
b. Minimum lot area (net) (square feet)	5,000	5,000	8,000	8,000
c. Minimum lot area (net) per dwelling unit (square feet)	5,000	2,500	<u>1,450</u> 1,600 or 1,000	-
d. Minimum lot width (feet)	50	50	65	65
e. Usable open space per dwelling unit (square feet)	-	-	400 ²	-

~~1. See Section 24.10.601.~~

~~2. Projects using Special Density Allowance may reduce usable open space to one hundred fifty square feet per dwelling unit.~~

2. Setback Requirements.

a. The minimum front yard setback shall be fifteen feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater, except that the front yard may be reduced to not less than six feet for a portion not to exceed fifty percent of the building frontage, providing that a total of ten square feet of front yard is provided for each lineal foot of total lot frontage. Such reduction of front yard depth shall not be permitted on a corner lot, within twelve feet of any side street lot line.

b. The minimum rear setback shall be ten feet, or one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater.

c. The minimum side yard setback shall be five feet for the first story and, ~~or~~ one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater for the second story and above.

d. There shall be no side yard required for townhouses, or interior lots except there shall be a minimum side yard setback at the interior end of a townhouse group of five feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

e. The minimum exterior side yard setback shall be eight feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

f. The Minimum Distance Between Buildings on the Same Lot. Between main buildings, including accessory dwelling units, shall be ten feet, or one foot of setback for each two feet of height, or portion thereof, of a structure, whichever is greater; between main buildings and one-story accessory buildings, six feet; between accessory buildings, six feet.

g. For any attached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line.

3. Other Requirements. Other regulations which may be applicable to site design in this zone are set forth in General Site Design Standards, Part 2, Chapter 24.12, and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

4. All new development adjacent to a "CON - Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district. (Ord. 88-41 § 2, 1988; Ord. 88-24 § 5, 1988; Ord. 85-05 § 1 (part), 1985).

Section 5. Chapter 24.10, Part 7B of the Santa Cruz Municipal Code is hereby amended as follows:

Part 7B: R-T(B) SUBDISTRICT B - MOTEL RESIDENTIAL

24.10.610 PURPOSE.

The purpose of Subdistrict B is to establish and control uses to ensure a compatible mixture of uses addressing the needs of residents and tourists. Dominant uses contemplated are motel and medium high-density residential uses. To encourage a mixture development which is attractive to both permanent residents and tourists, emphasis will be placed on compatibility ~~in~~ of design, landscaping, and a comprehensive review of use-siting site planning in compliance with the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan. (Ord. 85-05 § 1 (part), 1985).

24.10.612 USE PERMIT REQUIREMENTS.

1. The following uses are subject to approval of an Administrative Use Permit and a Design Permit and other requirements of the Municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. ~~Boardinghouses, lodging and rooming houses; residence halls.~~
- b. ~~Family animal farm.~~
- e. a. Multiple dwellings, townhouses, townhouse and dwelling groups, and condominiums, nine units or fewer (830).
- d. b. Single-family and duplex dwellings (800,810).
- e. c. Single-room occupancy (SRO) housing, fifteen units or fewer (860).
- f. d. Storage and equipment structures.
- g. e. Temporary structures and uses.
- h. f. The providing of board and room for not more than two paying guests per dwelling unit, when located within principal building.
- i. g. Accessory buildings containing plumbing fixtures subject to the provisions of Section 24.12.140.

2. The following uses are subject to approval of a Special Use Permit and a Design Permit and other requirements of the Municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Coffee shops. (280g)
- b. Large community care facilities. (850e)
- c. Large family daycare facilities. (510a)

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~~e.d.~~ Motel, hotel and bed-and-breakfast inn uses ~~including both permanent residential and transient residential uses.~~ subject to annual business license review. (300)

~~d.e.~~ Multiple dwellings, townhouses, townhouse and dwelling groups, and condominiums, ten units or more. (840)

~~e.f.~~ Public and private commercial parking. (940,950)

~~f.g.~~ Public and private noncommercial recreation areas, buildings and facilities such as parks ~~country clubs, golf courses, and riding, swimming and tennis clubs.~~ (710)

~~g.h.~~ Public and quasi-public buildings and uses of an administrative, recreational religious, cultural or public utility or service nature; but not including corporation yards, storage or repair yards, and warehouses. (500,510,530,540,570)

~~h.i.~~ Retirement homes or centers. (850b)

~~i.j.~~ Single-room occupancy (SRO) housing sixteen units or more. (860)

(Ord. 96-39 § 8, 1996; Ord. 93-21 § 1, 1993; Ord. 88-60 § 11, 1988; Ord. 88-25 § 6, 1988; Ord. 85-66 § 10, 1985; Ord. 85-05 § 1 (part), 1985).

24.10.614 USE DETERMINATION.

Any other use or service establishment determined by the zoning administrator to be of the same general character as the foregoing principal permitted uses, and which will not impair the present or potential use of adjacent properties, may be permitted. A use permit shall be required and processed pursuant to Part I, Chapter 24.08, Use Permits, of this title. (Ord. 85-05 § 1 (part), 1985).

24.10.616 DISTRICT REGULATIONS.

1. General.

Provision	Dwelling Unit Type			
	Medium High Density Residential			
	One-Family Detached	Duplex	3 or more units	Other Uses
a. Maximum Height of Buildings				
• Number of Stories	2	2	5 3	5 3
• Principal buildings (feet)	35 30	35 30	55 36	55 36
• Accessory buildings (feet)	15 25	15 25	15 25	15 25
b. Minimum lot area (net) (square feet)	5,000	5,000	8,000	8,000
c. Minimum lot area (net) per dwelling unit (square ft.)	5,000	2,500	1,450 1,600 or 1,000 ¹	-
d. Minimum lot width (feet)	50	50	65	65
e. Usable open space per dwelling unit (square ft.)	-	-	400 ²	-

1. See Section 24.10.601.

2. ~~Projects using Special Density Allowance may reduce usable open space to one hundred fifty square feet per dwelling unit.~~

2. Setback Requirements.

a. The minimum front yard setback shall be fifteen feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater; except that the front yard may be reduced to not less than six feet for a portion not to exceed fifty percent of the building frontage, providing that a total of ten square feet of front yard is provided for each lineal foot of total lot frontage. Such reduction of front yard depth shall not be permitted on a corner lot, within twelve feet of any side street lot line.

b. The minimum rear setback shall be ten feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

c. The minimum side yard setback shall be five feet for the first story and, or one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater, for the second story and above.

d. There shall be no side yard required for townhouses on interior lots except there shall be a minimum side yard setback at the interior end of a townhouse group of five feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

e. The minimum exterior side yard setback shall be eight feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

f. The Minimum Distance Between Buildings on the Same Lot. Between main buildings, including accessory dwelling units, shall be ten feet, or one foot of setback for each two feet of height of the tallest building, or portion thereof, of a structure, whichever is greater; between main buildings and one-story accessory buildings, six feet; between accessory buildings, six feet.

g. For any attached or detached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line.

3. Other Requirements. Other regulations which may be applicable to site design in this zone are set forth in General Site Design Standards, Part 2, Chapter 24.12. and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

4. All new development adjacent to a "CON - Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district. (Ord. 88-41 § 3, 1988; Ord. 88-24 § 6, 1988; Ord. 85-05 § 1 (part), 1985).

Section 6. Chapter 24.10, Part 6A of the Santa Cruz Municipal Code is hereby amended as follows:

Part 6A: R-H MULTIPLE RESIDENCE - HIGH-DENSITY DISTRICT

24.10.560 PURPOSE.

To promote the development of multifamily apartments, townhouses and condominiums at a high residential density of 30.1 to 55 units per acre in order to increase the supply of affordable and rental housing, and provide new market rate infill housing opportunities. This district provides a suitable environment for higher density households. This section of the Zoning Ordinance is also part of the Local Coastal Implementation Plan. (Ord. 96-36 § 1 (part), 1996).

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24.10.565 PRINCIPAL PERMITTED USES.

The following uses are permitted subject to a Design Permit for new structures in compliance with the Beach and South of Laurel Design Guidelines and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.) Additionally, environmental review must be conducted in accordance with city and state guidelines:

1. Apartments, townhouses, row houses and condominiums in one or more structures that meet minimum density requirements; (830,840)
2. Small community care residential facilities including daycare, foster homes, retirement homes, with six or fewer persons; (510a)
3. Small family daycare; (510a)
4. Large family daycare in single-family dwelling or duplex. (510a)

(Ord. 96-36 § 1 (part), 1996).

24.10.570 ACCESSORY USES.

Accessory uses are principally permitted subject to a Design Permit when they are a subordinate use to the principal use of the lot:

1. Garages and parking areas, private;
2. Home occupations subject to home occupancy regulations as provided in Section 24.10.160;
3. Residential accessory uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Sections 24.12.140 and 24.10.575.

(Ord. 96-36 § 1 (part), 1996).

24.10.575 USE PERMIT REQUIREMENT.

1. The following uses are subject to approval of an Administrative Use Permit and a Design Permit in compliance with the Beach and South of Laurel Design Guidelines and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.):

- a. Expansion of any existing single-family dwelling; (800)
- b. Two-family dwelling if the lot area allows only two. New single-family development is not permitted; (810)
- c. Temporary structures and uses;
- d. Accessory buildings containing plumbing fixtures subject to the provisions of Section 24.12.140;
- e. Single-room occupancy (SRO) housing, fifteen units or fewer. (860)

2. The following uses are subject to approval of a Special Use Permit and a Design Permit in compliance with the Beach and South of Laurel Design Guidelines and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.):

- a. Bed-and-breakfast inns subject to requirements contained in Part 9, Chapter 24.12; (300c)

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b. Community care facilities including daycare, foster homes, nursing and retirement homes for seven or more persons;(850e)

c. Health facilities for in-patient and out-patient psychiatric care and treatment; (410b)

d. Off-street parking facilities accessory and incidental to a contiguous commercial property with said parking not to exceed one hundred feet from the boundary of the site it is intended to serve; (930)

e. Public and private noncommercial recreation areas, buildings, and facilities such as parks; (710)

f. Public and quasi-public buildings and uses including recreational, educational, religious, cultural, public utility or public service uses; but not including corporation yards, storage or repair yards, and warehouses; (500,510,530,540)

g. Social halls, lodges, fraternal organizations, and clubs, except those operated for a profit; (570)

h. Single-room occupancy (SRO) housing, sixteen units or more. (860)
(Ord. 96-36 § 1 (part), 1996).

24.10.580 USE DETERMINATION.

Any other use or service establishment determined by the Zoning Administrator to be of the same general character as the foregoing uses, and which will not impair the present or potential use of adjacent properties, may be allowed by Use Permit.
(Ord. 96-36 § 1 (part), 1996).

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24.10.585 DISTRICT REGULATIONS.

1. General.

Provision	Dwelling Unit Type	
	Duplex	3 or More Units
a. <u>Maximum Height of buildings</u>		
• <u>Principal: Number of Stories (feet)</u>	<u>2 ½ & 30</u>	<u>4 & 48</u>
• <u>Accessory: (stories & feet)</u>	<u>1 & 15</u>	<u>1 & 15</u>
b. <u>Min. lot area (net) (sq. ft.)</u>	<u>4,000</u>	<u>5,000</u>
c. <u>Min. lot area (net) per dwelling unit (sq. ft.)</u>	<u>2,000</u>	<u>790</u>
d. <u>Minimum lot width (feet)</u>	<u>50</u>	<u>50</u>
e. <u>Usable open space per dwelling unit (sq. ft.)</u>	<u>-</u>	<u>250</u>
f. <u>Lot Coverage</u>	<u>45%</u>	<u>70%</u>

2. Setback and Height Requirements.

a. The minimum front yard setback shall be fifteen feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater, except that the front yard may be reduced to not less than six feet for a portion not to exceed 50% of the building frontage, providing that a total of ten square feet of front yard is provided for each lineal foot of total lot frontage. Such reduction of front yard depth shall not be permitted on a corner lot, within twelve feet of any side street lot line. Setback requirements may not be reduced for those portions of buildings that are three stories or taller.

b. The minimum rear setback shall be ten feet, or one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater.

c. The minimum side yard setback shall be five feet, or one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater.

d. Where a parcel abuts a public right-of-way on opposite sides of the parcel, the front yard setback requirements shall apply to the major street and the rear yard setbacks shall apply to the secondary street; however, in the case of the rear yard setback, the setback shall not be less than the average of the two adjacent lots.

e. The minimum exterior side yard setback shall be eight feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

f. Minimum Distance Between Buildings on Same Lot. Between main buildings, ten feet or one foot of setback for each two feet of height of the tallest building, or portions thereof, whichever is greater; between main buildings and accessory buildings, six feet; between accessory buildings, six feet.

g. For any attached or detached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line.

h. To preserve the view to Beach Hill down Front Street, the height of the first 25 feet of depth along Front Street shall be limited to 24 feet.

3. Design.

a. The site and building design shall conform to the General Site Design Standards, Part 2, Chapter 24.12 and the Beach and South of Laurel Design Guidelines.

b. The building façades that abut streets, public rights-of-way, or public open space,

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such as a river levee, shall have an articulated façade that creates a designed, three-dimensional rhythm to the building face. Architectural elements such as roof overhangs, roof slopes, building step backs and projections, bay windows, covered porches, entryways, decks and balconies, dormers, and single-story elements may be used to create the required three-dimensional architectural interest in the façade. In addition to the above, the front side and rear building façades that are visible from streets, public rights-of-way or public open space shall be composed of at least two separate building planes. A separate building plane is distinguished by an average horizontal difference of 4 feet measured perpendicular to the plane closet to the street or property line. These building planes shall occupy at least 20% of the total building elevation area but not exceed 60% of that area.

c. Maintain views to Beach Hill by creating view corridors through new structures. Portions of the building(s) shall not occupy more than 60 percent of the top 10 feet of the height limit, as viewed from the center of the Soquel Bridge. The minimum 40% open view to Beach Hill shall be reasonably distributed over the length of the building.

d. All open space, except balconies, shall be fully landscaped. To soften the massing of high density buildings, the building landscape shall include tree massing and/or landscape elements attached to the building such as trellises, arbors, espalier structures, etc.

e. All parking shall be fully screened from the roadways, pedestrian ways, and open space by building elements, walls, and landscaping.

4. All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district.

24.10.590 FINDINGS REQUIRED.

In addition to required Use and Design Permit findings, any development permit must also meet all the following findings:

1. The amenity level of the development, the quality of the architecture, and the landscaping provided substantially enhance the site;

2. The bulk, massing, height, and rooflines of the proposed development are found to be consistent with the Design Criteria and add to the architectural quality of the neighborhood; and

3. The siting, landscaping, access, and design of the proposed development demonstrate a sensitive relationship to the San Lorenzo River and maximize the natural attributes of this riverside location.

Section 7. Chapter 24.10, Part 7C of the Santa Cruz Municipal Code is hereby amended as follows:

Part 7C: R-T(C) SUBDISTRICT C - BEACH COMMERCIAL

24.10.618 PURPOSE.

The purpose of the R-T(C) Subdistrict is to establish standards for development of residential uses mixed with neighborhood commercial, motel, and regional tourist commercial use. These standards are designed both to improve existing uses and encourage new developments in a manner that maintains a harmonious balance between residential and regional commercial uses. It is the intent of this zoning that preservation of la Bahia be conducted in accordance with the measures described in the certified final Environmental Impact Report for the Beach and South of Laurel Comprehensive Area Plan.

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(Ord. 93-21 § 2, 1993; Ord. 85-05 § 1 (part), 1985).

24.10.619 PRINCIPAL PERMITTED USES.

1. The following uses are allowed outright, subject to a Design Permit and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Further refinement of uses within these categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. ~~Duplexes (810);~~
- b. a. Food and beverage stores (except liquor stores) (240);
- e. b. Lodging (300); Motel, hotel, and bed-and-breakfast inn uses subject to annual business license review (300);
- d. ~~Multiple family (3-9 units) subject to design standards in R-T(A) District standards (830);~~
- e. c. One or two multiple-family units when located above the first floor of permitted commercial uses with no additional parking required (820);
- f. d. Off-site parking fewer than five spaces (930);
- g. ~~Personal services (except contractors yards and mortuaries) (310);~~
- h. e. Small family day care facility in single-family home or duplex;
- f. Eating and Drinking Establishments without alcohol sales (280).
- i. ~~Triplexes (820).~~

2. Accessory Uses. Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, accessory buildings, and Section 24.10.620.

- a. ~~The providing of board and room for not more than two paying guests per dwelling unit when located within principal building.~~

(Ord. 96-39 § 9, 1996; Ord. 93-21 § 3, 1993).

24.10.620 USE PERMIT REQUIREMENT.

(1) The following uses require an Administrative Use Permit and Design Permit and are subject to other applicable requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Subcategories of uses within these use categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- (a) Accessory buildings containing plumbing fixtures subject to provisions of Section 24.12.140;
- (b) Acting/art/music/dance/studios/schools (610);
- (c) Apparel and accessory stores (250);
- (d) Churches (500);
- (e) Community organizations, associations, clubs and meeting halls (570);
- (f) Convenience stores, subject to alcohol regulations in Part 12 of Chapter 24.12 (240B);
- (g) Developed parks (710);
- (h) Undeveloped parks and open space (700);
- (h) (i) Eating and drinking establishments (except bars and fast-food restaurants) subject to alcohol regulations in Part 12 of Chapter 24.12 (280);
- (i) (j) Educational facilities (public/private) (510);
- (j) ~~Financial, insurance, real estate offices (420);~~
- (k) ~~Financial services (320);~~

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- (~~k~~) (k) General merchandise stores (drug and department stores) (230);
- (~~m~~) (l) Government and public agencies (530);
- (~~n~~) (m) Home furnishings (270);
- (~~o~~) (n) Liquor stores, subject to alcohol regulations in Part 12 of Chapter 24.12 (240B);
- (~~p~~) ~~Medical/health offices (410);~~
- (~~q~~) (o) Mixed residential, and commercial developments when multiple family units are located above first floor of commercial uses, subject to the R-T(A) District regulations (830);
- (~~r~~) (p) Multifamily 10+ (3-9 units) units subject to the R-T(A) District regulations (840);
- (~~s~~) (q) Museum and art galleries (600);
- (~~t~~) (r) Professional offices (400); associated with a visitor-serving use;
- (~~u~~) (s) Repairs, alterations, maintenance services to household items (except boat repair) (340);
- (~~v~~) (t) Single-room occupancy (SRO) housing, fifteen units or fewer (860);
- (~~w~~) (u) Specialty retail supply stores (290);
- (~~x~~) (v) Temporary structures and uses;
- (~~y~~) ~~Undeveloped parks and open space (700);~~
- (~~z~~) (w) Video rental (360B);
- (~~1~~) (x) Sports and Recreation facilities, without alcohol sales (720)
- (2) The following uses require a ~~s~~Special ~~u~~Use ~~p~~Permit and Design Permit and are subject to other applicable requirements of the municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Subcategories of uses within these use categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)
 - (a) Bars/taverns subject to alcohol regulations in Part 12 of Chapter 24.12 (280C);
 - (b) Communication and information (550);
 - (c) Large family daycare facilities;
 - (~~e~~)(d) ~~Fast-food restaurants or drive-in eating facilities subject to performance standards in Section 24.14.290 and~~ subject to alcohol regulations in Part 12 of Chapter 24.12 (280H);
 - (~~e~~) ~~Group quarters (850);~~
 - (e) Professional offices (400), except as associated with a visitor-serving use;
 - (f) Multifamily 10+ units subject to the R-T(A) District regulations (840);
 - (~~f~~) (g) Marine facilities and related uses (560E):
 - (i) Related research facilities (400L),
 - (ii) Related storage and warehousing (330),
 - (iii) Fish/seafood wholesale sales (200F);
 - (~~g~~) (h) Mixed residential and commercial developments with non-commercial uses on the ground floor, subject to the R-T(A) District regulations (830);
 - (~~h~~) (i) Nightclubs (live amplified music), subject to alcohol regulations in Part 12 of Chapter 24.12 (630);
 - (~~i~~) (j) Off-site public/private parking facilities, five or more spaces (930);
 - (~~j~~) (k) Single-family residences if lot size does not allow multifamily development (800);
 - (~~k~~) (l) Single-room occupancy (SRO) housing, sixteen units or more (860);
 - (~~l~~) (m) Sports and recreation facilities subject to alcohol regulations in Part 12 of Chapter 24.12 (720);
 - (~~m~~) (n) Theaters (620);
 - (~~n~~) (o) Utilities and resources (540);
 - (~~o~~) (p) Duplexes (810);

- (q) Personal services (except contractors yards and mortuaries) (310);
- (r) Triplexes (820);
- (s) Educational facilities (public/private) (510);
- (t) Financial, insurance, real estate offices (420);
- (u) Medical/health offices (410).

(Ord. 96-39 § 10, 1996; Ord. 96-08 § 1, 1996; Ord. 95-04 § 1, 1995; Ord. 93-21 § 4, 1993; Ord. 89-39 § 3, 1989; Ord. 88-60 § 12, 1988; Ord. 88-41 § 4, 1988; Ord. 88-26 § 1, 1988; Ord. 88-25 § 7, 1988; Ord. 87-22 § 1, 1987; Ord. 85-66 § 12, 1985; Ord. 85-05 § 1 (part), 1985).

24.10.622 USE DETERMINATION.

Any other use or service establishment determined by the Zoning Administrator to be of the same general character as the foregoing principal permitted uses, and which will not impair the present or potential use of adjacent properties, may be permitted. A Use Permit shall be required and processed pursuant to Part 1, Chapter 24.08, Use Permits, of this title. (Ord. 85-05 § 1 (part), 1985).

24.10.624 DISTRICT REGULATIONS.

1. General.

Provision	Dwelling Unit Type			
	One-Family Detached	Duplex	3 or More Units	Other Uses
a. Maximum Height of Buildings				
• Number of Stories	2	2	3	3
• Principal buildings (feet)	35 <u>30</u>	35 <u>30</u>	35 <u>36</u>	35 <u>36</u>
• Accessory buildings (feet)	25 <u>15</u>	25 <u>15</u>	25 <u>15</u>	25 <u>15</u>
b. Minimum lot area (net) (square feet)	5,000	5,000	5,000 <u>8,000</u>	5,000
c. Minimum lot area (net) per dwelling unit (square feet)	5,000	2,500	1,450 <u>1,600</u> or <u>1,100⁺</u>	--
d. Minimum lot width (feet)	50	50	65	65
e. Usable open space per dwelling unit (square feet)	—	—	400 ²	—

1. See Section 24.10.601.

2. ~~Projects using Special Density Allowance may reduce usable open space to one hundred fifty square feet per dwelling unit.~~

2. Other Requirements.

a. When located across a street from Subdistrict A, parking and loading facilities shall be at least ten feet distant from said property line, and buildings and structures at least fifteen feet from said property line.

b. The minimum distance between buildings on the same lot shall be ten feet, or one foot of setback for each two feet of height of, or portion thereof, a structure, whichever is greater.

c. For any attached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line, to the entrance of the garage.

d. Other regulations which may be applicable to site design in this zone are set forth in General Site Design Standards, Part 2, Chapter 24.12.

(Ord. 88-41 § 5, 1988; Ord. 88-24 § 7, 1988; Ord. 85-05 § 1 (part), 1985).

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e. Height:

e.1 Maximum Building Height: Uninhabitable mechanical penthouses shall be limited to 10% of the roof area and will be permitted an additional 10 foot height allowance, provided that they are set back from the face of the building by a minimum of 20 feet so as not to be visible by pedestrians.

• Architectural elements such as bell towers, spires, turrets, cupolas, chimneys, dormers, flag poles, etc. are limited to 15% of the roof area and may extend 10 feet above the height limitation, subject to Design Permit review.

e.2 Minimum Building Height: Not less than two stories, of which the first floor retail, restaurant and entertainment uses must have a minimum floor to floor height of 15 feet.

f. Design: All development must be in compliance with adopted Design Guidelines. Regulations which may be applicable to site design in this zone are set forth in General Site Design Standards Part 2, Chapter 24.12. and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

(Ord. 99-04 § 6, 1999; Ord. 88-41 § 6, 1988; Ord. 88-24 § 8, 1988; Ord. 85-05 § 1 (part), 1985).

f.1 The design of all new structures shall be based upon "Spanish Colonial Revival" architecture as well as Mission Revival and Mediterranean architecture as described in the Design Guidelines. "Fantasy Victorian" is encouraged for recreational and entertainment development.

f.2 Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, or appropriate materials that emulate the scale, proportions and look of wood, decorative iron and tile details or other features typical of Spanish Colonial Revival style.

f.3 Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.

f.4 Building walls shall be stucco and colored white, off-white or very light value, warm-toned hues. Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.

f.5 Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet. This parapet shall incorporate curvilinear decorative shapes and moldings.

f.6 Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance.

g. Siting:

g.1 Development shall be designed to create plazas and pedestrian spaces featuring amenities such as shade, benches, outdoor dining, fountains, gardens and performance spaces.

g.2 All store fronts, theater entries, hotel lobbies shall be located along streets, plazas, courtyards, or sidewalks in order to create visual interest to the pedestrian.

g.3 Building facades shall be articulated with wall offsets, recesses, openings, ornamentation, and appropriate colors and materials to add texture and detail to the streetscape.

h. Accessibility:

h.1 All retail uses must be directly accessible from a sidewalk, plaza, courtyard or other public open spaces.

h.2 Access must be aesthetically integrated within the development.

i. Setbacks: Development on this site should be designed to encourage and support activities which unify both sides of Beach Street. For that reason, development shall be required to build to the property line adjacent to Beach Street. Significant planter boxes and other narrow-scape concepts should be used to soften this edge but provide active pedestrian access.

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i. Parking:

i.1 Surface or structured parking may be constructed if the parking is visually screened and/or separated from the street by commercial development of at least 50 feet in depth.

i.2 Parking structure exteriors shall maintain the same high-quality architectural design and construction standards as all other commercial buildings.

• The large scale and mass of parking structures shall be alleviated through wall offsets, pilasters, arched opening and other distinctive design elements.

• Decorative elements such as cornices, balustrades, finish materials, colors and lighting shall be used to add interest and integrate the structures within the design character of the area.

i.3 Parking shall not be the dominant visual element of the site. Existing and/or expanded surface parking which is visible from the street or other areas exposed to public view must be screened and softened by landscaping, low screen wall or a combination of these elements.

i.4 Surface lots must be planted with trees to reduce heat and glare, that include at least 15% of the surface area to provide visual relief from broad expanses of paving. Shade trees shall be planted around the perimeter and within the lot.

i.5 Off-site parking may be permitted within this subdistrict if:

• the city establishes a parking district for the area, the district develops a suitable parking facility, and the development pays an in-lieu parking fee; or

• the development identifies and develops a suitable permanent parking facility;

or

• the development secures and provides evidence of a long-term lease from a suitable permanent parking facility.

k. Landscaping:

k.1 Interior courtyards and passages are encouraged and shall be planted with colorful, perennial and annual plant species. A combination of trees, shrubs and groundcovers shall be used to frame, soften and embellish the quality of the development, to screen undesirable views and to define development boundaries. Landscaping shall be maintained in an attractive condition.

k.2 Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas, and along the frontages of Beach Street and Riverside Avenue.

l. Transit: All development proposals within the RTC shall:

• discourage employee automotive use by instituting one or more of the following: carpooling requirements, transit subsidies, employee shuttle service, and/or

• provide a contribution and/or cost-sharing for shuttle and/or parking such as on the Depot site.

3. All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district.

24.10.624.1 FINDINGS REQUIRED.

In addition to required Use and Design Permit findings, any development permit must also meet the following findings. The proposed project:

1. Can be coordinated with existing and proposed development of the surrounding areas, and, if appropriate, particularly addressing the issue of transition to the adjacent RTA and RTB neighborhoods; and

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2. Shall provide the amenity level of the development, the quality of architecture, and the landscaping to meet the requirements listed above.

3. Shall be found to contribute to the overall economic health, vitality and general mix of uses in the beach area by providing diverse retail and merchandising for the area.

Section 8. Chapter 24.10, Part 7 of the Santa Cruz Municipal Code is hereby amended to add a new Section, Part 7C.1, as follows:

**Part 7C.1: R-T(C)/PER: SUBDISTRICT C - BEACH
COMMERCIAL/PERFORMANCE OVERLAY ZONE**

24.10.625.0 PURPOSE.

The purpose of the Beach Commercial Performance Overlay Zone is to identify areas of critical public interest in the Beach Commercial Area, and to provide regulations and standards necessary to achieve planning goals for development and/or redevelopment in a manner which protects neighborhood integrity while stimulating appropriate economic development.

The goal of RTC/PER is to promote the General Plan policy for the development of regional visitor serving uses in the Beach Area while accomplishing the directives of the adopted Beach and South of Laurel Comprehensive Area Plan. The Beach Commercial Performance Overlay zone establishes the requirement that a master plan for the entire 7.5 acre site developed under the Planned Development process before any new buildings are constructed on the site.

24.10.625.1 INTENT OF THE ZONE.

The RTC/PER is designed to accomplish the following:

(a) to incorporate a public process to ensure that development meets adopted public policy goals in terms of uses, design and public benefits;

(b) to permit a flexible development response to the local market as well as the regional visitor market, and to allow for the development of a variety of land uses and public activities to meet the needs and requirements of visitors, residents and workers;

(c) to provide a density of development which will be compatible with uses adjacent to the Beach Commercial Area;

(d) to encourage design which produces a desirable relationship between the development on this site and adjacent residential and commercial areas;

(e) to ensure conformance with the approved "Beach Commercial Design Guidelines" regarding facades, public spaces, location of store fronts, architectural variation, height, rooflines, and landscaping; and

(f) to allow change of use of existing buildings on the site that comply with the following permit requirements.

24.10.625.2 APPLICABILITY.

The RTC/PER overlay zone shall be applied to all the parcels that comprise the Main Beach Parking lot, which is bounded by Beach Street, Riverside Ave. Second and Cliff Streets and shall be subject to a community based planning process that is carried out to review the proposed uses, densities and design of proposed development within the Overlay district through a Planned Development process subject to the criteria and standards established by the Performance Overlay Zone. This process should take into account the proposed development's potential to meet the City's adopted economic development goals as well as neighborhood

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conservation goals for the area; its compliance with Local Coastal Zone policy and adopted design guidelines, and identify the benefits of the proposed project.

24.10.625.3 USE PERMIT REQUIREMENT.

1. The following uses are permitted subject to an Administrative Use Permit, a Design Permit and other requirements of the Municipal Code. The goal is to provide a continuity of activity and interest which will integrate existing commercial uses along Beach Street and the Boardwalk more fully. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes.)

a. Retail uses, including stores, shops, specialty shops, and neighborhood convenience retail, excluding department stores or discount malls up to a total of 125,000 square feet for the district. (230,250,290)

b. Eating establishments, not including drive-up or drive-in services, subject to alcohol regulations in Part 12 of Chapter 24. (280)

c. Off-site parking with fewer than five spaces (930).

d. Food and beverage stores (except liquor stores) (240).

e. Acting/art/music/dance/photography studios/schools (610) if such establishments are in compliance with the design guidelines and active people-orient uses are located adjacent to the street.

f. Administrative offices ancillary to a permitted use, if:

- offices in new structures are screened on street frontage by retail/recreational, and/or active people-oriented uses of at least 25 feet in depth, or
- offices in existing structures do not create a significant break in pedestrian activity and storefront continuity along the street, and are landscaped according to the Design Guidelines.

g. Indoor recreational uses and sports and health clubs, provided that the storefront adjacent to the street is designed in compliance with the guidelines, and active people oriented uses are located adjacent to the street. (720)

h. Temporary structures and uses.

2. The following uses are subject to approval of a Special Use Permit, a Design Permit and other requirements of the Municipal Code:

a. Public performance space, art galleries and/or museums which are open to the public, (600,610)

b. Lodging, hotel development provided it incorporates specialty boutiques and/or restaurant(s) on the ground floor, and does not create a significant break in pedestrian activity and storefront continuity along the street, (300).

c. Off-site public/private parking facilities, five or more spaces (930), so long as the total width of access to the facility is not more than 50 feet in width on any one street frontage, provided the parking is visually screened and separated from the street, and that access is designed in a manner not to disrupt retail continuity along Beach Street.

d. Residential development, located above the first floor of commercial uses, not to exceed 30 dwelling units/acre, if it includes a mixture of unit types (e.g. variety of unit sizes) and if 15% of the total number of units are affordable in accordance with Chapter 24.16 of the zoning regulations.

e. Theater and cinemas; exhibit space and libraries, auditoriums and conference space, providing significant pedestrian activity and storefront continuity is provided along the streets, (550g,570b,570c,600,620)

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f. Live performing arts theaters or nightclubs, establishments providing entertainment or permitting dancing and establishments serving alcoholic beverages provided that the storefront is designed in compliance with design guidelines and to include active people-oriented activities of visual interest to the pedestrian and subject to acoustical studies indicating that such uses can achieve the City's existing noise standards. (630)

24.10.625.4 USE DETERMINATION.

Any other use or service establishment determined by the zoning administrator to be of the same general character as the foregoing uses, and which will not impair the present or potential use of adjacent properties, may be permitted. A use permit shall be required and processed pursuant to Part 1, Chapter 24.08, Use Permits, of this title. Ord. 85-05 § 1 (part), 1985).

24.10.625.5 DISTRICT REGULATIONS.

These regulations apply to all development within the RTC/PER overlay district.

1. General.

a. Maximum allowable development of uses other than structured parking within the RTC/PER Overlay District may not exceed 125,000 square feet.

b. Maximum lot coverage by building development, exclusive of facilities meeting parking requirements, may not exceed 50% of lot area.

c. Minimum Public Use and/or Open Performance Space (percent of lot area): not less than 10% of the gross site area. Publicly accessible space in courtyards and plazas may be included in this calculation.

2. Other Requirements/Standards.

a. Height:

a.1 Maximum Building Height: Not to exceed 36 feet, unless twenty-five percent (25%) of the net lot area is dedicated public use and/or public performance space, then the maximum building height may not exceed 48 feet. Uninhabitable mechanical penthouses shall be limited to 10% of the roof area and will be permitted an additional 10 foot height allowance, provided that they are set back from the face of the building by a minimum of 15 feet so as not to be visible by pedestrians.

♦ Architectural elements such as bell towers, spires, turrets, cupolas, chimneys, dormers, flag poles, etc. are limited to 15% of the roof area and may extend 10 feet above the height limitation, subject to Design Permit review.

♦ Development eligible for additional height must step back a minimum of 15 feet from the face of the base height of 36 feet except for architectural elements at the street intersections, so as to maintain the visual impression of a three story façade on the street.

a.2 Minimum Building Height: Not less than two stories, of which the first floor retail, restaurant and entertainment uses must have a minimum floor to floor height of 15 feet.

b. Design: All development must be in compliance with adopted Design Guidelines. Regulations which may be applicable to site design in this zone are set forth in General Site Design Standards Part 2, Chapter 24.12. and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

(Ord. 99-04 § 6, 1999; Ord. 88-41 § 6, 1988; Ord. 88-24 § 8, 1988; Ord. 85-05 § 1 (part), 1985).

b.1. The design of all new structures shall be based upon "Spanish Colonial Revival"

architecture as well as Mission Revival and Mediterranean architecture as described in the Design Guidelines. "Fantasy Victorian" is encouraged for recreational and entertainment development.

b.2. Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, or appropriate materials that emulate the scale, proportions and look of wood, decorative iron and tile details or other features typical of Spanish Colonial Revival style.

b.3 Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.

b.4. Building walls shall be stucco and colored white, off-white or very light value, warm-toned hues. Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.

b.5 Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet. This parapet shall incorporate curvilinear decorative shapes and moldings.

b.6. Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance.

c. Siting:

c.1 Development shall be designed to create plazas and pedestrian spaces featuring amenities such as shade, benches, outdoor dining, fountains, gardens and performance spaces.

c.2 All store fronts, theater entries, hotel lobbies shall be located along streets, plazas, courtyards, or sidewalks in order to create visual interest to the pedestrian.

c.3 Building facades shall be articulated with wall offsets, recesses, openings, ornamentation, and appropriate colors and materials to add texture and detail to the streetscape.

d. Accessibility:

d.1 All retail uses must be directly accessible from a sidewalk, plaza, courtyard or other public open spaces.

d.2 Access must be aesthetically integrated within the development.

e. Setbacks: Development on this site should be designed to encourage and support activities which unify both sides of Beach Street, integrating the Boardwalk/Casino with the Main Beach lot development. Development shall be required to build to the property lines adjacent to Beach Street and Riverside Avenue. Significant planter boxes and other narrowcape concepts should be used to soften this edge but provide active pedestrian access.

e.1 On Beach Street and Riverside Avenue front line setbacks, not to exceed 15 feet in depth, may be permitted if it is designed to incorporate active outdoor uses, such as outdoor dining or public seating and/or landscaping.

e.2 A 15 foot landscaping setback/buffer is required on Second Street.

e.3 A 10 foot landscaping setback/buffer is required along Cliff Street.

f. Parking:

f.1 On Beach Street, Cliff and Riverside Avenue, surface or structured parking may be constructed if the parking is visually screened and separated from the street by commercial development of at least 50 feet in depth.

f.2 On Second Street, surface or structured parking may be constructed if the parking is visually screened and separated from the street by a 15 foot landscape buffer in conjunction with, or in place of, commercial development.

f.3 Parking structure exteriors shall maintain the same high-quality architectural design and construction standards as all other commercial buildings.

- ◆ The large scale and mass of parking structures shall be alleviated through wall offsets, pilasters, arched opening and other distinctive design elements.
- ◆ Decorative elements such as cornices, balustrades, finish materials, colors and lighting shall be used to add interest and integrate the structures within the design character of the area.

f.4 Parking shall not be the dominant visual element of the site. Existing and/or expanded surface parking which is visible from the street or other areas exposed to public view must be screened and softened by landscaping, low screen wall, or a combination of these elements.

f.5 At least 15% of the surface area of surface parking lots must be planted with trees to reduce heat and glare and to provide visual relief from broad expanses of paving. Shade trees shall be planted around the perimeter and within the lot.

g. Landscaping:

g.1 Interior courtyards and passages are encouraged and shall be planted with colorful, perennial and annual plant species. A combination of trees, shrubs and groundcovers shall be used to frame, soften and embellish the quality of the development, to screen undesirable views and to define development boundaries. All landscaping shall be maintained in an attractive condition.

g.2 Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas, and along the frontages of Beach Street and Riverside Avenue.

h. Transit: All development proposals within the Performance Overlay Zone shall:

- ◆ discourage employee automotive use by instituting one or more of the following: carpooling requirements; transit subsidies; employee shuttle service; and
- ◆ provide a contribution and/or cost-sharing for shuttle and/or parking such as on the Depot site.

Within the requirements above, certain exceptions may be considered as part of the design review process in order to encourage quality development and the provision of public facilities and amenities only if all relevant permit findings can be made.

3. All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district.

24.10.625.6 PROCEDURE.

It is the intent of the RTC/PER overlay to encourage comprehensive master planning, following either the Planned Development or Area Plan process outlined in the City Zoning ordinance. For this reason, all new development in the RTC/PER overlay shall require a pre-application review by City staff prior to submittal of a formal application. The intent is to review proposed plans at the conceptual stage, thereby enabling City staff to provide feedback on recommended site design elements, compatibility with other existing and planned uses within the RTC/PER overlay, and to identify other land use issues which may arise. Following the preapplication process the applicant shall submit for either an Area Plan or a Planned Development subject to the design criteria listed in the RTC/PER overlay zone, to achieve the desire to accommodate a community based planning process identified in the Beach and South of Laurel Comprehensive Area Plan.

24.10.625.7 FINDINGS REQUIRED.

In addition to required Use and Design Permit findings any development permit, must also meet the following findings. The application:

1. Can be coordinated with existing and proposed development of the surrounding areas, and, if appropriate, particularly addressing the issue of transition to the adjacent RTD, and RTE neighborhoods.
2. Provides an amenity level of the development, the quality of architecture, and the landscaping required by the above standards.

Section 9. Chapter 24.10, Part 7 of the Santa Cruz Municipal Code is hereby amended to add a new Section, Part 7B.1, as follows:

Part 7B.1: R-T (B)/PER - MOTEL RESIDENTIAL PERFORMANCE OVERLAY

24.10.617.1 PURPOSE.

The purpose of the Motel Residential Performance Overlay district is to establish and control uses to ensure development which protects neighborhood integrity while supporting appropriate uses. The goal of the RTB/PER District is to limit the future development of hotel or motel rooms in the district, but to allow ancillary hotel support facilities as well as additional residential development.

24.10.617.2 USE PERMIT REQUIREMENT.

The overlay district allows all of the uses identified in the underlying RTB zone with the exception that new motel or hotel rooms will not be allowed.

The following uses are allowed in the overlay district subject to a Special Use Permit and a Design Permit, in compliance with the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

- a. Indoor and outdoor recreation facilities and other facilities related to existing hotel or motel facilities.
- b. Bed and Breakfast Inns.

24.10.617.3 DISTRICT REGULATIONS.

1. General

Provision	Dwelling Unit Type			
	<u>One-Family Detached</u>	<u>Duplex</u>	<u>3 or More Units</u>	<u>Other Uses</u>
a. <u>Maximum Height of Buildings</u>				
• <u>Number of Stories</u>	<u>2</u>	<u>2</u>	<u>3</u>	<u>3</u>
• <u>Principal buildings (feet)</u>	<u>30</u>	<u>30</u>	<u>36</u>	<u>36</u>
• <u>Accessory buildings (feet)</u>	<u>15</u>	<u>15</u>	<u>15</u>	<u>15</u>
b. <u>Minimum lot area (net) (sq. ft.)</u>	<u>5,000</u>	<u>5,000</u>	<u>8,000</u>	<u>8,000</u>
c. <u>Minimum lot area (net) per dwelling unit (sq. ft.)</u>	<u>5,000</u>	<u>2,500</u>	<u>1,450</u>	
d. <u>Minimum lot width (feet)</u>	<u>50</u>	<u>50</u>	<u>65</u>	<u>65</u>
e. <u>Minimum open space per dwelling unit (sq. ft.)</u>			<u>400</u>	

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2. Setback Requirements.

a. The minimum front yard setback shall be fifteen feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater; except that the front yard may be reduced to not less than six feet for a portion not to exceed fifty percent of the building frontage, providing that a total of ten square feet of front yard is provided for each lineal foot of total lot frontage. Such reduction of front yard depth shall not be permitted on a corner lot, within twelve feet of any side street lot line.

b. The minimum rear setback shall be ten feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

c. The minimum side yard setback shall be five feet for the first story and one foot of setback for each three feet of height, or portion thereof, of structure, whichever is greater, for the second story and above.

d. There shall be no side yard required for townhouses on interior lots except there shall be a minimum side yard setback at the interior end of a townhouse group of five feet or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

e. The minimum exterior side yard setback shall be eight feet, or one foot of setback for each three feet of height, or portion thereof, of a structure, whichever is greater.

f. The Minimum Distance Between Buildings on the Same Lot. Between main buildings, including accessory dwelling units, shall be ten feet, or one foot of setback for each two feet of height of the tallest building, or portion thereof, whichever is greater; between main buildings and one-story accessory buildings, six feet; between accessory buildings, six feet.

g. For any attached or detached garage or carport fronting on a front or exterior side property line, the setback shall be twenty feet from said property line.

3. Other Requirements. Other regulations which may be applicable to site design in this zone are set forth in General Site Design Standards, Part 2, Chapter 24.12. and the design guidelines of the Beach and South of Laurel Comprehensive Area Plan. In addition, development on sites located within the district which fronts on West Cliff Drive shall conform to design standards governing development on West Cliff.

4. Siting

a. Development shall be designed to create plazas and pedestrian spaces featuring amenities such as shade, benches, outdoor dining, fountains, gardens and performance spaces.

b. Building facades shall be articulated with wall offsets, recesses openings ornamentation, and appropriate colors and materials to add texture and detail to the streetscape.

c. Any third story element of residential or support development shall be stepped back from the two story element by at least 15 feet, from the property lines at the streets.

d. Buildings design shall be encouraged to include significant building modulation and roof form articulation as specified within the design guidelines.

e. All required front setback areas shall be landscaped in accordance with the standards or the design guidelines.

5. All new development adjacent to a "CON - Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district. (Ord. 88-41 § 3, 1988; Ord. 88-24 § 6, 1988; Ord. 85-05 § 1 (part), 1985).

Section 10. Chapter 24.10, Part 24(A) of the Santa Cruz Municipal Code is hereby amended as follows:

Part 24(A): CBD SUBDISTRICT E - LOWER PACIFIC AVENUE

24.10.2360 PURPOSE.

The purpose of the Lower Pacific Avenue Subdistrict Zone is to encourage the development/redevelopment of the Lower Pacific corridor between Laurel and Front Streets as an important linkage between the Downtown and the Beach. Its intent is to extend the commercial and mixed use developments of the Downtown along Lower Pacific Avenue in a manner that is compatible with and linked to the regional tourist serving uses of the Beach.

24.10.2361 PRINCIPAL PERMITTED USES.

1. The following uses are allowed outright in the Lower Pacific Avenue Subdistrict, subject to a Design Permit and other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's Land Use Codes. Further refinement of uses within these categories can be found in the Land Use Codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Acting/art/music/dance school and studios (610);
- b. Apparel and accessory stores (250);
- c. Eating and drinking establishments (excepts bars, fast food), subject to alcohol regulations in Part 12 of Chapter 24.12.(280);
- d. Educational facilities (public/private) (510);
- e. ~~Financial, insurance, real estate offices (420);~~
- f. ~~Financial services (320);~~
- g. e. Food and beverage stores (except liquor and convenience stores) (240);
- h. f. General retail merchandise (drug and department stores) (230); not exceeding 16,000 square feet per individual store;
- i. g. Home furnishing stores (270);
- j. h. Lodging (300);
- k. ~~Medical/health offices (except veterinarians and ambulance services)(410);~~
- l. i. Multifamily residential units or mixed use projects residential and commercial developments when multiple family units are located above the first floor of commercial uses, subject to the minimum land area (net) per dwelling unit of the R-M District (840);
- m. j. Museums and art galleries (600);
- n. ~~Professional offices (400);~~
- o. ~~Professional/personal services (except contractors yards and mortuaries)(310);~~
- p. k. Repair, alterations, and maintenance services for household items (except boat repair) (340);
- q. l. Small community care residential facilities;
- r. m. Small preschool/childcare (12 or fewer) (510A);
- s. n. Specialty retail supply stores (290);
- t. o. Theaters (620);
- u. p. Video rental (360B), (Ord. 96-06 § 2 (part),1996).

24.10.2365 ACCESSORY USES.

Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, Accessory Buildings and Section 24.10.730. (Ord. 96-06 § 2 (part),1996).

24.10.2370 USE PERMIT REQUIREMENT.

1. The following uses are subject to approval of an Administrative Use Permit in the Lower Pacific Avenue Subdistrict and possibly other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's Land Use Codes. Further refinement of uses within these categories can be found in the Land Use Codes, but they are not intended to be an exhaustive list of potential uses.)

a. Auto supply stores (260C);
b. Bakery, micro-brewery (subject to alcohol regulations in Part 12 of Chapter 24.12), handicrafts or similar light manufacturing and assembly uses associated with retail sales, if floor area is less than 7,000 sq. ft. square feet and retail sale or service area occupies at least 30% of the floor area:

c. Brew pubs, subject to alcohol regulations in part 12 of Chapter 24.12;
d. Churches (500);
e. Communication and information services (550);
f. Community organizations, associations, clubs, and meeting halls (570);
g. Developed parks (710);
h. Fast-food restaurants or drive-in eating facilities subject to performance standards in Section 24.14.290 and subject to alcohol regulations in Part 12 of Chapter 24.12 and Section 24.14.290 (280H);

i. Foster family homes;
j. Government and public agencies (530);
k. Off-site public/private parking facilities (930);
l. Recycling collection facilities;
~~m. Single room occupancy (SRO) housing fifteen units or fewer (860);~~
~~n. m.~~ Sports, recreation and entertainment facilities subject to alcohol regulations in Part 12 of Chapter 24.12 (720);

~~o. n.~~ Temporary structures and uses;
~~p. o.~~ Utilities and resources (540);
~~q. p.~~ Veterinarians (410A).
q. Financial, insurance, real estate offices (420);
r. Financial services (320);
s. Medical/health offices (except veterinarians and ambulance services)(410);
t. Professional offices (400);
u. Professional/personal services (except contractors yards and mortuaries)(310).

2. The following uses are subject to approval of a Special Use Permit and Design Permit in the Lower Pacific Avenue Subdistrict and possibly other requirements of the Municipal Code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's Land Use Codes. Further refinement of uses within these categories can be found in the Land Use Codes, but they are not intended to be an exhaustive list of potential uses.)

a. Bar and cocktail lounges, subject to alcohol regulations in Part 12 of Chapter 24.12 (280C);

~~b.~~ Boat repairs (340D);
e b. Community care facilities;

- d c. Community care residential facilities;
 - e d. Contractor/building (310E);
 - f e. Convenience/liquor stores, subject to alcohol regulations of Part 12 of Chapter 24.12 (240B);
 - g f. Night clubs (amplified live entertainment), subject to alcohol regulations of Part 12 of Chapter 24.12 (630);
 - h g. Single-room occupancy (SRO) ~~housing sixteen units or more~~ (860);
 - h. General retail merchandise (drug and department stores) exceeding 16,000 square feet (230).
- (Ord. 96-06 § 2 (part), 1996)

24.10.2375 USE DETERMINATION.

Any other use or service establishment determined by the Zoning Administrator to be of the same general character as the established uses and which will not impair the present or potential use of adjacent properties may be permitted. An Administrative Use Permit will be required.

(Ord. 96-06 § 2 (part), 1996).

24.10.2380 LOWER PACIFIC AVENUE SUBDISTRICT REGULATIONS.

1. Height and Stepback Requirements. The maximum height of development throughout this subarea shall be ~~thirty-five~~ 35 feet or three stories with a minimum height of two stories for buildings along Pacific Avenue. Provision for sloping roofs and mechanical penthouses will be allowed to a maximum height of ~~forty~~ 40 feet, provided that penthouses are stepped back at least ~~twenty~~ 20 feet out of sight from pedestrian view. The second story shall be at least 50% of the first floor and shall be located toward the street frontage.

2. Build to Lines and Setbacks. New development along Pacific Avenue shall be set back from the property line to create a side-walk depth of at least ~~twelve~~ 12 feet. New development along Front Street shall be set back from the property line to create a sidewalk depth of at least ~~ten~~ 10 feet. Additional setbacks are permitted to provide landscaped or paved extensions of the side-walk area, gardens, out-door seating, or cafes. No sideyard setbacks are required. Building design at the corners of Pacific Avenue and Front Street should include strong architectural elements (such as a tower) at the corners to emphasize the entrance to Pacific Avenue.

3. Parking. Parking requirements set forth in Part 3 of Chapter 24.12 of the Zoning Ordinance shall apply, with the exception noted in subsection "c" below.

a. Surface parking lots within the South of Laurel subarea shall be well landscaped, and parking shall be visibly screened from the public sidewalk with low walls, planters, or hedges, and shall comply with other landscaping requirements of the zoning regulations.

b. Surface parking should be located to the rear of buildings or facing ~~collector~~ side streets. Driveways should be consolidated as much as possible, and cooperative easements should be formed to allow parking access at the rear of buildings.

c. Parking Waiver Provision. Undeveloped lots, 40 feet in width or less located along Lower Pacific Avenue, may meet established parking requirements off-site, based on specific building permit review and approval, identification of receiving parking resource, and payment of a yearly parking deficiency fee. The fee structure may reflect the deficiency fees established for the adjacent CBD.

4. All new development adjacent to a "CON - Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites

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abutting overlay district boundaries, to ensure compatibility with the established district."

~~Service Access. Trash storage areas shall be internal to the building or completely enclosed and screened from view. To the maximum extent practicable, no trash or loading areas shall be permitted adjacent to Pacific Avenue.~~

24.10.2385 LOWER PACIFIC AVENUE DESIGN GUIDELINES.

1. **Store Front Treatment.** The ground-level treatment of buildings and parking structures within the Lower Pacific Avenue subarea should generally comply with the guidelines for the Pacific Avenue retail subarea listed on pages 41-45 of the Downtown Recovery Plan, in terms of: storefront access, transparency, and variation; and the use of landscaping, awnings, and canopies. However, it is recognized that the Lower Pacific Avenue subarea has a more informal character than Pacific Avenues, and as such, more variation of ground-level treatment is envisioned and encouraged. The use of porches and terraced gardens as an intermediate space between the ground floor use and the sidewalk is permitted, as long as the finished floor elevation of the ground floor use is no more than ~~four~~ 4 feet above or below the sidewalk level and accessibility requirements are met.

2. **Ground Level Residential.** Within the Lower Pacific Avenue subarea, ground level residential uses are permitted in all areas and floors. Direct access between individual units and the street is strongly encouraged through the uses of porches and front "stoops."

3. **Upper Level Facade Treatment.** The treatment of upper level facades should generally comply with the guidelines of the Pacific Avenue retail district in terms of building rhythm, corner treatment, windows, roof treatment, building materials, colors, and planting. In recognition, however, of the area's village character, several special conditions are noted:

a. **Architectural Elements.** The use of architectural elements that promote the village character of the street is encouraged. Such elements could include, sloping roofs, chimneys, bay windows, dormers, recessed loggias, balconies, and porches.

b. **Articulations.** Facades should be highly articulated and varied; the introduction of moldings and trims, and changes in horizontal and vertical planes are strongly encouraged to create visual interest and variation in light and shadow. Residential development should be highly articulated and expressive of the individual units within the complex.

c. **Wood.** Building material can be more diverse and residential in character than those recommended for the Pacific Avenue District. The use of wood as a siding material is encouraged.

d. **Flowers and Planting.** The use of significant planters, trellises, and topiary treatment of buildings is encouraged to further enliven the area and to promote its unique village qualities. (Ord. 96-06 § 2 (part), 1996).

4. Parcels that are undeveloped at the time of the ordinance shall be well maintained; including landscaping and/or appropriate screening subject to the approval of the Director of Planning at the street frontages, within six months of adoption of this title.

Section 11. Chapter 24.10, Part 12 of the Santa Cruz Municipal Code is hereby amended as follows:

PART 12: C-B BEACH COMMERCIAL DISTRICT

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24.10.1100 Purpose.

To provide for commercial uses which are primarily coastal-dependent in nature and which serve tourists and visitors to the Santa Cruz coastal recreational areas. Also, to provide commodities and services to residents of such areas. The C-B District shall be applied only in areas designated in the General Plan and the Local Coastal Program. This section of the Zoning Ordinance is also part of the Local Coastal Implementation Plan.

(Ord. 94-33 § 37, 1994; Ord. 93-21 § 7, 1993; Ord. 85-05 § 1 (part), 1985).

24.10.1110 Principal Permitted Uses.

1. The following uses are allowed outright, subject to other applicable requirements of the municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Subcategories of uses within these use categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

- a. Acting/art/music/dance schools and studios (610);
- b. Apparel and accessory stores (250);
- c. Eating and drinking establishments (except fast-food restaurants), subject to alcohol regulations in Part 12 of Chapter 24.12 (280);
- d. Financial, insurance, real estate offices above first floor (420);
- e. Food and beverage stores (except convenience/liquor stores) (240);
- f. General merchandise (drug and department stores) (230);
- g. Handicraft shops and workshops;
- h. Lodging (300);
- i. Marine facilities (560E);
- j. Mechanical contrivances for amusement purposes, such as Ferris wheels, and roller coasters, south and east of Beach Street only;
- k. One or two multiple-family units when located above the first floor with no additional parking required (830);
- l. Museums and art galleries (600);
- m. Off-site public/private parking facilities, five or fewer spaces (930);
- n. Personal/professional services (except contractors yards and mortuaries) (310);
- o. Professional offices above first floor (400);
- p. Small family daycare facilities, in single-family home or duplex;
- q. Specialty retail supply stores (290); except thrift stores (290m);
- r. Sports and recreation facilities, subject to alcohol regulations in Part 12 of Chapter 24.12 (720);
- s. Theaters (620);
- t. Video rental (650).

(Ord. 97-17 § 5, 1997; Ord. 96-39 § 18, 1996; Ord. 95-04 § 6, 1995; Ord. 93-21 § 7, 1993; Ord. 88-26 § 10, 1988; Ord. 85-05 § 1 (part), 1985).

24.10.1120 Accessory Uses.

Other uses and buildings customarily appurtenant to a permitted use, subject to the provisions of Section 24.12.140, Accessory buildings and Section 24.10.1130.

(Ord. 93-21 § 7, 1993; Ord. 88-60 § 2, 1988; Ord. 85-05 § 1 (part), 1985).

24.10.1130 Use Permit Requirement.

1. The following uses require an administrative use permit and are subject to other applicable requirements of the municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Subcategories of uses within these use categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

a. Accessory buildings containing plumbing fixtures subject to the provisions of Section 24.12.140;

b. Community organizations, associations, clubs and meeting halls (570);

c. Convenience store, subject to alcohol regulations in Part 12 of Chapter 24.12 (240B);

d. Churches (500);

e. Educational facilities (public/private) (510);

f. Financial services (320);

g. Fish/seafood/wholesale (200F);

h. Government and public agencies (530);

i. Home furnishings (270B);

j. Liquor stores, subject to alcohol regulations in Part 12 of Chapter 24.12;

k. Mixed residential and commercial development involving permitted or administrative uses on the ground floor and from three to nine dwellings above the first floor, subject to the minimum land area (net) per dwelling unit of the R-M District (830);

l. Multifamily, three to nine units, subject to the minimum land area (net) per dwelling unit of the R-M District (830);

m. Parks and open spaces (700);

n. Repairs, alterations, maintenance services for household items (340);

o. Single-room occupancy (SRO) housing, fifteen units or fewer (860);

~~p. Small community care residential facilities;~~

~~q-p. Temporary structures and uses;~~

~~r-q. Thrift stores (290m).~~

2. The following uses require a special use permit and are subject to other applicable requirements of the municipal code. (Numerical references at the end of these categories reflect the general use classifications listed in the city's land use codes. Subcategories of uses within these use categories can be found in the land use codes, but they are not intended to be an exhaustive list of potential uses.)

a. Bars/taverns, subject to alcohol regulations in Part 12 of Chapter 24.12;

b. Large family daycare facilities;

~~b-c.~~ Fast-food restaurants or drive-in eating facilities subject to performance standards in Section 24.12.290 and subject to alcohol regulations in Part 12 of Chapter 24.12 (280H);

~~e-d.~~ Group quarters (850);

~~d-e.~~ Mixed residential and commercial developments with ten or more dwellings, either above the first floor or on the same parcel, subject to the minimum land area (net) per dwelling unit of the R-M District (840);

~~e-f.~~ Multifamily, ten+ units subject to the minimum land area (net) per dwelling unit of the R-M District (840);

~~f-g.~~ Nightclubs/dance halls (live amplified music), subject to alcohol regulations in Part 12 of Chapter 24.12 (630);

~~g-h.~~ Off-site public/private parking facilities, five or more spaces (930);

h.i. Refreshment stands and vehicles, when located on private property, in locations clearly incidental and adjacent to beach, park, campgrounds, or other major recreational and tourist facilities or activities;

h.j. Single-room occupancy housing (SRO), sixteen units or more (860).
 (Ord. 97-17 § 6, 1997; Ord. 96-39 § 19, 1996; Ord. 95-04 § 7, 1995; Ord. 93-21 § 7, 1993; Ord. 89-21 § 5, 1989; Ord. 88-60 § 23, 1988; Ord. 88-26 § 11, 1988; Ord. 87-22 § 6, 1987; Ord. 86-12 § 3, 1986; Ord. 85-05 § 1 (part), 1985).

24.10.1140 Use Determination.

Any other use or service establishment determined by the zoning administrator to be of the same general character as the foregoing principal permitted uses, and which will not impair the present or potential use of adjacent properties may be permitted. A use permit shall be required and processed pursuant to Part 1, Chapter 24.08 of this title.
 (Ord. 93-21 § 7, 1993; Ord. 85-05 § 1 (part), 1985).

24.10.1150 District Regulations.

1. General.

Provisions	Requirement
a. Height of Building – Maximum	
• Commercial and Mixed Use (stories and feet)	3 & 40
• Accessory	1 & 20
b. Lot area minimum (net) (square feet)	
• Commercial or residential	5,000
• Mixed Use	8,000
c. Setbacks	
• Front yard	0 ²
• Rear yard	0 ¹
Sideyard	
• Interior	0 ¹
• Exterior	0 ⁽¹⁾⁽²⁾
d. Open (Residential Only) Space Per Unit	
• Private (sq. ft.)	100
• Common (sq. ft.) and easily accessible to residential units	150
e. Distance between buildings on same lot	10

1. Except where abutting an R- District, then not less than the minimum yard required for the adjacent yard in the R- District.
2. Except where special street setback requirements for designated streets apply, then the setback shall not be less than the minimum setback listed in Section 24.12.115 for affected streets.

2. Additional Setback Requirement. In any C-B District directly across a street or thoroughfare, but not including a freeway, from an R- District, parking and loading facilities shall be at least ten feet distant from the property line, and buildings and structures at least twenty feet from the street; said setback space shall be permanently landscaped.

3. Other Requirements.

a. All uses shall be conducted wholly within a completely enclosed building, except for parking facilities, or other outdoor uses when appropriately screened and as approved by the zoning administrator.

~~b. Other regulations which may be applicable to site design in this zone are set forth in General Site Design Standards, Part 2, Chapter 24.12. (Ord. 93-21 § 7, 1993; Ord. 85-05 § 1 (part), 1985).~~

4. The following regulations are applicable to site design in the CB Zone north of Beach Street as set forth in General Site Design Standards, Part 2, Chapter 24.12, and the following:

a. Height:

a.1 Maximum Building Height: Maximum building height shall be 36 feet. Uninhabitable mechanical penthouses shall be limited to 10% of the roof area and will be permitted an additional 10 foot height allowance, provided that they are set back from the face of the building by a minimum of 20 feet so as not to be visible by pedestrians.

- Architectural elements such as bell towers, spires, turrets, cupolas, chimneys, dormers, flag poles, etc. are limited to 15% of the roof area and may extend 10 feet above the height limitation, subject to Design Permit review.

a.2 Minimum Building Height: Not less than two stories, of which the first floor retail, restaurant and entertainment uses must have a minimum floor to floor height of 15 feet.

b. Design: All development must be in compliance with adopted Design Guidelines. Regulations which may be applicable to site design in this zone are set forth in General Site Design Standards Part 2, Chapter 24.12. and the Design Guidelines of the Beach and South of Laurel Comprehensive Area Plan.

(Ord. 99-04 § 6, 1999; Ord. 88-41 § 6, 1988; Ord. 88-24 § 8, 1988; Ord. 85-05 § 1 (part), 1985).

b.1 The design of all new structures shall be based upon "Spanish Colonial Revival" architecture as well as Mission Revival and Mediterranean architecture as described in the Design Guidelines. "Fantasy Victorian" is encouraged for recreational and entertainment development.

b.2 Buildings shall be designed with stucco walls, courtyards, arches, towers, balconies, wood doors and windows, or appropriate materials that emulate the scale, proportions and look of wood, decorative iron and tile details or other features typical of Spanish Colonial Revival style.

b.3 Building forms shall suggest thick masonry reminiscent of Spanish Colonial Revival architecture and incorporate features such as recessed doors and windows.

b.4 Building walls shall be stucco and colored white, off-white or very light value, warm-toned hues. Multiple color combinations may be used provided they are subtle and consist of a limited number of colors. Variations in shade or tone can be used to articulate architectural features.

b.5 Roofs shall be hipped terra cotta tile roofs or flat roofs completely surrounded by a parapet. This parapet shall incorporate curvilinear decorative shapes and moldings.

b.6 Flat roofed buildings shall incorporate porches, window overhangs, trellises, wall and opening articulation or other features to avoid a bare box appearance.

c. Siting:

c.1 Development shall be designed to create plazas and pedestrian spaces featuring amenities such as shade, benches, outdoor dining, fountains, gardens and performance spaces.

c.2 All store fronts, theater entries, hotel lobbies shall be located along streets, plazas, courtyards, or sidewalks in order to create visual interest to the pedestrian.

c.3 Building facades shall be articulated with wall offsets, recesses, openings, ornamentation, and appropriate colors and materials to add texture and detail to the streetscape.

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d. Accessibility:

d.1 All retail uses must be directly accessible from a sidewalk, plaza, courtyard or other public open spaces.

d.2 Access must be aesthetically integrated within the development.

e. Setbacks: Development on this site should be designed to encourage and support activities which unify both sides of Beach Street. For that reason, development shall be required to build to the property line adjacent to Beach Street. Significant planter boxes and other narrowscape concepts should be used to soften this edge but provide active pedestrian access.

f. Parking:

f.1 Surface or structured parking may be constructed if the parking is visually screened and/or separated from the street by commercial development of at least 50 feet in depth.

f.2 Parking structure exteriors shall maintain the same high-quality architectural design and construction standards as all other commercial buildings.

- The large scale and mass of parking structures shall be alleviated through wall offsets, pilasters, arched opening and other distinctive design elements.

- Decorative elements such as cornices, balustrades, finish materials, colors and lighting shall be used to add interest and integrate the structures within the design character of the area.

f.3 Parking shall not be the dominant visual element of the site. Existing and/or expanded surface parking which is visible from the street or other areas exposed to public view must be screened and softened by landscaping, low screen wall or a combination of these elements.

f.4 Surface lots must be planted with trees to reduce heat and glare, that include at least 15% of the surface area to provide visual relief from broad expanses of paving. Shade trees shall be planted around the perimeter and within the lot.

f.5 Off-site parking may be permitted within this subdistrict if:

- the city establishes a parking district for the area, the district develops a suitable parking facility, and the development pays an in-lieu parking fee; or

- the development identifies and develops a suitable permanent parking facility;

or

- the development secures and provides evidence of a long-term lease from a suitable permanent parking facility.

g. Landscaping:

g.1 Interior courtyards and passages are encouraged and shall be planted with colorful, perennial and annual plant species. A combination of trees, shrubs and groundcovers shall be used to frame, soften and embellish the quality of the development, to screen undesirable views and to define development boundaries. All landscaping shall be maintained in an attractive condition.

g.2 Permanent containers for flowering plants, such as window boxes and planters, are encouraged for use in limited space areas, at entries and in courtyards and plazas, and along the frontages of Beach Street and Riverside Avenue.

h. Transit: All development proposals within the RTC shall:

- discourage employee automotive use by instituting one or more of the following: carpooling requirements, transit subsidies, employee shuttle service, and/or

- provide a contribution and/or cost-sharing for shuttle and/or parking such as on the Depot site.

5. All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district.

24.10.1160 Findings Required.

In addition to required Use and Design Permit findings, any development permit must also meet the following findings. The proposed project:

1. Can be coordinated with existing and proposed development of the surrounding areas, and, if appropriate, particularly addressing the issue of transition to the adjacent RTC and RTE neighborhoods; and
2. Shall provide the amenity level of the development, the quality of architecture, and the landscaping to meet the requirements listed above.
3. Shall be found to contribute to the overall economic health, vitality and general mix of uses in the beach area by providing diverse retail and merchandising for the area.

Section 12. Chapter 24.10, Sections 24.10.450(4), 24.10.550(4), and 24.10.1790(3) of the Santa Cruz Municipal Code are hereby amended as follows:

Chapter 24.10 Zoning District regulations for districts adjacent to the new "CON-Neighborhood Conservation" zones shall be amended to acknowledge the need to comply with the "CON" district standards for projects within 30 feet of that zone, by inserting the following text at the end of the "DISTRICT REGULATIONS." Section for all Districts described above and for Part 5: "Section 24.10.450(4) – RL-Multiple Residence Low Density," Part 6: "Section 24.10.550(4) – RM-Multiple Residence Medium Density," and Part 18b: "Section 24.10.1790(3) – PF-Public Facility."

"All new development adjacent to a "CON – Neighborhood Conservation District" overlay zone shall comply with Section 24.10.4060 standards for new construction on sites abutting overlay district boundaries, to ensure compatibility with the established district."

Section 12. The official Zoning Map of the Santa Cruz Municipal Code is hereby amended as shown on Attachments 1 and 2:

Section 13. This ordinance shall be in force and take effect thirty (30) days after its final adoption.

ZONING SUMMARY RESIDENTIAL ZONING

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Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>RTD: Beach Residential – Beach Flats Conservation Neighborhood [REVISED]</p> <p><u>Issues</u></p> <ul style="list-style-type: none"> • Need to preserve small scale character threatened by large development –up to 55 DU/A. 	<p>Conserve neighborhood: to ensure that new development is compatible with existing neighborhood; reduce density; limit large new development; require guidelines</p>	<ul style="list-style-type: none"> • allows up to 55 DU/A • no landscaping required • no bldg. articulation required 	<ul style="list-style-type: none"> • require consistency with Conservation Overlay requirements • reduce to compatible density - 27 DU/A • require design permit • bldg height setbacks • require landscaping • require parking and siting standards • need valid occupancy permit to sell • requires demolition control • City 1st right in tax sale 	<ul style="list-style-type: none"> • No commercial uses allowed • Restrict building envelope • Require design and scale conformance to standards • Need occupancy permit to sell to ensure health and safety standards of housing stock.
<p>RTE: Beach Flats [NEW ZONE]</p> <p><u>Issues</u></p> <ul style="list-style-type: none"> • Deteriorated housing stock • Need for new units 	<p>Encourage construction of new affordable & market rate housing</p> <p style="text-align: center;"><i>Certified Under STC-MAJ-1-01 (Part A)</i></p>	<p>Replace RTD which allows up to 55 DU/A</p> <ul style="list-style-type: none"> - no landscaping required - no bldg. articulation required 	<ul style="list-style-type: none"> • Reduce density to range of 30-36 DU/A • Establish 36' height • 1 pking space/unit – may be off-site • require design compliance • require bldg. articulation • require landscaping 	<ul style="list-style-type: none"> • Incentives provided: <ul style="list-style-type: none"> - 30-36 DU/A - allows off-site parking to be provided within 500 feet. • existing parking – required to phase in landscaping • flexible side yard setbacks • landscaping required • blding articulation • blding style – Sp. Colonial

ZONING SUMMARY

RESIDENTIAL ZONING (continued)

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Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>CON: Conservation Neighborhood Parking Overlay [NEW ZONE]</p> <p><u>Issues</u></p> <ul style="list-style-type: none"> -Neighborhoods at risk - need to preserve character & uniqueness - deteriorated housing stock -Need to ensure compatible development on boundaries of residential neighborhood 	<p>Conserve neighborhood: -encourage multifamily rehab; - restrict commercial, - promote maintenance of existing residential & homeownership</p> <p style="text-align: center;"><i>certified under STC-MAJ-1-01 (Part A)</i></p>	<ul style="list-style-type: none"> • no conservation standards - only standards of underlying zones 	<ul style="list-style-type: none"> • require compatibility with existing structures • require buffering between uses • all residential uses of underlying zone permitted • frt yd setbk needs to be compatible • pking within or behind bldg. • 3+units pking landscaping plan reqd. • housing condition status report within 24 months • becomes targeted neighborhd of City's housing stock improvement programs • regulations for new construction on sites within 30' abutting overlay district 	<ul style="list-style-type: none"> • inventory of all boarded up/vacant housing in 24 months • notice to owners to comply • city rt. to purchase tax delinquent sales • no demolition permit without assessment by Planning Dept. • eligible for City's 1st time homebuyer/homeownership program • occupancy permit required to sell or transfer property

ZONING SUMMARY
RESIDENTIAL ZONING (continued)

Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>RTA: Beach Hill High (Medium) Density [REVISED]</p> <p><u>Issues</u> -high density incompatible with existing residentl. -due to density, pressure for "tear downs" to replace with larger structure -maximum height = 55'</p>	<p>Existing goal "high density residential": promote neighborhood characteristics.</p> <p>Proposed goal: medium density residential: promote unique neighborhood characteristics</p>	<ul style="list-style-type: none"> allows up to 55 DU/A allows special density bonus some inappropriate uses single family & duplex units: up to 35' height 3+ units : up to 55' height 	<ul style="list-style-type: none"> General Plan states area should be medium density = 30 DU/A: recommend reduction to medium density delete density bonus provision delete inappropriate uses require conformance with design guidelines & historic requirements reduced permitted height to 36', with flexibility for Victorians & their architectural elements 	<ul style="list-style-type: none"> reduced density to be consistent with General Plan. Reduced building height Deleted inappropriate uses such as family animal farms. Require conformance with Design Guidelines
<p>RTB: Beach Hill Motel-Residential [REVISED]</p> <p><u>Issues</u> -Need to ensure compatibility between resid. & motel uses. -zoning too dense up to 55 DU/A -allows permanent uses - motel/hotel -maximum height = 55'</p>	<p>Existing goal: compatible mix of motel and "high" density residential uses.</p> <p>Proposed goal: protect medium density residential</p>	<ul style="list-style-type: none"> allows up to 55 DU/A allows special density bonus allows permanent & transient uses in motel, hotel some inappropriate uses 1-2 units: up to 35' height 3+ units: up to 55' height 	<ul style="list-style-type: none"> General Plan: medium density = 30 DU/A: recommend reduction to medium density. delete inconsistent definitions for motel, hotel and b&bs delete inappropriate uses require conformance with design guidelines & historic requirements reduced permitted height to 36' 	<ul style="list-style-type: none"> reduced density to be consistent with General Plan reduced building height require conformance with design guidelines require annual licensing renewal for all lodging to health/safety & fire code compliance

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ZONING SUMMARY RESIDENTIAL ZONING (continued)

Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>RH/MU: High Density Residential/ Mixed Use Commercial [REZONE From RM/MU]</p> <p><u>Issues</u> - General Plan designates Riverbend for Regional Visitor Serving & Sycamore housing site for High Density residential</p> <p>- B/SOLA states need high density housing in South of Laurel – not at Sycamore but at Riverbend</p>	<p>Existing Goal: to promote housing at density of 20.1 to 30 DU/A</p> <p>Proposed Goal: to remove high density from borders of SOLA conservation neighborhoods & place adjacent to River to increase residential rental/ ownership supply</p>	<p>Existing zoning, RM/MU</p> <ul style="list-style-type: none"> • allows 20 - 30 DU/A • 3+ units: up to 35' height • allows commercial uses 	<ul style="list-style-type: none"> • eliminate country clubs, golf courses, riding, and swimming and tennis clubs. • Delete duplexes & smaller units • Maintain commercial uses to encourage live/work • Add market rate housing to encourage live/work • 55 DU/A • increase height of 3+ units from 44' to 48' • require use permit and design permit • require design guidelines 	<ul style="list-style-type: none"> • preserves General Plan desire create “high” density residential in SOLA – but removes it from low scale neighborhoods • encourages creation of live/work environment in SOLA

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ZONING SUMMARY COMMERCIAL ZONING

Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p><i>Tourist Residential</i></p> <p>RTC: Beach Commercial [REVISED]</p> <p>Issues</p> <ul style="list-style-type: none"> - no development standards/design controls for principal Regional Visitor serving uses 	<p>Existing Goal: regional & tourist commercial use mixed with residential</p> <p>Proposed Goal: establish standards to ensure harmonious balance</p>	<ul style="list-style-type: none"> • no design standards • no bldg articulation or architectural style required • no landscaping • no allowance of public performance space, art galleries and/or museums • no requirement for public spaces • allows visible surface parking • allowable height 36': PD = 45' 	<ul style="list-style-type: none"> • require conformance with design guidelines • require bldg. articulation and architectural controls • permit performance space/art galleries, etc. • require public spaces & plazas • require landscaping for all surface parking • require landscaping for all development • maintain existing height with exception of strictly limited architectural elements 	<ul style="list-style-type: none"> • stringent development controls • findings required • encourages creation of public spaces & requires landscaping • encourages quality architecture
<p>RTC/PER: Beach Commercial Performance Overlay: Main Beach Lot [NEW]</p> <p>Issues</p> <ul style="list-style-type: none"> - need to ensure compatible development on Main Beach Lot - need to control density & bulk 	<p>Existing Goal: to encourage regional serving uses</p> <p>Proposed Goal: to reduce density & to provide standards which protect neighborhood integrity & appropriate economic development</p>	<ul style="list-style-type: none"> • no design standards • no bldg articulation or architectural style required • no landscaping • no allowance of public performance space, art galleries and/or museums • no requirement for public spaces • allows visible surface parking • allowable height 36': PD = 45' • allows up to 400,000 sf development 	<ul style="list-style-type: none"> • require public process • encourages street level retail, restaurant, enter-tainment, arts, museum • creates pedestrian environment • allows 2nd floor residential development • makes distinction between character of surrounding streets & allowable uses • allows 125,000 sf development • maintain existing height with exception of strictly limited architectural elements 	<ul style="list-style-type: none"> • detailed design and use controls • retail required on 1st floor at street frontages • minimum bldg height 2 stories • require transit/carpooling &/or contribution to shuttle/depot.

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ZONING SUMMARY COMMERCIAL ZONING (continued)

Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>RTB/PER : West Cliff Overlook Performance Zone [NEW]</p> <p style="text-align: center;"><u>Issues</u></p> <p>-Need to ensure compatible development -Need to reduce impact of commercial uses on residential West Cliff area</p>	<p>Existing Goal: to encourage mix of motel/hotel and "high" density residential uses.</p> <p>Proposed Goal: to limit future hotel/motel rooms but allows residential & hotel support facilities. Reduce to medium density residential</p>	<ul style="list-style-type: none"> • encourage motel/hotel & residential • allows up to 55 DU/A • allows special density bonus • allows permanent & transient uses in motel, hotel • some inappropriate uses • 1-2 units: up to 35' height • 3+ units: up to 55' height 	<ul style="list-style-type: none"> • Not allow hotel/rooms rooms • Reduce residential density to 30 DU/A • Allows hotel support facilities, i.e. recreational facilities • New design and siting requirements • Reduces height to 36' • Require public spaces and landscaping • Require bldg. articulation • Step back 3rd story by 15' 	<ul style="list-style-type: none"> • Encourages residential development, • limits commercial development • Requires development/design standards • Restricts density & scale

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ZONING SUMMARY COMMERCIAL ZONING (continued)

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Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>CBD/E: Lower Pacific Avenue [REVISED]</p> <p style="text-align: center;"><u>Issues</u></p> <ul style="list-style-type: none"> - small, narrow lots cannot develop - need to continue pedestrian streetscape & restrict surface parking lots 	<p>Existing goal: to continue CBD down Pacific at a smaller scale</p> <p>Proposed goal: to implement goal above and to create an attractive pedestrian environment & linkage between CBD & Beach</p>	<ul style="list-style-type: none"> • permit retail, office and residential up to 30DU/A • use permit for auto supply stores, recycling facilities, • setbacks for sidewalks - no side yard setbacks • on-site parking required • design guidelines like CBD • permit grd level residential 	<ul style="list-style-type: none"> • delete quasi-industrial and auto uses • encourage live/work environment • continue stringent design standards • permit flexibility in location of off-site parking 	<ul style="list-style-type: none"> • encourage continuation of retail/pedestrian environment from CBD to Beach • restrict surface parking lots and permit off-site parking for small lots • require stringent design standards compatible with downtown on a smaller scale

Zone	Goal	Existing Standards	Proposed Standards & Incentives	Policy Issues
<p>CB: Beach Commercial</p> <p>Issues: to ensure compatible development and provide development standards on a design control similar to the rest of the B/SOLA area, north of Beach Street</p>	<p>Goal: to provide opportunity for primarily coastal-dependent and tourist serving uses, while maintaining compatibility with the adjacent neighborhoods.</p>	<ul style="list-style-type: none"> • no design standards • no building articulation or architectural style required • no landscaping requirements • no requirement for public spaces • permits visible surface parking • allows heights to 45' w/ PD • allows ferris wheels and roller coasters 	<ul style="list-style-type: none"> • prohibit ferris wheels and roller coasters north and west of Beach Street • require conformance w/ design guidelines • require building articulation and architectural controls • require public spaces and plazas • require landscaping for all surface parking (15% of the land area) • require landscaping for all development • allow professional offices associated with a visitor serving use 	<ul style="list-style-type: none"> • encourages development standards and design controls • findings required • encourages creation of public spaces and requires landscaping • encourages quality architecture

