

LAND USE ELEMENT - LOCAL COASTAL PLAN
SAN LUIS OBISPO COUNTY GENERAL PLAN



April 2006



**Board of Supervisor's
Approved Draft**

**Cambria and San Simeon Acres Community
Plans of the North Coast Area Plan**

SAN LUIS OBISPO COUNTY DEPARTMENT OF PLANNING & BUILDING

COUNTY OF SAN LUIS OBISPO

THE LAND USE ELEMENT
OF THE SAN LUIS OBISPO COUNTY GENERAL PLAN

**Cambria and San Simeon Acres
Community Plan Update
April 2006**

NORTH COAST AREA PLAN

FIRST ADOPTED BY
THE SAN LUIS OBISPO COUNTY BOARD OF SUPERVISORS
MARCH 1, 1988

PROGRAM CERTIFIED BY
THE CALIFORNIA COASTAL COMMISSION
FEBRUARY 25, 1988



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Preface

This 'Board of Supervisor's Approved Draft' for review is a continuation of the County's efforts to update the North Coast Area Plan. On May 6, 2003, the Board of Supervisors directed staff of the Department of Planning and Building to update the Community Plans for Cambria and San Simeon Acres separately from the remainder of the North Coast Area Plan. The Board further directed that the draft update be subject to public hearings before the County Planning Commission, Board of Supervisors, and the California Coastal Commission.

This document combines provisions from the existing plan, previous draft plans, and recommendations from the California Coastal Commission. Where possible, the material was made current and updated with the most recent information available. Previous proposed land use changes requested by property owners, the county, and California Coastal Commission are also included.

This document includes a number of proposed text and mapping changes to the Cambria and San Simeon Acres portions of the North Coast Area Plan along with relevant background information to establish the setting based on updated information. No changes to the Rural Area standards, programs, or combining designations are proposed as part of this project. Also proposed are changes to sections of the Cambria Design Plan and the Coastal Zone Ordinance.

On the next page is a review guide to help the reader understand the symbols and organization of the document. The public is encouraged to comment on this document. Comments in writing are preferred. If there are questions, please contact:

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Review Guide

Cambria and San Simeon Acres Community Plans of the North Coast Area Plan

Introduction. The following is a guide for the reader of the update of the Cambria/San Simeon Community Plan portions of the North Coast Area Plan. This draft contains specific language and symbols that:

1. Identify what remains unchanged in the existing North Coast Area Plan.
2. Shows what is proposed to be changed and updated.

The purpose of this guide is to help the reviewer understand the organization of the draft plan update.

How the amendments are organized. To amend an adopted plan, the county must show all text and maps proposed for addition or deletion. The chapter and page organization were written to closely follow the existing North Coast Area Plan document, which is in wide circulation, and familiar to many participants.

How to read the symbols. The current amendment proposal uses the same symbols as other general plan amendments, and the former North Coast Area Plan Update. They are:

- Text *not* proposed for any change looks like this.
- Text proposed to be added looks like this.
- ~~Text proposed to be deleted looks like this.~~

Special Symbols. To identify the source of some of provisions in the draft plan, special symbols may be found at the end of each paragraph.

- ★ (The Rural Area is not part of this Plan Update) ★ highlights areas of the plan that pertain to the Rural Area and are not part of this Plan Update
- (Mod#) means a proposed change *related to* a State Coastal Commission recommended modification adopted as findings in 1998.
- (Rec#) means a proposed change *related to* a State Coastal Commission recommendation during Periodic Review adopted as findings in 2001.
- (CDP) is text related to the Cambria Commercial Design Plan.
- (RDP) is text originating from unadopted work relating to the Residential Design Plan.

CHAPTER 1: INTRODUCTION & GENERAL GOALS



A. Introduction

The North Coast Planning Area extends from the Monterey/San Luis Obispo County Line on the north, to Point Estero on the south, and inland generally to the main ridge of the Santa Lucia range. The planning area includes the communities of Cambria and San Simeon Acres. Several small tourist-oriented facilities are also located along Highway One, with Hearst Castle and the scenic shoreline being the principal attractions. The major land holding is Hearst Ranch, which occupies nearly half of the 261 square mile planning area, which is the subject of a specific master plan discussed in detail in the text. This report describes county land use policies for the North Coast Planning Area, including regulations which are also adopted as part of the Land Use Ordinances and Local Coastal Program.

This Area Plan allocates land use throughout the planning area by land use categories. The land use categories determine the varieties of land use that may be established on a parcel of land, as well as defining their allowable density and intensity. A list of allowable uses is in Chapter 7 (Coastal Table O,) of Framework for Planning.

Figure 1-1 shows the North Coast Planning Area in relation to the rest of the County.

The Planning Areas

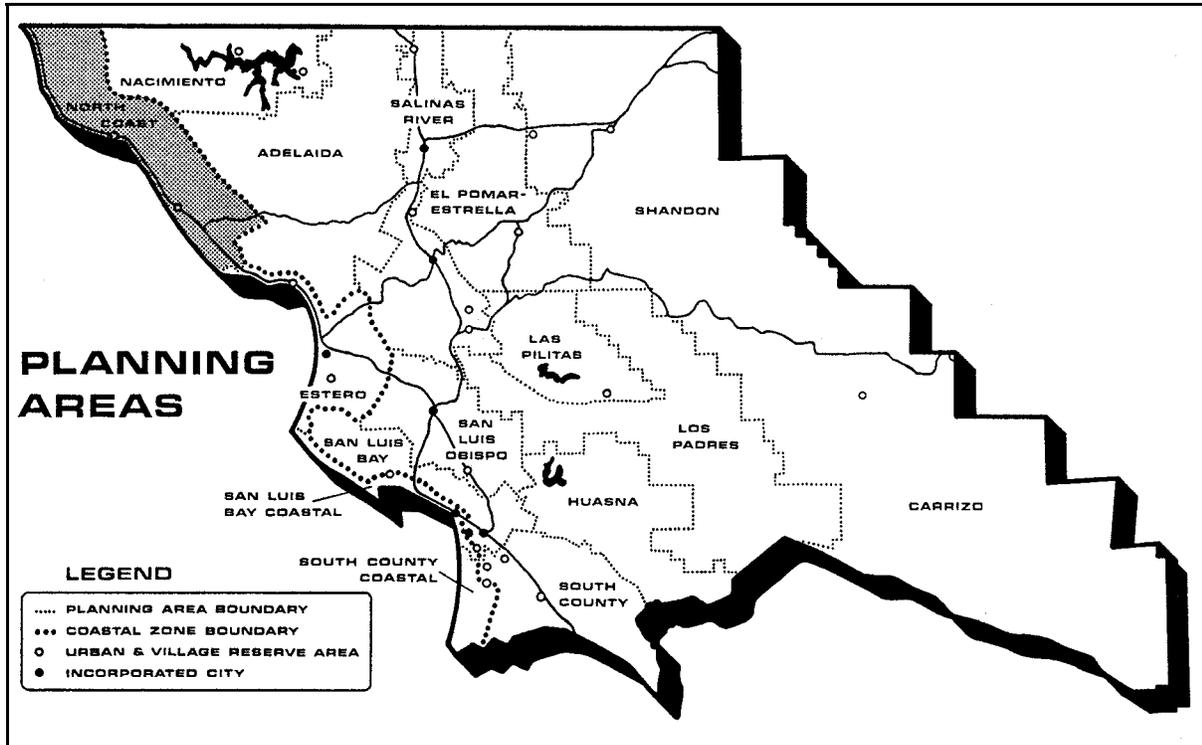


Figure 1-1: The Planning Areas

B. General Goals for Cambria and San Simeon Acres

The goals in this section provide the direction for the communities of Cambria and San Simeon Acres. These general goals, together with other parts of the Land Use Element (LUE) and Local Coastal Program (LCP), provide the basic plan for the area for the next 20 years. These goals also further refine the goals found in *Coastal Zone Framework for Planning*. The background information, programs, and standards portions of the Plan are intended to carry out these goals. Proposed amendments and updates to the Plan should also carry out these goals.

The goals also function as criteria to help determine consistency of a development proposal with the LUE/LCP. New development should be located, designed and built in a manner that furthers these general goals, and shall comply with all other applicable regulations. The following are the general goals for planning in Cambria and San Simeon Acres:

- 1. Environment.** Maintain and protect a living environment that is safe, healthful and pleasant for all residents by:
 - A.** Assuring the protection of coastal resources such as wetlands, coastal streams, forests, marine habitats, and wildlife, including threatened and endangered species.
 - B.** Conserving nonrenewable resources and replenishing renewable resources.
 - C.** Balancing the capacity for growth allowed by the Plan with the sustained availability of resources.
 - D.** Avoid or mitigate to the maximum extent feasible, any adverse impacts from development using the best available methods.
 - E.** Preserving and protecting the air quality by seeking to attain and maintain State and federal ambient air quality standards by determining, and mitigating where feasible, potential adverse air quality impacts of new residential, commercial, and recreational development.
 - F.** Preserving and protecting water quality by mitigating where feasible, potential adverse water quality impacts of new residential, commercial, and recreational development.
 - G.** Supporting the efforts of the Monterey Bay National Marine Sanctuary, or future local marine sanctuaries.

2. Orderly Development. Provide for an environmentally and economically sustainable rate of orderly development within the planned capacities of resources and services by:

- A.** Developing and maintaining information on supply and demand for resources such as water, and population growth rates.
- B.** Establishing a growth management system that protects communities and resources from the adverse effects of growth.
- C.** Establishing a growth rate consistent with the growth management ordinance and the anticipated population provided for in this Plan.
- D.** Providing the lead time necessary to fund and implement the public services necessary to support population growth, considering the County's and taxpayers' financial ability to provide them.

3. Distribution of Land Uses. Encourage an urban environment that is an orderly arrangement of buildings, improvements, and open space appropriate to the size and scale of development for each community by:

- A.** Maintaining a clear distinction between urban and rural development.
- B.** Ensuring the protection of agricultural land containing both prime and nonprime soils, for the production of food, fiber, and other agricultural commodities and maintain the long term viability of agriculture.

4. Location and Timing of Urban Development. Plan for a land use pattern and population distribution that is consistent with the ability of communities to provide and maintain necessary services and facilities by:

- A.** Encouraging the phasing of urban development in a compact manner, and maintaining the "hard edge" between urban and rural uses on the perimeter of Cambria and San Simeon Acres
- B.** Encouraging the voluntary transfer of potential development from antiquated subdivisions, agricultural lands, environmentally sensitive habitat areas and hazard lands to areas identified in the TDC program.

5. Location and Timing of Development within Cambria

- A.** First, infill development from the CCSD water waiting list;

- B. Second, infill on existing vacant parcels within the Urban Services Line, including transfers of water meters out of forested areas;
- C. Third, on other parcels suitable as "receiver areas," accepting development from sender areas through a Transfer of Development Credits Program.
- D. Encouraging establishment of a Park or Open Space District to purchase small substandard lots in Cambria in order to reduce overall buildout and the corresponding need for costly new services and infrastructure.

6. Residential Land Uses. Preserve and enhance the quality of residential areas by:

- A. Locating urban residential uses within Cambria Urban or San Simeon Village Reserve Lines in areas near employment.
- B. Protecting residential areas from incompatible land uses and protecting the residential character of single-family areas.
- C. Preserving desirable neighborhood characteristics such as compatible uses, open views, yard areas, sense of scale, landscaping, pedestrian ways, and other amenities.
- D. Requiring major developments and long range plans to create a balance between available jobs and housing by locating housing in areas that reduce the need for commuting.

7. Residential Development in Cambria.

- A. Achieve residential design that avoids or minimizes impacts to the pine and oak forest.
- B. Respect the natural features and limitations of each site with site-sensitive design.
- C. Ensure that building size, massing and location are compatible with the site, neighborhood, and character of the community.
- D. Assist buyers of properties in determining whether sites are suitable for their vision of residential development (RDP).
- E. Ensure that buildings are constructed in accordance with all applicable building and safety codes.

8. Residential Design in Cambria.

- A.** Reflect the character of Cambria with residential design.
- B.** Keep the physical scale of proposed buildings and site design consistent with site constraints and resources.
- C.** Preserve the native forest ecosystem. (RDP)
 - 1.** Maintain and enhance the urban forest as a distinctive feature of Cambria.
 - 2.** Design structures to accommodate the extent of upper- and lower-story canopy trees, their density and age structure.
 - 3.** Avoid significant, exposed excavation or fill slopes. (RDP)
- D.** Building Height and Floor Area.
 - 1.** Reflect the site context and the surrounding natural environment with building forms.
 - 2.** The height and size of a house should be reflective of its site and the character of the community. (RDP)
- E.** Residential Setbacks and Open Areas.
 - 1.** Locate open areas to enhance the forest and coastal terraces on a site.
 - 2.** Create a feeling of open space within a site, coordinated with the design of structures and adjacent sites.
 - 3.** Open space should provide a visual flow of open space from the site into the roadways. (RDP)
- F.** Parking and Access.
 - 1.** Minimize the extent of hardscape and impervious surfaces.
 - 2.** Avoid garage structures that dominate the site and building design.
 - 3.** Respect open space, views and activity areas within a site when designing parking. (RDP)
- G.** Building Design.

1. Avoid massive structural forms that dominate the site and neighborhood with site-specific and contextual design.
2. Promote a diversity of architectural designs.
3. Restrain the variety of details and forms on a single building.
4. Design night lighting to avoid impacts to neighbors and wildlife and to preserve the night sky. (RDP)

H. Landscape Design.

1. Renew the urban forest and provide for upper and lower canopy trees.
2. Use native, drought-tolerant and fire retardant plants accustomed to growing along the Central Coast.
3. Maintain a sense of informality and discovery along the street.
4. Consider and respect the neighbors with landscape designs. (RDP)

9. Commercial and Industrial Land Uses. Designate commercial and/or industrial areas that are compatible with overall land use by:

- A.** Designating visitor-serving and community-serving commercial areas that are located near existing similar development and their users.
- B.** Designating commercial and/or light industrial areas compatible with overall land uses that are convenient to users, and are realistically related to market demand and the needs of the community.
- C.** Creating and preserving desirable neighborhood business characteristics, such as compatible uses, safe employment areas, sense of scale, attractive landscaping, pedestrian ways, and other amenities.

10. Visitor Serving, Recreation and Resort Development. Preserve and enhance visitor opportunities in appropriate locations as an important part of the economy by:

- A.** Implementing the California Coastal Act by acknowledging that public opportunities for recreation have priority over private residential, nonvisitor-serving commercial or industrial development, but not over agriculture or coastal dependent industry. (Mod 3A)
- B.** Recognizing that while visitor-serving uses are encouraged by the Coastal Act and Local Coastal Program, they shall not exceed resource capabilities, conflict with agricultural uses, or be permitted when unavoidable adverse environmental impacts may result.
- C.** Balancing carefully the needs of visitors with those of the local residents. Emphasis should be on careful management of diminishing resources such as water, sewage disposal, and traffic capacity.

11. Public Services, Parks and Facilities. Provide additional public resources, services and facilities to serve existing communities in sufficient time by:

- A.** Avoiding the use of public resources, services, and facilities beyond their renewable capacities.
- B.** Planning for and monitoring new development through the Resource Management System and Growth Management Strategies, to ensure that resource demands will not exceed existing and planned capacities or service levels.
- C.** Financing the cost of additional services and facilities from those who benefit, including local residents, businesses, public agencies, and visitors. When consistent with federal, State and local laws, finance methods may include dedications, development impact fees, in-lieu fees, or other exactions to assure that adequate services are available.
- D.** Locating new public service facilities as close as possible to the users.
- E.** When consistent with the LCP, provide for community recreation needs according to adopted park plans.

12. Circulation. Integrate Land Use and Transportation Planning by:

- A.** Coordinating with cities to ensure that traffic and transportation demands can be safely and adequately accommodated.
- B.** Designing a transportation system that provides safety within feasible economic and technical means, preserves important natural resources and features, promotes the aesthetic quality of the region, and minimizes adverse environmental impacts.
- C.** Develop alternative transportation modes such as pedestrian facilities bikeways, car-pooling, transit systems, and other methods that serve the resident and visitor needs of the coastal areas.
- D.** Exploring the possibility of using Transportation Management Systems before making decisions that will significantly alter the character of coastal roads and highways, and the communities they serve.

13. Open Space. Preserve urban open space as an irreplaceable resource for future generations by:

- A.** Encouraging cooperation among governmental agencies, landowners, and nonprofit organizations in the preservation of open space.
- B.** Recognizing the value of open space and passive recreation as both a coastal resource, and an economic asset contributing to the desirability of the area as a place to live, an agricultural production area, wildlife habitat, and a visitor destination area.
- C.** Encouraging better access to the coast through the acquisition and development of coastal accessways, trails, and neighborhood parks, in areas that do not impact agriculture or coastal resources.

14. Resource Use and Energy Conservation. Support the conservation of energy resources by:

- A.** Requiring energy conservation through a balanced system of transportation and land use. Subdivision and building regulations should require that energy conservation methods be incorporated into the design of land divisions and buildings.
- B.** Decreasing reliance on environmentally costly energy sources, increasing conservation efforts, and encouraging use of alternative energy sources.

- C. Recognizing the impacts of land uses and water consumption levels that may be inappropriate in an area with a semi-arid climate.
- D. Encouraging land use and transit measures that reduce the use of nonrenewable resources such as petroleum.

15. Property Rights. Recognize and protect individual property rights by:

- A. Seeking to maintain a balance between the rights of property owners and efforts to plan for the community.
- B. Not taking property without just compensation.
- C. Recognizing and protecting the rights of all property owners, individuals, and groups to comment and participate in coastal planning and land use decisions.
- D. According to Section 30001.5 of the California Coastal Act:
 - 1. Assure orderly, balanced use and conservation of coastal zone resources taking into account the social and economic needs of the people of the state; and
 - 2. Maximize public access to, and along, the coast and maximize public recreation opportunities in the coastal zone consistent with sound resource conservation principles and constitutionally protected rights of private property owners. (Mod five, part two)

16. Economics. Promote a strong, diverse, and viable local economy by:

- A. Pursuing planning policies that balance economic, environmental, and social needs of coastal areas.
- B. Recognizing the importance of tourism and agriculture as significant parts of the economic base of the coastal areas.
- C. Considering the economic and fiscal effects of land use planning decisions. New development projects should employ locally based workers in order to reduce traffic and resulting air pollution.

17. Implementation and Administration.

- A. Encouraging maximum public participation in the land use decision-making process when plans are developed and when development is being reviewed.

Encourage comments from individuals, property owners, residents, community advisory councils, interest groups, districts, and other agencies.

- B.** Developing clear policies, programs, and performance standards that promote the most desirable living and working environment for the residents of the County.

18. Public Access to the Shoreline. Provide for public access, consistent with the need to protect natural resource areas from overuse, by:

- A.** Maximizing public access to and along the coast through the following:

1. Developing all feasible vertical and lateral pedestrian access easements to and along the shoreline, consistent with other public access goals of this plan;

2. Developing a Coastal Trail through the length of the Planning Area.

3. Developing all other feasible pedestrian circulation systems in the coastal zone, consistent with other public access goals of this plan;

4. Providing a bike path system for the Planning Area;

5. Providing conspicuous signage for all public access easements;

- B.** Preventing interference with the public's right of access to the sea, whether acquired through use or legislative authorization, including, but not limited to, the use of dry sand and rocky coastal beaches to the first line of terrestrial vegetation.

- C.** Requiring new development between the nearest public roadway and the shoreline and along the coast to provide public access consistent with sound resource management and consistent with public safety, military security needs, and the protection of fragile coastal resources.

- D.** Carefully balancing the public's right of access to the sea with constitutionally protected private property rights. (Mod 6)

19. Vision for Fiscalini Ranch. Recognize and support the Vision of the approved Management Plan for the Ranch, by furthering the following objectives:

- A. Striving for minimum disturbance to the natural qualities of the Ranch while allowing appropriate public access and recreation.
- B. Protecting sensitive habitats and species in all areas of the Ranch, including coastal bluffs, coastal terrace, pine forest, riparian and creek corridors, wetlands, and other unique and valuable resources.
- C. Creating restoration, enhancement, and management guidelines for the long-term protection of natural resources.
- D. Creating design standards and management guidelines for long-term public access improvements.
- E. Providing a method for environmentally sound vegetation management, which provides for adequate wildfire defensible space.
- F. Creating management guidelines for allowed uses and activities on the Ranch.
- G. Providing a public trail system that allows balanced and strategic access, and provides linkages to other local trail systems in the community and to the Coastal Trail.
- H. Siting and design all improvements in ways that protect sensitive habitats and the scenic and visual quality of the Ranch.
- I. Identifying methods to access the Ranch, including ADA-compliant parking and transit service that provide necessary public access while avoiding undue impacts to surrounding neighborhoods.
- J. Reducing risk and hazards to Ranch users and surrounding neighbor properties, including fire protection, erosion, noise, trespassing, and litter.
- K. Providing guidance on implementation activities, including roles and responsibilities of the Cambria Community Services District and North Coast SWAP or their successor, for operational and maintenance issues, and prioritization of activities.

C. Features of the North Coast Area Plan

- 1. Background Information.** The North Coast Area Plan provides information on population, land use, availability of resources and public services, and environmental characteristics. This information is the basis for many of the decisions made in the

Plan and is current at the time of the last update.

- 2. Resource Management System (RMS).** This report ~~Plan~~ also includes initiates the use of the Resource Management System (RMS) (see Chapter 3) which inventories the resources of water supply, sewage disposal, schools, roads/circulation, and air quality in the Planning Area. ~~The report contains~~ RMS provides estimates of population thresholds at which potential resource capacity problems may occur within the Planning Area. The RMS estimates are updated annually and reviewed by the County Board of Supervisors. at the three levels of severity for the resources of water supply, sewage disposal, schools and roads.
- 3. Planning Programs.** ~~Proposed "Programs" are also noted included~~ at the end of the chapters on Public Facilities, Services and Resources services (Chapter 3), Land Use (Chapter 6 4), Circulation (Chapter 4 5), and Combining Designations (Chapter 7 6). Programs are nonmandatory actions recommended to be initiated by the communities, ~~through~~ the County, or other specified public agencies, to work toward correcting addressing local problems or conditions issues of concern. They are also intended to support community objectives in implementing the General Plan. Because many recommended programs involve public expenditures, their initiation will be dependent upon the availability of funding.
- 4. Plan Maps.** Land use, combining designation, and circulation maps are shown following Chapter 7. They include the following:

 - A. Land Use Categories.** The land use categories determine the allowable uses for every piece of property, including the density and intensity of potential development.
 - B. Combining Designations.** The Plan designates areas of hazards, sensitive resource areas, environmentally sensitive habitat areas, historic and archaeologically sensitive areas, and public facilities.
 - C. Circulation.** The Plan designates highways, streets, and roads, according to their existing and proposed transportation function.

Because of limitations of scale, the foldout maps in this report are for reference purposes only. The official maps are at a more suitable scale at a size of 1-inch=400 feet (Urban) and 1 inch = 1000 feet (Rural). They are available at the County Department of Planning and Building.
- 5. Planning Area Standards.** Specific development "standards" are included in ~~this report~~ Chapter 7: Planning Area Standards to address special ~~problems~~ issues and conditions ~~in~~ relevant to individual communities. ~~Chapter 7 standards for public services, circulation, and land use are found in Chapter 7 (Planning Area Standards)~~

~~and provide criteria for detailed evaluation of proposed development projects. The text of this Plan may also be used as a basis for decisions concerning development and subdivision applications. Report, other than is for general planning guidance only and is not to be used as a basis for approval or disapproval of development or land division proposals. Careful reading of the planning area standards will assist in creating projects that are consistent with adopted policies and regulations.~~

- 6. Combining Designation Standards.** In addition to the land use categories, programs, and standards, one or more Combining Designations have been applied to specific areas. Combining Designations identify potential natural hazards and locations of notable resources. The designations are shown on the maps following Chapter ~~7~~ 8.
- 7. Coastal Access.** Chapter 8 of this Plan provides a list of County goals, policies, standards and ordinances pertinent to coastal access in the Planning Area.
- 8. Coastal Access Appendix A.** Appendix A contains a detailed inventory and maps of existing accessways and offers-to-dedicate lateral and vertical access to the coast. The attached appendix provides background information but is not formally adopted as part of the Plan. San Luis Obispo County Parks, in coordination with the Department of Planning and Building maintains this inventory for the entire County. Please contact County Parks for the most current information.

D. Relationship of the North Coast Area Plan to the Local Coastal Program.

- 1. Introduction.** The following section provides a brief outline of related State and local provisions guiding the preparation and update of the North Coast Area Plan.
- 2. State Law - Zoning and Planning.** Key provisions from the California Government Code and California Coastal Act guide the preparation and adoption of local plans.
 - A. The California Government Code.** The Government Code guides the preparation and adoption of general plans, zoning ordinances, and regulations for subdivision and environmental review. The Code establishes mandatory elements of the General Plan, and various content and procedural requirements.
 - B. The California Coastal Act.** Policies in the California Coastal Act of 1976 guide the conservation and development of California's 1,100 mile coastline, with the goal of protecting California's coastal resources and providing for their wise use. The Act establishes the California Coastal Commission as a permanent State coastal management and regulatory agency and requires each of the State's coastal cities and counties to adopt a long-term management plan, known as a Local Coastal Program. Each Local Coastal Program consists of a land use plan, zoning ordinances and other implementing actions. The Act also requires 'maximum public participation' in the preparation of proposed plans.
Key policy sections include Public Access, Recreation, Marine Environment, Land Resources, Development and Industry. These policies function as criteria for finding consistency of both development proposals and Local Coastal Plans with the Coastal Act.
- 3. County Organization of the Local Coastal Program (LCP).** The LCP is part of the County General Plan and Zoning Ordinance, and applies to those areas lying within the Coastal Zone. Consistent with State law, the County's Local Coastal Program also functions as the mandatory Circulation Element and Land Use Element of the General Plan.

A. The LCP Policy Document. The Local Coastal Program Policy document contains 13 chapters that respond to policies contained in the California Coastal Act. They are:

- Shoreline Access
- Recreation & Visitor Serving
- Energy & Industrial Development
- Commercial Fishing
- Environmentally Sensitive Habitats
- Agriculture
- Public Works
- Coastal Watersheds
- Scenic Resources
- Hazards
- Archeology
- Air Quality

Each chapter discusses the relationship to Coastal Act policies and issues, and formulates locally-based policies that implement the Act.

B. Framework for Planning. Framework for Planning serves as an organizational document, linking land use, resource management, and circulation. It also provides general goals for County decisions on land use, development, and circulation. A key part is ‘Coastal Table O - Allowable Land Use Chart’ that shows which land uses are allowable in each of the 13 different land use categories.

C. Coastal Zone Land Use Ordinance (CZLUO). The CZLUO provides provisions typically found in zoning ordinances. They include permit and appeal requirements, site design, development and operational standards, and enforcement provisions.

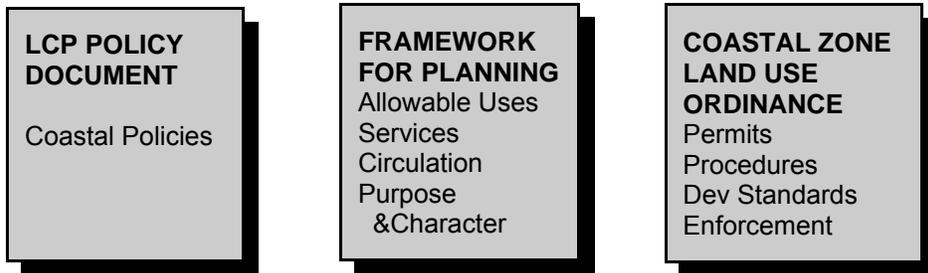
D. The Area Plans. The Area Plans replaced the former community general plans, and provided goals, objectives, policies, programs and standards unique to each area. The Coastal Zone of San Luis Obispo County is divided into four segments: North Coast, Estero, San Luis Bay, and South County. The Area Plans work with the other elements of the LCP and County General Plan. Key provisions found in the Area Plans are land use maps, programs, and standards guiding development. In addition, standards are provided to guide resource management, conservation, environmental protection, and community character.

Plan Organization

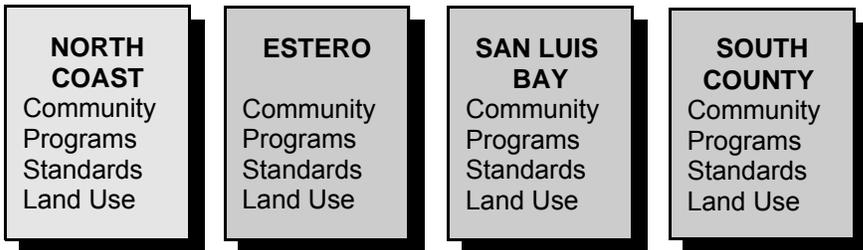
State Law - Planning and Zoning



County of San Luis Obispo Land Use Element and Local Coastal Program



Area Plans



CHAPTER 2: POPULATION, HOUSING & ECONOMY



A. Introduction

This chapter provides background information, goals and objectives about the population, housing and economy of the North Coast. Although Cambria, San Simeon Acres, and the rural area are unique and separate communities, they are connected in terms of their economy. Unless otherwise noted, all information in this chapter is based on 2000 census data. This chapter provides information about Cambria and San Simeon Acres, as well as information about the entire North Coast Planning Area, the county and the State for comparison purposes.

B. Population

- 1. Historic Population Growth.** The North Coast Planning Area, like as is true of the other coastal areas, has grown at a faster rate than the county average. The population ~~has grown~~ grew from approximately 2,100 in 1970 to over 3,600 in 1980. By 1990, the North Coast had a population of 5,936 (see Table 2-1). The present population of Cambria is around 3,300.

Table 2-1
Historical Population Growth

	1970	1980	1990	2000
San Simeon Acres	50	150	206	253
Rural	334	350	348	867
Cambria	1,716	3,100	5,382	6,218
Total	2,100	3,600	5,936	7,338

The year 2005 population for Cambria is approximately 6,400. The population of San Simeon Acres ~~is estimated to slightly exceed 100~~ in 2005 is estimated to be 250. The rural population is between ~~500 and 600~~ 800 and 900 persons. ~~Based on past past growth rates, the projected population for the North Coast Planning Area will exceed 6,700 by the year 2000, with most of this growth occurring in the Cambria area.~~

~~Over the past several decades, Cambria's growth was slow but steady, resulting in an average annual increase of about 45 persons per year. In 1950, Cambria had 788 residents, a figure that rose to 1,260 by 1960, and 1,716 by 1970. The population in Cambria was 3,100 in 1980.~~

The North Coast displays a disproportionate age structure compared to that of the county and state. Older people make up a larger proportion of the population, reflecting the area's attractiveness for retired persons.

~~The distribution of age groups in Cambria differs markedly from San Luis Obispo County and from the country as a whole. In 1964, approximately 21% of the Cambria population was over 65; in 1970 this had risen to 25%. The percentage of school age children had correspondingly declined from 22% to about 19% during the same period. These figures suggest that elderly or retired persons remain a major age group and are probably increasing in relation to the entire community population.~~

~~Cambria's growth can be attributed to its role as a recreational and vacation community, with large proportion of the housing supply serving as second homes. This has resulted in a vacancy factor of 28%, or only 72% of the existing dwellings being occupied by permanent residents.~~

~~At present the permanent population at San Simeon Acres is limited as very little year-round residential development has occurred. Anticipated future growth will be in multiple family units to provide second homes or retirement units, or to serve those who work in the local businesses.~~

2. Population Profile.

A. Total Population. The county grew at a much faster rate in the 1980's than did the state. Much of the growth is attributed to in-migration from other areas of the state. Cambria grew at a much faster rate than the rest of the county. Estimates for San Simeon Acres are approximate, since figures had to be extracted from census data that includes the rural area (see Table 2-2).

Table 2-2
Total Population - 2000

	San Simeon Acres		Cambria		County		State	
1980 population	150		3,100		155,435		23,667,902	
1990 population	206		5,382		217,162		29,760,021	
2000 population	247		6,218		246,681		33,871,648	
1980-2000 change	97	65%	3,118	101%	91,246	59%	10,203,746	43%

B. Age and Race Profile. Based on the 2000 census, the following table indicates differences in age and race groups for the Cambria urban areas as compared to the county and state. No census data is available for San Simeon Acres. In Cambria, 42 percent of the population is over 55 years of age, as compared to 23 percent for the county (see Table 2-3).

Table 2-3
Age and Race Profile - 2000

Age Group	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
0-4 years	239	3.84	12,358	5.01	2,486,981	7.34
5-9	287	4.61	14,912	6.05	2,725,880	8.05
10-14	308	4.94	16,174	6.56	2,570,822	7.59
15-19	308	4.94	20,893	8.47	2,450,888	7.24
20-24	218	3.50	22,647	9.18	2,381,288	7.03
25-34	415	6.66	28,177	11.42	5,229,062	15.44
35-44	709	11.38	38,416	15.57	5,485,341	16.19
45-54	1,135	18.21	36,150	14.65	4,331,635	12.79
55-59	488	7.83	11,787	4.78	1,467,252	4.33
60-64	465	7.46	9,482	3.84	1,146,841	3.39
65-74	920	14.76	18,094	7.34	1,887,823	5.57
75-84	606	9.72	13,415	5.44	1,282,178	3.79
85+	134	2.15	4,176	1.69	425,657	1.26
Total	6,232	100.00	246,681	100.00	33,871,648	100.00
Racial Group	Cambria		County		State	
White	5,676	91.08	208,699	84.60	20,170,059	59.55
Black or African American	22	0.35	5,002	2.03	2,263,882	6.68
American Indian	62	0.99	2,335	0.95	333,346	0.98
Asian	72	1.15	6,568	2.66	3,697,513	10.92
Pacific Islander	8	0.13	286	0.12	116,961	0.35
Other	251	4.03	15,312	6.21	5,682,241	16.78
Two or more races	141	2.26	8,479	3.44	1,607,646	4.75
Total	6,232	100.00	246,681	100.00	33,871,648	100.00
* Hispanic or Latino (of any race)	874	14.02	40,196	16.29	10,966,556	32.38

* Census data does not list Hispanic or Latino as a subset of race. Rather, Hispanic or Latino of any race is analyzed as a separate category and the percentage of population of Hispanic or Latino background is factored as a percentage of the total population.

C. Education Attainment. Residents of the North Coast area communities are generally better educated than county- and state-wide populations. The following figures in Table 2-4 are for persons 25 years or older.

**Table 2-4
Education Attainment- 2000**

	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Less than 9 th grade	177	3.62	7,836	4.92	2,446,324	11.49
9 th to 12 th grade, no diploma	251	5.13	15,128	9.50	2,496,419	11.72
High school graduate (includes equivalency)	804	16.42	34,728	21.81	4,288,452	20.13
Some college, no degree	1,591	32.50	44,514	27.96	4,879,336	22.91
Associate degree	337	6.88	14,483	9.10	1,518,403	7.13
Bachelor's degree	1,162	23.73	27,648	17.37	3,640,157	17.09
Graduate or professional degree	574	11.72	14,859	9.33	2,029,809	9.53
Total	4,896	100.00	159,196	100.00	21,298,900	100.00

D. Household Profiles. Table 2-5 shows that household sizes in Cambria are smaller than those of the county and state. A household is an occupied housing unit.

**Table 2-5
Total Households - 2000**

	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Households in 1990	2,386		80,281		10,381,206	
Households in 2000	2,816		92,739		11,502,870	
1990-2000 change	430	18.02	12,458	15.52	1,121,664	10.80
Persons per Household						
	Cambria		County		State	
1990 average	2.25		2.533		2.79	
2000 average	2.21		2.49		2.87	

E. Household Types. The following Table 2-6 and graph shows household types in Cambria as compared with the rest of the county and the state.

**Table 2-6
Households by Type - 2000**

	Cambria		County		State	
		%		%		%
Family Households:	1,882	66.83	58,654	63.25	7,920,049	68.85
- Married-couple family	1,622	57.60	46,769	50.43	5,877,084	51.09
- Male householder, no wife present	60	2.13	3,425	3.69	594,455	5.17
- Female householder, no husband present	200	7.10	8,460	9.12	1,448,510	12.59
Non-Family households	934	33.17	34,085	36.75	3,582,821	31.15
- Male householder	373	13.25	15,881	17.12	1,718,168	14.94
- Female householder	561	19.92	18,204	19.63	1,864,653	16.21
Total Households	2,816	100.00	92,739	100.00	11,502,870	100.00

F. Household Income. The following Table 2-7 shows household income in Cambria as compared with the rest of the county and the state.

**Table 2-7
Household Income - 2000**

Income Level 1990	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
\$0-\$10,000	178	6.33	8,382	9.04	967,089	8.40
\$10,000-\$14,999	160	5.69	6,115	6.59	648,780	5.64
\$15,000-\$24,999	278	9.89	11,515	12.42	1,318,246	11.45
\$25,000-\$34,999	418	14.86	11,970	12.91	1,315,085	11.42
\$35,000-\$49,999	529	18.81	15,133	16.32	1,745,961	15.17
\$50,000-\$74,999	576	20.48	18,718	20.19	2,202,873	19.14
\$75,000-\$99,999	228	8.11	9,735	10.50	1,326,569	11.52
\$100,000-\$149,999	265	9.42	7,258	7.83	1,192,618	10.36
\$150,000-\$199,999	93	3.31	1,945	2.10	385,248	3.35
\$200,000 or more	87	3.09	1,961	2.11	409,551	3.56
Total	2,812	100.00	92,732	100.01	11,512,020	100.00
2000 median income (\$)	45,000		42,428		47,493	

C. Population Projections

With countywide growth management provisions, existing shortages of important resources such as water and public services, and the inability of the natural environment to sustain full buildout, the North Coast Planning Area is not expected to reach buildout of the land use described by this Plan in the 20-year term of the Plan.

- 1. Land Use and Growth.** Historic growth rates in the North Coast have been higher than the countywide average. However, like the county-wide average, growth rates in Cambria and San Simeon Acres have decreased during the last ten years. This reduction in growth rates is due primarily to resource constraints and development restrictions as there is still an ample supply of existing vacant lots, within both Cambria and San Simeon.

While buildout is theoretically possible, it is not very likely because of resource constraints such as water supply, traffic capacity, and public facility limitations. Development of the large amounts of water capacity necessary to sustain a large population would be very costly, difficult, and time-consuming. The environmental sensitivity and location within the Coastal Zone also makes permitting and implementation of infrastructure projects difficult.

More importantly, community input has indicated that sustaining past growth rates is neither wise nor acceptable. Past growth rates, together with increasing difficulty on the part of the community to provide necessary funding and improvements for more water, traffic capacity, schools, and other services, have left the North Coast area with several serious resource deficiencies.

During the CCSD Board of Director's July 24, 2003 meeting, a motion by the Board confirmed a maximum of 4,650 connections as the ultimate buildout of Cambria. This total was based on 3,812 existing connections as of the end of 2002 plus 165 connections in process at that time, plus 670 future connections from the CCSD wait list. The CCSD's Buildout Reduction Plan will include an implementation program to permanently retire lots so that the maximum buildout will match their identified goal of 4,650 dwelling units. Based on historic population rates for Cambria, this would result in a population between 7,724 and 10,469 people.

- 2. Growth Management and Anticipated Growth Rates.** The current county growth rate for dwelling units is set annually. Countywide, the number of new dwelling units allowed yearly is generally 2.3 percent of the existing county dwelling units. There are a few types of housing that are not subject to this limitation, such as farm worker and affordable housing projects. During some years, fewer allocations may be allowed due to water shortages. Since 1999, the County limited the allocations for Cambria to a maximum 1 percent rather than 2.3 percent. This one percent limit will continue due to Cambria's water shortages. Due

to limited water supplies and the community's support over the years for growth control, it is assumed that the average growth rate over the life of this Plan would be a maximum of one percent. The CCSD is currently in the preliminary stages of a lengthy process to permit, build, and implement a desalination plant. Once a desalination plant is implemented, it would accommodate 650 additional units in Cambria. The remaining buildout potential in Cambria is to be retired through implementation of the CCSD's Buildout Reduction Plan. Requests for allocations to build new units for San Simeon Acres and the rural area are calculated as part of the County total.

A. Cambria. There were approximately 4,000 dwellings in 2005, with approximately 7,900 potential additional units allowed by the existing 1988 plan. Theoretically, all of this growth has the potential to occur during the life of the existing plan since the existing plan does not include growth rates.

With the public purchase of Fiscalini Ranch and change of developable acreage to Recreation and Open Space, approximately 738 potential dwellings were removed. Further reductions are proposed to result in an approximately 31% decrease in dwelling units from the total potential

Should the plan never be updated again, the theoretical buildout would be approximately 6,130. Based on historic population rates for Cambria, this would result in a population between 10,180 and 13,790 people. However, through the standard development review process and future plan updates, development levels would continue to match available resources such as water and public services. For example, verification of water and sewer service from the CCSD is required prior to acceptance of an application for processing.

Water is one of the most important limiting factors to growth in Cambria. The CCSD has begun efforts to reduce water demand and to secure a reliable water supply. During the CCSD's Board of Director's July 24, 2003 meeting, a motion by the Board confirmed a maximum of 4,650 connections as the ultimate buildout of Cambria. This total was based on 3,812 existing connections at the end of 2002, 165 connections in process at that time, and 670 future connections from the CCSD wait list. This also approximates the number of dwelling units to be served by a proposed desalination project that was the subject of an August 2000 advisory ballot.

As summarized in detail in Chapter 3, Cambria Water Supply, the CCSD has embarked on a phased approach toward updating its Water Master Plan and improving upon its water system. The Water Master Plan EIR will include a Buildout Reduction Program which is the tool to cap the maximum number of potential water service connections within the CCSD service area to 4,650. Based on historic population rates for Cambria, this would result in a

population between 7,724 and 10,469 people.

B. San Simeon Acres. There were approximately 320 dwelling units in San Simeon acres in 2005. There are sufficient number of existing lots that are vacant or partly developed to allow approximately 530 additional dwelling units. Taking into account growth management at a 2.3% rate per year, the proposed plan projects a total of 530 units in San Simeon. Based on the range of historic population rates for San Simeon, this would result in a population between 400 and 740 people in San Simeon. The community has made substantial efforts to retrofit plumbing, resulting in greatly reduced water usage and sewage generation. However, constraints such as water supply, sewage disposal, school capacity, and Highway One capacity will continue to limit growth.

Table A contains population projections for the planning area, excerpted from the countywide projections found in Table C of Framework for Planning. For comparison, Table B contains the projected Absorption Capacity, which is the potential planning area population resulting from unconstrained growth and fully-occupied development to the maximum permitted in each land use category. (Framework for Planning offers a more detailed discussion of Absorption Capacity.)

TABLE A
POPULATION PROJECTIONS
NORTH COAST PLANNING AREA

Year	Cambria	Rural Area	Planning Area	% of County
1979	3,087	530	3,617	2.50
1980	3,274	540	3,814	2.55
1985	3,722	694	4,416	2.61
1990	4,232	4,896	5,128	2.76
1995	4,776	1,089	5,865	2.90
2000	5,391	1,320	6,711	3.05

TABLE B
ABSORPTION CAPACITY[†]
NORTH COAST PLANNING AREA

Land Use Categories	Rural Area	Cambria	San Simeon Acres	Total Planning Area
Agriculture	2,597	=	=	2,597
Rural Lands	154	=	=	154
Residential Rural	=	=	=	=
Residential Suburban	=	259	=	259
Residential Single Family	=	17,471	=	17,471
Residential Multi-Family	=	3,667	588	4,255
Office & Professional	=	128	=	128
ABSORPTION CAPACITY	2,751	21,525	588	24,864
Existing Population	436	3,274	104	3,814
POTENTIAL ADDED POPULATION	2,315	18,251	484	21,050
† Potential population at build-out by land use category.				

D. Housing

- 1. Introduction.** The North Coast Planning Area, like other areas, has a number of constraints to providing affordable housing for all segments of the community. Affordable housing is not easily available to those with lower incomes. With tourism and agriculture as the largest segment of the economic base in the area, it is essential that housing needs of employees in these industries, such as motel and farm workers, be addressed. Other groups with unmet housing needs include minimum wage earners, young people, renters, and elderly persons needing care.

- 2. Housing and Population.** The quality of life of the North Coast has also attracted many retired people and self-employed younger families. The largest age group is 35 to 54 year-olds, which comprises about 30 percent of the total population. The second largest group is the over-65 group, at about 27 percent of the population. Cambria is the largest community on the North Coast, with housing mostly provided by single-family dwellings. Housing is in very good condition, with a high percentage of single-family and owner-occupied units. An abundance of vacant lots, existing infrastructure, steady growth rate, and desire for many to live in the North Coast will likely continue this pattern.

- 3. Housing Approaches in this Plan.** Providing for current and future housing needs is essential for the well-being of residents and the future of the economy. The Area Plan works with the *Housing Element* to further the housing goals of the County. The segments of the population with unmet housing needs may best be provided with multi-family housing, such as apartments, group housing, and mobile homes. While government actions may remove some obstacles to housing development, and encourage programs for financial assistance, there are still a number of additional factors, such as interest rates and construction costs, that affect housing availability and are beyond the scope of this Area Plan.

This Plan designates land in both Cambria and San Simeon Acres for multi-family residential use. Both communities have ample land area and encourage mixed-use development projects, such as projects which provide rear-lot apartments behind a commercial-frontage. While resources such as water have been a problem in the past, this Plan allows a limited amount of water previously allocated to visitor-serving uses to be re-allocated to affordable housing projects in Cambria. This Plan also ensures that land potentially supporting multi-family housing is in close proximity to transit opportunities. Other programs and ordinances already allow farm worker housing in agricultural areas. In addition, there are requirements in the Coastal Zone Land Use Ordinance that 15 percent of the units in large new residential projects shall be affordable housing units. These measures will help encourage affordable housing opportunities in the Planning Area, however, further measures will probably need to be implemented in order to provide the amount of affordable housing that will be needed in the future.

Individual strategies to meet affordable housing needs are listed below:

A. Sufficient Land. Ensuring that there is sufficient land designated for multi-family housing is an important objective of this Plan.

1. San Simeon Acres. The majority of land in San Simeon Acres is within the Commercial Retail or Residential Multi-Family land use categories, both of which allow multi-family residential development. Approximately 40 acres of vacant and partly developed land are located within the village, with a potential for over 500 additional residential units. This area lacks school capacity and support facilities, and a sharp increase in new residential development in the area has the potential to conflict with the tourist economy. An additional important concern is providing a dependable and affordable long-term water source.

2. Cambria. This Plan allows a potential for 975 additional residential units to result under the 20 year term of the plan. Cambria has approximately 50 acres of vacant and partially developed land in the Residential Multi-family and Commercial Retail categories, with the potential for 616 additional dwelling units.

B. Mixed Use Development. Standards in this plan allow mixed use development in the downtown areas of both Cambria and San Simeon Acres. An advantage to this strategy is that it locates housing within walking distance of shopping, services, and Main Street, where transit may be available. This also works to improve the jobs/housing balance and places the local workforce near local jobs.

C. Reservation of Capacity. The Plan includes standards aimed at ensuring affordable housing projects aren't unnecessarily delayed because of water allocation issues and requires a program that reserves sufficient water and sewer capacity to serve affordable housing.

4. Housing Profile.

A. Types of Housing Units. Approximately 93 percent of the housing units in Cambria are single-family detached homes. This is a higher percentage than the rest of the county or state, but is to be expected, since much of the community consists of small lots created in the 1920's. The dominance of small lots and single-family dwellings reflects the market demand for this housing type over multi-family dwellings. It also shows that there is less variety of housing types in the area (see Table 2-8).

Table 2-8
Housing Unit Type - 2000

Type	Cambria		County		State	
		%		%		%
1 unit, detached	3,366	89.76	67,120	64.89	6,883,493	56.35
1 unit, attached	118	3.15	6,096	5.89	931,873	7.63
2 units	84	2.24	3,022	2.92	327,024	2.68
3-4 units	38	1.01	5,150	4.98	697,779	5.71
5-9 units	12	0.32	3,508	3.39	722,827	5.92
10-19 units	28	0.75	2,323	2.25	619,092	5.07
20 or more units	37	0.99	5,054	4.89	1,462,793	11.98
Mobile homes	61	1.63	11,168	10.80	538,423	4.41
Boat, RV, van, etc.	6	0.16	728	0.70	31,245	0.26
Total	3,750	100.00	103,441	100.71	12,214,549	100.00

B. Housing Costs. The percentage of owners paying at least 30 percent of their income for housing in Cambria is about equal to the percentage in the county and the rest of the state (see Table 2-9).

Table 2-9
Housing Costs - 2000

Category	Cambria		County		State	
Median home value (\$)	\$ 305,600	%	\$ 230,000	%	\$ 211,500	%
Owners paying > 30% of income for housing	596	30.8	13,892	31.7	1,726,959	31.2
Source: 2000 Census						

C. Rental Housing Costs. Rents in Cambria are higher than in the rest of county and the state (see Table 2-10).

Table 2-10
Rental Housing Costs - 2000

Category	Cambria		County		State	
Median rent (\$)	\$868		\$719		\$747	
Renters paying > 30% of income for housing	300	44%	16,865	48%	2,079,695	40%
Source: 2000 Census						

D. Year Housing Built. The housing stock in Cambria is quite new, with 73 percent built since 1970. In the year 2005, there were approximately 4,000 dwellings within Cambria's Urban Reserve Line (see Table 2-11).

Table 2-11
Year Housing Built - 2000

Occupancy	Cambria		County		State	
		%		%		%
1999-March 2000	89	2.37	2,407	2.35	191,345	1.57
1995-1998	416	11.09	6,595	6.45	541,056	4.43
1990-1994	310	8.27	8,463	8.27	845,325	6.92
1980-1989	1,206	32.16	25,338	24.77	2,098,028	17.18
1970-1979	728	19.41	26,096	25.52	2,504,157	20.50
1960-1969	460	12.27	13,112	12.82	2,047,205	16.76
1940-1959	342	9.12	13,912	13.60	2,834,883	23.21
1939 or earlier	199	5.31	6,352	6.21	1,152,550	9.44
Total	3,750	100.00	102,275	100.00	12,214,549	100.00

E. Occupancy Type. Approximately 21 percent of the homes in Cambria are occupied only on a seasonal basis. Those homes are not usually available for local permanent residents. Occupancy type describes who actually occupies a dwelling unit-and is also referred to as “tenure” (see Table 2-12). The percentage of homes not occupied permanently is the “vacancy rate.”

Table 2-12
Tenure of Housing Units - 2000

Occupancy	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Occupied Units:						
- Occupied by owners	2,080	73.86	57,001	61.46	6,546,334	56.91
- Occupied by renters	736	26.14	35,738	38.54	4,956,536	43.09
Total Occupied Units	2,816	100.00	92,739	100.00	11,502,870	100.00
Vacant Units:						
- for rent	74	7.91	1,183	12.41	190,321	26.74
- for sale	39	4.17	619	6.49	92,197	12.95
- rented or sold	29	3.10	621	6.51	50,846	7.14
- seasonal use	738	78.85	6,179	64.80	236,857	33.28
- held for migrant workers	0	0.00	27	0.28	2,205	0.31
- other vacant	56	5.98	907	9.51	139,253	19.57
Total Vacant Units	936	100.00	9,536	100.00	711,679	100.00

E. Economy

1. Background History.

In the years since its founding, Cambria's principal economic activities have experienced broad changes. Once an important service center for pioneer residents of the adjacent coastal region, ~~valuable products were locally produced~~ products that included whale oil, lumber, mercury, gold, and dairy products. Many of these were exported. Eventually, depletion of mineral resources and the replacement of coastal shipping by inland transportation diminished Cambria's position of economic importance in the county.

While Cambria continues to provide limited services to nearby agricultural areas, its role as a resort and retirement community has steadily grown in importance since the 1920's. Today, visitors come to enjoy the pleasant natural setting, the seashore, and numerous recreational opportunities, such as art, craft and antique shops, and fine restaurants. ~~that are Cambria's most valuable economic assets.~~ The sale of lots and construction of homes also adds significantly to income-producing activities ~~carried on~~ in the community.

Besides retirement, the economy of the North Coast Planning Area is oriented to serving the surrounding agricultural areas and tourists visiting the central California coast. The major tourist attraction is Hearst San Simeon State Historical Monument, which attracts over one million 950,000 visitors annually. Also, the beauty and tranquility of the coastal area extending from Cambria to Big Sur continues to draw ~~the~~ travelers along the coastal corridor. Most of the tourist visits occur during the summer vacation months; however, year-round tourist use is increasing. San Simeon Acres and Cambria both are oriented to serving the traveling public. ~~Tourist-serving commercial centers, primarily lodges, are identified for several small areas within Hearst Ranch.~~

Most of the local work force is involved in service or retail trade. The lack of economic diversity and high dependence on tourism is a potential problem affecting Cambria's future economic stability. With tourism as the major economic base, efforts in this plan and by community groups are directed toward strengthening this base by adding new attractions, and diversifying the economy. Another sector for employment may be providing goods and services for a growing number of retirees. ~~Cambria as a major tourist center.~~

Economic development is oriented to tourism and recreation. Motels, restaurants, and specialty retail shops serving visitors to the central coast have been developed. Continued economic growth of this type is anticipated in accordance with increasing visitor demand and successful implementation of programs to enhance basic services, such as water supply. Expansion of attractions ~~that would~~ to keep visitors in the central coast area for longer periods of time would substantially also increase

economic benefits to the area.

2. Goals and Objectives.

The following section reiterates the general goals for Cambria and San Simeon Acres in this Plan concerning economics:

A. Economy. Promotes a strong, diverse and viable local economy by:

- 1.** Pursuing planning policies that balance economic, environmental and social needs of coastal areas.
- 2.** Recognizing the importance of tourism and agriculture as significant parts of the economic base of the coastal areas.
- 3.** Considering the economic and fiscal effects of land use planning decisions. Encourage new development projects to employ locally-based workers in order to reduce traffic and resulting air pollution.

B. Visitor Serving, Recreation and Resort Development. Preserve and enhance visitor opportunities in appropriate locations as an important part of the economy by:

- 1.** Implementing the California Coastal Act by acknowledging that public opportunities for recreation have priority over private residential, non-visitor serving commercial or industrial development, but not over agriculture or coastal dependent industry.
- 2.** Requiring that new destination resorts and recreational development such as hotels, conference centers and outdoor recreation to be located within or adjacent to urban village areas, or to existing visitor destination points. (Mod 7)
- 4.** Balancing carefully the needs of visitors with those of the local residents. Emphasis should be on careful management of diminishing resources such as water, sewage disposal, and traffic capacity.

C. Commercial and Industrial Land Uses. Designate commercial and/or industrial areas that are compatible with overall land use by:

- 1.** Designating commercial and/or light industrial areas compatible with overall land uses that are convenient to users, and are realistically related to market demand and the needs of the community.
- 2.** Designating visitor-serving and community-serving commercial areas that are located near existing similar development and their users.
- 3.** Creating and preserving desirable downtown and neighborhood business characteristics, such as compatible uses, safe employment areas, sense of scale, attractive landscaping, pedestrian ways and other amenities.

3. Employment Profile.

The following data from the 2000 census describes the profile of workers in Cambria as compared to the county and the state.

A. Labor Force.

Table 2-13
2000 Labor Force (16+ Years) - 2000

	Cambria	County	State
Males in labor force	1,503	63,325	8,765,269
- Civilian males employed	1,432	59,451	8,045,350
- Civilian males unemployed	68	3,670	587,862
Females in labor force	1,430	53,543	12,998,409
- Civilian females employed	1,394	50,218	6,673,578
- Civilian females unemployed	36	3,241	522,412
Labor Force Population in Armed Forces	2	288	148,677
Total Population not in Labor Force	2,472	83,704	9,618,265
Total Population in Labor Force	2,932	116,868	15,977,879

B. Industry and Employment.

**Table 2-14
Industry (16+ Years) - 2000**

	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Agriculture, forestry, fishing and hunting, and mining	75	2.65	4,134	3.77	282,717	1.92
Construction	216	7.64	8,642	7.88	915,023	6.22
Manufacturing	46	1.63	7,772	7.09	1,930,141	13.11
Transportation/ Warehousing/ Utilities	95	3.36	4,975	4.54	689,387	4.68
Wholesale trade	34	1.20	2,721	2.48	596,309	4.05
Retail trade	436	15.43	13,561	12.37	1,641,243	11.15
Finance, insurance, real estate, and rental and leasing	145	5.13	5,545	5.06	1,016,916	6.91
Information	42	1.49	2,907	2.65	577,463	3.92
Professional, scientific, management, administrative, and waste management services	315	11.15	10,336	9.42	1,711,625	11.63
Educational, health and social services	441	15.61	23,923	21.81	2,723,928	18.51
Arts, entertainment, recreation, accommodation and food services	705	24.95	12,500	11.40	1,204,211	8.18
Other services (except public administration)	158	5.59	5,883	5.36	761,154	5.17
Public administration	118	4.18	6,770	6.17	668,811	4.54
Total	2,826	100.00	109,669	100.00	14,718,928	100.00

C. Type of Worker.

**Table 2-15
Type of Worker - 2000**

	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Private wage and salary workers	1,629	57.64	60,720	71.45	11,257,393	76.48
Government workers	505	17.87	3,281	3.86	2,158,071	14.66
Self-employed workers in own not incorporated business	612	21.66	12,639	14.87	1,249,530	8.49
Unpaid family workers	80	2.83	8,341	9.82	53,934	0.37
Total	2,826	100.00	84,981	100.00	14,718,928	100.00

D. Place of Work. In the county, a high percentage of people work in the same community in which they live (see Table 2-16). This situation reflects a better jobs/housing balance than that in the rest of the state. Cambria has a better jobs/housing balance than either the county or the state.

**Table 2-16
Place of Work - 2000**

	Cambria		County		State	
	Persons	%	Persons	%	Persons	%
Work in community of residence	1,114	53.38%	35,237	45.28%	5,061,526	39.78%
Work outside community of residence	973	46.62%	42,578	54.72%	7,661,711	60.22%

E. Travel Time to Work. Travel time to work is related to the balance of jobs and housing in an area. Based on 2000 census data, most working residents in Cambria also work in their community.

Table 2-17
Travel Time to Work - 2000

	Cambria		County		State	
		%		%		%
0-9 minutes	1,050	41.57	23,665	23.30	1,605,146	11.49
10-14 minutes	398	15.76	19,655	19.35	1,930,263	13.82
15-19 minutes	305	12.07	15,431	15.19	2,157,970	15.45
20-24 minutes	127	5.03	13,648	13.44	2,004,060	14.35
25-29 minutes	76	3.01	5,538	5.45	782,241	5.60
30-34 minutes	141	5.58	10,560	10.40	2,025,657	14.50
35-44minutes	210	8.31	4,118	4.05	894,530	6.40
45-59 minutes	114	4.51	4,040	3.98	1,151,598	8.24
60-89 minutes	48	1.90	2,494	2.46	933,123	6.68
90 or more minutes	57	2.26	2,430	2.39	483,698	3.46
Total workers who did not work at home	2,526	100.00	101,579	100.00	13,968,286	100.00
Mean travel time (minutes)	19		21.1		27.7	

4. Tourism and Overnight Units. Tourism continues to be a major portion of the economic base for the North Coast area. Major attractions are Hearst Castle and the scenic coastline. Overnight units are hotel, motel, and bed & breakfast rooms in existing facilities. There are also approximately 200 campsites at San Simeon State Park in the north part of Cambria. In 2005, there were approximately 240 vacation rental units in Cambria.

A. Existing Hotel/Motel Units. The following table shows existing overnight units in the North Coast as compared to other coastal communities.

**Table 2-18
Existing Hotel/Motel Units - 2003**

North Coast		Coastal Communities	
Area	Units	Area	Units
San Simeon Acres	706	North Coast	1,418
Cambria	733	Cayucos	122
North Coast Rural	45	Los Osos	48
		Morro Bay	946
		Pismo Beach	1,855
		San Luis Obispo	1,885
Total	1,484.00	Total	6,274.00

CHAPTER 3: PUBLIC FACILITIES, SERVICES, AND RESOURCES



(Note that former Chapter 5, RESOURCE MANAGEMENT and Chapter 3, PUBLIC SERVICES AND FACILITIES have been combined and updated in this new chapter.)

Population growth and changes in land use can occur without creating community problems if the public facilities, services, and resources necessary to support such change are adequate, and integrated with community design. This chapter describes the facilities, services and resources available in the Planning Area. The *Framework for Planning*, a separate document, contains a general discussion of the appropriate levels of such services for both urban and rural areas.

In the case of resources such as water supply and sewage disposal, ‘planned capacities’ are determined largely by environmental factors, although other community values may also be involved. For example, in the absence of a decision to import water from outside the Planning Area, this Plan is based upon the assumption that water supply for the entire north coast, including Cambria and Sam Simeon Acres, is limited to that which can be can be obtained locally from groundwater extraction, desalination, reclamation, and conservation.

A. Relationship to Planning Goals, Policies and Programs

The General Plan and Local Coastal Program contain goals that apply to public facilities, services, and resources. Several goals of the plan are:

- 1. Balancing growth and resources.** Balance the capacity for growth allowed by the Land Use Element and Local Coastal Plan with the sustained availability of resources.
- 2. Provide timely services.** Provide additional public resources, services and facilities to serve existing communities in sufficient time to avoid overburdening existing resources, services, and facilities.
- 3. Planning and resource management.** Avoid the use of public resources, services, and facilities beyond their renewable capacities. Plan for and monitor new development through the Resource Management System and Growth Management Strategies to ensure that resource demands will not exceed existing and planned capacities or service levels.
- 4. Financing and facility planning.** Finance the cost of additional services and facilities from those who will benefit, such as residents, businesses, public agencies, and visitors. When consistent with local, State and federal law, finance methods may include dedications, development impact fees, in-lieu fees, assessment districts, or other exactions to assure that adequate services are available.
- 5. Facility locations.** Locate new public service facilities as close as possible to the users.

B. Service Providers in the Planning Area

Public services are provided to county residents by a variety of jurisdictions, including incorporated cities, community service districts (CSD's), county service areas (CSA's), single-purpose special districts, school districts, and by the County of San Luis Obispo. A community service district (CSD) is a locally-governed body authorized to provide a variety of public services, with the exception of land use planning. A CSD typically has an elected governing board with full financial and operational responsibilities.

~~Appropriate levels of service for urban, suburban and rural areas are discussed in Chapter 5 of Framework for Planning. The following is a discussion of service concerns that directly affect the North Coast planning area.~~

~~A. SPECIAL DISTRICTS~~

~~Three special districts provide a wide range of services to the North Coast planning area. The Cambria Community Services District was formed in 1976 through reorganization of a number of single-purpose districts, and provides water, sewers, street lighting, fire protection and garbage collection services.~~

~~San Simeon Acres Community Services District provides street lighting, sewer, water, street maintenance and fire protection.~~

~~Cambria Public Cemetery District serves the entire northwestern portion of the county. The cemetery is located north of the community of Cambria.~~

Principal services in the Planning Area are provided by two community service districts, a school district, and the County. The following list indicates the major service providers in the Planning Area and the services they currently provide:

Cambria Community Service District

Water Supply

Sewage Disposal

Street Lighting

Surf/Ocean Rescue Services

Fire/Emergency Services

Garbage Collection

Parks & Recreation

San Simeon Acres Community Service District

Water Supply

Sewage Disposal

Road Maintenance

Street Lighting

Fire Protection

Coast Unified School District

Kindergarten through twelfth grade

Cambria Cemetery District

Cemetery Maintenance and Management

Cambria Community Health Care District

Ambulance Service

County of San Luis Obispo

Parks and Recreation

Planning and Building

Engineering and Road Maintenance

Fire Protection

Police Protection

Social Services

Health Services

Tax Collection

Libraries

Animal Regulation

Municipal Courts

District Attorney

State of California

CalTrans (Highway One and 46)

State Department of Parks and Recreation

(Hearst Castle, W.R. Hearst Memorial State Beach,

San Simeon State Park campground)

California Department of Forestry and Fire Protection

Future special districts may be needed to provide services in the planning area. Proposed developments on the Hearst Ranch, for example, will necessitate the provision of water, sewage disposal, and other facilities managed by an agency.

C. Monitoring the Availability of Resources: The Resource Management System (RMS)

The Resource Management System (RMS) provides an alert process for timely identification of potential resource deficiencies, allowing sufficient lead time to correct or avoid a problem. Potential and actual resource deficiencies are given the following "level of severity" designations indicating the relative urgency of each situation.

The RMS System - Level of Severity Designations:

Level of Severity I - Resource Capacity Problem:

Level I is an early threshold when data suggests that the capacity of the resource will be reached within a specified time period, but where sufficient time remains to plan and implement corrective measures before the problem becomes critical.

Level of Severity II - Diminishing Resource Capacity:

Level II occurs when the current rate of resource use will deplete the resource before its capacity can be increased. This is a point at which a public works project must be initiated, and if necessary, actions taken that will extend the time available to correct the resource deficiency.

Level of Severity III - Unavoidable Resource Deficiency:

Level III occurs when the capacity of a resource has been met or exceeded. This situation suggests that actions may need to occur to protect public health and safety.

Resources monitored by the Resource Management System are:

Water Supply
Schools
Air Quality

Sewage Disposal
Roads

The resource capacities and levels of severity contained in this Plan reflect conditions as of the end of 2003. However, resource supply and demand factors are constantly changing. The *Annual Resource Summary Report*, prepared by the County, is updated each year to reflect these changes and to recommend appropriate levels of severity. The Resource Management System is also discussed in *Coastal Zone Framework for Planning*.

Relationship to the Capital Improvements Program (CIP)

The Resource Management System (RMS) provides early information to decision-makers about alternative actions that may be taken to avoid a resource deficiency. Where the funding of a capital project may be needed to correct the deficiency, the RMS links that needed project to the Capital Improvement Program in the budget of the agency responsible for funding construction of the project. If the necessary project is not or cannot be funded, an advisory memo is sent to the Board of Supervisors identifying alternative actions that may be necessary to avert the deficiency or to prevent it from worsening until the needed capital project can be constructed.

~~The primary purpose of the Resource Management System (RMS) is to provide an alert process for a timely identification of potential resource deficiencies so that sufficient lead time is allowed for correcting or avoiding a problem. This chapter initiates the resource management system by summarizing assessments of the major resources of water supply, sewage disposal, schools, and road capacity. In conjunction with those assessments, population thresholds have been estimated for three levels of severity for each resource. Since population thresholds are estimates, however, changes in population growth, resource consumption or other factors may change the estimated thresholds. Data developed for this report will be reviewed and updated annually as part of the general plan review process.~~

~~Resource capacity information is included in this area plan to support ongoing county review of needs for capital programs and provide information to the public on the status of county resources. This information is not to be used for reviewing individual development proposals or their consistency with the general plan. The use of resource capacity information by the county to evaluate individual development proposals can only occur through separate hearings and enactment of ordinances outside of the general plan. (An explanation of this procedure is in Framework for Planning, Chapter 4.)~~

D. Areawide: Status of Public Facilities, Services and Resources

Because many services for the Cambria and San Simeon Acres urban areas are inextricably linked to areawide services, this section provides an overview of services and resources for the entire North Coast Planning Area. Facilities, services and resources for the communities of Cambria and San Simeon Acres are discussed in more detail in later sections. Services included in the Resource Management System are identified by "(RMS)" following the heading. The information is the best available at the time of preparing the Plan, however, the status of the RMS resources is frequently updated as new information becomes available. Current information, including levels of severity, may be found in the most recent edition of the *Annual Resource Summary Report*.

The RMS resources that appear to be experiencing deficiencies are summarized in Table 3-1 C. The table includes population thresholds, which indicate how the level of severity changes as demand for a resource increases with population growth, assuming no increase in resource capacity. Verification of the level of severity will occur after public hearings and Board of Supervisors' action to certify the documentation on which these assessments are based.

~~TABLE C
RESOURCE SEVERITY LEVELS & POPULATION THRESHOLDS
NORTH COAST PLANNING AREA~~

CAMBRIA URBAN AREA			
Resources	I	H	HH
Water Resources	*	*	*
Water System	*	*	*
Sewage Treatment Plant	*	*	*
Schools †			
Elementary	2,700	2,800	3,500
High School	4,600	4,900	5,700
Roads/Circulation	**	**	**

~~*Facilities will be adequate through year 2000.~~

~~**Data difficult to correlate to residential absorption due to heavy use by visitor traffic. Require project review to determine availability of road capacity.~~

Notes:

~~†Population thresholds are for the North Coast planning area, with the effects of Cayucos' population on high school enrollments taken into account.~~

Table 3-1
2004 Recommended Resource Severity Levels & Population Thresholds
North Coast Planning Area

Resource	Levels of Severity (1)		
	I	II	III
	POP	POP	POP
WATER SUPPLY			
CAMBRIA (2)	X	X	X
SAN SIMEON ACRES (3)	X	X	X
SEWAGE TREATMENT			
CAMBRIA (4)	8,982	9,141	9,936
SAN SIMEON ACRES (5)	362	398	641
SCHOOLS			
CAMBRIA GRAMMAR (7)	X	X	X
SANTA LUCIA MIDDLE (7)	X	X	X
COAST UNION HIGH (6)	X	X	12,900
AIR QUALITY (8)	X	X	
ROADS/CIRCULATION			
Highway One (9)	X	X	X
Main St., Cambria	X	X	X

NOTES: Data from 2000 Census, and 2004 County Annual Resource Summary Report.

X LEVEL OF SEVERITY FOR CATEGORY ALREADY EXCEEDED.

- (1) DATE AND PROJECTED POPULATION BASED ON 1990 CENSUS DATA, 2.3% ANNUAL GROWTH RATE EQUIVALENT TO HISTORIC GROWTH RATE.
- (2) SAN SIMEON AND SANTA ROSA CREEKS ARE PRIMARY WATER BASINS. BECAUSE OF REOCCURRING DRY SEASON SHORTAGES, OVERALL RMS LEVELS IS III.
- (3) BASED ON SEASONAL SHORTAGES AND SSCSD MORATORIUM.
- (4) TREATMENT PLANT AT 64% OF CAPACITY.
- (5) TREATMENT PLANT AT 35% OF CAPACITY, SINCE COMPLETION OF RETROFIT PROGRAM.
- (6) SCHOOL SERVES OTHER AREAS OF NORTH COAST AND ESTERO PLANNING AREAS. LEVEL III POPULATION THRESHOLDS ARE: CAMBRIA, 7,650; CAYUCOS, 4010; RURAL NORTH COAST, 1,240
- (7) CAMBRIA STUDENTS ONLY. CURRENTLY THE DISTRICT IS ON SCHEDULE TO OPEN THE NEW ELEMENTARY SCHOOL IN THE FALL OF 2005. THE SCHOOL WILL ADEQUATELY ACCOMMODATE THE CURRENT K-5 ENROLLMENT AND WILL BE ABLE TO GROW TO A FUTURE ENROLLMENT OF 500 STUDENTS. THE RECOMMENDED LEVEL OF SEVERITY IS EXPECTED TO CHANGE FROM LEVEL III TO OK FROM 2004 TO 2005.
- (8) AIR QUALITY FOR ENTIRE COUNTY IS LEVEL II, 75% OF THRESHOLD LEVEL. NO ESTIMATE FOR RMS LEVEL III.
- (9) BASED ON CAL TRANS ESTIMATES OF LEVEL OF SERVICE 'D', WITH LEVEL DECLINING TO 'E', IN SUMMER MONTHS.

~~Adequate water and sewer capacity exists to the year 2000. However, Cambria and the rural areas may experience potential school deficiencies before year 2000. San Simeon Acres appears to have adequate water supply and sewage capacities for projected growth and will not contribute significantly to school enrollments. Proposed tourist facilities will require development of water and sewage disposal facilities, but these resources and needs will be studied in conjunction with preparation of development plans. The impact of community growth and development of tourist facilities and vacation homes on Highway One traffic will also require study.~~

1. Water Supply (RMS)

~~Water Resources~~

Ensuring adequate water is essential for the future development of the North Coast Planning Area. Decisions involving ~~the need for providing provisions for~~ additional water supplies will have far-reaching implications ~~for all of the area.~~

Estimates of the adequacy of water resources in the North Coast Planning Area vary, depending on the definition of "water supply" upon which they are based, and upon assumptions about "demand." Estimates of the adequacy of water resources in the North Coast Planning Area vary depending upon the volume and timing of seasonal rainfall, as well as the balance that must be maintained between municipal, agricultural, and environmental issues. All of the north coast creeks that are tapped for such uses could be subject to dry season limitations depending upon the type of rainy season that occurs, as no imported water is provided to the area. Summer months also have the highest demands due to increased occupancy, tourism, and irrigation requirements. Cambria, for example, based on its legal entitlement, does not have enough water to support its residential and commercial uses without demand management, and it is likely that the actual amount of water available is less than the legal entitlement. Thus, as a practical matter, the community's seasonal demand is now greater than the available supply in years of below average rainfall. All of the north coast creeks are subject to summertime limits due to reduced rainfall. Summer months also have the highest demand for water because of increased occupancy of vacation homes and hotel rooms, and tourism.

~~Local water resources in the North Coast Planning Area appear to be adequate for growth needs through the year 2000. The conflict between expanding domestic uses and expanding agricultural uses of water is minimal; lessened because of limitations of climate and topography for expanding irrigated uses, and; There are, however, concerns about the natural environmental impacts resulting from reduced groundwater tables and stream flows.~~

~~B. UTILITY SERVICES~~

~~Most of the groundwater basins in the northern part of the North Coast Planning Area are presently undeveloped; have relatively small storage capacities and are maintained by percolation of stream flow and precipitation. The four major five basins are San Carpoforo,~~

Arroyo de la Cruz, Pico, San Simeon, and Santa Rosa, each named for the principal streams which traverse them. Villa Creek forms the Planning Area's southern boundary. These groundwater basins are shown in Figure 3-1. The maximum safe yield of the four basins is estimated to be at least 4,700 acre-feet per year.

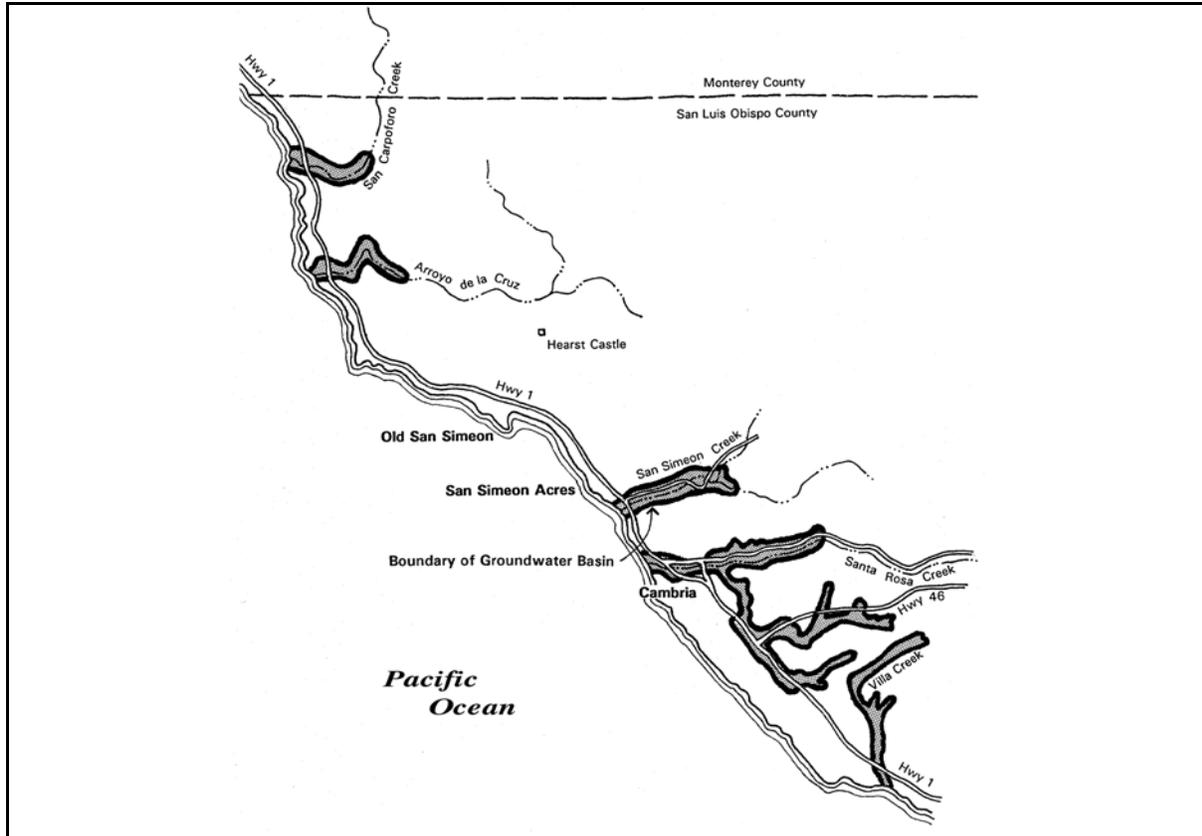


Figure 3-1: North Coast Groundwater Basins

~~Although irrigated agriculture uses some of the available water, only minimal expansion of irrigated agriculture is foreseen. Growth in water demands will occur due to the expected influx of population and tourists.~~

Long-term demands for supplemental water should be integrated into the planning process for both the north and central coast areas. ~~The most A likely Possible future water sources for population centers of the North Coast Planning Area are is either the Nacimiento project, or surface storage developed on, or near, one of the coastal creeks, and desalination.~~ In the near term, the most feasible approaches appear to be conservation, retrofit programs, reclamation of waste water for irrigation, and desalination.

~~Groundwater basins and safe yields are indicated in Table D~~

While the use of desalinated seawater could prevent potential over-pumping of shallow aquifers such as Santa Rosa Creek, it would be much less expensive to continue to rely on

the creeks for more water, there must be adequate conditions on the pumping of the creek aquifers to prevent negative impacts to the area's natural resources and agriculture in the short and long-term. As community growth continues, and with it increased demand for water, care must be taken to protect the ground water basin from salt water intrusion and loss of capacity, and to preserve natural plant, animal, and aquatic habitats.

GROUNDWATER RESOURCES - NORTH COAST PLANNING AREA		
GROUNDWATER BASIN	PRINCIPAL USE	SAFE-YIELD ACRE-FT/YR
SANTA ROSA CREEK (1)	AGRICULTURE	875
	DOMESTIC USE/ GAMBRIA	518
SAN SIMEON CREEK (1)	DOMESTIC USE/ GAMBRIA	1,230
PICO CREEK (2)	DOMESTIC USE/ SAN SIMEON ACRES	140
ARROYO DE LA CRUZ (3)	NOT USED	430
SAN CARPOFORO CREEK (3)	NOT USED	*

NOTES:

(1) Coastal Valley Engineering Inc., "Report on Proposed Water System Improvements and Master Plan," Cambria County Water District, 1976.

(2) Master Water and Sewerage Plan, County of San Luis Obispo, 1972-1986.

In addition to groundwater resources, the planning area contains long range possibilities for water supply reservoirs on the principal creeks. The following table is based on information from the Master Water and Sewerage Plan indicates potential alternative sites and safe yields. Alternatively, smaller water conservation projects could be established on these creeks or smaller creeks or tributaries for local needs. These projects, however, are presently considered unlikely because of unfavorable cost/benefit ratios and environmental impacts.

Table E
Potential Reservoir Sites in
North Coast Planning Area

DAM AND RESERVOIR	STREAM	GROSS STORAGE (AF)	SAFE YIELD (AFY)
BALD TOP	SAN CARPOFORO	20,000	10,400
UPPER RAGGED POINT	SAN CARPOFORO	30,000	17,500
SAN SIMEON	SAN SIMEON CREEK	60,000	18,200
YELLOW HILL (ALTERNATIVE DAM HEIGHTS)	ARROYO DE LA CRUZ	20,000 50,000 80,000	13,100 22,900 27,300
SANTA ROSA (ALTERNATIVE DAM HEIGHTS)	SANTA ROSA CREEK	15,000 25,000 35,000	7,300 9,200 11,000
-SOURCE: ——— MASTER WATER AND SEWERAGE PLAN, COUNTY OF SAN LUIS OBISPO, 1972.-			

The most likely source of future water supplies, with Cambria as the principal potential user in the North Coast planning area, is additional extraction from San Simeon Creek or other local water courses. Cambria has not requested an allocation from the Nacimiento water project.

***(The Rural Area is not part of this Plan Update.) ***

(The paragraph that is in the current North Coast Area Plan relates to the rural area and is not part of this Plan update.)

2. Sewage Disposal (RMS)

~~Sewage disposal in rural and agricultural portions of the planning area is served entirely by septic tanks.~~ Sewage disposal in Cambria is provided by the Cambria Community Services District (CCSD). The San Simeon Acres Community Services District provides service for San Simeon Acres and Hearst San Simeon State Historical Monument. Sewage disposal in Cambria and San Simeon Acres is discussed in more detail in sections E.2 and F.2 below.

3. Solid Waste Disposal

Rural areas must rely on individual disposal for solid waste. Solid waste disposal is by ~~franchise private~~ contractors serving San Simeon State Park and Beach, Hearst Castle, Cambria U.S. Air Force Station, and San Simeon Acres. The Cambria Community Services District contracts for disposal service within the district boundary. The Los Osos Landfill has been closed since 1988, and the former disposal site located north of Cambria has closed several years before that. ~~and a~~All waste is now being hauled to the Cold Canyon Los Osos landfill site 7.5 miles south of San Luis Obispo. ~~A centralized transfer station is needed near Cambria. Consideration should be given to a new site within the planning area to avoid long trips and wasting energy.~~

A regional composting facility exists to process greenwaste, including the disposal of significant amounts of cut trees and stumps exists at the Cold Canyon Landfill. Currently, greenwaste disposal is available in Cambria but not San Simeon Acres or rural area. As residential growth continues in the Cambria Monterey pine forest, trees are removed to accommodate new homes. Likewise, with increased awareness of dangerous tree situations and disease, more trees are likely to be cut. Infected material will need special attention to prevent further spread of diseases such as pine pitch canker and bark beetles.

4. Drainage

Drainage courses include many coastal streams leading from the mountains to the ocean. The more significant ~~watersheds ones~~ are Santa Rosa Creek, San Simeon Creek, Pico Creek, Little Pico Creek, Arroyo de la Cruz and San Carpoforo Creek. These are anadromous fish streams ~~primarily known for steelhead which provide habitat for, among other species,~~ steelhead trout (*Oncorhynchus mykiss*) and red-legged frog (*rana aurora draytonii*), a species listed by the federal government as threatened on the California south-central coast. Flood hazards exist during periods of intense or prolonged rainfall on portions of these watersheds.

Santa Rosa Creek is the most significant in this regard because it passes through Cambria. Precautions must be taken with development in minor tributaries and swales ~~as well, for~~ because these areas also collect substantial runoff. Increased runoff in the urban areas can be expected with further development of paved streets and residential construction.

Portions of Cambria, along Main Street (West Village), have been classified by the Federal Emergency Management Agency (FEMA) as being located within 100-year flood hazard

zone on Santa Rosa Creek. The West Village was under several feet of water during 1995 storms.

The combination of the area's steep topography, lack of drainage facilities, unpaved roads, and location of residential parcels below the street grade has resulted in localized poor drainage and/or flooding around some residences, buildings, and roadways.

Flooding problems along Santa Rosa Creek in the West Village are being addressed by the construction of a by-pass channel for Santa Rosa Creek. The by-pass channel will allow overflows to move slowly through the by-pass channel and then rejoin the Santa Rosa Creek downstream.

C. EMERGENCY AND SOCIAL SERVICES

5. Police Service

~~The entire North Coast Planning Area is served by the County Sheriff and California Highway Patrol. Response times vary with distance from the main station on Highway One at Camp San Luis Obispo.~~

Law enforcement services for the North Coast Planning Area are provided by the California Highway Patrol (CHP), County Sheriff, and the State Parks Ranger (State Park property). A mutual aid agreement exists between the three agencies. Response times vary due to the location of units.

The CHP is primarily responsible for traffic-related calls along Highway 1 and the local streets in Cambria. The California Highway Patrol dispatches one unit to the Cambria area from the Templeton Station. They will not investigate, take action or respond to crimes in progress in residential, commercial or industrial areas. They may respond upon request as backup to the Sheriff's Department Response, if available, however the CHP doesn't normally provide police services.

The County Sheriff responds to civil and criminal enforcement calls and provides coroner services for the County. There is currently one Sheriff patrol car assigned to an area, which extends from Cayucos to the County line north of San Simeon.

6. Fire Protection

~~Fire protection for the rural area is provided by the California Department of Forestry with the headquarters located in Cambria. A lookout base is maintained on Rocky Butte.~~

The Cambria CSD Fire Department provides fire protection within th CCSD boundaries. The California Department of Forestry and Fire Protection (CDF) and the San Luis Obispo County Fire Department (County Fire) provide fire protection to all unincorporated lands

outside the CCSD boundaries. The station has one wildland engine, one structure engine and one rescue engine and is located at the north end of Cambria to serve the entire North Coast Planning Area. Response times within the North Coast Planning Area range from 5 to 30 minutes, varying with distance from the station. The CDF provides an additional fire engine during the declared wildland fire season, normally May through October. An automatic aid agreement exists between CDF and the Cambria CSD Fire Department. (The Cambria Fire Department is discussed in greater detail in the Cambria section below.)

Fire protection in San Simeon Acres is provided through a contract with CDF/County Fire in Cambria. an all volunteer fire unit of the Community Services District. Equipment is maintained at the California State Department of Forestry CDF Station in Cambria.

7. Emergency Medical Services

Emergency medical assistance is provided by all fire service organizations in the North Coast Planning Area. The majority of firefighters in the CDF/County Fire Department are EMT trained. Typically, the fire service units are on the scene prior to law enforcement and ambulance units because of the fire station locations. Response times range from 5 to 30 minutes.

County Service Area No. #15 provides ambulance service to the southern extremes of the North Coast planning area. The remainder of entire North Coast Planning Area, and a portion of southern Monterey County, is served by the Cambria Community Hospital Health Care District which operates both in and out patient clinic facilities and provides paramedic ambulance service. The CCHD provides one full-time ambulance at all times that is staffed with EMT personnel. At the current time, emergency medical service has been considered barely adequate. Expansion of service be required as population growth and visitor influx create increased demand.

8. Human Services

All County offices for provision of human services are located in the City of San Luis Obispo. A North Coast Regional Center in the Morro Bay area, to include the provision of human services, has been proposed as a long-range capital improvement project. Presently, in Morro Bay, there is a small health services center operated by the County. The decision as to which services or facilities will be built, and when, must ~~will~~ be reviewed and coordinated through the ongoing County Capital Improvements Program.

9. Schools (RMS)

The North Coast Planning Area is part of the Coast Joint Union High School District. The district maintains a senior high school providing grades 9 through 12 for the coastal area

~~extending north from Cayucos and including a large portion of the southern coastal area of Monterey County . The high school, located on the east side of the community on the north side of Santa Rosa Creek, presently serves around 267 students. Major improvements at the facility are being planned, no additional sites will be needed.~~

~~The Cambria Elementary School District provides education for grades K-8. The grammar school is located in the center of the community on Main Street. One additional school will be needed to serve the future population. In addition, may become necessary. The intermediate school is located on Schoolhouse Lane and serves grades 7 and 8. It can be expanded to accommodate projected growth. Students from the surrounding rural and agricultural areas are bused to these central locations in Cambria.~~

Prior to July 1, 1997, two school districts served the North Coast Planning Area: Cambria Union Elementary School District and Coast Union High School District. Effective on that date, the two districts were combined as the Coast Unified School District. Current facilities of the Coast Unified School District include the Cambria Grammar School, Santa Lucia Middle School, Coast Union High School and Leffingwell Continuation High School.

A new elementary school with a capacity of 500 students opened in 2005 with an enrollment of approximately 335 students. District offices will move into the main building of the former grammar school on Main Street after renovation. Future use of the other buildings has not been determined at this time.

School districts serving the North Coast Planning Area are shown in Figure 3-2.

10. Library

A new library was recently completed to serving the entire North Coast Planning Area The site is located on Main Street adjacent to the Joslyn Adult recreation Center in Cambria. Property has been purchased on Cornwall Street for the future site of a new library.

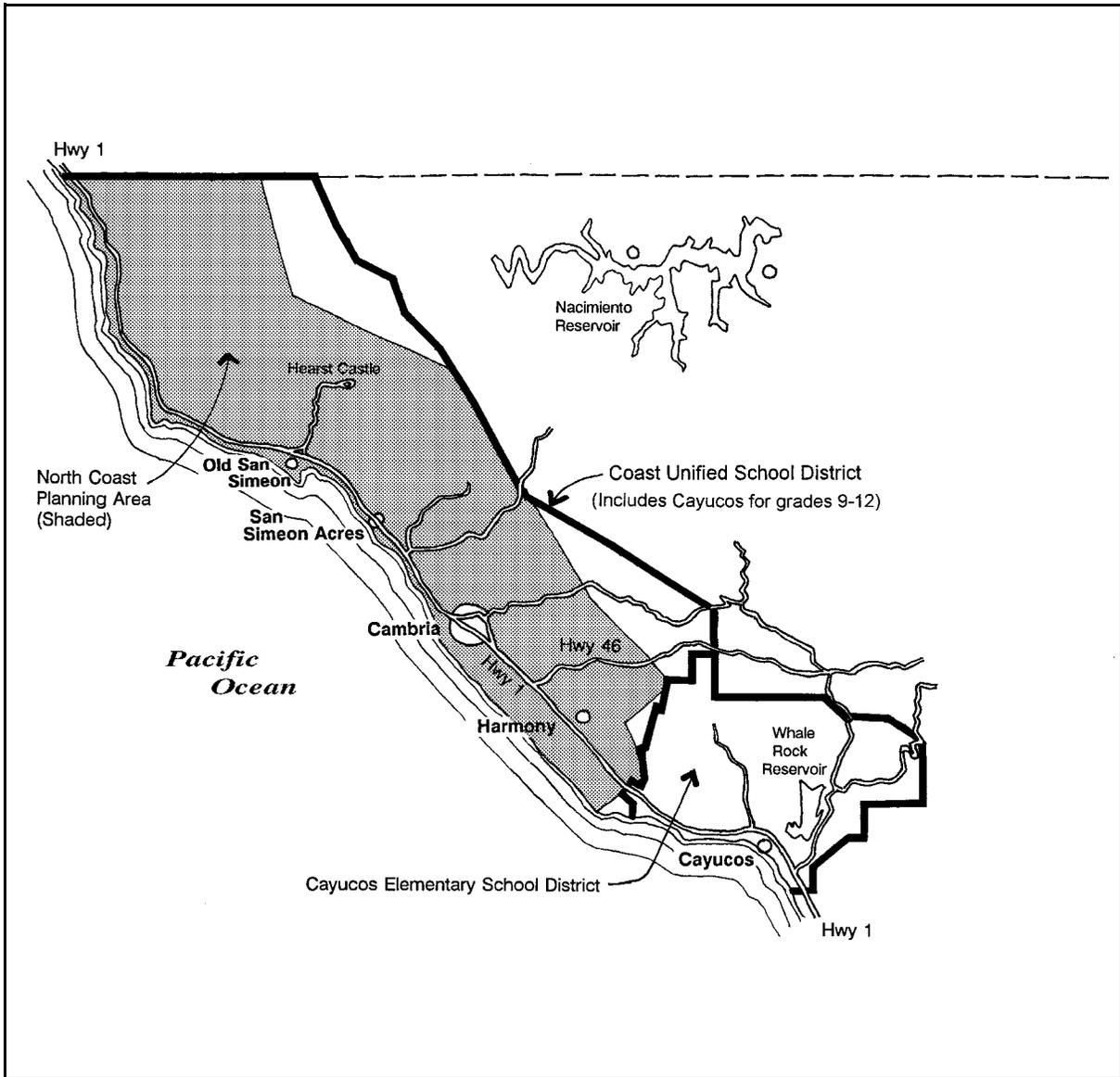


Figure 3-2: School Districts Serving the North Coast Planning Area

11. Recreation Services

The scenic ocean shoreline is the main feature attracting residential growth in Cambria and San Simeon Acres. Some of the beaches in Cambria and to the north, wherever Highway One closely follows the shoreline, are accessible for public recreation. Designated public shoreline areas; ~~however~~, are Shamel County Park in Cambria and State Beaches extending north to San Simeon Creek. Day use facilities are available at the County Park, Leffingwell ~~Creek~~ Landing, San Simeon State Park, Beach and W.R. Hearst State Beach at San Simeon, while overnight camping is available at the mouth of San Simeon Creek in San Simeon State Park Beach. The goal of the California Coastal Trail is to provide one continuous trail along the entire coast of the State of California. Developing a trail the length of the Planning Area would provide spectacular recreation opportunities not currently available to the public. Other recreational resources include the Jocelyn Center and Community Center.

Recreation and leisure time opportunities are also afforded by numerous tourist-oriented facilities, including Hearst Castle tours, restaurants, recreation facilities, and points of interest. The pine forests, creeks, and ocean shoreline of Cambria provide an excellent setting for ~~such~~ simple recreation activities such as walking and bicycle riding. The boardwalks along Moonstone Beach Drive and the coastal edge of Fiscalini Ranch are wheelchair accessible. As the community grows, however, additional neighborhood and regional parks and trails will be needed to provide conveniently located recreation facilities for persons of all ages and varied interests.

According to the County General Services Department, the north coast is deficient in neighborhood, community, and regional park facilities. Several neighborhood and community parks are needed in San Simeon Acres and Cambria, to make up an estimated deficiency of approximately 73 acres. One additional regional park facility is needed to make up a deficiency estimated at 269 acres. The East Ranch Park and the Cambria Historical Center/Park are in the planning stages and may be able to fill this deficiency. Increasing residential growth in San Simeon Acres will require consideration of a local park. The Plan suggests a rest area with picnic facilities and parking for shoreline access to serve both residents and tourists.

12. Roads/Circulation (RMS)

The impact of the combination of resident and tourist traffic on Highway One is a concern for the future, especially on the two-lane highway between the ~~Cambria~~ Hearst Castle and Cayucos areas. ~~However~~, ~~since~~ many residents of Cambria are ~~mostly~~ retired and/or locally employed ~~persons~~, highway usage tends to be more evenly distributed during the day; that is, the effect of morning and evening commuter rush hour traffic is ~~largely eliminated~~ somewhat diminished. Tourist traffic along Highway One between the Monterey Peninsula and San Luis Obispo, on the other hand, ~~may~~ tends to be somewhat heavier in the early morning and late afternoon. The principal need will be for safer circulation ~~to and from~~ entering, exiting, and crossing Highway One in Cambria and the surrounding area. Data should be compiled to determine the appropriate methods and timing for progressively needed appropriate safety measures.

13. Air Quality (RMS)

In January 2004, San Luis Obispo County was designated by the California Air Resources Board as being in attainment of the state ozone standard. This designation is based on the clean ozone record for years 2000 through 2002, when no violation of the state hourly ozone standard was measured at any of the county's six community based monitoring stations. Although two exceedences were reported in 2003, they do not constitute violations and do not jeopardize the attainment status. The county is however designated as a "non-attainment" area for the California PM10 (fine particulate matter) standard. There are no air quality monitoring stations in the North Coast Planning Area. Ozone and PM₁₀ levels recorded at the Atascadero, Morro Bay and Paso Robles stations are shown in Tables 3-3 and 3-4.

Ozone air quality appears to be improving countywide over the past ten years, which suggests that ongoing industrial and vehicular pollution controls are accomplishing their intended goals. Ozone, the primary constituent of smog, is formed in the atmosphere through complex photochemical reactions involving reactive organic gases (ROG) and oxides of nitrogen (NOx) in the presence of sunlight. Short-term exposure to higher concentrations of ozone can cause or contribute to a variety of respiratory ailments, while long-term exposure to lower concentrations may result in permanent lung damage. In San Luis Obispo County, the primary sources of ROG are motor vehicles, organic solvents, the petroleum industry, and pesticides. The primary sources of NOx are motor vehicles and fuel combustion by utilities, the petroleum industry, and other sources.

PM₁₀ is fine particulate matter ten microns or less in diameter, and consists of many different types of particles which vary in their chemical activity and potential toxicity. It can be emitted directly to the air by man-made and natural sources, or be formed in the atmosphere as a by-product of complex reactions between gaseous pollutants. PM₁₀ is particularly important from a health standpoint due to its ability to bypass the body's air filtering system, traveling deep into the lungs where it can lodge for long periods. Major sources of PM₁₀ in San Luis Obispo County include vehicle travel on paved and unpaved roads, demolition and construction activities, agricultural operations, fires, mineral extraction and wind-blown dust.

Table 3-3
Maximum Ozone Concentrations

Location	Number of Days Exceeding State Standard												
	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002
Atascadero	2	3	2	2	2	1	7	0	2	0	0	0	0
Morro Bay	0	1	1	0	0	0	0	0	0	1	0	0	0
Grover Beach	0	0	1	0	0	0	0	0	0	0	0	0	0
Nipomo	0	0	0	1	0	0	1	0	N/A	0	0	0	0
Paso Robles	0	0	0	0	1	5	9	0	25	1	0	0	0

Table 3-4
Maximum PM10 Concentrations

Location	Number of Days Exceeding State Standard (PM10 measurements are taken once every six days, or sixty times each year. Thus, a year in which six days had exceedences would have exceedences for 10% of all measured days.)												
	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002
Atascadero	3	3	0	5	1	3	0	1	0	<u>0</u>	<u>2</u>	<u>2</u>	<u>2</u>
Nipomo	3	0	0	1	1	1	0	N/A	0	<u>0</u>	<u>0</u>	<u>2</u>	<u>2</u>
Paso Robles	N/A	4	2	2	0	3	0	1	1	<u>1</u>	<u>2</u>	<u>2</u>	<u>2</u>
San Luis Obispo	0	1	0	1	1	1	0	2	0	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Ralcoa Way (Callendar-Garret)	12	10	8	19	12	14	12	16	12	<u>5</u>	<u>16</u>	<u>17</u>	<u>26</u>
Morro Bay	N/A	0	0	2	0	0	0	1	0	<u>0</u>	<u>0</u>	<u>0</u>	<u>1</u>
Guadalupe Road (Nipomo)	8	10	8	10	6	4	6	5	4	<u>4</u>	<u>7</u>	<u>9</u>	<u>5</u>

State law requires that emissions of nonattainment pollutants and their precursors be reduced by at least 5 percent per year until the standards are attained. The 1991 Clean Air Plan (CAP) for San Luis Obispo County was developed and adopted by the Air Pollution Control District to meet that requirement. The CAP is a comprehensive planning document designed to reduce emissions from traditional industrial and commercial sources, as well as from motor vehicle use.

Motor vehicles account for about 40 percent of the precursor emissions responsible for ozone formation, and 50 percent of direct PM₁₀ emissions. Thus, a major requirement in the CAP is the implementation of transportation control measures and land use planning strategies designed to reduce motor vehicle trips and miles traveled by local residents. All jurisdictions are expected to incorporate applicable strategies in their land use planning and project review process, to ensure that motor vehicle use and emissions resulting from existing and new development are minimized to the maximum extent feasible. As described in the County's Resource Management System, the County will implement applicable transportation and land use planning strategies recommended in the CAP through incorporation of these strategies in the County General Plan, focusing on the land use and circulation elements and updates of those elements for each of the County's planning areas.

E. Cambria: Status of Public Facilities, Services and Resources

This section discusses in more detail the status of water supply, sewage disposal, fire protection, recreation services, and roads/circulation in the Cambria area. For a discussion of solid waste disposal, drainage, police services, emergency medical services, human services, schools, library services, and air quality in Cambria, please see the relevant sections in Part D: Areawide Status of Public Facilities, Services and Resources.

The Cambria Community Services District (CCSD) operates a water system, sewage treatment plant, and fire department for the community of Cambria. The CCSD also provides street lighting and garbage collection. The CCSD service area extends beyond the Cambria Urban Reserve Line and is shown in Figure 3-3.

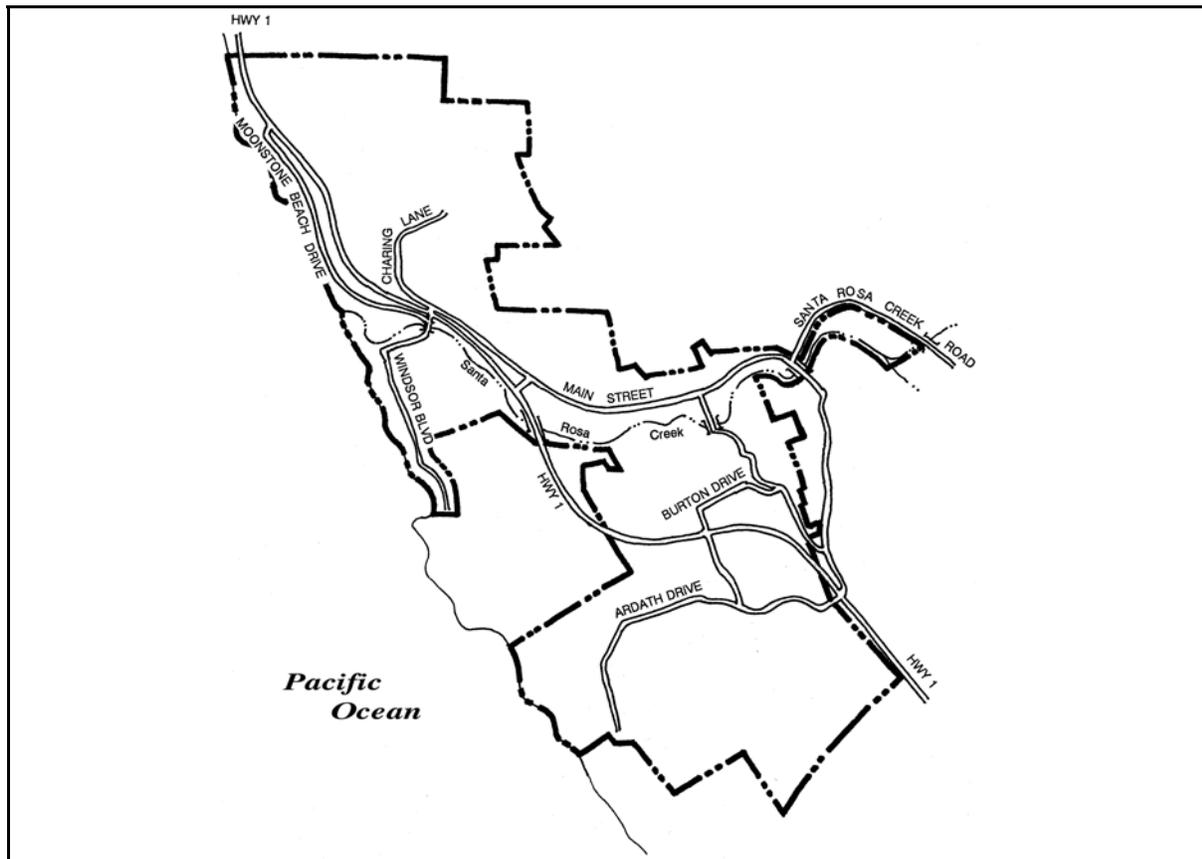


Figure 3-3: Cambria Community Services District Boundary

1. Water Supply (RMS)

Domestic water for Cambria was is provided by the Cambria Community Services District from wells located along Santa Rosa Creek and San Simeon Creek.

Water is provided to the community by The Cambria Community Services District, which was formed in 1976 to consolidate all community services. Due to water quality concerns and reduced yield during the prolonged drought, new wells were established on San Simeon Creek. The latest estimate of safe-yields for domestic use are 2,260 acre-feet per year for the Santa Rosa Creek groundwater basin and 1,040 acre-feet per year for the San Simeon Creek groundwater basin. There is a strong possibility that the water resource capacity will be greater on these than present estimates.

The Cambria Community Services District has received an apportionment permit from the State Water Resources Control Board of 1,230 acre-feet to be withdrawn 1,230 acre-feet per calendar year from San Simeon Creek. To protect the creek basin and ensure that withdrawal does not exceed the safe yield, the decision permit stipulates that only 572 370 acre-feet may be withdrawn from July 1 to November 20 cessation of streamflow at the Palmer Flats Gaging Station until October 31 each year. Generally, streamflow ceases during June, though it can occur earlier during drought conditions.

In November 1989, the State Water Resources Control Board approved a permit for the District to operate three wells on the Santa Rosa Creek basin and withdraw 518 acre-feet of water per year, with the condition that a maximum of 260 acre-feet be withdrawn from May 1 through October 31 each year. The permit further requires the District to monitor water quality and ground subsidence, and maintain in-stream habitat. The combined dry-season entitlement from both the San Simeon and Santa Rosa Creek basins is 630 acre-feet. Until recently, water was drawn from three wells located on Santa Rosa Creek. A severe water shortage due to the recent drought and poor water quality from the Santa Rosa Creek groundwater basin resulted in relocation of the community water supply to the San Simeon Creek basin, where the Cambria Community Services District has obtained water rights for 1,230 acre feet per year. These water rights will establish a reliable source of water for the community. Treated wastewater from the sewage treatment plant is to be returned to the groundwater basin by sprinkler irrigation or direct release from the Van Gordon Creek wastewater pond.

It is not clear whether or under what conditions the CCSD dry-season entitlement of 630 acre-feet may actually be available. It is known that during the droughts of 1975-76 and 1987-91, mandatory conservation was necessary in order that the CCSD could provide its customers with a meager baseline supply. Between 1990 and 1998, Cambria saw the construction of over 500 new dwelling units. Many of these units were built under an off-site retrofit program that required a new unit to provide low water-use plumbing fixtures in existing dwelling units. In theory, each new unit would result in no additional net water use when combined with the savings attributed to the retrofitted units. However, a 1999 evaluation of the retrofit program found it to be less effective than originally assumed. Dry-season water use in the seven-year

period from 1992 through 1999 increased by an average of about 2 percent per year. Some of this increase was probably due to a gradual erosion of “drought-consciousness” on the part of local water customers, but some portion is also attributable to the increase in the number of service connections over that period.

The CCSDs conservation efforts have continued to evolve since 1999. In 2003, the production of the CCSD wells totaled 793 acre-feet. This is a 2-percent decrease from 2002 and 3.4-percent less than the CCSD’s 1988 total production of 819.5 acre-feet. Further water conservation opportunities such as using non-potable water for outdoor landscaping and irrigation are currently being pursued.

One consequence of building regulations and retrofit programs that require low water-use plumbing fixtures is that a community’s ability to respond to a drought becomes somewhat diminished. For example, in a house without low water use fixtures, a reduction in the number of times a toilet is flushed each day saves up to 7 gallons per flush. The same reduction in a house with low-flow fixtures saves only 1.6 gallons per flush. Thus, the more efficiently a community uses its water on a day-to-day basis, the less flexibility it has to respond to a severe drought. This is certainly true in Cambria, where more efficient water use and a significant number of additional dwelling units combine to raise concerns about the community’s ability to respond adequately to droughts.

Thus, the CCSD has an entitlement to a water supply that may be sufficient to support a modest amount of additional development in years when rainfall is average or better, but which may not be adequate to meet even the existing demand in a year when precipitation is much below average. The CCSD is considering construction of a desalination unit with sufficient capacity to provide supplementary water to existing customers and future customers on the CCSD water wait-list during droughts.

The California Coastal Commission adopted an allocation plan which provides that 20 percent of the CCSD’s permitted water production capacity be reserved for visitor-serving and commercial uses. This plan identified the maximum number of units the 1,230 acre-feet per year could serve to be a total of 5,250 units (according to coastal development permit 4-28-10). As future permits are considered, this number may be reevaluated to address changes in annual per capita use, percentage of leakage or other factors which may identify changes in the number of persons or units which could be served.

The regional coastal commission adopted an allocation plan which provides 80% of the safe yield for residential uses and the remainder for tourist and commercial uses. This plan identified the maximum number of units the 1,230 acre-feet per year could serve to be a total of 3,800 units. As future permits are considered, this may be reevaluated to address changes in annual per capita use, percentage of leakage or other factors which may identify changes in the number of persons or units which could be served.

The Cambria Community Services District’s Feasibility Report of 1976 projected a daily use of 140 gallons per capita per day. The condition established by the Coastal Commission requiring

the 20 percent reserve for nonresidential use, leaves 504 acre-feet for residential use during the dry season. Based on CCSD's Water Master Plan Phase 2 report, the average residential use per connection is approximately 144 gallons per day. Applying this water consumption figure to the total dry-season residential allocation of 1,230 504 acre-feet, indicates that the total population number of dwelling units served would be 7,885 approximately 4,120 people. This represents an increase of 120 units above the 4,000 unit total for January 2005, as reported by the CCSD.

After reviewing reliability conclusions of the CCSD's Water Supply Analysis during a November 15, 2001 meeting, the CCSD Board declared a Water Code 350 emergency and enacted a moratorium for new connections with an exception for certain projects that were already in process. These "pipeline" projects amounted to approximately 202 EDUs at the time of the November 15, 2001 moratorium. Since then, approximately 80 EDUs out of the 202 EDUs have been connected.

Water Supply in Dry Years

Cambria's annual dry-season water shortage has long been a cause for concern. Customers of the Cambria Community Services District were under a mandatory conservation program from early in the summer of 1990 until the end of the drought in 1993. The conservation program resulted in reduced consumption of approximately 28 percent compared to 1989. This reduced consumption allowed the community's usage to remain within the limits of its dependable supply. However, if no conservation program had been in effect, consumption might have exceeded the dependable monthly supply.

The 1998 USGS Report provides additional details regarding Cambria's seasonal water shortage. "The water supply for the Cambria area is vulnerable to drought because the ground-water basins provide the only supply of water during the dry season and because ground-water storage capacity is small relative to the demand for water." The USGS report evaluated various drought scenarios as follows:

- A. Single Long Dry Season.** "If the dry season were exceptionally long and pumping continued unabated, wells could go dry or subsidence or seawater intrusion could occur before recharge begins the following winter. Partly for these reasons, there are (regulatory) limitations on annual and seasonal quantities of municipal pumpage for both basins. The longest dry season on record for San Simeon Creek (269 days in 1977) has an estimated recurrence interval of about 20 years. The longest dry season on record for Santa Rosa Creek (289 days in 1977) has an estimated recurrence interval of about 52 years."
- B. Single Winter with Incomplete Recharge.** "If streamflow is insufficient during winter, ground-water recharge will be incomplete and water levels will not return to the levels of the preceding winter. The consequences become evident toward the end of the succeeding dry season, when upstream wells in the Santa Rosa Creek basin are likely to go dry and subsidence is likely. In the San Simeon Basin, water levels in the CCSD sprayfield are likely to decline below sea level, resulting in seawater intrusion. Many

wells are likely to go dry or to experience a decline in yield. Crop losses in the upper part of the valley would be significant. A year with less than the minimum amount of stream discharge necessary to completely recharge the ground-water basin is likely to occur once in 18 years in the Santa Rosa Basin and once in 25 years in the San Simeon Basin. A winter as dry as 1976 or 1977, when basin recharge did appear to be incomplete, is likely to occur once in about 25 to 26 years. Even allowing for uncertainty, the recurrence interval of incomplete recharge is clearly short enough to warrant consideration during water-supply planning.”

C. Successive Winters with Incomplete Recharge. “Given that the consequences of even a single winter with incomplete recharge can be fairly severe, the consequences of two successive winters with incomplete recharge could be devastating. The likelihood of this occurrence would be an important factor in designing water storage facilities. However, the estimated recurrence interval of such an event is about 210 years for Santa Rosa Creek and about 430 years for San Simeon Creek.”

The CCSD spray field operation was changed to a percolation pond system during the early 1990s. The percolation ponds serve to raise water well levels in this area while serving as a hydraulic mound to slow fresh water outflow at the ocean boundary. This operational change also improved the water balance slightly from that shown in the 1998 USGS report by minimizing evaporative loss at the spray field.

The CCSD has developed a three-stage restricted water use plan to deal with these situations. The restricted water use plan includes various methods for achieving the necessary demand reductions, including consumer education by news media and direct mail, and prohibition of wasteful water use practices such as vehicle washing, certain irrigation practices, use of potable water in construction activities, and refilling of swimming pools. Severe demand reductions of 40 percent or more are achieved by water rationing, prohibiting new construction, establishing high rates for water use in excess of allotments, and by imposing severe penalties for violations.

In addition, all new development must participate in an off-site plumbing fixture retrofit program, through which conventional plumbing fixtures are replaced with low-flow fixtures throughout the community.

These periodic seasonal supply problems caused by a combination of limited storage capacity and shortage of rainfall have resulted in a recommendation of RMS Level of Severity III for Cambria’s water supply.

Demand and Additional Supply Alternatives

An aggressive conservation program and plumbing fixture retrofit requirements for new construction have helped to moderate the problem of insufficient water resources by reducing per capita demand. Since 1990, there have been more than 500 additional hookups to the CCSD water system. These homes were constructed with low water use fixtures, and the CCSD’s retrofit program has resulted in the installation of similar fixtures in more than 2,500 existing

homes. The program has been somewhat less effective than originally envisioned, primarily because it was not designed to reduce the amount of water used to irrigate residential landscapes. Nonetheless, the retrofit program has reduced per capita demand. However, because the program has reduced demand, it has also reduced the likelihood that the community could achieve the same degree of emergency conservation that was possible during the last drought.

In addition, as of October 1999, there were about 130 new residential units in the plan approval and construction process. The occupancy of these units could make it more difficult to achieve the communitywide water savings necessary to endure a prolonged drought.

In 1999 the CCSD undertook several initiatives intended to achieve more efficient use of available water supplies and to evaluate options for increasing supplies in the future. The CCSD board authorized the preparation of a Water Master Plan to project future demand and evaluate various options for increasing supply to meet that demand.

Due to its limited resources, the CCSD embarked on a phase approach towards updating its water master plan. The CCSD has since commissioned several key reports and related work efforts as part of its phased Water Master Plan approach.

The following summarizes key water master planning studies and reports that the CCSD has commissioned to improve upon its water system

Phase 1 - Land Use and Buildout Analysis. The District completed an extensive base mapping effort in order to obtain an accurate map of its service area. The base mapping effort consisted of completing an aerial survey, post-flight processing of the aerials to convert images into digital-orthogonally corrected images and digitization of key map features into a geographic information system (GIS). After the GIS information was gathered, the CCSD developed a model to assess the likely and non-likely buildable vacant lots for use in a subsequent build-out reduction plan. The GIS information developed under phase 1 is currently being expanded upon and used as part of the District's on-going build-out reduction plan.

Phase 2 - Water Supply Availability Analysis. The Phase 2 work of the Water Master Plan update is summarized in a report entitled "Baseline Water Supply Analysis," December 8, 2000, prepared by Kennedy/Jenks Consultants. This work included an assessment of the District's existing water supply, an analysis of aquifer hydrology, the development of a water supply and demand model, and recommendations for water shortage emergency criteria. The supply and demand findings from this work effort were subsequently used during completion of phase 3 and 4 of the water master plan. For example, the baseline report included a statistical analysis that found at least 93 percent of the time, groundwater levels at the start of the dry season will be at elevation 15.2 feet or higher in San Simeon well field (a conservatively low elevation for the start of the dry season.) From the hydrologic model, elevation 15.2 also corresponded with a total dry season capacity of approximately 286 acre-feet at the CCSD's San Simeon aquifer well field. In developing sizing recommendations for a supplemental water supply, the

subsequent Phase 4 report uses a dry season capacity of 286 acre-feet from the San Simeon aquifer and zero acre-feet from the Santa Rosa aquifer. These are conservatively low aquifer capacities when compared to the CCSD's permitted dry season capacity of 370 acre-feet from the San Simeon aquifer and 260 acre-feet from the Santa Rosa aquifer. However, the use of a conservatively low capacity value provides assurances that any new water supply will be adequately sized and less vulnerable to drought conditions as well as other unforeseen circumstances that could impact the existing supply.

Phase 3 - Potable and Recycled Water Distribution System Analysis. The Phase 3 work of the water master plan was split into two reports: one of the potable water distribution system; and one on a proposed recycled water distribution system. The potable water distribution system analysis focused on system improvements to improve fire fighting capabilities that are described in a report entitled: "Final Task 3 Report: Potable Water Distribution System Analysis," dated July 2004. This report analyzed fire flow criteria that were approximately 250 to 350 percent higher than the current system capacity due to concerns over multiple fires from the high fuel loads and close proximity to adjacent structures in Cambria. Report recommendations resulted in three levels of priorities based on the value of the improvements towards improving fire-fighting capabilities. The highest priority level 1 recommendations have been incorporated into the CCSD's capital improvement program and are in various stages of completion. For example, construction of a new distribution main across the west ranch area is completing construction. Replacement of the Pine Knolls storage tanks was recently bid but is currently on hold due to permitting concerns by the Coastal Commission. Because of the concern for public safety, the potable water distribution system analysis report was the first of the phased water master plan reports to be started. Therefore, this report also contains discussion on projected demands and multiple buildout scenarios. The four buildout projections are further described within the potable water distribution analysis report.

The second Phase 3 report consisted of a recycled water distribution report that analyzed the reuse of highly treated wastewater effluent for landscape irrigation. Main candidate sites for recycled water use included the elementary and middle schools, future park site, and larger commercial landscaped areas.

Phase 4 - Assessment of Long-term Supply Alternatives. The Phase 4 assessment of long-term supply alternatives reviewed all the various supply alternatives developed by the CCSD over a period of approximately 20 years. Two levels of screening were developed based on tangible and intangible factors. In addition, sizing recommendations considered four levels of unit demand for residential customers for improved "quality of life" allowances to provide current and future customers a level of relief from existing water shortage restrictions. The increases in residential demands considered were 10, 20, 30 and 50 percent above the existing consumption level of 0.161 acre-feet per residential connection identified in the Phase 2 Report. The total estimated supply requirement also allowed for the existing Coastal Commission permit requirement of at least 20 percent of the CCSD's supply being available for visitor serving and commercial services.

The Phase 4 report recommendations included increased conservation efforts, the use of recycled water for landscaping and seawater desalination. Summary tables presented within the report allow some flexibility in determining the size of a desalination facility based on desired buildout capacity and increase in unit consumption above the current baseline consumption above the current baseline consumption rate. For example, a buildout scenario limited to 4,650 existing and future dwelling units with a 50 percent increase in residential demand, requires a desalination system capacity of about 602 acre-feet during the dry season. These are the same conditions the CCSD Board directed staff to pursue during a July 24, 2003 meeting.

Program-Level Environmental Impact Report. The CCSD has commissioned RBF Consulting to complete a program-level EIR for its Water Master Plan. A public scoping session was held on July 15, 2004. RBF is also developing a detailed build-out reduction plan for Cambria. The CCSD's intent is to incorporate recommendations from the buildout reduction plan into the program level EIR as mitigations to offset the growth-inducing impacts of a new water supply.

~~Growth beyond roughly 6000 population will require the planning of additional water supplies. Alternatives include additional water extraction from the San Simeon Creek groundwater basin if it is found to have a larger safe yield, renewed extraction of water from the Santa Rosa Creek basin accompanied by necessary treatment of mixing, local reservoirs such as the one on Santa Rosa Creek, or importation of water from Lake Nacimiento.~~

~~Current improvements to the Cambria water supply system are designed to serve the existing subdivided areas. The potential build-out population for existing building sites in Cambria is well beyond the year 2000 projected population of the community. Although improvements and extensions will occur during the intervening years, these would not require the long lead times necessary for installing major new systems.~~

~~In addition to the above sources of water, the county has 17,500 acre feet of water per year allocated from Lake Nacimiento. This allocation is not presently used, but could be used in a number of areas of the county, including the North Coast.~~

2. Sewage Disposal (RMS)

~~Sewage disposal service is provided for Cambria by the Cambria Community Services District. Sewer lines are constructed for the existing subdivided area, but due to limitations of the existing wastewater disposal facilities and the need for future increased plant capacity, major improvements were completed in 1995, are currently underway under construction. Sewage disposal service is provided throughout the entire District, except for some large parcels, primarily in the Leimert subdivision, to Park Hill, Happy Hill, Moonstone Beach, the downtown area and new additions to the north of Weymouth Street and Lodge Hill area. Future development will be linked to the existing network and expanded as needed.~~

~~The sewage treatment facility has been expanded to one million gallons per day. The project required the development of the alternative spray disposal area in the vicinity of Van Gordon Creek and San Simeon Creek. Wastewater disposal is being accomplished by spray irrigation and by surface water discharge from the proposed reservoir. In addition, discharge to this basin will be designed to prevent seawater intrusion resulting from well water withdrawals. The existing treatment plant capacity has been expanded from 250,000 to 1,000,000 gallons per day. The permit from the Environmental Protection Agency allows a maximum of 125 permits for residential hookups per year. This must be allocated between single family residential use and multiple family residential use. Treated wastewater will be collected in wastewater pond and from there disposed of through spray irrigation on adjoining ranches, and secondarily by direct release into Van Gordon Creek, a tributary of San Simeon Creek.~~

~~The project, when completed, would serve a population of 11,000 persons at the wastewater flow rate of 100 gallons per capita per day (gpcd). Use of treatment facilities is to be apportioned between permanent residents and tourists according to a Coastal Commission permit. The capacity of the treatment plant would accommodate persons for the permanent population, persons equivalent for the tourist population using local facilities. This computation is based on the seasonal increases of approximately 30% that result during the summer peak period of recreational use.~~

In 1995, improvements were completed that increased the existing sewer plant capacity to one million gallons per day. The current dry weather flow average is between 650,000 and 700,000. The wastewater disposal process entails pumping effluent to a reservoir pond and then into four percolator ponds located near San Simeon Creek where it percolates into the ground water basin below. Discharge into the basin also serves as a “water dam” to prevent seawater intrusion into existing drinking water wells. Reuse of wastewater for non-potable uses is limited to withdrawals from an extraction well located in the middle of the effluent field.

There are no current deficiencies or capacities problems to note for the wastewater system. Ample capacity exists and preliminary calculations indicate that up to 6,150 connections may be provided prior to exceeding capacity.

Current and future issues for wastewater include removal of nitrates to meet California requirements. Currently, the biosolids are hauled in liquid form (approximately 2%) to Kern County for disposal. The District will be required to dewater its biosolids at the treatment plant to lower disposal costs. In the future, the District may further treat the dewatered biosolids to produce a “Class A” exceptional quality product that allows for local land disposal and reclamation as a soil amendment.

Other noted considerations include modifications to an existing lift station within a flood zone as well as responding to additional water quality regulations that are continuing to be set forth by the State. The addition of advanced treatment processes at the wastewater plant for the production of recycled water is also described in a Task 3 water master plan report. These facilities may include additional filtration, advanced oxidation and disinfection.

C. SCHOOLS (RMS)

(See Schools discussion, page 3-17)

The North Coast planning area is served by the Cambria Union Elementary School District providing for grades K-8. The 1978 enrollment was 280 students and the present district capacity is only 300 students. A level II severity problem is evident. Additional elementary classroom space will be needed within a few years. It is quite possible, however, that remaining classroom space could be occupied within one year if there should be a sudden surge in residential building activity and/or more permanent occupancy of existing housing.

Students from San Simeon Acres attend schools in Cambria. The small permanent population, the anticipated slow rate of growth, and residency primarily attracting retired persons would indicate an insignificant impact on school district enrollments.

The Coast Joint Union High School located in Cambria serves all the North Coast planning area as well as Cayucos, and the southern Monterey County coast. Although 40% of the present enrollment is from Cayucos, the number of additional students from this source is anticipated to be low due to the physical limitations of the community for much additional growth. The 1978 and 1979 high school enrollment was 275 students, and the school capacity, is 400 students.

The present population of the North Coast planning area is 3,560 permanent residents. Relating high school enrollments directly to the population of the planning area, taking into account modest growth in Cayucos, a Level III severity problem may occur when the population reaches 5,700 persons. At total build-out of designated residential uses in the North Coast planning area, and assuming that Cayucos students might someday use more localized facilities in the Estero planning area, the present high school site should be adequate for ultimate school expansion needs.

3. Traffic and Circulation (RMS)

Both residents and tourists contribute to traffic in the Cambria area. While Highway One divides the community, it carries mostly tourist traffic in the summer months, while also providing west side residents with a second access to downtown.

In the summer months, tourist traffic increases substantially on Main Street, Burton Drive, and Moonstone Beach Drive. Both Burton Drive and Main Street are at capacity year round.

Construction of the Main Street Enhancement Plan began in 2000. Implementation of this Plan will help to ease congestion. The *North Coast Circulation Study* calls for Main Street to be improved to three lanes, and construction began in 2000. Transportation Management Measures may also be employed to accommodate the growing level of traffic anticipated by this Plan. Resident traffic will increase with the moderate increase in new homes and gradually declining housing vacancy rates. Visitor traffic will increase due to statewide growth.

Traffic and circulation issues are discussed in greater detail in Chapter 5: Circulation.

4. Fire Protection

~~Fire protection service has been provided locally since 1957. Cambria is served through the Cambria Community Services District with a fire stations located in the downtown village. At present, the district is served by one full-time permanent employees and a volunteer force. An additional fire station is proposed the Lodge Hill area to replace the Burton Drive station.~~

Fire protection service in Cambria is provided by the Cambria Community Services District (CCSD) Fire Department. The headquarters station is located on Burton Drive and is manned seven days a week, 24 hours a day. The CCSD has an automatic aid agreement with CDF/County Fire. All emergency calls originating in Cambria are responded to by both the Cambria fire protection and CDF/County Fire. As part of the automatic aid agreement, the Cambria Fire Department also responds to emergency calls in the County areas outside the CCSD.

Although there has been no large-scale fire in Cambria since the 1800's, several contributing factors confirm that a high fire danger exists throughout the community. When considering the fire environment, fire fighters and experts recognize four components that are clearly applicable to Cambria: weather, topography, fuel and the human factor. These components affect the likelihood of a fire starting, the speed and direction at which a wildfire will travel, the intensity at which a wildfire burns and ability to control and extinguish a wildfire.

The conditions in the community and nature of the north coast area will continue to present challenges for the fire department. The Cambria Fire Department relies significantly upon the County's General Fund for funding.

5. Recreation Services

Parks and recreation facilities are provided by the county and by the CCSD. In 2003, the CCSD formed a Parks and Recreation Department to develop, manage, and maintain the District's parks, recreation, and open space amenities and facilities. The Parks, Recreation, and Open Space Commission (PROS) is a board-appointed advisory group reporting to the CCSD Board of Directors. Green Space, a Cambria land conservancy, has also been active in the creation, acquisition and maintenance of neighborhood parks in the Cambria Community Services District.

The County's Parks and Recreation Master Plan, adopted in 1988, contains recommended standards for the park acreage in relation to the size of the population. Standards for regional parks dictate that such parks should be within a one-hour drive from urban areas, approximately 200+ acres in size and will serve both residents and visitors (see Table 3-5). Cambria does not currently meet this standard. The San Luis Obispo County parks and Recreation Element proposes creation of a Cambria Regional park of approximately 200 acres to be located in the North Coast Area.

Table 3-5
Recommended Park Standards

Type of Park	Acres per 1000 people	Size range (Acres)	Service Area Radius	Access by (Street Type)	Population of Service Area
Mini-Park	0.5	0.18 - 5	0.12-0.25 mile	Local/collector	500 to 2,500
Neighborhood	1.0	5 - 25	0.25-0.5 mile	Local/collector	2,500 to 5,000
Community	5.0	25+	1-2 miles	Clctr/arterial	5,000+
Regional	15-20	200+	< 1-hr drive	Clctr/arterial	30,000+

Source: Recreation, Park and Open Space Standards and Guidelines; National Recreation and Parks Association, 1983

There are currently two County parks within the Cambria CSD. Lampton Cliffs Park is a 2.2 acre neighborhood park with trails and coastal access. Shamel Park is a 6-acre County community park located on Windsor Boulevard. Shamel park provides picnic areas, play equipment, a swimming pool and coastal access. There are also developed coastal access ways at Wedgewood Street and Harvey Street. Green Space, The Cambria Land Trust, owns 11 pocket parks in Cambria as well as Strawberry Canyon, a 16.0-acre open space area, and the Center Street 1.6-acre open area.

In November 2000, the CCSD took title to the more than 400-acre Fiscalini Ranch with the intention of using the ranch for public recreation and open space. The Parks and Recreation Department is currently facilitating the development of a community “active recreation” park on the East Ranch. The community park could include a community center in addition to other sports and recreation facilities. The CCSD also owns the majority of retired lots in the Fern Canyon Area of Lodge Hill. Acquisition of the remaining lots in Fern Canyon and development of a trail through this area could provide an important link to Cambria’s growing trail system.

Regional Parks. The Master Plan estimated that the county as a whole, including the North Coast Planning Area, met the standard for regional facilities. However, because the standards recommend that a regional park be no further that one hour from the users, Cambria does not meet the standard. Current estimates indicate that a regional park is needed in the north coast.

Neighborhood and Community Parks. Tabulation of existing neighborhood and community park acreage, including 50 percent credit for school playgrounds, indicates that Cambria's 1999 supply of park acreage is approximately 85 percent of the recommended standard. Recreation acreage in Cambria is summarized in Table 3-6.

Table 3-6
Neighborhood and Community Park Status
North Coast Planning Area

Community	Current Supply (Acres)	Current Need (Acres)	Estimated Need, Buildout (Acres)
Cambria	29	34	121
San Simeon Acres	0	2	4
Total	29	36	125

F. San Simeon Acres: Status of Public Facilities, Services and Resources

The San Simeon Acres Community Services District (SSCSD) operates a water system and sewage treatment plant for the community of San Simeon Acres. The SSCSD also provides road maintenance, street lighting and fire protection. The District's service area is shown in Figure 3-4.

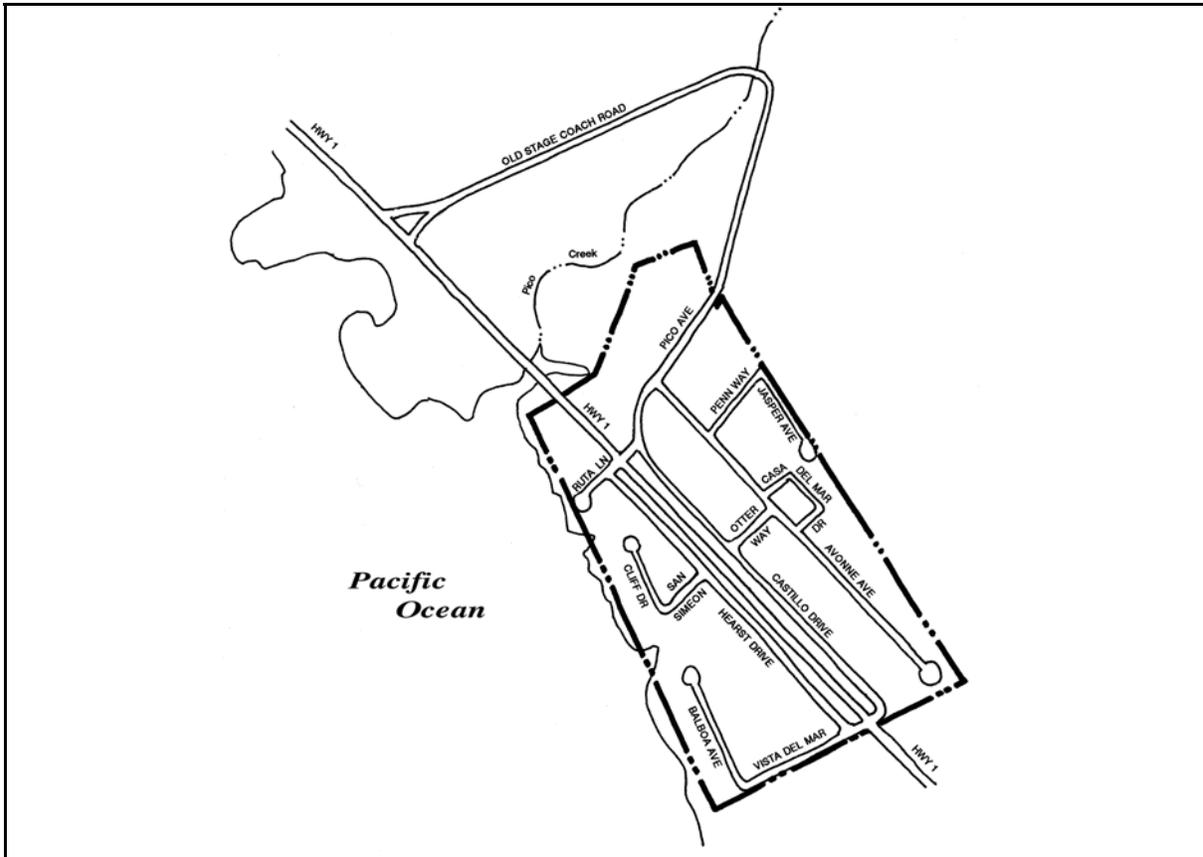


Figure 3-4: San Simeon Acres Community Services District

1. Water Supply (RMS)

The existing supply of water for San Simeon Acres is provided from two SSCSD wells along Pico Creek. The existing permit from the County Health Department allows a total production withdrawal of 140 acre-feet per year from these wells. ~~No determination has been made of t~~ The safe yield of the Pico Creek groundwater basin is estimated to be about 120 to 130 acre-feet per year. ~~Current~~ The 1998-99 production withdrawal is by the SSCSD was approximately ~~107 80~~ 107 80 acre-feet per year. This water is fully consumed; ~~for none of the~~ wastewater is returned to the groundwater basin. In addition, the Hearst Corporation

withdraws about 16 acre-feet per year from the Pico Creek basin for the watering of livestock, does not presently have any competing uses for water. An environmental concern is protection of stream flows for anadromous fish and riparian wildlife habitat.

San Simeon Acres had an adequate water supply through 1985. In January 1986, a moratorium was placed on new construction or remodeling which required additional water/sewer hook-ups. In August 1988, outdoor water usage was restricted and in December 1988, it was prohibited. In May 1989, a program was initiated which mandated that all bathrooms be retrofitted with positive shut-off ultra low flush toilets. These were supplied by the District. Also, beginning in June 1989, many establishments began purchasing irrigation water from outside sources. The low flush toilet program has reduced water use by about 30 percent.

The San Simeon Community Services District provides water services to this area from two wells located along Pico Creek. Total production capability of the District's wells is over 400 500 acre-feet per year. During periods of below-average rainfall, the capacity of storage facilities and peak periods of use must be monitored to insure a continuous water supply.

Because of the large number of second homes and resulting high vacancy rates, the actual permanent population of San Simeon Acres is difficult to estimate. According to the U.S. census, the 1978 1990 permanent population of San Simeon Acres was 134 128 people. ; and, b More recent estimates place the number of permanent residents at about 250. Residential water demand is estimated to be approximately 120 gallons per capita per day (gpcd), 30 gallons lower than the 150 gallon estimate contained in the 1986 Master Water Plan Update. The difference results from permanent conservation measures implemented by the SSCSD. Based on an estimated per capita consumption of 0.151 acre-feet per year, the water demands of these permanent residents is 20 acre-feet per year. Water use attributable to the community's motel rooms, restaurants, and other commercial facilities fluctuates with the seasons. Estimates of unrestricted use are based on an average year-round motel room occupancy rate of 50 percent and a consumption rate of 100 gallons per room per day.

This leaves approximately 60 acre-feet per year as the portion consumed by seasonal residents and tourists. Projections of permanent population and tourist demands for water indicate that the maximum water allotment of 140 acre-feet will suffice past the year 2000 unless future safe-yield estimates turn out to be lower than anticipated.

The absorption buildout capacity of San Simeon Acres, based on residential land use in the Land Use Element, is 980 1,229 people. The necessary water supply to support this population would be 148 160 acre-feet per year, not including tourist demands. Tourist facilities at buildout will require an additional 80 acre-feet per year. Total build-out of both visitor-serving uses and residential growth may will consequently create a substantial deficit over the allowed production withdrawal of 140 acre-feet per year. Projected water demand is shown in Figure 3-5.

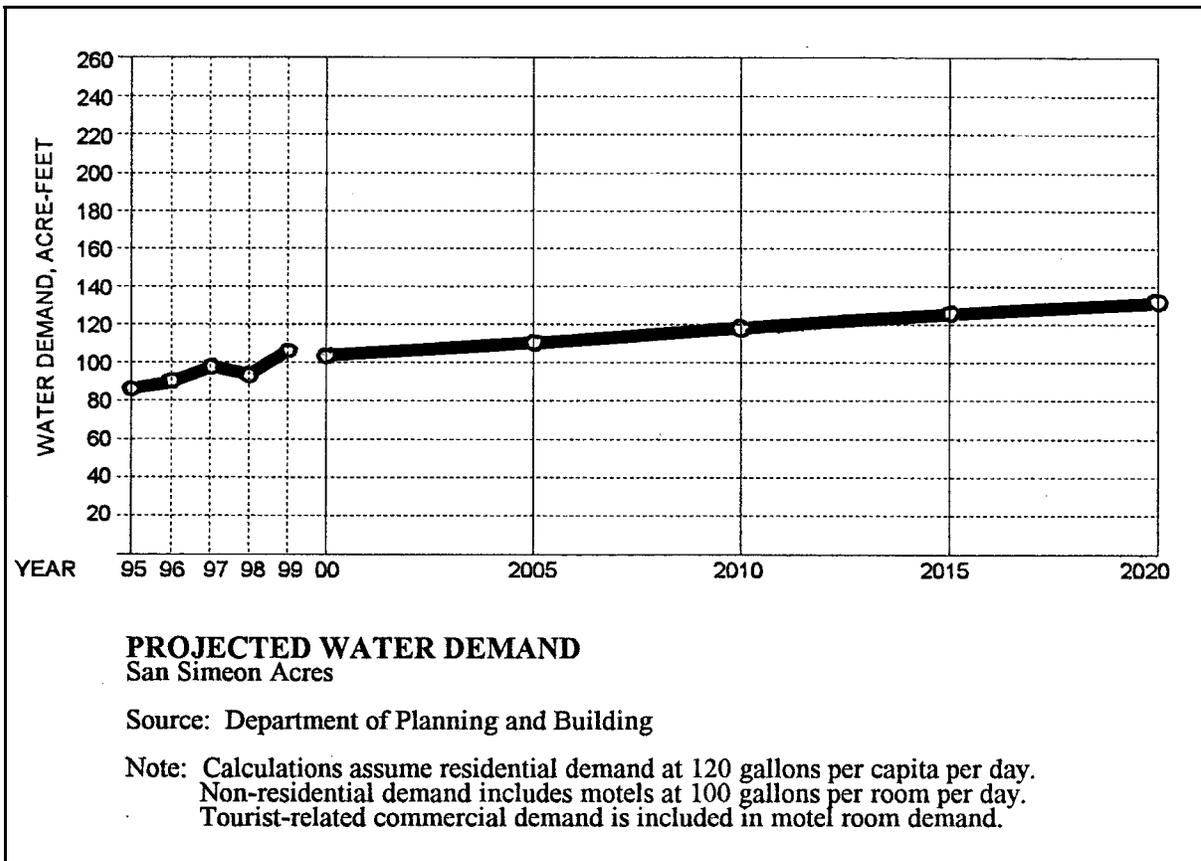


Figure 3-5: Projected Water Demand, San Simeon Acres

Although the present water supply appears to be adequate over the 20 years, a surge in residential growth or expansion of tourist facilities could reduce the time for seeking additional water supplied. The principal need is to determine the safe yield of the Pico Creek groundwater basin.

Several long-term projects have been considered for increasing the community's water supply, including a subsurface barrier at Pico Creek, a wastewater reclamation project, participation in the Nacimiento water project, and participation with the Cambria CSD in a desalination plant

The San Simeon Acres water distribution system needs to be monitored to insure availability of water during peak periods of use. It is estimated that the present peak resident and tourist population (motel rooms fully occupied) is around 800 people, while the current permanent population is estimated to be 140 persons.

2. Sewage Disposal (RMS)

The San Simeon Acres Community Services District provides sewer service ~~for the community to the development~~ in this area. The ~~present sewer plant~~ wastewater treatment facility has a capacity of 200,000 ~~150,000~~ gallons per day. An ocean outfall line is used for disposal. ~~One-fourth~~ third of the capacity (50,000 GPD) has been purchased by the state to serve the Hearst San Simeon State Historical Monument ~~leaving 100,000 gpd capacity for San Simeon Acres.~~ The ~~breakdown of the total 80,000 gpd sewage flows in 1978 are 13,400 gpd for the permanent population and 50,600 gpd for the tourist facilities at San Simeon Acres and 16,000 gpd for Hearst San Simeon State Historical Monument.~~ In 1989, the plant was operating at 98 percent of its capacity. By 1992, in response to the toilet replacement program and water use restrictions, average dry-weather flow had dropped to 44 percent of capacity. During the June-October dry season, which coincides with the peak tourism season, average daily flows were 88,000 gallons for both 1993 and 1994. During 2002-2003, the average daily flow was estimated at 84,000 gallons per day. It is estimated that about 35 percent of the flow is contributed by San Simeon Acres permanent and seasonal population, 49 percent by motels and businesses patronized by tourists, and 16 percent by Hearst Castle.

Based on the projected increase in residential units and tourist facilities and a corresponding increase in sewage flow from the Hearst Castle Visitors' Center, it is estimated that average dry-weather flow at buildout would equal or exceed the current capacity of the treatment plant. An increase in the rate of flow per capita or per motel room could result in peak flows 25 percent higher than the plant's capacity. particularly t To handle these peak flows, further expansion will be necessary. With modifications and upgrades, the current system could handle 400,000 gpd. that occur during the summer tourist season. The ultimate plant capacity is limited to 300,000 gallons per day due to the limited space of the existing site. The buildout flow could be accommodated by a plant of this size. If a larger plant should be needed in the future, T the Master Water and Sewerage Plan identifies two alternatives: first, 1) a new plant designed to serve San Simeon Acres (within the Village Reserve Line) and immediate vicinity; and, second, 2) joint use and expansion of the Cambria wastewater treatment plant. Final sizing of plant expansion should be limited to that needed to serve San Simeon Acres, Hearst Castle, and Department of Parks and Recreation staging area facilities. In addition, beneficial use of treated effluent should be considered rather than continuing use of the ocean outfall line. Projected sewage flow is indicated in Figure 3-6.

~~San Simeon Acres should be adequately served by the existing sewage treatment capacity beyond the year 2000. According to the projection, the community will have 20,000 gpd~~

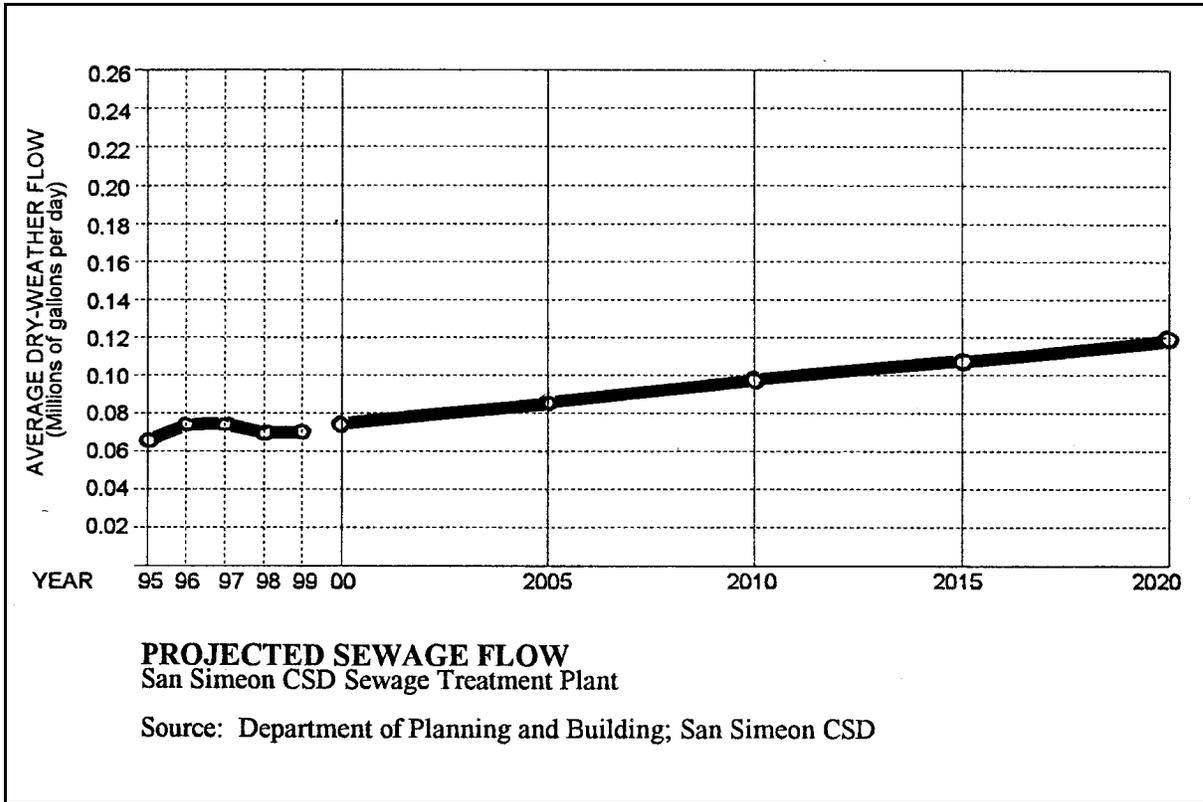


Figure 3-6: Projected Sewage Flow, San Simeon Acres

capacity still in reserve by year 2000. There also appears to be plentiful capacity remaining for Hearst Castle tourist facilities when comparing the present 16,000 gpd existing sewage flows to the 50,000 gpd capacity available, for it would seem that the number and size of Hearst Castle tours cannot be increased a great deal over recent years' peak usage.

The maximum plant capacity could be expanded to 300,000 gpd on the existing site. Present modifications are being made to allow effluent storage during periods of peak flow for more efficient plant operation. A substantial sewage plant capacity exists at present, and there is plentiful time for planning plant expansion or studying other alternatives for area sewage disposal facilities.

G. Planning Area Service Programs

"Programs" are nonmandatory actions or policies recommended by the Land Use Element to achieve community or areawide goals and objectives identified in this Area Plan. The implementation of each LUE program is the responsibility of the community, through the County or other public agency identified in the program itself. Because programs (some of which include special studies) are recommended actions rather than mandatory requirements, implementation of any program by the County ~~should~~ will be based on consideration of community needs and substantial community support for the program and its related cost.

The following public service programs for the North Coast Planning Area are grouped under general headings identifying the service they each address.

Areawide Programs

Police Service

1. **Cambria Substation.** The County should consider future location of a Sheriff's substation in Cambria to provide shorter emergency response times to the community and north coast area.

Schools

- ~~2. **Site Dedication and Facilities Financing.** The County should adopt an ordinance under the provisions of Government Code Section 66478 to require school site dedication where necessary in conjunction with new Subdivisions.~~

Water Supply

2. **Overall Water Supply.** The Community Services Districts should continue to look for new water sources sufficient for, but not exceeding, the growth anticipated by the Local Coastal Plan or the environmentally sustainable yield of the area water sources. Efforts should be made to monitor and verify the reliability of estimates of water savings from retrofit programs and other conservation measures, to improve the effectiveness of existing programs, and to initiate new programs to achieve greater efficiency in the use of limited water resources. Consideration should be given to such programs as site-specific collection of rainwater for later use in landscape irrigation, system-wide water reclamation and distribution, rate structure and billing systems designed to encourage conservation, and enhanced leak detection and repair programs.

3. **Establish Watershed/Basin Management Programs.** Establish Coordinated

Resource Management Programs (CRMP) to promote watershed management, including resource identification and water quality monitoring, and to address competing rural and urban uses in the North Coast groundwater basin.

4. Hearst Ranch

★(The Rural Area is not part of this Plan Update.) ★

- ~~6. **Long-Term Supply and Importation.** Although Cambria and San Simeon Acres have adequate local water supplies for the near future, the community services districts should work with the county to identify long-range needs, costs and benefits of participating in shorter term water importation projects developed for communities in the Estero Planning Area if they involve either Nacimiento water or new reservoirs located in the North Coast Planning Area.~~

Sewage Disposal

5. **Water Recycling - Restored Wetlands and Park Irrigation Golf Course Use.** The Community Service Districts should work with property owners, public agencies, and developers of any golf courses proposed in the Cambria/San Simeon Acres areas to explore possibilities for using treated wastewater from sewage treatment plants.
6. **Sewage Sludge Disposal.** The Community Services Districts need to identify long-term sites for disposal of sewage sludge. Additional coordination, environmental study, and permits may be required.

Solid Waste Disposal

7. **Provision of Services.** The County should work with CalTrans and the sState Department of Parks and Recreation Department to allocate responsibilities for organized trash collection and disposal service for road turn-outs shoreline accesses along Highway One and other shoreline roads.
8. **Community Recycling Programs.** The County should work with the Community Service Districts to establish and maintain an areawide recycling program.
9. **Regional Greenwaste and Composting Facility.** The County should work with the community, Community Service Districts, and property owners to establish a facility to accept and recycle greenwaste from the area. The facility should provide a place for wood waste and other trimmings. In addition, the facility should help contain pine pitch canker spread from material currently being hauled to other areas, or disposed of improperly.

Cambria Programs

- 10. Cambria Community Services District Boundaries.** The County and CCSD should coordinate with the Local Agency Formation Commission during the Sphere of Influence Update to analyze the feasibility of making the Service Area and Sphere of Influence of the District co-terminus with the URL.
- 11. Water Master Plan for Cambria.** The Cambria Community Services District should avoid issuing intent to serve letters for new development which relies on additional water supplied by San Simeon or Santa Rosa Creeks until the following tasks have been completed: (Mod107)

 - A. In-stream flow management study.** An in-stream flow management study for Santa Rosa and San Simeon Creek should be conducted. The study should identify a sustainable amount of withdrawals for new development that may be accommodated which will not adversely affect riparian and wetland habitat or agricultural activities.
 - B. Water management strategy.** A water management strategy, which includes water conservation, reuse of wastewater, alternative water supply (e.g., desalinization), and potential off-stream impoundments should be completed. The amount of new development should be limited to that which can be supported by the implementation of the strategy.
 - C. Small lot reduction ballot measure.** The County and CCSD should cooperate to place a small lot reduction ballot measure before the Cambria electorate.(Mod 107) & (Rec 2.13)
- 12. Service Needs/Deficiencies Study.** The County should work with the CCSD and other appropriate entities to formulate programs to address needs and deficiencies related to drainage, police services, fire protection, emergency medical, and recreation services.
- 13. Fern Canyon Recreation Improvements.** The County should work with the CCSD, willing land-owners, and other appropriate entities to help the CCSD acquire or obtain access easements through the remaining lots in Fern Canyon for development as a link to the open space trail system.

Table 3-7 indicates recommended timing and responsible agencies for proposed Service Programs:

Table 3-7
North Coast Planning Area
Schedule for Completing Recommended Service Programs

AREA	PROGRAM	RESPONSIBLE AGENCIES	POTENTIAL FUNDING	TIME FRAME	TARGET DATE	PRIORITY
AREA WIDE						
1.	SHERIFF SUBSTATION	COUNTY	FEES	2005-2010	2010	HIGH
2.	WATER AUGMENTATION PROGRAMS	CCSD, SSCSD	FEES, GRANTS	2005-2010	2010	HIGH
3.	COORDINATED RESOURCE MANAGEMENT PROGRAM	COUNTY, CCSD	FEES, GRANTS	2005-2015	2010	MEDIUM
4.	HEARST RANCH (Not part of this update)					
5.	WATER RECYCLING	CCSD, SSCSD	DEVELOPER AGREEMENTS	2005	ON-GOING	HIGH
6.	SEWAGE SLUDGE DISPOSAL	CCSD, SSCSD,	FEES	2005	ON-GOING	MEDIUM
7.	SOLID WASTE DISPOSAL	CALTRANS, STATE PARKS, COUNTY	FEES	2005	ON-GOING	MEDIUM
8.	SOLID WASTE RECYCLING	CCSD, SSCSD, COUNTY	FEES	2005	ON-GOING	MEDIUM
9.	COMPOSTING & GREENWASTE FACILITY	COUNTY	FEES, GRANTS	2005-2010	2007	MEDIUM
CAMBRIA						
10.	CCSD BOUNDARY CHANGE	COUNTY, LAFCO CCSD	N/A	2005-2010	2010	HIGH
11.	WATER MASTER PLAN	CCSD	FEES	2004-2005	ON-GOING	HIGH
12.	SERVICE NEEDS/ DEFICIENCIES STUDIES	COUNTY, CCSD	FEES, GRANTS	2005-2010	ON-GOING	MEDIUM
13.	RECREATION IMPROVEMENTS	COUNTY, CCSD	FEES, GRANTS	2005-2010	ON-GOING	MEDIUM

CHAPTER 4: LAND USE



This chapter addresses land use issues affecting the North Coast planning area. The chapter is divided into three sections: rural, urban and village areas. The "rural" portion of the text discusses the area outside of urban and village reserve lines; the "urban" portion discusses the Cambria area within the urban reserve line; and the "village" portion discusses land within the San Simeon Acres village reserve line.

The LUE official maps separate the planning area into land use categories, which define regulations for land uses, density and intensity of use. Land use "programs" at the end of this chapter recommend actions by the county or other public agencies. Chapter 7 contains development standards related to the land use categories to assist in guiding planning area development. Standards define actions required for new development to achieve consistency with the general plan.

Table F 4-1 summarizes the acreage for each land use category in the planning area.

Table 4-1
Land Use Acreage - North Coast Planning Area

Land Use Category	San Simeon Acres	Cambria	Rural North Coast	Total Study Area
Agriculture	-	39.78	99,037.35	99,077.35
Rural Lands	-	-	3,338.51	3,338.51
Recreation	-	41.88	1,750.21	1,792.09
Open Space	-	515.02	-	515.02
Residential Rural	-	-	-	0.00
Residential Suburban	-	57.54	-	57.54
Residential Single-Family	-	893.99	-	893.99
Residential Multi-Family	39.21	70.73	-	109.94
Office & Professional	-	13.05	-	13.05
Commercial Retail	41.81	64.61	36.70	143.12
Commercial Service	-	21.81	-	21.81
Industrial	-	-	-	0.00
Public Facilities	.73	72.06	44.19	116.98
TOTAL (NET) ACRES	81.75	1,790.47	104,207.18	106,079.40
TOTAL ACRES	114	2,350.94	105,679.42	108,144.36
Notes:	1. Net acres reflect areas within individual property ownerships. Is used to compute absorption capacity and buildout. 2. Total acres are net areas plus areas within road rights of way, and beach areas along the bay or ocean. Difference between gross/net is: (32.25 ac SAN SIMEON), (560.47 ac CAMBRIA), and (2,078.48 ac RURAL AREA).			

TABLE F

LAND USE ACREAGE				
NORTH COAST PLANNING AREA				
	Rural		San Simeon	
Land Use Categories	Area	Cambria	Acres	Total
Agriculture	155,812	-	-	155,812
		168	-	
Rural Lands	8,761	-	-	8,761
		-	-	
Recreation	593	134	3	730
		-	-	
Open Space	-	207	-	207
		-	-	
Residential Rural	-	-	-	-
		-	-	
Residential Suburban	-	132	-	132
		-	-	
Residential Single Fam	-	1,047	-	1,047
		-	-	
Residential Multi Fam	-	126	30	156
		-	-	
Office & Professional	-	10	-	10
		-	-	
Commercial Retail	43	53	70	166
		-	-	
Commercial Service	-	23	-	23
		-	-	
Industrial	-	-	-	-
		-	-	
Public Facilities	88	84	-	172
		-	-	
				Total 165,297
1,816	103	167,216		

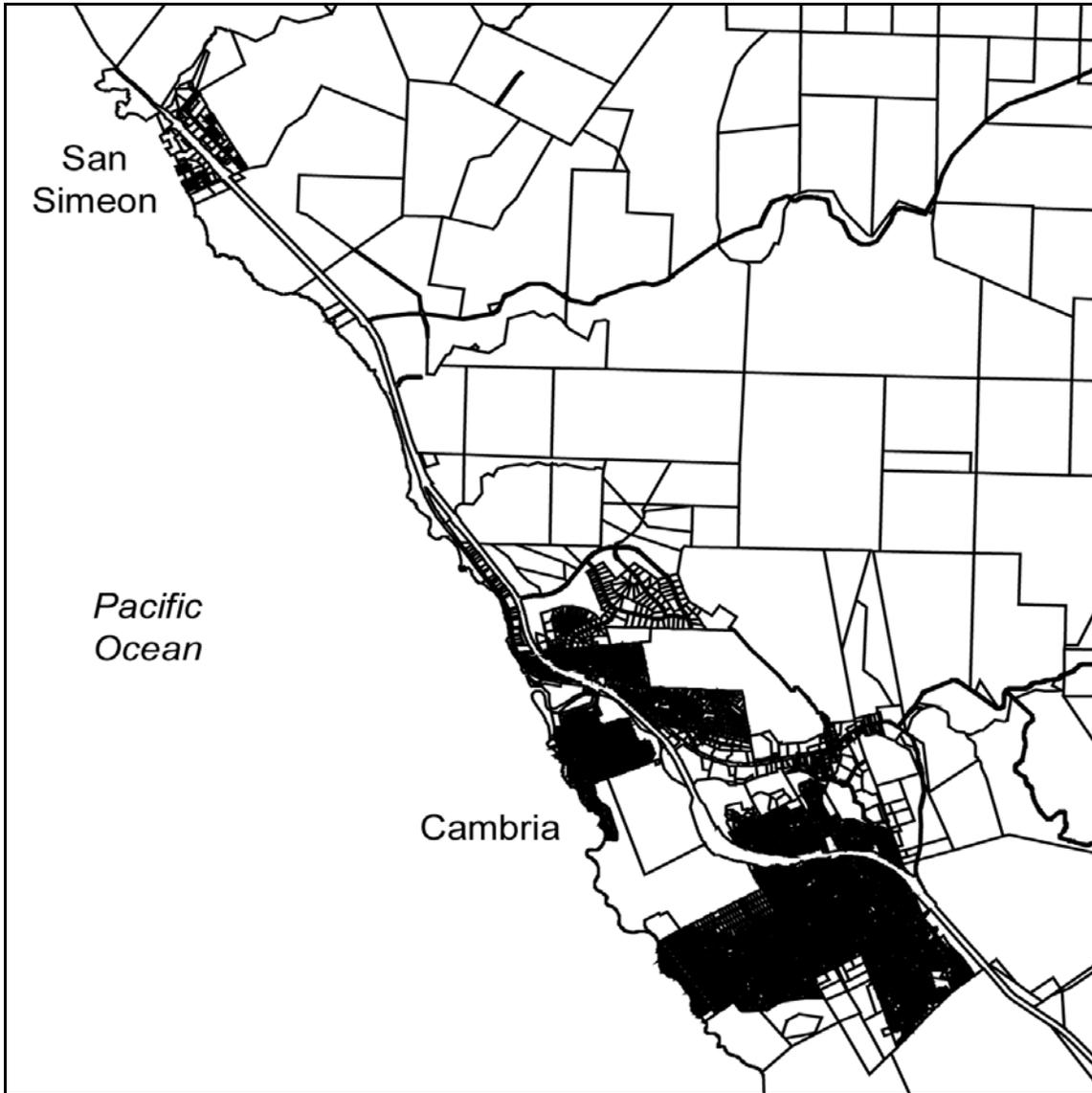


Figure 4-1: Location Map Cambria-San Simeon Acres

A. Rural Area Land Use

★(The Rural Area is not part of this Plan Update.) ★

B. Cambria Urban Area Land Use

Cambria is an unincorporated community located 20 miles north of Morro Bay and 23 miles south of the Monterey County line. Cambria's outstanding natural environment is its most precious resource, ~~with~~ including native forests of Monterey Pine, creek side areas, and a scenic coastline with interesting beaches. The surrounding outlying areas are devoted to agricultural uses, primarily grazing, which contribute to the unique setting of Cambria.

Cambria is located within Rancho Santa Rosa, an original Mexican land grant. The town was established in the late 1860's to accommodate shipping of mining and agricultural products in the central coast region. The hills around the original commercial area were subdivided in the late 1920's into 25-foot lots by the Cambria Development Company, which also installed the original water supply and distribution system. Cambria today is an attractive center for both retired persons and tourists visiting the central coast.

The Urban Reserve Line (URL) of Cambria encompasses approximately ~~1,764~~ 1,800 net acres available for ~~urban land~~ development, and approximately 2,400 gross acres for all uses. The Urban Services Line (USL) indicates the area to which urban services, particularly water and sewer, should be extended when available. The Cambria Urban Area includes the ~~proposed~~ Fiscalini Ranch area ~~development~~ in the central part of the community which is planned for ~~to include single and multiple family residential, public recreation, and open space uses following acquisition by~~ These land uses shown on the plan map and described separately in the text, are based on a generalized concept plans and may be subject to changes in configuration when the detailed development plans is are prepared. nonprofit organizations and agencies in 2000.

The Cambria Urban Area is divided into neighborhoods. These areas, ~~which~~ shown in Figure 4-2, are named to provide a convenient aid to location in the text discussion of land uses. ~~, are shown in the following map:~~

Open Space

Areas in Cambria designated as Open Space include the state-owned flood plain and riparian vegetation at the mouth of Santa Rosa Creek west of Highway One, and portions of the Fiscalini Ranch. Areas of the Fiscalini Ranch ~~which~~ should remain in a ~~the~~ natural state with uses limited to passive forms of recreation and public coastal

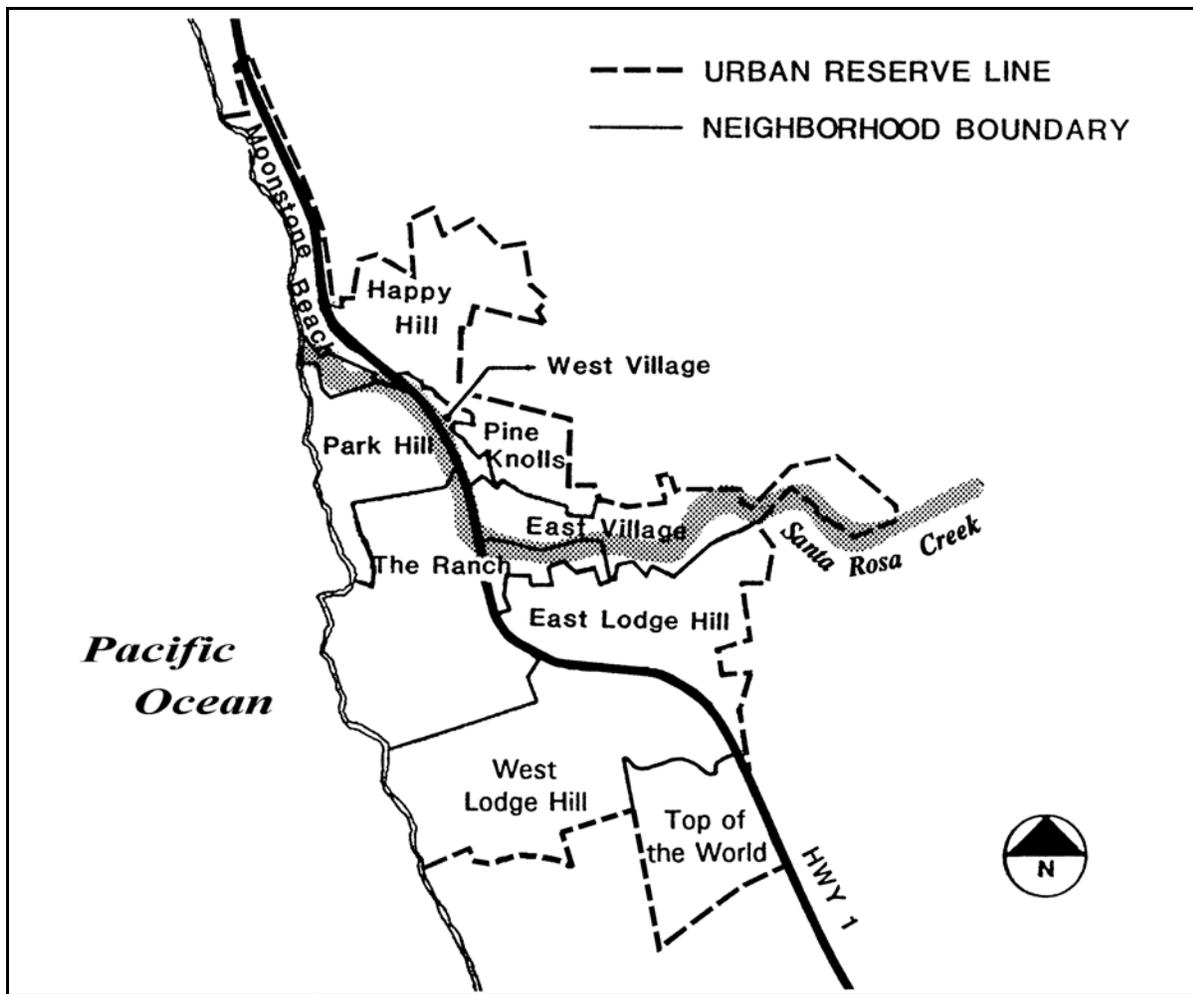


Figure 4-2: Cambria Neighborhoods

access. Open Space areas include most of the shoreline west of the proposed Windsor Boulevard emergency extension, flood-prone areas along Santa Rosa Creek, and significant pine stands, some of which are on slopes too steep for residential development. The land use map shows generalized the location of designated Open Space areas. ~~areas, and during specific planning of The Fiscalini Ranch, natural features should be studied in more detail. This is also true for other large properties within the Urban Reserve Line, where cluster development should be encouraged to preserve pine stands in common open space for local residents, or possible siting of possible public parks sites.~~

Commercial Retail

East Village. Commercial activities in Cambria are concentrated in two main areas, East Village and West Village. East Village, lying in the narrow valley carved by Santa Rosa Creek, remains Cambria's principal shopping and service center. ~~Here,~~ Along Main Street,

Bridge Street, and Burton Drive are found the businesses and residences that recall Cambria's past.

Two ~~problems~~ issues affect commercial development in the core area of East Village. The first ~~of these~~ is the lack of adequate parking space for ~~vehicles associated with existing or proposed new businesses~~. Part of the area is in the flood plain for Santa Rosa Creek, and is not suitable for development. The small size of many properties in East Village makes it difficult for prospective businesses to furnish the required number of parking spaces on-site. Although there are several private parking lots available on a lease basis, these ~~two~~ are now at capacity. ~~A Village Special Study undertaken in 1972,~~ Recent citizen concerns demonstrate ~~identifies~~ the need for parking areas scattered within East Village and around its periphery, that can serve as public parking and as employee parking. ~~parking for employees~~ Local businesses should undertake ~~active~~ steps to form a district for the eventual development of adequate parking, and their efforts should include a prompt designation of preferred parking areas so that these may be preserved for that use.

The second ~~problem facing issue affecting the~~ East Village ~~stems from the related is also related to the~~ shortage of land within the ~~downtown~~ area. Considering existing land uses and the small size of many lots, it will be increasingly difficult for larger businesses to be located in the village. ~~One positive result is that~~ As a result, most ~~of the~~ new businesses located in the core area will be small in scale and ~~will be~~ therefore consistent with the "village theme." Ultimately, many of the homes now occupying this area will be replaced by, or converted to, commercial activities. In addition to its role as the central business district, the East Village provides convenient shopping for residents living nearby.

Mid-Village. The Mid-Village area is located between the East and West Village. The Mid-Village lies on the crest of a hill separating the two community centers, and is characterized by a mix of uses, large parcel sizes, and a lack of building presence on Main Street. Santa Rosa Creek flows along the southern edge of the Mid-Village area.

West Village. The second commercial area, West Village, is situated between Cambria Road and State Highway One and it is ideally located to serve ~~the traveling public: tourists visiting the area.~~ Access to and from the highway is excellent and the The area benefits from high visibility and close proximity to the highway. Once called "the flats," most commercial uses now present, including arts and crafts shops, realties estate offices, small markets, and auto services, depend on the traveling public tourists as their chief source of business. In addition, day-to-day service to the surrounding residential areas is appropriate convenient. Growth in the West Village has steadily increased, ~~Major improvements to Main Street have occurred.~~ and Main Street has recently been the subject of major improvements designed to service this growth. However, available space for future expansion is severely limited by topography, flood hazards, Highway One, and other established uses. ~~Full development is likely to occur within the time frame of the plan.~~ Because of flooding issues, site planning will have to include careful consideration of flood hazards, and measures to

protect both the structure and flood plain will have to be incorporated into project design.
(Mod 21)

~~The need for additional retail commercial should be evaluated periodically. Possible relocation of the elementary school to an alternate site may make this area available for reconsideration through LUE amendment.~~ (Mod 22)

Office and Professional

~~Many office and professional uses are integrated in East and West Village. In addition, Two areas along Main Street are designated for professional office development have the Office and Professional land use category. On the south side of Main Street, across from the Cambria Community Health Care Hospital District office, are several medical offices. Additional professional offices should be developed. The second area designated for professional office development is located in the largely-vacant transition area between the two commercial centers along Main Street. This area is largely vacant. Many office and professional uses are also integrated in East and West Village in areas presently designated Commercial Retail.~~

Commercial Service

At present, most ~~service~~ commercial service uses are scattered within the East Village. The proposed area for concentrating this use to meet future site demands is the south side of Santa Rosa Creek, adjacent to the central business district. Some types of commercial services would be inappropriate in Cambria because of the community's desire to maintain an attractive residential and tourist-oriented business setting. ~~Certain large space users uses that require large amounts of space, such as auto and mobile home dealers, are neither essential nor in-scale compatible with the character of the community character.~~ There is a need, however, to provide adequate space for the local construction industry. The areas best suited for these uses, if adequate circulation can be provided, ~~are~~ is the south side of Santa Rosa Creek opposite the main business district. ~~Certain small service commercial uses such as repair shops could locate in retail commercial areas if they meet design standards contained in the Land Use Ordinance.~~

Recreation

Substantial areas of Cambria are designated for Recreation uses to provide for the combination of tourist food and lodging facilities. ~~and resident and tourist recreation activities.~~ The ocean shoreline, creek sides and pine forests provide varied scenic settings for both active and passive forms of recreation. Fiscalini Ranch is the largest area, with the eastern portion of the ranch planned for a community park and active recreational uses, while the western portion of the ranch is intended for more passive uses, such as trails. Moonstone Beach has for many years been recognized as one of Cambria's foremost scenic resources and a major recreation attraction to visitors and residents. The use and protection of this portion of coastline has been a priority for planning efforts by the State, County, and

community. The bluff tops and beaches on the west side of Moonstone Beach Drive are owned by the State of California as public recreation areas. The area between Moonstone Beach Drive and Highway One north of Weymouth Street, is presently the principal area of Cambria providing large commercial lots and community services suitable for development of motels and tourist-oriented commercial recreational use. Emphasis is on development of selected uses essential to the area, consistent with the scenic setting.

The County maintains a community park, Shamel Park, in Park Hill along Windsor Boulevard Road. The park site includes a community swimming pool, barbecue facilities and active play areas. The site is adjacent to the State of California holdings and provides access to the State Beach. ~~An additional park site is Lampton Park, located in southwestern Lodge Hill, has been improved using grant funds for minor landscaping, pathways, benches, and beach access. More community recreation facilities are needed in Cambria, including playgrounds, trails, and pocket parks.~~ The need for additional recreation areas is identified on the plan map.

Two group campground facilities also are ~~found~~ located in Cambria. Camp Pinecrest Yeager is maintained by Huron-Coalinga Elementary School District. Camp Ocean Pines is a group campground located southwest of Lodge Hill.

~~An additional area designated for recreational use is the Cambria Pines Lodge is also designated for recreational use, and This existing facility has been a major visitor-serving use facility in Cambria. A renovation program has been is underway proposed that would will upgrade and expand the facility. recreational use in conjunction with development of a multiple family project. Future development plans should retain the steeper, tree-covered slopes would be retained in their natural state.~~

~~Tourist facilities are proposed on the Fiscalini Ranch area along Santa Rosa Creek on both the east side and west sides of Highway One. Planning for the property on the west side of Highway One could include a 9-hole golf course, if enough water were available. The open rolling slopes with views over the ocean and fringed with pine forests could enhance the setting for proposed residential development, and single family residential development would provide an excellent setting for this use.~~

Fiscalini Ranch. ~~This area is the larger part of the Fiscalini Ranch located west of Highway One between Park Hill and Lodge Hill. and proposed in a preliminary development or concept plan primarily for single family residential use. Steeper slopes, forested areas, and the shoreline are proposed for recreation and or open space uses. On the western portion of the ranch, the large open areas may be suitable for passive park uses at an intensity in keeping with environmental constraints. On the eastern portion of the ranch, open areas may be suitable for active park uses at an intensity in keeping with environmental constraints. The large open rolling areas, however, may be suitable for both passive and active park uses at an intensity in keeping with environmental constraints. are proposed for single family lots with some multiple family condominium units. development of The Fiscalini Ranch will require major improvements to circulation and the expansion of~~

~~community services. The timing of development in these presently undivided areas should be in response to a need for additional types of development. Eventual development, however, is logical in that it would fill in the large, undeveloped area of central Cambria and discourage expansion in urban fringe. The entire project, however, will require careful design, coordination of facilities, and mitigation of impacts on the existing community.~~

Residential Multi-Family

Only a few areas of designated for multiple family units are presently available located in Cambria. Much land previously zoned for multiple-family development has been developed with single-family dwellings. Multiple family units provide an alternative housing type option, that may be appropriate in several areas of Cambria. ~~and several areas within Cambria are appropriate for these uses.~~ This Plan recognizes that the Residential Multi-Family category plays an important role in providing opportunities for affordable housing. Therefore, development of the Residential Multi-Family category with single-family dwellings is discouraged, because of the loss of suitably-designated property and corresponding loss of affordable housing opportunities. In addition, the Residential Multi-Family category has the potential to provide for a number of unmet housing needs, such as elder care, small sized dwellings, housing for singles, and service employees housing. Areas designated as Residential Multi-Family are discussed in more detail below:

Cambria - Main Street. Two areas located on Main Street are ~~identified~~ designated for multiple family residential uses. The first is located between the two commercial centers and is currently occupied by two mobile home parks. It should ultimately be developed as a transition from the commercial areas to the surrounding residential areas. Development will require sensitivity to ~~the~~ natural features, particularly those areas with significant steep slopes on the north side of Main Street. The second Residential Multi-Family area on Main Street is at the eastern fringe edge of the street. It is presently a mixture of uses including condominiums, apartments, motels, and single-family dwellings. This area is easily accessible and has convenient services in the Old Village area. Multi-family residences are allowed as a principal use on Main Street with Residential Multi-Family Standards applied.

Lodge Hill. Multiple family uses are also shown designated in both East and West Lodge Hill. ~~The~~ Within West Lodge Hill, the plan map identifies an area of existing multiple family use near the intersection of Ardath Drive and Highway One, and provides for expansion of this use. East Lodge Hill is ~~not~~ presently developed with some multiple family units, ~~but~~ and the area in the vicinity of Cambria Pines Lodge contains some larger lots and multiple lots under single ownership which would be suitable for multiple family use.

~~**The East Ranch.** The Fiscalini Ranch concept plan proposes a cluster of condominiums within the mostly single-family residential area west of Highway One, and a multiple-family residential area on the south side of Santa Rosa Creek. in close in to the central business~~

~~district. The steeper, wooded hillsides, and the immediate creek area, should remain in their natural state.~~

~~**Tract 226.** A third commercial area is available for serving neighborhood needs. This is This tract 226, is located in the East Lodge Hill area, This area and was subdivided into 32 small commercial lots with a central, commonly-owned lot for parking. Opportunity exists to develop these lots as they exist now, or after resubdivision. Detailed review development standards are discussed in a separate section of Chapter 7: Planning Area Standards, included to ensure adequate architectural design planning, site planning, parking, accessibility, and compatibility with the neighborhood. (Mod 19, 20)~~

Residential Single-Family

The majority of Cambria is within the Residential Single Family land use category designated for single-family residences. Extensive tracts of subdivided residential lands are a dominant feature of Cambria. ~~Top of the World, East and West Lodge Hill, Park Hill, Pine Knolls and Happy Hill are~~ the major residential neighborhoods. However, residences are also found in East Village and the Moonstone Beach residential area.

These ~~sprawling~~ residential subdivisions occupying the pine covered hills and open coastal terraces were created largely during the 1920's when many thousands of small lots, typically 25' x 70', were platted. Part of the area is visible from Highway One, with resulting visual effects. Much of the area was laid out with little regard for natural features of the land and is served by a confusing ~~and incongruous~~ system of roads, some much of which remain unpaved and substandard today. With a high residential density, there is little land in public ownership that can be used for parks. Some areas have also experienced serious bluff erosion problems. Proper treatment development of the small lots is critically needed to maintaining the attractiveness of Cambria.

~~More recent development in the Pine Knolls, Happy Hill, and Ranch areas has created standard sized lots that blend with the natural features. Adequate access to these subdivisions is a prerequisite to their development.~~

~~The total holding capacity of the existing subdivided with approximately 5,400 would range from 9,000 to 12,000 permanent residents. In addition, it is roughly estimated that there remains within the unsubdivided portion of the urban reserve area enough land for the additional potential development of an additional 1,700 units.~~

Each of the existing residential areas presents a unique residential setting, and is Each is discussed separately in the following sections.

~~Top of the World occupies the most southeasterly portion of the subdivided part of Cambria. On the south it is bounded by open and tree-covered agricultural land, to the northeast by Highway One, and on the north and northwest by Ardath and Burton Drives, which separate~~

it from West Lodge Hill. On its west side are found several parcels of land too small for agricultural purposes, but which are appropriate sites for residences on large lots. While dense forests are present on the western third of Top of the World, much of it is comparatively open. The eastern portions are nearly devoid of trees and are highly visible to travelers approaching Cambria from the south. The open and exposed character necessitates particularly careful arrangement and architectural treatment of residences to avoid detrimental aesthetic impacts. Rustic landscaping and reforestation on the eastern portions of this sub-area can enhance visual appearances and benefit property values.

West Lodge Hill. This is an extensive residential area located on the south and west side of Highway One, ~~west of the Top of the World area.~~ Topography is varied with numerous ridges and gullies, steep slopes, and nearly flat areas along the marine terrace. These features, combined with the large overall size of the West Lodge Hill neighborhood, the winding streets, and ~~differences in the variety of~~ vegetation types, give the area a diverse character. The most pressing ~~problem issues associated with~~ of West Lodge Hill is a lack of permanent open space, and the impact to residential densities that the vast number of small lots would create at buildout, and a lack of permanent open space. The visual impact of development, drainage problems, and tree removal are also major concerns. Buildout of all existing legal parcels would result in a significant loss of trees, and cause the demise of the forest as an important and valuable habitat area. Many of the lots with steep slopes will be very difficult to build on. If preserved in their natural state, they would reduce ~~the~~ ultimate residential densities and provide needed open space.

East Lodge Hill. This area is located south of the Santa Rosa Creek, between Highway One and Main Street. East Lodge Hill exhibits considerable diversity in its pattern of residential subdivision and development. Like other residential areas in the community, it has numerous 25-foot lots, yet it also contains more recent subdivisions with standard-sized lots. East Lodge Hill includes the former Air Force housing facility. ~~compound~~

Several areas of steep slopes have been subdivided with lots which today are nonconforming. Most notable are the steep areas between Highway One and Ramsey Drive, and the canyon between Burton Circle and Wilton Drive. Development of ~~such~~ these areas must conform ~~with~~ to engineering safety standards, and avoid unsightly hillside cuts and fills.

East Village. The East Village occupies narrow strips of land on both sides of Main Street. Residential development consists partly of ~~in large part of~~ older single-family homes on various sized lots. Nearly all of the residential buildings remaining from the community's early period are found here, and are concentrated along Bridge, Burton and Main Streets. Most of these structures have been maintained in sound condition, although there are some obvious exceptions. Older single-family homes will continue to exist in the downtown area and contribute to the village atmosphere. ~~As time progresses, it will be desirable to maintain the historically interesting homes in good condition.;~~ A number of these structures have historical significance, and are recognized by the Historic Combining Designations in the LUE. Efforts should be made to maintain the historically significant homes in good

condition. This Plan recognizes that some of these historical buildings will be converted to commercial uses associated with the tourist industry. As conversion occurs, care should be taken to preserve the historical characteristics of the structures.

Pine Knolls. This area is situated north of Main Street between East and West Villages. On its northern side are ~~adjacent~~ dense pine forests. The southeastern portion of Pine Knolls is a moderate to steeply sloping grassy hillside that fronts on Main Street and contains several homes on large parcels. Residential development is concentrated predominantly in the northeastern portion of the ~~sub-area~~ neighborhood where many new homes have been constructed on generous sized lots. The more westerly portions of Pine Knolls contain the nonconforming 25-foot lots characteristic of the community.

Several areas within Pine Knolls contain excessively steep slopes and should ~~either not be built upon at all or should require~~ be developed, if at all, only after careful technical investigation ~~first~~. The most notable of these is the triangular-shaped area bounded by Pine Knolls Drive, Hartford Street, and Main Street where steep slopes, made steeper by excavation, may preclude the possibility of residential construction. Another area of very steep slopes is the westerly-draining ravine lying between Hartford and Hillcrest Drives.

Happy Hill. This area forms the most northeasterly residential area of Cambria. It lies on the wooded hills adjoining Pine Knolls and West Village on the east side of Highway One. Approximately half the subdivided area is made up of nonconforming lots while the northern portion includes three recent subdivisions with medium to large sized lots.

~~The plan recognizes the long-term desirability of balancing the over-supply of nonconforming lots with large lots for providing greater diversity in living styles. Thus, a large area north of the Happy Hill subarea is designated as Rural Lands. The pine forests should be protected from excessive tree removal that would be caused by higher density development. Clustering of units close to the urban reserve line would be required to minimize the impact of development and establish a firm urban rural boundary.~~

Moonstone Beach. This area is located south of Weymouth Street, and is primarily developed as a single-family residential area, with a few nonconforming commercial uses. It is designated as a residential area with development to be compatible with the remainder of the Moonstone Beach area.

Park Hill. Like West Lodge Hill, this area includes both open ocean terraces and wooded, hilly terrain. It is bounded on the northeast by Highway One and on the west by the Pacific Ocean. Environmental amenities of Park Hill are enhanced ~~immeasurably~~ by open space land, both in public and private ownership, that now nearly surrounds the ~~sub-area~~ neighborhood. This Plan supports the continued distinctiveness and integrity of Park Hill ~~by designating~~ through designated open space areas on all sides of the ~~sub-area~~ neighborhood, with the exception of the ~~presently existing developed area~~ development at Park Hill's junction with West Ranch.

Residential Suburban

~~Two areas are identified for suburban residential development. Within Cambria, one area, located in the eastern portion of the community, is identified for residential suburban development the eastern portion of the community, between Main Street and Burton Drive on the south side of Santa Rosa Creek. It is presently in agricultural use, so small hobby farms ranging from one 1 acre to five 5 acres would be appropriate, along with residential uses. (Mod 18)~~

~~The second area is in the southwestern portion of the planning area. It serves as a transition from the denser portions of the community to the surrounding agricultural and rural areas. Particular care is essential in design of lots to suit the topography and setting. (Mod 18)~~

Public Facilities

~~Numerous public uses are identified on the plan map.~~

~~Cambria Village includes a number of public uses facilities. The fire station, Community Services District offices and yards, library, and post office serve the entire community. The Cambria Community Health Care Hospital District provides maintains an in-and-out-patient clinic and ambulance service in for the area. As Cambria grows, additional health care facilities will be needed to serve both the tourists and the local population. ~~village~~ Two cemeteries located in Cambria include the original Catholic church and cemetery, ~~in which a museum of local history is planned.~~ The California State Department of Forestry and Fire Protection maintains a year-round fire station in at Happy Hill. Community meeting facilities are available at the Veterans Memorial Building, and Mid-State Bank, and complementary uses are found at the Joslyn Adult Recreation Center and Community Center, ~~to include a proposed library in the adjacent area.~~~~

~~The Cambria Grammar School near Highway One opened in the fall of 2005. Future use of the former grammar school on Main Street has not been determined at this time. A number of community facilities will be needed to serve the anticipated population growth. Two elementary schools are is proposed, one of which may replace the existing school site on Main Street. If the elementary school relocates, future use of the site and the surrounding area for commercial purposes should be reconsidered through a general plan amendment. Neighborhood parks should be developed in conjunction with these school sites.~~

Agriculture

~~Within Cambria, one area, located in the eastern portion of the community, is designated as the Agriculture land use category. This area is appropriate for continued commercial agriculture as well as hobby farms or the keeping of animals such as horses.~~

C. San Simeon Acres Village Land Use

San Simeon Acres is a small commercial village developed to provide tourist and recreation services along the central coast. It provides food and lodging facilities for Hearst Castle visitors as well as tourists driving the scenic Highway One route between San Luis Obispo and the Monterey Peninsula. San Simeon Acres evolved from a 1940 sale of the area by W.R. Hearst to permit facilitate recreational development. Present uses are concentrated on the frontage roads along Highway One.

The Village Reserve Line (VRL) encompasses approximately ~~100~~ 80 net acres. The surrounding areas are devoted to grazing. Future residential, commercial, and motel development can occur within the presently subdivided area.

Commercial Retail

Commercial development in San Simeon Acres is oriented to ~~the traveling public~~ tourists using Highway One while visiting the central coast. The village provides accommodations for visitors to Hearst San Simeon Historical Monument. While the annual number of visitors to Hearst San Simeon Historical Monument varies, the number of visitors can exceed one million annually. ~~The village provided the nearest accommodations for the more than 950,000 persons that visited Hearst San Simeon Historical Monument annually.~~ Frontage roads to the east and west of Highway One provide access to the commercial areas. Peak visiting periods occur in the summer months; however, visitor use throughout the year is expected to increase.

~~Infilling on the~~ vacant lots located on the immediate frontage will occur as demand continues to grow. Of concern to the appropriate development of San Simeon Acres is the present current proliferation of signs. Future development should ~~use single sign advertising~~ be restricted to advertising with a single sign. Mixed architectural styles are also quite evident. Landscaping the highway frontage with trees and shrubs, and providing better highway crossings and pedestrian paths, would provide an attractive unifying element to the area.

Residential Multi-Family

The areas beyond the commercial frontage are designated for multiple family residential uses. These include the existing mobile home park and scattered multiple family units. It is anticipated that expansion of multiple family units will include triplex, apartments, and townhouse developments. In addition, expansion of commercial uses such as motels beyond the frontage may be appropriate ~~following developmental review~~.

Public Facilities

The only public facility ~~use~~ in San Simeon Acres is the existing sewage treatment plant. ~~and the Community Services District office~~. No additional public facilities are anticipated.

D. Planning Area Land Use Programs

"Programs" are nonmandatory actions or policies recommended by the Land Use Element to achieve community or areawide goals and objectives identified in this area Community Plan. The implementation of each ~~LUE~~ program is the responsibility of the community, through the County or other public agency identified in the program itself. Because programs (some of which include special studies) are recommended actions rather than mandatory requirements, implementation of any program by the County should be based on consideration of community needs and substantial community support for the program and its related cost.

Land use programs for the ~~North Coast Planning Area~~ are grouped first under names of the communities or rural areas, and then under land use categories or other location headings to identify specific areas where they each apply.

Rural Area Programs

★(The Rural Area is not part of this Plan Update.) ★

Cambria Urban Area Programs

The following programs apply within the Cambria Urban Reserve Line, to locations in the land use categories listed.

Commercial Retail

- ~~1. **Village Plans.** The County should work with property owners and the community to prepare Specific Plans for the East and West Village. The plans should address details of parking, landscaping, desired design themes (if any), pedestrian and vehicle circulation, and preservation of any identified historical sites.~~

Residential Single-Family and Multi-Family

- 1. Tract 226.** The County should work with property owners in Tract No. 226 toward resubdivide consolidation and redistribution of the lots (with no net addition of lots) for a multi-family or mixed use project. ~~commercial use before development occurs.~~ (Mod24)
- 2. Affordable Housing.** The County and the Cambria Community Services District (CCSD) should work together to ensure that affordable housing project proposals are not unnecessarily delayed because of water allocation issues.

3. **Residential Character.** The community and the County should work together to preserve and protect the residential character of single-family areas from potential impacts of vacation rentals.

4. **Transfer of Development Credits Program.** The County, the CCSD, local nonprofit organizations, and the local Land Conservancy should promote expansion of the TDC Program to include more areas within the community of Cambria. Sensitive habitat areas should have the first priority for inclusion as sending sites for transferring development credits. Possible agencies that could participate include The Land Conservancy of San Luis Obispo County, Greenspace, Department of Fish and Game, U.S. Fish and Wildlife Service, Cambria Parks and Recreation Commission, and County Parks Division.

5. **Buildout Reduction Program.** The County, other agencies, and the community should work together with the CCSD in their implementation of the CCSD's Buildout Reduction Program. The Buildout Reduction program will identify various programs in which the County could provide assistance.

Along with other buildout reduction measures identified in the CCSD's Buildout Reduction Program, a program should be initiated to encourage lot consolidation through voluntary mergers and other mechanisms, and to retire vacant lots through acquisition. An open space district should be formed through a cooperative effort between the County, the community of Cambria and others, to begin purchasing small and substandard lots. The objective of this district would be to retire development rights, protect resources, preserve the forest, reduce the number of potential homes, improve fire clearance and reduce impacts on limited resources. Purchased lots could also be considered for a variety of purposes, such as pocket parks, viewsheds, habitat preservation and other uses to benefit the community.

6. **Penalties for unpermitted tree removal.** Fines for unpermitted removal of trees should be increased to be a more significant deterrent. The Department of Planning and Building may evaluate using fines to fund an agency to monitor replacement trees on and off-site, to identify areas for off-site reforestation, to supervise off-site planting, and provide incentives to owners of built lots to reforest their lots. (RDP)

Recreation

7. **Parks Planning and Development.** The County, nonprofit organizations, and CCSD should continue to coordinate prepare neighborhood park development plans for residents recreation uses at the county park on Lodge Hill in Cambria.

San Simeon Acres Village Programs

The following programs apply within the San Simeon Acres Village Reserve Line, to locations in the land use categories listed.

Communitywide

- 1. Frontage road view corridor and landscaping.** The County should work with property owners to improve the views from Highway One to the frontage road with sidewalks, street furniture, trees, and decorative street lights.

Commercial Retail

- ~~**2. Landscaping.** The San Simeon Acres Community Services District and the State Department of Transportation should jointly undertake a program to landscape the Highway One frontage road corridor.~~

Recreation

- 2. Park Sites.** A small roadside park site and overlook should be acquired by the County or the San Simeon Acres Community Services District in the northwestern part of San Simeon Acres for the development of picnic facilities, restrooms, and beach access for residents and tourists.

Table 4-2
Cambria and San Simeon Acres
Schedule for Completing Recommended Land Use Programs

SCHEDULE FOR COMPLETING SERVICE PROGRAMS						
#	PROGRAM	RESPONSIBLE AGENCIES	POTENTIAL FUNDING	TIME FRAME	TARGET DATE	PRIORITY
CAMBRIA						
1.	TRACT 226 RESUBDIVISION	PROPERTY OWNERS COUNTY	FEES	2005 -2007	2007	LOW
2.	AFFORDABLE HOUSING	COUNTY, CCSD, NONPROFITS	FEES, GRANTS	ONGOING	2007	HIGH
3.	RESIDENTIAL CHARACTER	PLANNING & BUILDING; COMMUNITY GROUPS	GENERAL FUND	2005-2007	2007	MEDIUM
4.	TRANSFER OF DEVELOPMENT CREDITS PROGRAM	PLANNING & BUILDING; SLO CO. LAND CONSERVANCY; COMMUNITY GROUPS	GENERAL FUND; TDC PURCHASES	2005-2007	2007	HIGH
5.	BUILDOUT REDUCTION PROGRAM; LOT CONSOLIDATION AND RETIREMENT PROGRAM	PLANNING & BUILDING; COMMUNITY GROUPS; CCSD; LAFCO	FEES; GRANTS	2007-2009	2009	HIGH
6.	PENALTIES FOR UNPERMITTED TREE REMOVAL	PLANNING & BUILDING; COMMUNITY GROUPS	GENERAL FUND	2005-2007	2007	HIGH
7.	COMMUNITY PARKS PLANS	COUNTY, CCSD	FEES, GRANTS	2005 -2007	2007	MEDIUM
SAN SIMEON ACRES						
1.	FRONTAGE ROAD VIEW CORRIDOR	SSCSD, CAL TRANS, OWNERS	FEES, GRANTS	2005 -2007	2007	MEDIUM
2.	COMMUNITY PARK SITES	COUNTY, SSCSD	FEES, GRANTS	2005 -2007	2007	MEDIUM

CHAPTER 5: CIRCULATION ELEMENT



Land use and transportation planning support each other so that development is served by a defined system of circulation. Highways, roads, airports, railroads, bikeways, walking paths, and other methods of transportation make up the circulation system in the County.

The California Government Code requires that the General Plan include a Circulation Element which describes the location and extent of existing and proposed major thoroughfares, transportation routes, terminals, and other public facilities. This chapter is the Circulation Element for the North Coast Planning Area. It is meant to work together with the North Coast Area Plan and *Coastal Zone Framework for Planning, Part One of the Land Use Element.*

This chapter includes issues and objectives about circulation in the North Coast Planning Area, taken from the *Regional Transportation Plan (RTP)* adopted by the San Luis Obispo Area Coordinating Council. The RTP has a countywide perspective and contains detailed technical information on transportation issues, alternatives, and needs. Also included are data from the *Updated North Coast Circulation Study (NCCS)*, prepared by the County Engineering Department and the *Final Environmental Impact Report (Certified December 1996)* for the North Coast Planning Area Update. This Plan contains data, analysis, alternatives, and recommendations for funding and improvement of circulation routes in the Planning Area.

~~The circulation system of the North Coast planning area is planned to accommodate anticipated traffic. A concern for the future is the increasing volume of traffic along Highway 1 resulting from the popularity of the scenic drive between the Monterey Peninsula and San Luis Obispo and the associated recreation areas and tourist facilities. Past plans for widening the highway to accommodate more traffic have been rejected to preserve the existing scenic character of the route. The California Coastal Act requires that Highway 1 remain a two-lane road in the rural portions of the state. Within Cambria, circulation concerns include safer accesses and crossings of Highway 1, needed street improvements, better downtown parking, and bus service connecting Cambria with San Luis Obispo and other coastal communities to the south.~~

~~A. ROADS~~

~~The LUE maps show functional classifications of major existing and proposed roadways in the planning area. Improvements will be required with proposed land divisions by the county Real Property Subdivision Ordinance, and the standards in this area plan.~~

~~The following is a listing of the major proposals for the road system. These and other improvements are shown on the plan map; the listed order does not imply any priority.~~

The overall goal for circulation in Cambria and Sam Simeon Acres is to provide a comprehensive, integrated, multi-modal transportation system that allows convenient, flexible and efficient use of all transportation alternatives.

A. Circulation Issues and Objectives

The circulation system ~~of~~ for the North Coast Planning Area, as described in this chapter, is ~~planned~~ intended to accommodate growth anticipated by this Plan.

- 1. Road Capacities and Growth.** Circulation systems for the Planning Area are affected by two primary user groups: members of the local population and visitors to the area. Traffic congestion is especially apparent during the summer tourist season on popular routes such as Highway One, a national scenic byway, and on Main Street in Cambria.

Objectives:

- A. Monitoring.** Monitor roadway capacities and land use to insure that future growth does not contribute to the erosion of safe and convenient traffic levels. Coordinate proposed development with monitoring by the Resource Management System, and phase necessary improvements as growth occurs. Strive to maintain or improve levels of service as the area grows.
- B. Funding.** Develop and maintain funding methods that are linked to growth, such as updating road fees annually, and implementing capital improvement programs.
- C. Expanding road capacity.** Focusing strictly on expanded roadway capacity is not financially feasible, possible, or desirable. Consider Transportation System Management (TSM) and Transportation Demand Management (TDM) methods first, as a means to reduce additional demand on circulation systems. Consider roadway widening and other conventional improvements as a less desirable alternative, and only when consistent with community character and needs.

- 2. Financing Needed Improvements.** Plans will not be effective if funding measures are not in place to finance needed transportation and road improvements.

Objectives:

- A.** Circulation improvement needs for local population growth should be funded by traffic impact fees, through developer exactions and dedications, and assessment districts.
- B.** Proposed visitor-serving development such as hotels, motels, visitor attractions, and other resort development should improve roadway capacity when development occurs.

- 3. Transit Planning.** Opportunities exist to utilize transit to minimize visitor-serving impacts from Hearst Castle, proposed resorts, and other recreational uses in the area.

Objectives:

- A.** Encourage commercial and resort land uses in locations where users may benefit from transit opportunities.
- B.** Study the feasibility of transportation systems such as a jitney system, park and ride lots, dial-a-ride, and other alternatives, to help avoid or postpone the need for costly conventional road improvements.
- C.** Locate community-serving commercial, institutional, and housing uses near existing or proposed transit facilities. Discourage land uses that require excessive dependence on individual automobile trips.
- D.** Require employee housing to be provided near large developments, or close to existing mass transit opportunities.
- E.** Link Hearst Castle Visitor Center with the motel areas of San Simeon Acres and Cambria by a shuttle bus system to reduce congestion on Highway One.

- 4. Emergency Access.** An efficient circulation system is necessary to serve residents and visitors to the area. Emergency conditions that affect transit routes are flooding, forest fires, and traffic accidents in the Planning Area.

Objectives:

- A.** Since Highway One is the main access to and through the region, maintain, to the maximum extent possible, levels of service on Highway One.
- B.** Establish and construct new emergency collector routes in Cambria to provide access in case of a forest fire or flooding of Santa Rosa Creek. An example is the planned emergency access road between the Marine Terrace and Park Hill residential areas.
- C.** Improve residential street widths and vertical clearance in accordance with the State of California Fire Codes.

B. Circulation Needs and Deficiencies

The North Coast is served by a network of two-lane roadways in the rural area and many local and collector streets in the Cambria area. The principal arterial route is State Highway One, running north and south through the Planning Area. State Highway 46 enters the planning area from the southeast, and terminates at Highway One. Main Street in Cambria functions as an arterial road. Examples of collector roads are Burton and Ardath Drives and Windsor Boulevard.

Peak traffic volumes occur during summer weekdays (typically Fridays) due to local and visitor-serving demands. Highest traffic volumes are on Highway One at the southerly portion of the planning area. Within Cambria, highest volumes are on Main Street. Average summer traffic volumes are higher than other seasons, with holiday weekends being the period of highest volumes.

Levels of service (LOS) are determined for each roadway by comparing the traffic volume carried on the roadway to its capacity. LOS values range from Level 'A' (best) to 'F' (worst). Levels of Service 'A', 'B', and 'C' are generally considered acceptable. LOS 'D' is considered marginally acceptable, and indicates need for improvement. LOS 'F' represents unacceptable conditions (see Table 5-1).

Table 5-1
Level of Service Definitions

Level	Description
A	Low volume, primarily free flow. Drivers can freely maneuver and maintain their desired speeds with little or no delay.
B	Stable flow, some speed restrictions due to traffic. Maneuvering is only slightly restricted.
C	Stable flow, but ability to maneuver is more restricted. Relatively satisfactory operating speeds. Longer queues can cause delays.
D	Small increases in traffic could cause substantial delays. Maneuverability and speed selection are restricted for most drivers.
E	Unstable flow. Potential for brief stoppages. Average travel speeds are one-half to one-third the free flow speed.
F	Forced flow, frequent approach delays at critical signalized intersections. Speeds reduced. Lengthy stoppages due to downstream congestion.

Future growth in both the County and the State will affect Main Street and Highway One. With approximately one-third of Cambria's downtown land underdeveloped or vacant, substantial commercial buildout may occur during the term of this Plan. Preparation of future Specific Plans and LCP amendments should carefully address these issues before a commitment to substantial development is made.

The 2005 Cambria and San Simeon Acres Community Plans of the North Coast Area Plan Final Environmental Impact Report examined peak hour volume and levels of service for various roadway segments in the Planning Area. Existing and forecast traffic volume and corresponding levels of service are shown on Table 5-2.

Emergency response is a significant unmet need, and the ability of residents in certain areas in Cambria to evacuate in case of a forest fire is limited. In the future, a number of collector roads may be needed to guide traffic out of the three major residential areas to Highway One, and away from any advancing fire. Similarly, a connection is needed between Park Hill and Lodge Hill to enhance safety and emergency access.

Table 5-2
Existing and Future Peak Hour Segment Analysis

Roadway Segment	Existing		Buildout	
	Peak Hour Volume	LOS	Peak Hour Volume	LOS
Hwy One, south of Hwy 46	924	C	1,328	D
Hwy One, Hwy 46 to Ardath Dr.	1,305	C	2,048	D
Hwy One, between Burton Drive and Ardath Drive	1,043	E	1,528	E
Hwy One, between Main Street and Burton Drive	814	D	*	*
Hwy One, north of Cambria	710	C	1,030	C
Hwy One, south of County Line	260	B	380	B
Hwy One, between Main Street and Cambria Drive	*	*	1,246	E
Hwy One, between Cambria Drive and Burton Drive	*	*	1,302	E
Hwy 46, east of Hwy One	484	B	832	C
Main St., Cambria Drive and Burton Drive	817	B	1,053	B
Main St., Burton Drive and Hwy One	613	C	883	C
Main St., between Hwy One and Cambria Drive	798	B	938	B
Ardath Dr., west of Hwy One	481	B	761	C

Source: Final *EIR Cambria and San Simeon Acres Community Plans of the North Coast Area Plan, 2005.*

C. Circulation Alternatives

As population and tourism increases in the Planning Area, there will be a corresponding increase in trip demand; that is, the need for people to travel from home to work, to shopping, to school and to a variety of other destinations. Most of these additional trips will be made by private automobile, and they will result in a degradation of levels of service on the existing roadway network. The function of the Circulation Element is to identify and implement strategies to maximize the capacity of existing roadways (Transportation System Management, or TSM), moderate growth in trip demand by increasing the convenience of transportation modes other than the private auto (Transportation Demand Management, or TDM), and, when necessary, to increase capacity by widening existing roads and constructing new ones. In most cases, using these strategies, acceptable levels of service can be maintained.

Efforts to reduce traffic congestion will also promote better air quality for the Planning Area. Transportation System and Demand Management methods are closely related to the goals and recommendations of the County *Clean Air Plan*.

There are several management methods that may be utilized to accommodate existing and new local and visitor-serving growth anticipated by this Plan. It is the objective of this Plan to look at System Management and Demand Management methods first, and new roadway construction and road widening second. The goals of Chapter 1: Introduction and General Goals are promoted by the following specific objectives.

- 1. Transportation System Management.** Utilize engineering methods to improve traffic flow on the road network to maximize its efficiency while minimizing expenditures for capacity expansion. Examples are traffic light synchronization, channelization of street intersections, and designation of one-way streets, left and right turn lanes, passing lanes, and on-street parking limitations.
- 2. Transportation Demand Management.** Promote the use of transit for visitors between lodging and camping areas, Hearst Castle, and downtown Cambria. Adopt measures to make it more convenient for local residents to participate in ridesharing, use of public transit, bicycling, and other alternative forms of transportation.

Transit Opportunities. An inter-community transit system provides service between Cambria and San Luis Obispo. Regional Transit Authority (RTA) provides service between San Simeon Acres and San Luis Obispo. The This transit system should includes a limited route through Cambria for the convenience of local residents. In addition to the RTA System, the Regional Handicapped System (RUNABOUT) offers door-to-door service throughout the County. With increasing population growth, the frequency of scheduled transit trips should be increased in response to demand.

Dial-A-Ride. These systems are currently in service in other areas, and could be introduced when the need arises. There is a Senior Van service in Cambria which

provides door to door service for medical and other appointments. This service is limited to Cambria seniors and requires advanced booking.

Jitney/Bus Service. As alternative transportation measures are considered to reduce traffic on Main Street, a jitney/bus service should be evaluated as a management method to reduce congestion. The Cambria Community Service District contributes to and the County provides bus service by the Cambria Trolley, connecting downtown with Moonstone Beach and residential neighborhoods. A possible extension of this system could link Hearst Castle, San Simeon Acres and Cambria motel areas, and eventually downtown Cambria. Trip reductions and a corresponding decreases in automobile trips on Highway One could be one direct benefit of such a system, even if it only operated during peak summer months. A decrease in the number of car trips would also relieve parking pressure in these areas.

Car-Pooling - Park & Ride Lots. These are transfer areas where people may drive to a lot, park, and car-pool to another area. Car-pooling is encouraged by the *Clean Air Plan* and the *Regional Transportation Plan* as an alternative to single-occupancy vehicle use.

Bikeways. Bikeways have been proposed for the Planning Area in a number of past planning efforts. Many regional cyclists travel through the area along Highway One. One goal of the *1990 Regional Transportation Plan* that is applicable to the Planning Area is "to provide a safe and efficient pathway system that promotes cycling and walking."

Local bicycle use is minimal, due to the hilly terrain and difficult climbs. However, other areas, such as Cambria's Main Street, Moonstone Beach Drive, Windsor Boulevard, and San Simeon Acres, are more suitable. An ongoing issue is safety of bicycle and pedestrian travel along these routes as traffic levels increase. Bikeways should be included in street construction projects when feasible, to help reduce automobile trips.

A series of ~~needed~~ Class II and III bikeways ~~are is~~ identified within the ~~North Coast~~ Planning Area ~~as part of the *County Bikeways Plan*.~~ This area is ~~much in~~ often used by touring bicyclists throughout the year, and most roads and streets are considered scenic. ~~Rural~~ Roads designated for bikeways include ~~are~~ Highways One and 46, and the ~~coastal valley section of Santa Rosa Creek Road.~~

The scenic character of Cambria provides many different choices for resident and tourist bike riders, but the most popular areas will be the nearly-level shoreline routes including downtown Cambria, Moonstone Beach Drive, and Windsor Boulevard. ~~when extended~~ A bicycle/pedestrian path (Class I bikeway) ~~at~~ along Santa Rosa Creek between the Cambria High School and these oceanfront roads would be ~~both~~ highly functional as well as scenic.

Pedestrian Transportation. Walking shares some of the opportunities and problems of biking in the Planning Area. Walking is usually limited to shorter distances. New residential, commercial, and resort developments should be designed and located to encourage walking opportunities, and minimize automobile use when possible.

- 3. Land Use Planning Methods.** Planning for communities and resort areas should emphasize land use arrangements that reduce the need to drive. Policies and standards should be formulated that require new development to mitigate its effects on the circulation system. The requirement for major resort and other developments to provide employee housing will reduce the need to commute, as well as protect the availability of rental housing. A second strategy in this Plan allows for multi-family housing in the downtown, close to employment areas.

There are a number of land use methods proposed in this Plan that will act to reduce existing and future traffic congestion. The increased use of telecommunication technologies could reduce the need for commuting. Tele-commuting can reduce trips through a variety of methods, including accessing information in a way which avoids the need to drive, increasing the use of home offices, and establishing telecommunications facilities.

- 4. Highways, Streets and Roads.** This section lists major improvements that may be needed to accommodate growth anticipated by this Plan. The LUE Circulation maps in the back of this document show functional classifications of major existing and proposed roadways in the Planning Area. Improvements are typically required as development is approved, through application of County subdivision and land use ordinances, community and area plan standards, development impact fees, and other methods. A road improvement fee program for the North Coast Planning Area was first adopted by the Board of Supervisors in January 1992.

The timing of these improvements is important, and there should be a demonstrated need for each improvement prior to approval and construction. In addition, road improvements involving new roads and widening of existing roads should be carefully planned to be in character with the community, and to minimize environmental impacts on forests, creeks, wetlands and other sensitive areas. The following is a listing of the major proposals for the road system:

Principal Arterials

Principal Arterial Highways function to carry traffic between regions and major population areas. These are typically freeways and State Highways, such as Highway

One in the Planning Area. Highway One was designated a Scenic Highway by the State in 1999, and a National Scenic Byway in 2003. (Rec7.14)

Highway One. This highway is the main route through the Planning Area, serving area residents, the agricultural community, and as well as tourists driving the scenic coast route between San Luis Obispo and the Monterey Peninsula. A concern for the future is the increasing volume of traffic along Highway One resulting from the existing attractions such as Hearst Castle, and proposed new resort facilities and outdoor recreation. (Mod25) (Mod26a) popularity of the scenic drive between the Monterey Peninsula and San Luis Obispo, and the associated recreation areas and tourist facilities.

Past plans for widening the highway to accommodate more traffic have been rejected to preserve the existing scenic character of the route. The California Coastal Act requires that Highway One remain a two-lane road in the rural portions of the State. Highway One is, however, required by statute to remain a two lane, scenic road in rural areas of the coastal zone. In the North Coast, this is all of the planning area outside of the urban services lines of Cambria and San Simeon Acres. This places added importance on the use of Transportation Demand Management methods, to insure that levels of service remain acceptable. In addition, Highway One should be realigned landward in order to maintain the road as a scenic highway, provide continuing access to and along the North Coast of the County, and limit the amount of shoreline protection devices that may otherwise be needed to prevent damage to the highway from bluff erosion. (Mod26) In order to maintain the scenic quality of the highway, and to protect the State scenic highway status, only minor safety improvements are proposed, in rural areas such as adding signals, channelizing traffic, and adding a limited number of passing lanes.

Within Cambria, Highway One circulation concerns include safer access and crossings of the Highway. Highway One bisects Cambria, with intersections located at ~~South~~ Main Street/Ardath Drive, Burton Drive, Cambria Drive Road, North Main Street/Windsor Boulevard/Moonstone Beach Drive, Weymouth Street, and North Moonstone Beach Drive. Currently there are signals at Windsor Boulevard, Ardath Drive, Main Street and Burton Drive. Future grade separations are recommended. ; needed street improvements, better downtown parking, and bus service connecting Cambria with San Luis Obispo and other coastal communities to the south. Because the highway also carries a substantial amount of local traffic in the urban area, the installation of passing lanes, where possible, is recommended in the North Coast Circulation Plan.

In San Simeon Acres, landscaping between the highway and frontage roads is recommended to improve community appearance. Because Highway One is a substantial barrier dividing the town, better vehicle and pedestrian crossing facilities are needed.

Arterial Roads

Arterial Roads function to carry traffic between Principal Arterial roads, population centers, and to serve large volumes of traffic.

Main Street. Main Street was the former Highway One route through Cambria. It now serves as the main access to the central business district, and to nearby commercial and public uses. Limited operational improvements, such as signs, turn pockets, and bike and pedestrian ways, are necessary to accommodate traffic. The proposed Main Street Enhancement Plan should provide these improvements. Any improvements should maintain and enhance the character of the “streetscape”, while providing convenient access to businesses. Also, an off-street parking program, public transit and limited road widening are ~~is~~ needed in commercial areas to help alleviate traffic congestion.

Collector Roads

Collector Roads function to connect arterial roads with smaller and less-traveled local or neighborhood streets.

Ardath Drive and Windsor Boulevard. This alignment provides the principal access to the extensive existing and proposed residential areas west of Highway One. A limited extension of Windsor Boulevard for pedestrian, bicycle and emergency access is planned to connect Marlboro Lane in the Marine Terrace area to Windsor Boulevard in the Park Hill area.

Charing Lane, Buckley Drive, ~~Weymouth~~ Cambria Pines Road, etc. These streets, together with planned future extensions, will provide a collector street system to serve ~~ing~~ the north Cambria residential area. Cambria Pines Road provides additional access to Highway One from the north side of Cambria.

Windsor Blvd. Extension. This extension is limited to emergency vehicles, bicycles, and pedestrians. The extension in the short term serves as an emergency/fire access road linking Lodge Hill and Park Hill. (Mod27)

Burton Drive. This collector road provides major access for Lodge Hill. Operational improvements are necessary to keep the level of service from deteriorating further as Lodge Hill develops with more homes.

~~Fiscalini Ranch Road:~~ ~~This proposed collector road is also planned to serve development on West Ranch and would extend from Lodge Hill to Highway One.~~

Cambria Drive. This collector is also located in West Village and links the west side of Cambria by a fourth access point to Highway One and the downtown areas. A signal is planned on Highway One at this intersection.

Local Streets

Many of the streets within the Cambria Urban Reserve Line are unpaved, too narrow, poorly maintained, and lack proper drainage facilities. Numerous local public streets do not have vertical and horizontal clearances required by current fire code standards. These conditions will need to be corrected or improved through continuing street improvement programs, such as the cooperative road improvement program, and use of development impact fees.

The internal roads of San Simeon Acres are developed and maintained ~~through~~ by the local Community Services District and continued improvements should be completed in response to growth within the community.

5. Other Transportation Facilities.

★(The Rural Area is not part of this Plan Update.) ★

Figure 5-1 shows the existing roadway network in Cambria and the additional streets proposed to serve the increased traffic volumes projected for the next 20 years.

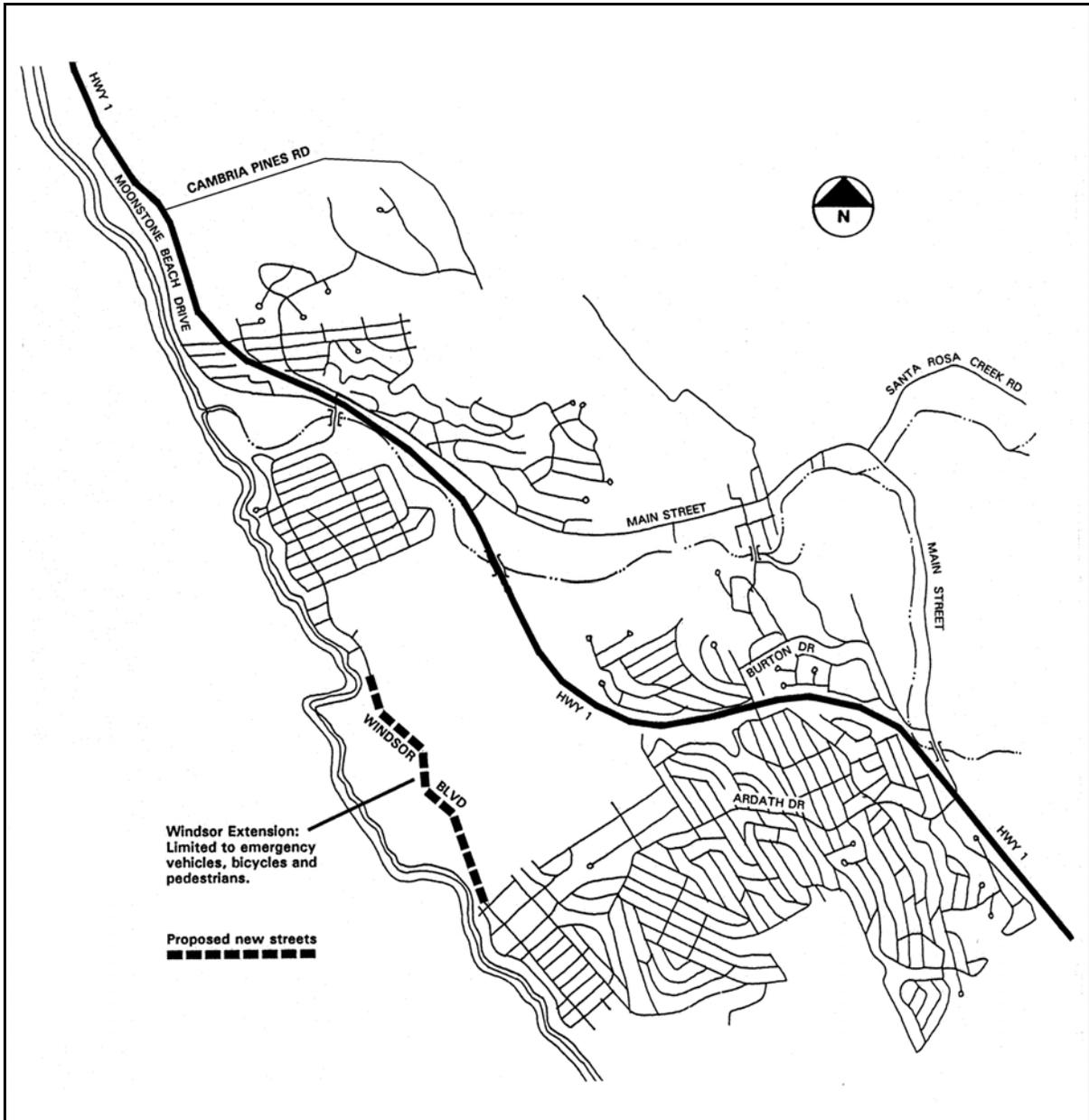


Figure 5-1: Existing Roadways & Proposed Windsor Blvd. Extension, Cambria

D. Circulation Programs

"Programs" are nonmandatory actions or policies recommended by the Land Use Element to achieve community or areawide goals and objectives identified in this Area Plan. The implementation of each LUE program is the responsibility of the community through the County or other public agency identified in the program itself. Because programs (some of which include special studies) are recommended actions rather than mandatory requirements, implementation of any program by the County should be based on consideration of community needs and substantial community support for the program, available funding, and its related cost.

The following circulation programs for the Planning Area are grouped under general headings indicating a community or other location in the Planning Area where they each apply. The approximate priority and suggested timing are also noted at the end of each program.

Areawide

1. **Directional Signing.** The availability of tourist services such as food and lodging facilities, recreational areas, and coastal access should be clearly indicated by State or County highway signs consistent with the County's sign ordinance, near each access to reduce or eliminate the need for conspicuous on-site signs. (Medium priority/Long term) (Mod31)

2. **Trails.** ~~In areas where there is interest in establishing equestrian trails,~~ To maximize the provision of public trails, the County should work with interested equestrian groups, property owners, agriculturalists, and other groups to determine if access rights-of-way may be secured to serve this need while respecting adjacent uses and ownerships. Opportunity exists to establish the California Coastal Trail, connecting each end of the Planning Area. More discussion on the Coastal Trail is found in Chapter 8: Coastal Access. (Medium priority/Long term)(Mod32)

3. **Highway One.** Highway One should be improved to ensure the safe flow of traffic, yet not detract from the scenic nature of the highway.
 - A. **Further Study and Improvement Funding.** The County, State, community, and other developers should cooperate to fund a study that will determine the actual origin and destination of Highway One users, and other related characteristics. The study should be the basis for appropriating fair shares of assessments to fund improvements, or other programs that might be necessary to offset the impact of new development on the highway. (Origin and destination funding alternatives)

 - B. **Cambria Urban Area.** The County should continue to monitor the need for passing lanes, traffic signals, and channelization of the southerly section of Main Street/Ardath Drive, and Cambria Drive intersections. Any planning for flood

prevention and/or control may include lengthening the bridge over Santa Rosa Creek to enable a 100-year flood flow to pass unimpeded. (Low priority/Long term) (Mod35)

- C. Hearst Castle.** The County should work with the State to develop transit opportunities to reduce traffic on Highway One generated by visitors to Hearst Castle. One option might be a summer (peak) bus service between the Castle and motel areas in San Simeon Acres and Cambria, and Cambria’s downtown. (Medium priority/Medium term)
- D. Highway One - Scenic Highway Enhancement.** The County and CalTrans should work together to develop an enhancement plan and identify funding sources to enhance the scenic qualities of the Highway One corridor. Examples of enhancement may include placing utilities underground, purchasing existing billboard signs, consolidating signs, planting trees and other vegetation, and screening unsightly features such as quarry sites or buildings. (Medium priority/Long term) (Mod38)
- 4. Jitney/Bus Service.** The ‘Cambria Trolley’ service should be continued and expanded to help relieve some of the peak-season traffic on Main Street in Cambria, and other areas. The opportunity also exists to link Hearst Castle and other resorts to the north, with the motels and commercial areas in San Simeon Acres and Cambria. (Medium priority/Long term)
- 5. Ridesharing - Park and Ride Lots.** A Park and Ride program should be established, including parking lots in appropriate areas, to help reduce single- occupancy vehicle trips in the area. Two suggested sites are near the Highway One/Ardath Drive and Cambria Drive/Highway One intersections. (Medium priority/Long term)
- 6. Bicycle Improvements.** The County should pursue development of a regional bikeway system, including the provision of Class I and II bikeways where feasible. Road improvements should include provisions for bicycle travel. The improvements should be integrated with the Coastal Trail where possible. (High priority/Short term with all road improvements) (Mod39)
- 7. Pedestrian Travel.** New development and road improvement projects should include provisions for safe and convenient pedestrian ways. (High priority/Short term)

Cambria

- 1. Road Alignment Reservation.** The County Engineering Department should continue to work with the community and property owners to identify and reserve the appropriate alignment for a public road connection between Burton Drive and the eastern extremity of Main Street.

1. **Highway One - Cambria Segment.** Highway One should remain a two-lane road within Cambria until traffic levels warrant the installation of passing lanes. The additional lanes should not be considered until other management methods are found not to be feasible or effective. (Monitor by RMS System/Long term)
2. **Road Improvement Financing.** The County has established ~~Engineering Department should work with the community to establish~~ a system for financing and installing road improvements in Cambria as needed.
3. **Cambria Pines Road Extension.** This segment currently connects north Cambria with Highway One to the west and should be maintained for access by vehicles, bicycles, and pedestrians.
4. **East Village Parking.** The County should continue to work with the community to establish an in-lieu parking fee and assessment district to construct a parking lot, including restrooms for visitors, in this area. Development of the parking area should not require the demolition of historic structures. (High priority/Short term)
5. **Fiscalini Ranch - Emergency Circulation.** Extension of Windsor Boulevard as an emergency, pedestrian, and a bicycle road are planned as part of the development of this property. (High Priority/Long term) (Mod42)
6. **Transit Opportunity Expansion.** The County should continue to work with the community to determine the need to establish a dial-a-ride system to augment the existing community busses. (High priority/Short term)
7. **Voluntary Trip Reduction Programs.** The County should work with the community, businesses, and affected agencies to establish voluntary trip reduction plans. For example, the County may amend applicable regulations to allow up to a 20 percent reduction in parking spaces where it is demonstrated that car-pooling or other transit measures incorporated into a project design will reduce overall trips by the same amount. (High priority/Short term)
8. **Pedestrian Improvements - The Cross-Town Trail.** The County and the community should continue to provide pedestrian trails, walkways, and sidewalks where appropriate to serve residents and visitors in the community. (Medium priority/On-going)
9. **Transit Center.** The County should work with the community, businesses, and appropriate agencies toward constructing a transit center to serve the community. This facility should include a bus turnout, shelter, benches, restrooms, information kiosk, secure bicycle parking, and public telephones. (High priority/Short term)

San Simeon Acres

1. **Pedestrian Improvements.** The San Simeon Acres Community Services District and the County should work with motel owners and residents to develop a clearly-defined walkway system that links residences, motels, and beach areas. The network may include sidewalks, bikeways, street lights, landscaping, and beach stairs. (Medium priority/Medium term)

2. **Highway One Crossing.** The County, CalTrans, and the community should discuss a safer and more convenient pedestrian crossing to link residential and motel areas east of the highway with the beach areas. The crossing may include channelization for left turn lanes at Vista Del Mar and Pico Avenue. Also, a traffic light may be needed on Highway One to stop high-speed traffic at an appropriate location. (Medium priority/Medium term)

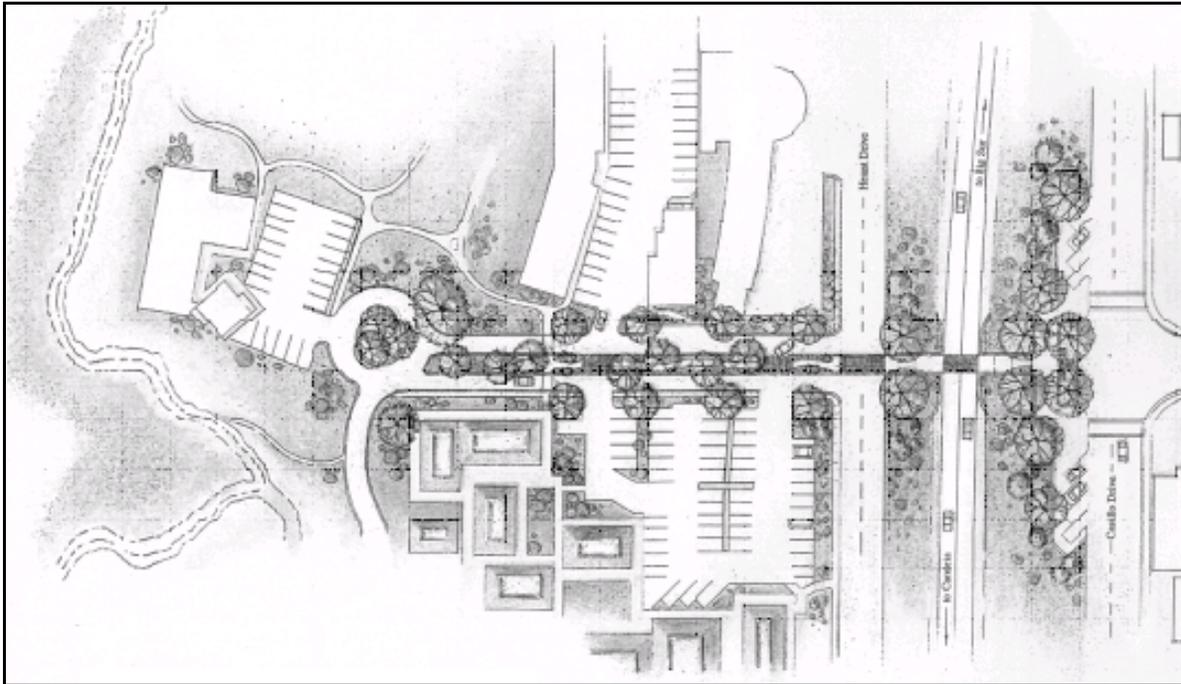


Figure 5-2: Highway One Crossing Plan, San Simeon Acres

Table 5-3
Schedule for Completing Circulation Programs
North Coast Planning Area

PROGRAM NUMBER	PROGRAM	RESPONSIBLE AGENCIES	POTENTIAL FUNDING	TIME FRAME	TARGET DATES	NOTES TIMING/PRIORITY
AREA WIDE						
1.	DIRECTIONAL SIGNING	CAL TRANS COUNTY	COUNTY/ FEES	2005-2007	2007	MEDIUM PRIORITY LONG TERM
2.	COASTAL TRAIL	COUNTY, STATE, NON-PROFITS	COUNTY	2005-2015	2015	MEDIUM PRIORITY LONG TERM
3.A	HIGHWAY ONE STUDIES	STATE, COUNTY, DEVELOPERS,	FEES	2005-2007	2007	ORIGIN & DESTINATION FUNDING ALTERNATIVES
3.B	HIGHWAY ONE - CAMBRIA URBAN AREA	COUNTY	FEES/GRANTS	2005-2015	2015	LOW PRIORITY LONG TERM
3.C	HIGHWAY ONE - HEARST CASTLE	STATE, COUNTY	FEES/GRANTS	2005-2007	2007	MEDIUM PRIORITY MEDIUM TERM
3.D	HIGHWAY ONE - SCENIC HIGHWAY ENHANCEMENT PLANS	COUNTY, CAL TRANS	GRANTS	2005-2010	2010	INCLUDES FUNDING AND IMPLEMENTATION MEDIUM PRIORITY LONG TERM
4.	JITNEY/BUS SERVICE	COUNTY	FEES/ GRANTS	2005-2010	2010	MEDIUM PRIORITY LONG TERM
5.	PARK AND RIDE	CAL TRANS	FEES/ GRANTS	2005-2010	2010	MEDIUM PRIORITY LONG TERM
6. & 7.	PEDESTRIAN/BICYCLE PLAN/IMPROVEMENTS	COUNTY	DEVELOPER, TRAFFIC IMPACT FEES	2005-2010	ON-GOING	WITH ALL ROAD IMPROVEMENTS HIGH PRIORITY SHORT TERM
CAMBRIA						
<u>NO SECTION</u>	MAIN STREET ENHANCEMENT PLAN	COUNTY	FEES	2005-2010	2010	SELECTED IMPROVEMENTS
1.	CAMBRIA SEGMENT/HWY ONE	CAL TRANS/ COUNTY	FEES	2005-2010	2010	MONITOR RMS LONG TERM
2.	ROAD IMPROVEMENT FINANCING	COUNTY	FEES	ON-GOING	ON-GOING	ON-GOING
3.	CAMBRIA PINES ROAD EXTENSION	COUNTY	FEES/GRANTS	ON-GOING	ON-GOING	ON-GOING
4.	EAST VILLAGE PARKING DISTRICT/LOT	COUNTY	ASSESSMENT DISTRICT	2005-2010	2010	HIGH PRIORITY SHORT TERM
5.	FISCALINI RANCH EMERGENCY ACCESS	CCSD	CCSD	2005-2015	2015	HIGH PRIORITY LONG TERM
6.	TRANSIT EXPANSION	COUNTY	COUNTY/GRANTS	2005-2007	2007	SHORT TERM HIGH PRIORITY
7.	VOLUNTARY TRIP REDUCTION PROGRAMS	COUNTY	COUNTY/ DEVELOPER	2005-2010	ON-GOING	ON-GOING SHORT TERM HIGH PRIORITY
8.	CROSS-TOWN TRAIL	COUNTY/CCSD	COUNTY/GRANTS	2005-2007	ON-GOING	MEDIUM TERM MEDIUM PRIORITY
9.	TRANSIT CENTER	COUNTY	COUNTY	2005-2010	2010	HIGH PRIORITY SHORT TERM

PROGRAM NUMBER	PROGRAM	RESPONSIBLE AGENCIES	POTENTIAL FUNDING	TIME FRAME	TARGET DATES	NOTES TIMING/PRIORITY
SAN SIMEON ACRES						
1.	PEDESTRIAN SYSTEM & STREET LIGHTS	COUNTY & SSACSD	FEES/GRANTS	2005-2010	2007	ONGOING PROGRAM MEDIUM PRIORITY MEDIUM TERM
2.	HIGHWAY ONE CROSSING	COUNTY, CAL TRANS SSACSD	FEES/GRANTS	2005-2010	2007	MAY INCLUDE TRAFFIC LIGHT MEDIUM PRIORITY MEDIUM TERM

CHAPTER 6: COMBINING DESIGNATIONS & PROPOSED PUBLIC FACILITIES

A. Combining Designations



Combining Designations are special overlay land use categories applied in areas of the county with potentially hazardous conditions or significant natural resources. In these areas ~~such area~~ more detailed project review is needed to avoid or minimize adverse environmental impacts, or effects of hazardous conditions on proposed projects. ~~The following areas are subject to special Combining Designations. Geologic Study Areas and Sensitive Resource Areas on Hearst Ranch were modified by the 1977 Environmental Data Base, prepared by Envicom Corporation.~~ In some cases, specific standards affecting land use and development have been adopted for an area where a Combining Designation is applied. Those standards are found in Chapter ~~7~~: 8 Planning Area Standards and apply to development proposals in addition to the standards of Chapter 23.07 of the *Coastal Zone Land Use Ordinance*.

A. Combining Designations

1. **Geologic Study Hillside Areas (GSA)**. This designation for rural areas basically includes moderate to high landslide risk areas and moderate to high liquefaction hazard areas as identified in the Seismic Safety Element. ~~Two areas, however, were reevaluated by Envicom Corporation. The Environmental Data Base for Hearst Ranch removed some former high risk rated areas from the coastal lowlands. The Geologic Study Area for the Cambria Urban Area, formerly identified as a moderately high risk area, is limited to those unsubdivided portions of the community where slopes are greater than 20% encompasses the entire Cambria Urban Reserve Area. These areas of steep slopes must be evaluated for engineering problems associated with building and access construction related to lot size as well as possible aesthetic adverse visual impacts of caused by hillside grading for roads and building sites.~~
2. **Arroyo de la Cruz, San Carpoforo, Pico, San Simeon, Santa Rosa, and Perry, and Creeks (FH)**. These are identified areas of potential flood hazards; development and fill in the creeks should be avoided, or incorporate mitigation measures. Maintenance of the creek habitats is essential to protect many coastal resources. These creeks support a number of declining species, such as the Tidewater Goby, Striped Garter Snake, Western Pond Turtle, Red-legged Frog, and Steelhead Trout. (Mod43a)
3. **North Coast Shoreline (SRA)**. The entire shoreline is a valuable scenic and natural resource which must be protected from excessive and unsightly development. Most of the coastline consists of low marine terraces with accessible beaches and coves, interspersed with rocky shorelines and steep bluffs ~~providing for a variety of passive recreation uses.~~ Offshore are ~~found~~ rocks, reefs, and kelp beds. The Monterey Bay Marine Sanctuary provides protection for the rich offshore marine habitat, and extends from 35 degrees 33 minutes North latitude (a point on the West Ranch in Cambria, approximately 1600 feet south of SeaCliff Estates) northward through Monterey County. The rugged; Sierra Nevada, San Simeon and Piedra Blancas points are of significant visual and environmental importance. The entire North Coast between Ragged and San Simeon Point and Reef also sustains important are marine habitats, and provides for a variety of passive recreation uses, resources of particular importance. North of San Carpoforo Creek, steep-sloped mountains rise abruptly from the ocean, limiting public use to the scenic views from Highway One.
4. **Monterey Pine Forests (SRA)(TH)**. ~~Native Monterey Pines occur in only a few areas along the California coast from north of Santa Cruz south to Cambria, and on one of the Channel Islands off the Santa Barbara County Coast. While widely grown in the Southern Hemisphere as a commercial timber, the Monterey pine forest occurs in only three areas of its native California. The southernmost stand in California is the 2,500 acres surrounding Cambria, with another isolated 500 acres at Pico Creek. These stands are extremely important as a "gene pool," due to genetic variations found there that~~

protect some trees from pine pitch canker, a disease that is causing rapid loss of Monterey pine trees. Relatively undisturbed stands occur on the Cambria fringe area and in isolated pockets to the north. Monterey pine forests cover most of the Cambria Urban Area. The larger remaining stands in undeveloped areas should be retained intact as much as possible, by use of designing cluster development at very low densities in open areas or areas of sparse tree cover. and p Preservation of finer specimen stands is recommended through the use of open space easements, avoidance by development, and direct purchase. The introduction of hybrid species of pines is discouraged in the forest.

- 5. North Coast Creeks (SRA)(ESHA).** Portions of Santa Rosa, San Simeon, Pico, and Little Pico, Arroyo de la Cruz, and San Carpoforo Creeks are anadromous fish streams which should be protected from impediments to steelhead migration and spawning. Adjacent riparian and wetland areas provide important wildlife habitat. Ground water and surface waters are linked, and maintenance of the creek habitats is essential to protect many coastal resources. These creeks support a number of declining species, such as the Tidewater Goby, Striped Garter Snake, Western Pond Turtle, Red-legged Frog, and Steelhead Trout.
- 6. Local Coastal Program (LCP).** The Coastal Zone encompasses all lands within the. Cambria and almost all of the North Coast Planning Area. The LCP Combining Designation identifies specific programs to ensure that access to the shoreline is provided and that coastal resources are protected in accordance with the policies of the Local Coastal Program.
- 7. Bluff Erosion (GSA).** Portions of the coastline where bluff erosion poses a concern for siting new development have been noted. Development should be located so that it can withstand 75 years of bluff erosion, without the need for a shoreline protection structure that would substantially alter the landform, affect public access, or impact sand movement along the beach.
- 8. Archaeologically Sensitive Areas (AS).** The Archaeologically Sensitive Combining Designation identifies urban and rural areas known for the potential to contain cultural resources. Applicants of development proposals in these areas are required to obtain a records check and possibly a surface search prior to approval. Standards to protect resources are described the LCP Policy Document, and Chapter 7: Combining Designation Standards of the Coastal Zone Land Use Ordinance.
- 9. Visitor Serving Areas (V).** The commercial and recreation land use categories along Main Street in Cambria possess unique, visually pleasing characteristics which serve as visitor destination points.
- 10. San Simeon Creek Lagoon (SRA).**

★(The Rural Area is not part of this Plan Update.) ★

11. San Simeon Point (SRA).

★(The Rural Area is not part of this Plan Update.) ★

12. Arroyo de la Cruz (SRA).

★(The Rural Area is not part of this Plan Update.) ★

13. Piedras Blancas Dunes (SRA).

★(The Rural Area is not part of this Plan Update.) ★

14. San Simeon Creek Lagoon (SRA).

★(The Rural Area is not part of this Plan Update.) ★

B. Historic Combining Designations (H):

The following structures and sites are identified as potential historic resources. Historic designations are meant to protect the historic structure or resource, and the site directly related to the resource. Typical repair and maintenance activities are usually exempt from the standards and permit requirements. Permit requirements for development affecting historic sites are found in CZLUO Chapter 23.03 Permit Requirements and Section 23.07.100 Historic Site.

- 1. The Lull House (H).** Also known as the Music House, it was among the first structures built in Cambria. Grant and Lull operated a general merchandise store on the lower floor and George Lull lived upstairs. It was converted into a residence in the early 1870s and was the Music family residence for sixty years. (Located at 2581 Main Street, reference APN 013-241-014).
- 2. The Olallieberry Inn (H).** Also known as the Mandersheid/Franklin/Smithers House, this structure was built in the 1870's for the Manderscheid brothers who were apothecaries from Prussia . It was also the home of Benjamin and Blanche Music Franklin (great nephew of Benjamin Franklin) and Amos and Ida Terrill Smithers. (Located at 2476 Main Street, reference APN 013-242-005).
- 3. The Leffingwell House (H).** Also known as the Utley/Leffingwell/Goodall House, this house was built in 1875 for Merit Utley, a rancher, miner, and owner of the Cambria Meat Market. Subsequently, it was owned by Joseph Warren Jr. and then William and May Woods Leffingwell, Jr. The Leffingwell's daughter, Anna Berganini's estate sold it to Lloyd and Arminta (Minnie) Gregg whose daughter, Helen Gregg Goodall lived in the house for thirty years. (Located at 2420 Main Street, reference APN 013-242-002).
- 4. The Old Santa Rosa Chapel (H).** This church was built in 1870, and was the first church in the County built outside of mission grounds. A walk through the adjacent cemetery reveals the names of many of the Swiss, Portugese, and other early settlers who settled in the area in the mid-1870's and established thriving dairy farms. (Located at 2353 Main Street, reference APN 013-241-022).
- 5. The Thorndyke House (H).** Also known as the Williams/Thorndyke/Bright House and home to several popular restaurants in recent years, this house on Bridge Street was built around 1880 by Henry Williams, a prominent carpenter. Captain Thorndyke, the first lighthouse keeper at Piedras Blancas, and his wife moved in 1914. The Bright Family bought it after Mrs. Thorndyke's death in 1929 and occupied the residence for nearly fifty years. (Located at 4286 Bridge Street, reference APN 013-241-004).
- 6. The First Presbyterian Church (H).** The little white church on Bridge Street was built in 1876 by Henry Williams and Merrit Trace and his son, Verne. In 1930, former

President Calvin Coolidge attended this church while visiting W.R. Hearst. (Located at 4314 Bridge Street, reference APN 013-241-003).

7. **The Bank of Cambria (H).** The brick building at the corner of Main and Bridge Streets was built in 1928. It housed the Bank of Cambria until 1933 and subsequently the Bank of America. (Located at 2255 Main Street, reference APN 013-232-007).
8. **Soto's Market (H).** This structure was built for Joaquin and Agnes Maggetti Soto in 1939. Three generations of the Joaquin Soto family owned and operated the store. (Located at 2244 Main Street, reference APN 013-263-002).
9. **Camozzi's (H).** Of the many saloons that historically served the community of Cambria, this is the only one still in operation. It was originally built in 1922 for Adriano and Rosa Filliponi Camozzi as a two story hotel, card parlor/pool hall, and barbershop complex. The bar was added in 1933 after Prohibition ended. (Located at 2262 Main Street, reference APN 013-263-002).

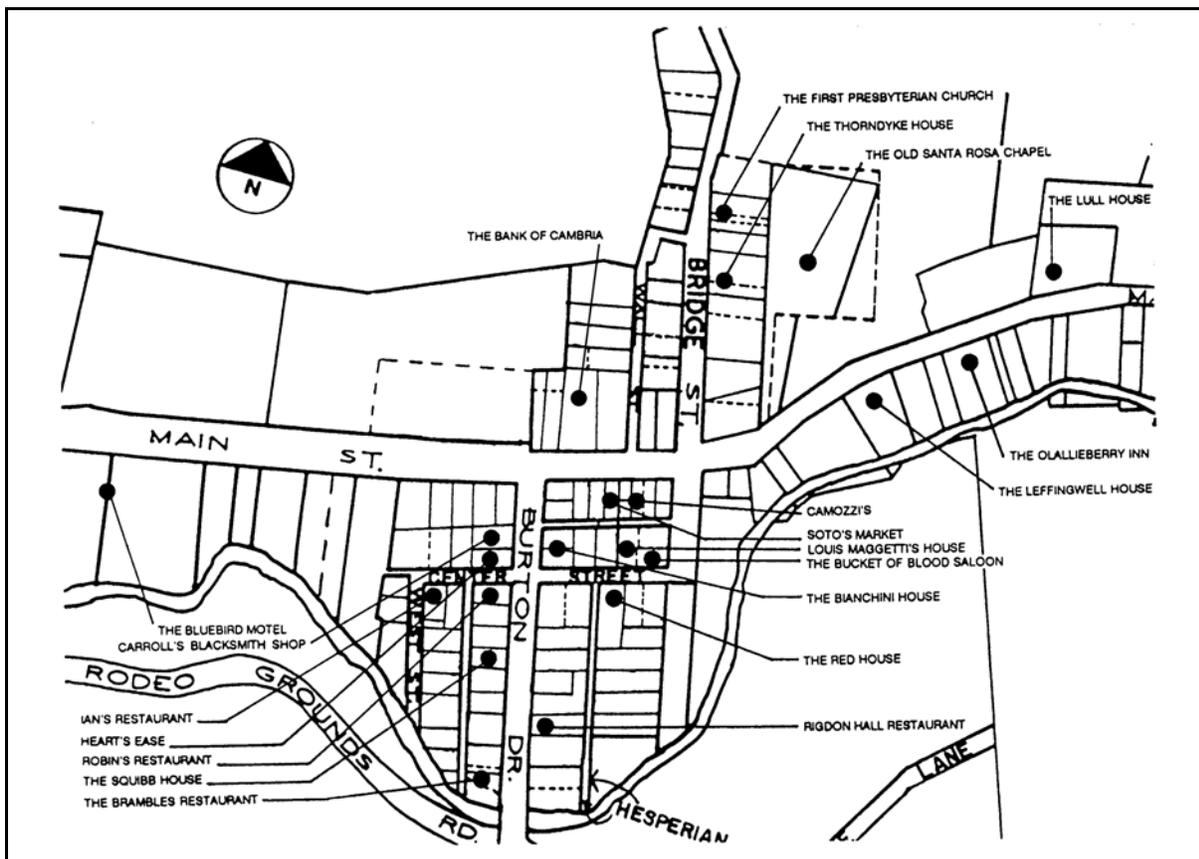


Figure 6-1: Location Map - Cambria/East Village Historic Designations

- 10. The Bucket of Blood Saloon (H).** The structure was built as a saloon in 1893 and has housed a blacksmith shop, a skating rink, an art studio, and a newspaper print shop. (Located on corner of Center and Bridge Streets, Reference APN 013-263-011).
- 11. Louis Maggetti House (H).** The tiny blue house on Center Street was built in the early 1870's. It is named in honor of the gentleman who added the second story in 1900 to make room for his six children. (Located at 2261 Center Street, reference APN 013-263-010).
- 12. The Red House (H).** This structure is located on what was once the site of Cambria's Chinese Community Center. The structure referred to generally as the "Red House" was assembled by combining several structures. One portion was the B.F. Franklin Building, used as Cambria's first high school around 1890. The portion commonly referred to as the "Joss House" is the only portion that remains on site. This portion, built in 1899, and is considered to be the oldest remaining Chinese temple in Southern California. (Located at 2264 Center Street, reference APN 013-264-014).
- 13. The Bianchini House (H).** Also known as the Guthrie/Bianchini House, the original structure was built in the mid-1860's. ~~this structure was built~~ In 1882 the house was bought by a merchant named Samuel Guthrie. In 1914 he sold the house to Eugenio Bianchini, a Swiss immigrant who came to the country in 1878 in search of opportunity= and found it. After farming, mining, dairying, and running a butcher shop, he retired to this home. The structure is listed on the National Register of Historic Places. The property has been unoccupied since 1970 due to legal problems and disagreement among the Bianchini heirs.(Located at 2251 Center Street, ref: APN 013-263-007).
- 14. Carroll's Blacksmith Shop (H).** This board and batten redwood building was built between 1895 and 1900 and is an example of the false-fronted shops that once lined the streets of Cambria and most other western towns. (Located at 4121 Center Street, reference APN 013-261-010).
- 15. Heart's Ease (H).** Also known as the Taylor House, this building was constructed in the 1870's by G. W. Proctor, and is reminiscent of nineteenth century New England. During the 1870's Cambria was described as having a neat, New England appearance. There is a one hundred year old rose bush growing beside the house. The Taylor's, whose timber ranch was sold in 1927 to the Cambria Development Company and became Lodge Hill, Park Hill, Happy Hill, and the West Village, bought the house in 1880. (Located at 4101 Burton Drive, reference APN 013-261-010).
- 16. Robin's Restaurant (H).** Also known as the Souza House, this restaurant was built as a residence in 1935 by Frank Souza, a construction supervisor at Hearst Castle. (Located at 4095 Burton Drive, reference APN 013-262-006).

- 17. Ian's Restaurant (H).** Also known as the Westendorf/Dickie House, the original portion of the structure was built in 1902 for William Westendorf. Mr. Westendorf sold the house to George and Dena Storni Dickie in 1915. In the 1980's the structure was moved from the corner of Center Street and Burton Drive to Center and West Streets. (Located at 2150 Center Street, reference APN 013-262-001).
- 18. The Squibb House (H).** This structure was built in 1877 by Fred E. Darke, a former civil war drummer boy who became a teacher when he moved to California. The structure to the right of the house was a carpentry shop built by Alex Paterson in 1889. Paul and Louise Squibb, founders of the Midland School of Santa Ynez Valley, purchased the property in 1953 and lovingly have preserved the house and grounds. The buildings and site have been converted to a Bed and Breakfast facility. (Located at 4063 Burton Drive, reference APN 013-262-007).
- 19. Rigdon Hall Restaurant (H).** Also known as the Rigdon House, the original structure was built in the early 1880s by the Bright family. This restaurant is located on the site of the two-story house once occupied by Rufus Rigdon, one of the town's original settlers. His son, Elmer Rigdon, became a California State Senator in 1917 and won approval for the construction of the Coast Highway between San Simeon and Monterey. (Located at 4036 Burton Drive, reference APN 013-264-007).
- 20. The Brambles Restaurant (H).** Also known as the Mora/Lowell House, the original three bedroom structure was built in the mid-1870's. In the early 1900's, Dr. Lowell lived in the house and ran his practice from a porch added on to the left side. (Located at 4005 Burton Drive, reference APN 013-264-014).
- 21. The Bluebird Motel (H).** This structure is located just west of old downtown Cambria, and was built around the home that George Lull built for his bride in 1880. At the time it was built, it was considered to be the most pretentious home in town. (Located at 1880 Main Street, reference APN 013-251-004).
- 22. Arthur Beale House (H).** More commonly known as 'Nitwit Ridge', this is a unique house constructed from discarded objects, and ~~is under consideration for recognition in the National Historic Register.~~ was declared a State Historic Landmark in 1981. (Located at 881 Hillcrest Drive, reference APN 022-131-031).
- 23. Hearst Castle (H)**

★(The Rural Area is not part of this Plan Update.) ★

24. The Sebastian Store (H)

★(The Rural Area is not part of this Plan Update.) ★

25. Van Gordon Archaeological Site (H)

★(The Rural Area is not part of this Plan Update.) ★

26. Hearst Ranch Headquarters (H)

★(The Rural Area is not part of this Plan Update.) ★

C. Proposed Public Facilities Combining Designations

The Land Use Element designates approximate locations of major proposed public facilities as Combining Designations. These include schools, parks, water and sewage treatment works, and other facilities necessary to serve the population proposed in the Plan. Full review of projects which might be proposed to implement these facilities is ~~would be~~ necessary, because as recognition as a Combining Designation does not assume that a project will be found consistent with the Local Coastal Program plan and other environmental policies and regulations.

- 1. Cambria Facilities.** Proposed public facilities for the Cambria Urban Area are designated symbolically indicated on the Combining Designations maps. ~~These include generalized site areas for future elementary schools and parks.~~ These include a new elementary school and several community-serving park sites. Other facilities, such as a new post office and police substation, are not shown on the maps because a number of locations may prove to be suitable.

- 2. Community Park Facilities.** Both Cambria and San Simeon Acres have a need for parks that primarily serve residents in the area. Local park plans have more detailed recommendations for locations and facilities than the designations found in this Community Plan. The Combining Designation maps show approximate locations where public parks would be desirable.

- 3. Regional Park Needs.** The regional parks nearest to the Planning Area are El Chorro Regional Park near San Luis Obispo, Heilmann Regional Park located in the community of Atascadero, and Santa Margarita Lake Regional Park near Santa Margarita. Because of the distance, there is a need for an additional facility in the North Coast or the Estero Planning Area. Any future park location should be further studied to assess potential impacts to adjacent uses and the surrounding area, the recreational needs and priorities of these Planning Area, and the ability for the proposed regional park site to contain recreation consistent with definition in the Parks and Recreation Element.

D. Combining Designation Programs

"Programs" are non-mandatory actions or policies recommended by the Land Use Element to achieve community or area wide goals and objectives identified in this area Plan. The implementation of each LUE program is the responsibility of the community, through the County or other public agency identified in the program itself. Because programs (some of which include special studies) are recommended actions rather than mandatory requirements, implementation of any program should be based on consideration of community needs and substantial community support for the program and its related costs.

Combining Designation Programs for the ~~North Coast~~ Planning Area are grouped under names of communities ~~or rural areas~~, and then under combining designations or other location headings to identify specific areas where they each apply.

Areawide

- 1. Shoreline Access - Vista Points (LCP).** The California Department of Transportation, State Department of Parks and Recreation, or other appropriate organizations should improve ~~existing~~ vehicle turnouts and accept ~~offers to dedicate~~ between Oak Knoll Creek and Point Piedras Blancas dunes public access easements in the Planning Area, where such improvements would be consistent with the County's LCP Public Access policies and ordinances. (Mod45) The improvements could ~~should~~ include:

- A. Bluff Protection.** Bluff revegetation and stabilization.
- B. Parking.** Defined parking areas paved with permeable surface material.
- C. Refuse Disposal.** Trash receptacles.
- D. Trails.** Designated trails to the beach.
- E. Bluff safety.** ~~Wooden~~ guard rails along bluff edge outside of the designated trails to the beach.
- F. Signs.** Interpretive signs, displays, and coastal access signs on Highway One, and along the shoreline.
- G. Restrooms.** Public restrooms.

- 2. Public Acquisition - Piedras Blancas Point.**

★(The Rural Area is not part of this Plan Update.) ★

- 3. Piedras Blancas Dunes Coastal Access.**

★(The Rural Area is not part of this Plan Update.) ★

4. **Coastal Access Implementation Plan (LCP).** As part of the public access component of this Plan, the County should develop an implementation plan for opening and managing access easements acquired through existing and future accepted offers-to-dedicate for public access. The implementation plan should identify access priorities, appropriate management entities, funding sources, and a schedule for opening accessways. (Mod48a) Also as part of the implementation plan, the County should assist other appropriate government agencies and interested groups in documenting areas known to have been historically used for public use. (Mod48b)

5. **Coastal Bluffs.** The County Planning and Building Department should seek grant funding for and prepare a program to avoid permanent armoring of the shoreline and minimize impacts to the shoreline in developed areas using a long-term, comprehensive approach. The program should include preparation of an areawide shoreline erosion and bluff management plan focusing on annual bluff erosion rates and sand supply; bluff retreat and setbacks; emergency armoring procedures; and shoreline protection structure design, engineering, monitoring, and maintenance. The management plan should help determine whether it would be appropriate to allow construction of seawalls where seawalls already exist on abutting properties on each side. If so, the county should initiate an amendment to the Local Coastal Program, after consultation with the Coastal Commission, to accordingly revise the Coastal Plan policy and implementing Coastal Zone Land Use Ordinance provisions regarding construction of shoreline structures. (Rec 7.08)

6. **San Simeon Creek Lagoon Resource Protection (LCP).** The State Department of Parks and Recreation should designate the lagoon as a natural preserve to ensure resource protection. Trails in proximity to the wetlands shall be designed to protect the habitat values of the lagoon.

Cambria Programs 8 through 11 apply only to programs within the community of Cambria.

7. **Shoreline Access Improvement - San Simeon State Park (LCP).** The State Department of Parks and Recreation should continue to maintain provide improvements to San Simeon State Beach at Leffingwell Landing, including a small parking area, small-scale boat launching facility, and signs.

- ~~9. Shoreline Access Signs. The State Department of Parks and Recreation should maintain public access signs on the state park properties in Park Hill.~~

- ~~10. Shoreline Access Improvements. The county should apply for a Coastal Conservancy grant to provide improvements to the three acre county park at Windsor Drive and Lampton Avenue. The improvements should include: parking area, trail, trash receptacles, signs and benches.~~

- 8. Shoreline Access - Street Improvements - West Lodge Hill (LCP).** The County ~~or other appropriate entity~~ should ~~improve~~ provide a public pathway and overlook at the following street ends west of Sherwood Drive with signs and walkways when development and maintenance funds are available: Castle Street, Emmons Road and Jean Street. ~~Warren Road and Harvey Street are each 50-foot rights-of-way and should be improved for limited public parking consistent with assuring bluff stability and the visual amenities of the area.~~
- 9. County-owned Surplus Lots (LCP).** To reduce impacts on traffic, water use, and the Monterey pine forest, substandard lots acquired by the County in the small lot subdivisions, such as Park Hill, Happy Hill and Lodge Hill areas, should be kept in public ownership and developed as a public access resource if appropriate. If such development is not appropriate, lots shall be offered for sale to adjacent owners when merged with existing lots to be used as yard areas or open space or transferred to appropriate public agencies or non-profit groups to be kept in permanent open space. (Mod48)
- 12. Lodge Hill Beach Maintenance.** ~~The county will accept maintenance responsibility for its holdings of the beach areas adjacent to Lodge Hill.~~

~~Sensitive Resource Areas (SRA)~~

- 10. Site Design (SRA).** Where development may be allowed in shoreline and other Sensitive Resource Areas, the County should encourage the use of cluster development.
- 11. Monterey Pine Forest - Forest Management District (SRA)(TH).** ~~Monterey Pine Forest (SRA) - Coastal Conservancy Project. The county will work with the State Coastal Conservancy to complete a study for the preservation of the most heavily forested and steep slope areas of Lodge Hill. The county should work to define the project areas and to designate wildlife corridors that could be preserved. The project should include evaluation of:~~
- ~~a. Lot consolidation. - This would require a study of the economic feasibility of lot consolidation to estate size lots for resale as a single building site or as a site for cluster residential development.~~
 - ~~b. TDR. - Analysis of the feasibility of a Transfer of Development Rights Program which may include portions of the unsubdivided areas of Cambria as transfer areas. Interested property owners should be invited to participate in the formulation of recommendations. This could allow for preservation of a portion of the presently subdivided areas of Cambria (particularly the Lodge Hill area) and a transfer of the development to more desirable areas without slope or heavy tree coverage concerns.~~
- ~~Based on the information provided in the study, the actual implementation program (e.g., purchase, consolidation and resale) could be completed by the conservancy, by private development, or a joint project.~~

c. ~~Erosion Control, Landscaping, and Development Manual.~~

The County should assist in the formation of a Forest Management District or other appropriate organization based upon the recommendations of the Forest Management Plan. The purpose of the District would be to implement the recommendations of the Plan, and provide for the long-term management of the forest. On-going functions of the District might include grant management, education, agency coordination, open space management, tree inspection and removal advice, restoration efforts, erosion control, and other activities. (Rec4.37)

The County should establish a Forest Management District, or an equivalent, setting up a new forest impact mitigation fee and fund using fees collected for removal of Monterey pines. The fees shall be established in compliance with provisions of the Mitigation Fee Act and may be used for creation and start-up of a Forest Management District, habitat enhancement, inspection, enforcement, education, and other forest management efforts. After establishment of the appropriate fee structure, new development may be required to pay reasonable fees to offset the impact of tree removal in the pine forest. The fund shall be designated for Monterey pine habitat protection, restoration, and enhancement. No mitigation fee would be required for removal of dead or diseased trees, or trees in dangerous condition as determined by the County or its representative. All mitigation fees are in addition to any other fees for inspection or permits, and shall be paid prior to approval of any State or County permit.

12. Erosion Control Program (LCP). ~~The County should will prepare a coordinated resource management program as part of Phase IV of Local Coastal Program implementation-Development to reduce erosion in the Lodge Hill Cambria Monterey pine forest. The program should be integrated with the Forest Management Program, and should utilize the best available management methods may include road system improvements, establishment of a formalized drainage system, stabilization of cut banks or other techniques to protect the forest and to reduce areawide erosion and sedimentation problems impacts.~~

Siltation shall be controlled and sediment shall be prevented from entering Santa Rosa Creek, or damaging other coastal resources, to the maximum extent feasible. Implementation of the certified program and establishment of a long-term funding source will require an erosion control/forest management fee established in compliance with the provisions of the Mitigation Fee Act. of \$400 per new dwelling unit shall be paid to a special trust fund established by the County for use in the implementation of an erosion control programs. for Lodge Hill. In addition, a manual will shall be prepared by the County as a part of the program Phase IV of Local Coastal Plan Development to address special development issues pertaining to the Monterey pine forest. The manual will emphasize techniques that may be used to prevent erosion and enhance and preserve the landscape, and will recommend special development regulations. affecting development. (Rec3.04)

~~5. **Specific Plan - Lodge Hill.** Based on the results of the Coastal Conservancy studies, the county should request Local Coastal Program Phase IV funds for the preparation of Specific Plans for those blocks that are: 1) heavily forested, 2) with slopes greater than 20%, 3) where existing development is very low. These studies require the county to work with the property owners to evaluate alternatives that would preserve as much as possible the physical characteristics of the area through clustering residences to the areas with the least impact.~~

13. Small Lot - Open Space District (LCP). As one component of a larger build-out reduction program, the County, CCSD, the community of Cambria, and other stakeholders should work together to establish an Open Space District to begin purchasing and maintaining many of the small and substandard lots in Cambria. The objective of the District would be to preserve the Forest from being physically displaced as small lots are developed with residential units. Secondary benefits would include lower building density, better fire clearance, more privacy between homes, larger yards, and more landscaping in neighborhoods.

Opportunity exists to begin a program to purchase and provide on-going maintenance for some of the small substandard lots in Lodge Hill, Happy Hill, and Park Hill on an annual basis, and then commit them to open space. In conjunction with the Forest Management District, the lots could be used for a variety of purposes such as pocket parks, viewsheds, habitat preservation, and other uses beneficial to the community. The program would enhance the value of properties located near open space lots, as well as reduce crowding of buildings, traffic congestion, and demand for water and other services.

14. Lot Consolidation for Monterey Pine Forest Protection (SRA)(TH). The County should review its procedures and mechanisms and, where necessary, suggest legislative changes to encourage lot consolidation within subdivisions that contain the substandard subdivided area lots.

15. Transfer of Development Credit Program (TDC) (LCP). The Transfer of Development Credits Program has shall have the objective of reducing potential build-out in the Special Project Areas (see Chapter 7: Planning Area Standards) for residential single family development, by transferring the development potential of these areas to the remainder of Cambria small lot divisions. the Cambria area, especially the substandard lot areas of Lodge Hill, to be within the public service capacity of the area and, where possible, new development should be transferred from the more environmentally sensitive areas to areas where less impact may occur. The County will continue to work with the State Coastal Land Conservancy of San Luis Obispo County, or other program administrator approved by the County, to establish a two-year pilot program for encourage voluntary transfer of development credits from Special Project Areas, designated preservation areas in Lodge Hill. In conjunction with the Conservancy, the county shall prepare ordinances to implement the transfer of Development Credits

program and to permit payment of a fee to offset acquisition costs for lots in the preservation areas. (Rec4.41)

(The following TDC text has been moved to Chapter 7; Planning Area Standards)

- ~~c. Establishment of Preservation Areas. The TDC program shall establish preservation areas that best reflect characteristics of Lodge Hill deemed most sensitive, but shall include the steeper slopes, heavy tree covered areas, Highway 1 view corridor and areas with potential to function as wildlife corridors.~~

- ~~d. Relation of Special Project Areas and Preservation Areas. The Special Project Areas represent the best present approximation of the most sensitive portion of the potential Preservation Areas based on existing information. The standards for the Special Project Areas shall apply to the Preservation Areas when the latter are designated. The Preservation Areas finally adopted will need to be larger to achieve a reduction of build-out potential to a level that is more consistent with available service capacities.~~

- ~~e. Change of TDC Program Requires LCP Amendment. Any change to the certified TDC program shall constitute an amendment to the LCP.~~

- ~~f. Permitted Development Area for Lodge Hill Projects Involving TDC's. Permitted development areas for projects involving TDC's shall be determined from Table 2 by:
 - ~~(1) Taking the lot area of the site where the development is to occur (the subject lot) and adding to it the lot area of the lot(s) to be retired (the TDC lots) to create a sum, and~~

 - ~~(2) Determining the permitted footprint and Gross Structural Area indicated for this sum for the category of the subject lot as shown in Table 2.~~

 - ~~(3) In no case shall the permitted footprint and Gross Structural Area be allowed to exceed respectively 45% and 90% of the area of the subject lot.~~~~

- ~~g. Criteria for Retirement of Lots and Gaining Additional Square Footage. Lots within Special Project Areas may qualify for additional footprint area and Gross Structural Area only by retiring lot(s) within a Special Project Area. In no case shall a TDC be transferred to a building site within a Special Project Area from outside the area.~~

- ~~h. Eligible Purchasers of TDC'S. Purchasers of TDC's may include the following:
 - ~~1. **Owners of small lots within Lodge Hill.** Through retirement of another lot, a TDC could be given that would allow an increase in the permitted coverage/gross structural area on the original parcel.~~~~

-
- ~~2. **Owners of properties within the Cambria Urban Reserve Line.** Lots that are not presently provided with sewer or water service by the Cambria Community Services District (CCSD), through participation in the retirement of lots on Lodge Hill, transferring of water and sewer credits that would otherwise have been developed on the original lots may be appropriate.~~
- ~~3. **Joint Retirement.** By owners in (1) and (2) a portion of the cost of retirement could be borne by each group, with the owners on Lodge Hill receiving permission for additional residential square footage and the owners of unserviced areas receiving water and sewer credits.~~
- ~~I. **Payment of Fees.** Under this program, a fee may be paid to the Conservancy or another appropriate nonprofit organization who would be responsible for purchase and retirement of the lots. The fee must be adequate to allow for purchase of typical lots within the preservation areas, plus sewer assessments and administrative costs.~~
- ~~J. **Resale Provisions.** With the concentration of purchases in specific preservation areas, the program may propose lot consolidation and eventual sale of portions of the preserved area, as estate-sized parcels with appropriately sited building sites. Using this approach, money collected from the sale of the estate lots would allow for retirement of additional lots.~~

16. Santa Rosa Creek (SRA) (ESHA) (SRV). Santa Rosa Creek supports a rich diversity of plant and animal species. The County should encourage the establishment of a Creek Enhancement Plan that may include:

- A. Restoration.** Methods may include restoration of summer stream flows, removal of exotic invasive plants, open space acquisition, protection of instream habitat, and an appropriate level of passive recreation use.
- B. Creek Enhancement - Suggested Goals.** Goals and objectives for the Enhancement Plan may include:
- 1. Conservation of watershed;**
 - 2. Protection and enhancement of habitat values;**
 - 3. Preservation of the natural flood plain;**
 - 4. Reduction of non-source point pollution and sedimentation;**
 - 5. Improvement of water quality and quantity;**
 - 6. Acquisition of lands for open space and/or parks;**
 - 7. Restoration of Santa Rosa Creek lagoon;**
 - 8. Development and maintenance of an information data base;**

- 9. Determination of areas where creek setbacks from development shall be maintained without adjustment or variance;
- 10. Increase in capacity to handle flooding;
- 11. Enhancement of the basin as a municipal water source for Cambria, consistent with protection and enhancement of habitat values. (Mod49)

17. Historical Survey. The County should work with the Cambria Historical Society, property owners, and others to conduct a historical survey and prepare documentation on the historical significance of properties in the East Village of Cambria.

San Simeon Acres. Programs ~~6 through 7~~ 18 and 19 apply only to access programs within the community of San Simeon Acres.

~~5. Shoreline Access Signs. The California Department of Transportation should provide signs on Highway 1 indicating beach access at Ruta Lane. The California Conservation Corps should provide a sign at the Ruta Lane stairway.~~

18. Shoreline Access - Acceptance and Maintenance (LCP). The San Simeon Acres Community Services District or other appropriate agency should accept and maintain public access easements offered through recorded offers-to-dedicate required for new development along the shoreline.

19. Shoreline Access - Pico Avenue Ruta Lane Stairway (LCP). The San Simeon Acres Community Services District should continue to maintain the stairway at Pico Avenue at ~~Ruta Lane~~ for public access. Public parking should be provided. ~~at the end of Ruta Lane.~~

Table 6-1
Schedule for Completing Combining Designation Programs

PROGRAM NUMBER	PROGRAM	RESPONSIBLE AGENCIES	POTENTIAL FUNDING	TIME FRAME	TARGET DATES	NOTES
COMMUNITYWIDE						
1.	SHORELINE ACCESS PROGRAMS	COUNTY, CSD'S, STATE	FEES/GRANTS	ON-GOING	ON-GOING	MULTIPLE LOCATIONS
2.	PUBLIC ACQUISITION (Rural Area, not a part of this update)					
3.	COASTAL ACCESS (Rural Area, not a part of this update)					
4.	COASTAL ACCESS IMPLEMENTATION	COUNTY, COMMUNITY GROUPS, AGENCIES	GRANTS/FEES	ON-GOING	ON-GOING	MULTIPLE LOCATIONS
5.	COASTAL BLUFFS	COUNTY, STATE	GRANTS	2005-2010	2010	NEW PROGRAM
6.	SAN SIMEON CREEK LAGOON RESOURCE PROTECTION	STATE PARKS	FEDERAL/ STATE/ COUNTY	2005-2025	2025	
CAMBRIA						
7.	LEFFINGWELL LANDING	STATE PARKS	GRANTS	2010	2010	
8.	WEST LODGE HILL COASTAL ACCESS	COUNTY	GRANTS	2010	2025	
9.	SURPLUS LOTS	COUNTY	GRANTS	ON-GOING	ON-GOING	
10.	SRA SITE DESIGN	COUNTY	FEES	ON-GOING	ON-GOING	
11.	FOREST MANAGEMENT PLAN/DISTRICT	COUNTY	FEES	2005-2010	2010	HIGH PRIORITY
12.	EROSION CONTROL PROGRAM	COUNTY & RCD	FEES/GRANTS	2005-2010	2010	COMBINE WITH FOREST MANAGEMENT
13.	SMALL-LOT OPEN SPACE DISTRICT	COUNTY, NON-PROFIT	FEES/GRANTS	2005-2010	2010	NEW PROGRAM
14.	LOT CONSOLIDATION	COUNTY	NONE REQUIRED	ON-GOING	ON-GOING	
15.	TDC PROGRAM	COUNTY/LAND CONSERVANCY	FEES/GRANTS	2005	ON-GOING	EXISTING PROGRAM
16.	SANTA ROSA CREEK ENHANCEMENT	COUNTY/NON-PROFIT	GRANTS	2005-2015	2015	NEW PROGRAM
17.	HISTORICAL SURVEY	COUNTY, PROPERTY OWNERS, HISTORIC SOCIETY	GRANTS	2005-2015	2015	
SAN SIMEON ACRES						
18.	SHORELINE ACCESS	SSACSD/COUNTY	FEES/GRANTS	ON-GOING	ON-GOING	
19.	PICO AVENUE STAIRWAY	SSACSD/COUNTY	FEES/GRANTS	ON-GOING	ON-GOING	

CHAPTER 7: PLANNING AREA STANDARDS



This chapter contains special “standards” for the North Coast planning area. Standards are mandatory requirements for development, designed to handle identified problems in a particular rural area, or to respond to concerns in an individual community. Planning area standards can range from special setbacks in one neighborhood, to limits on the kinds of land use normally allowed by the LUE (Table O, or Coastal O Part I) because of certain community conditions.

These standards apply to the planning and development of new land uses, and must be satisfied for a new land use permit to be approved, and for a newly constructed project to be used. All standards listed in this area plan may not affect the entire planning area. Most apply to specific communities or rural locations.

North Coast area standards are organized under several headings describing locations in the planning area where they apply. Standards are grouped first by community, then by land use category, and finally under headings naming a specific area where the standard must be satisfied.

These requirements apply to proposed projects in addition to provisions of the Land Use Ordinances. Where these standards conflict with the LUO, or CZLUO, these standards control. In any case where this area plan designates a property in the Open Space or Recreation land use categories, in the Sensitive Resource or Historic Area combining designations, or where the LUE identifies a need for open space preservation through easement, contract or other instrument, such designation does not in and of itself convey or imply any right of public use, access, trespass or violation of privacy.

Land dedications and other exactions identified in this Plan are intended to mitigate the impacts of specific development proposals, and to ensure consistency with the Local Coastal Program. In some circumstances the County may conduct a nexus and proportionality study, to provide the practical and legal basis for the proposed exactions. Dedications and exactions will be pursued consistent with Section 30001.5 of the California Coastal Act considering the need to:

1. Assure orderly, balanced use and conservation of coastal zone resources taking into account the social and economic needs of the people of the state; and
2. Maximize public access to and along the coast and maximize public recreation opportunities in the coastal zone consistent with sound resource conservation principles and constitutionally protected rights of private property owners. (Mod51)

Chapter Organization

A. RURAL AREA STANDARDS. Sections contain standards that apply only to land outside of urban and village reserve lines.

★(The Rural Area is not part of this Plan Update.) ★

B. CAMBRIA URBAN AREA STANDARDS. Sections contain standards that apply only to land within the unincorporated urban area of Cambria.

C. SAN SIMEON ACRES VILLAGE STANDARDS. Sections contain standards that apply only to land within the unincorporated village area of San Simeon Acres.

Within each area (Rural, Cambria Urban, or San Simeon Acres Village), standards which apply to Combining Designations are listed first, followed by communitywide standards. Communitywide standards are followed by standards that are listed by land use category, as follows:

- | | |
|-----------------------------------|-------------------------------------|
| <u>1. Open Space</u> | <u>6. Recreation</u> |
| <u>2. Agriculture</u> | <u>7. Residential Multi-Family</u> |
| <u>3. Commercial Retail</u> | <u>8. Residential Single-Family</u> |
| <u>4. Office and Professional</u> | <u>9. Residential Suburban</u> |
| <u>5. Commercial Service</u> | <u>10. Public Facilities</u> |

Unless a sub-heading identifies specific areas or properties where the standards apply, Land Use Category standards apply to all of the land within the applicable land use category.

**Example outline of
Planning Area Standards**

B. CAMBRIA URBAN AREA STANDARDS *(major section)*

Commercial Retail *(land use category)*

- 1. Standard one.** *(standard)*
- 2. Standard two.** *(standard)*
 - A. Permit requirement.** *(standard)*
 - B. Building setbacks.** *(standard)*

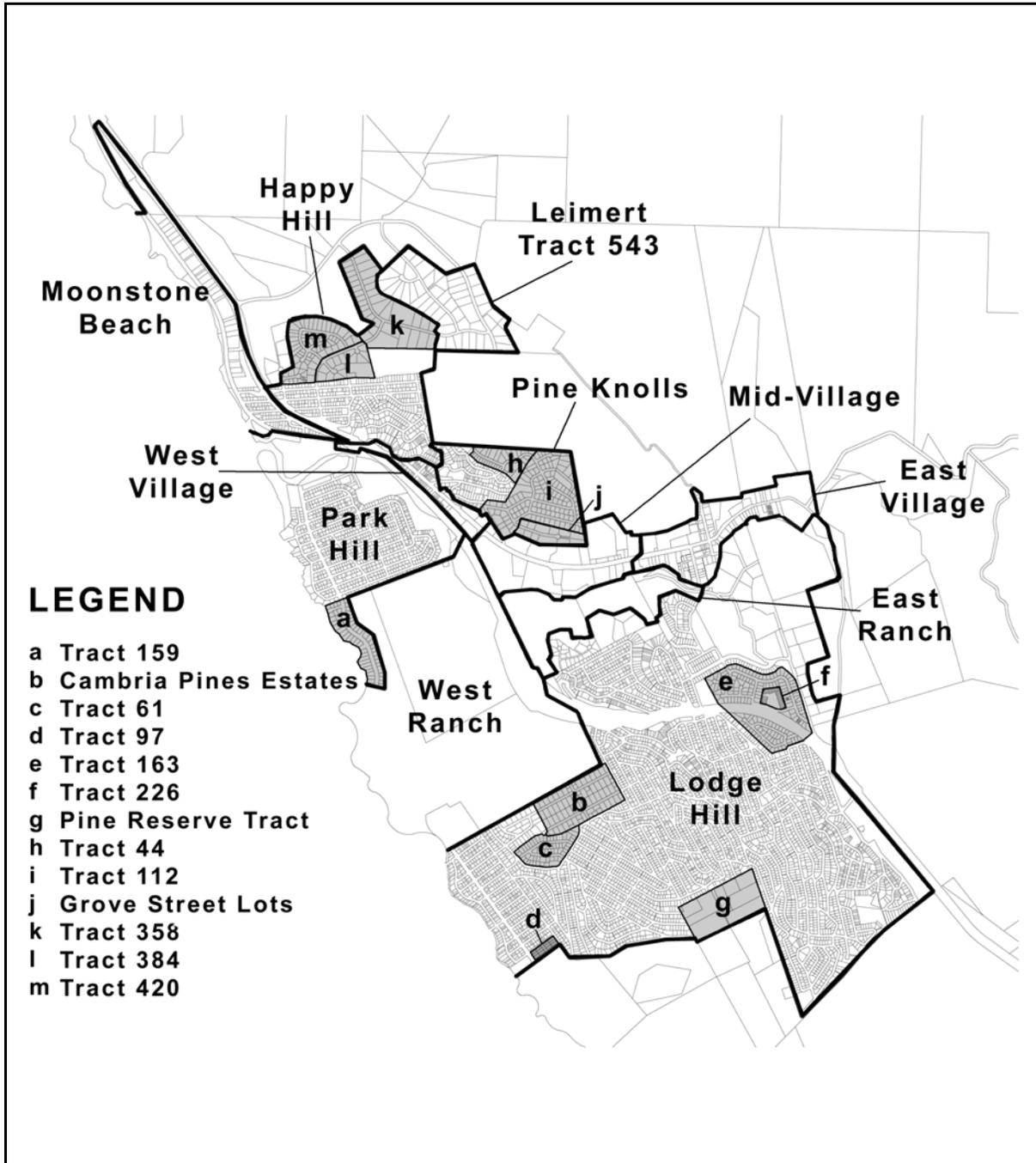


Figure 7-1: Cambria Location Map

A. NORTH COAST RURAL AREA STANDARDS

★(The Rural Area is not part of this Plan Update.) ★

B. CAMBRIA URBAN AREA STANDARDS

The following standards apply to development on lands within the Cambria Urban Reserve Line in to the land use categories or specific areas listed, and as shown in Figure 7-1.

COMBINING DESIGNATIONS: The following standards apply to areas of special concern as shown on official maps.

1. **Marine Habitat (SRA) - Projects with Point-Source Discharges.** The richness, sensitivity, and unspoiled character of the marine habitats within the Cambria URL demand particularly rigorous measures to ensure the protection of these special resources. Accordingly, no surface point-source discharges into the marine environment are allowed, except as follows: (Mod68)

Exceptions:

- A. **Cambria Community Services District.** Any capacity expansions needed to serve permitted growth within the existing Cambria Community Services District (CCSD) service areas, provided that any new outfall is consistent with Monterey Bay National Marine Sanctuary (MBNMS) provisions.
- B. **Proposed CCSD desalination project.** Any brine discharge facilities provided that:
 1. Brine discharge is consistent with MBNMS, Environmental Protection Agency (EPA) and Regional Water Quality Control Board (RWQCB) regulations.
 2. The discharge point is located south of San Simeon Point, and where it will not adversely impact any kelp bed or intertidal habitat.
 3. The discharge point is designed to maximize rapid mixing of the brine with ambient seawater in order to minimize hypersaline concentrations.

C. Passthrough Discharges. Aquaculture seawater passthrough discharges, provided that:

1. Discharge is consistent with MBNMS, EPA, RWQCB and California Department of Fish & Game (CDFG) Regulations.
2. The discharge point is located south of San Simeon Point.
3. The discharge method will not result in a eutrophic concentration of nutrients, and will not result in adverse impacts to wild abalone populations or other native marine organisms.

D. Seawater Passthrough Devices. Seawater passthrough discharges for public aquaria, and for scientific research facilities, provided that:

1. Discharge is consistent with MBNMS, EPA, and RWQCB Regulations.
2. The discharge method will not result in adverse impacts to kelp beds or other native marine organisms.

E. Water Quality Enhancement. Discharges to streams, for the purpose of hydrologic replenishment and/or stream water quality enhancement, provided that:

1. Discharge is consistent with NMFS, U.S. Fish & Wildlife Service (USFWS), EPA, RWQCB, and CDFG Regulations.
2. The discharged waters will be of appropriate temperature and quality so as not to disrupt the steelhead runs, nor the in-stream habitat for any other sensitive species including, but not limited to, the red-legged frog and tidewater goby nor will impact adjacent agriculture. (Mod68)

- 2. Monterey Pine Forest Habitat (SRA) (TH) - Purpose.** The following standards and procedures provide guidance for development in the Monterey Pine Forest in Cambria. The purpose of these standards is to minimize tree removal and impacts to the sensitive pine forest habitat. While the intent of these standards is to reduce redundancy and provide consistency in the planning process, a biological report may be required pursuant to the Coastal Zone Land Use Ordinance.
- A.** The project applicant shall demonstrate that no vegetation outside of the “project limit area” shall be removed, except for removal consistent with fire safety standards or trees identified as hazardous by a qualified professional.
- B.** Where a report is required by CZLUO Section 23.07.170, the required topics shall be supplemented with an analysis and map of the site constraints that should be considered, and a recommendation of alternative design measures that will minimize loss of Monterey Pines, oaks and forest habitat.
- C. Plan Requirements.** All site, construction and grading plans submitted to the County shall identify by species and diameter all Monterey pine trees that are six inches or more in diameter 4.5 feet above ground and oak trees four inches or more in diameter 4.5 feet above ground identified by species and diameter. The plans shall indicate which trees are to be retained and which trees are proposed for removal. In addition, such plans shall clearly show: (Mod126)
- 1.** The “project limit area” shall include all areas of grading (including cut and fill areas) and vegetation removal, the development footprint (i.e., all structures and/or site disturbance within the “project limit area”) necessary fire clearances and staging areas for all construction activities, the location of those activities, and areas for equipment and material storage.
 - 2.** Sturdy and highly visible protective fencing that will be placed along the 'project limit area'. Plan notes shall indicate this fence should remain in place during the duration of project construction to protect vegetation from construction activities.
 - 3.** Plan notes shall indicate native trees and undergrowth outside of the “project limit area” shall be left undisturbed. (Mod125)
 - 4.** Identify any necessary tree trimming. Plan notes shall indicate a skilled arborist, or accepted arborist's techniques, will be used when removing tree limbs.
 - 5.** Plan notes shall indicate wherever soil compaction from construction will occur within driplines that the compacted root zone area shall be aerated by using one of the following techniques:

- a. Injecting pressurized water.
 - b. Careful shallow ripping that radiates out from the trunk (no cross-root ripping).
 - c. Other County-approved techniques.
- 6.** Plan notes shall indicate no more than one-third of the area of the drip line around any tree to be retained should be disturbed, or as recommended in an arborist's or biologist's report. (RDP)
- D.** **Notice of pending tree removal application.** Where Plot Plan approval is required for a tree removal permit, a notice shall be posted by the property owner or representative near the front property line on the subject site. The notice shall be maintained in good condition by the property owner until permit issuance. (RDP)
- E.** **Construction Practices.** Construction practices to protect Monterey pines, oak trees and significant understory vegetation shall be implemented. These construction practices are to include at minimum; (RDP)
- 1.** All plan notes required in Standard 3 above shall be implemented.
 - 2.** **Protective Measures.** Practices to protect root systems, trees and other vegetation shall include but not be limited to: methods prescribed in the Cambria Forest Management Plan; avoiding compaction of the root zone; installing orange construction fencing around protected areas shown on the site plan; protecting tree trunks and other vegetation from construction equipment by wood fencing or other barriers or wrapping with heavy materials; disposing of waste, paints, solvents, etc. off-site by approved environmental standards and best practices; and using and storing equipment carefully. (RDP)
 - 3.** **Stockpiling of Materials.** Materials, including debris and dirt, shall not be stockpiled within 15 feet of any tree, and shall be minimized under tree driplines as required by the land use permit and the Fire Safety Plan. Stockpiled materials shall be removed frequently throughout construction. All stockpiled materials shall be removed before final inspection. (RDP)
 - 4.** **Construction Practices.** Excavation work shall be planned to avoid root systems of all on-site trees and trees on abutting properties. Any trenching for utilities that may occur within the dripline of trees on the project site shall be hand dug to avoid the root system of the tree. (RDP)

5. **Driveway Placement.** When remodeling or replacing existing residences, avoid moving established driveways if trees or significant vegetation would be negatively impacted. (RDP)

F. Replacement of Vegetation. Any Monterey pine trees that are six inches or more in diameter 4.5 feet above ground removed shall be replaced at a 4:1 ratio. Any oak trees that are four inches or more in diameter 4.5 feet above ground removed shall be replaced at a 6:1 ratio. All open areas of the site disturbed by project construction are to be seeded with native, drought and fire resistant species that are compatible with the habitat value of the surrounding forest. Replacement vegetation shall be planted in conformance with the following measures:

1. A replanting plan shall be prepared as a part of the application. Elements of this plan shall include the type, size and location of vegetation; a description of irrigation plans; and plan notes indicating compliance with the standards of this section. Any proposed on-site or off-site replanting plan must be approved by the County Planning and Building Department prior to issuance of building permits.
2. Container sizes for all replacement seedlings shall be one gallon, unless approved by the Director of Planning and Building. *Pinus radiata var. macrocarpa*, the native Monterey Pine tree, shall be used for replanting of any pine tree removed. **No out of area Monterey Pine stock shall be used.**
3. New trees shall be planted to reinforce the forest character on the site and in the street frontage, and to screen proposed development.

If insufficient area exists to plant all or any of the replacement vegetation on site, then the replanting plan shall identify an appropriate off-site area and owned or managed by an appropriate government agency or nonprofit organization.

If an off-site replanting is chosen, the replanting must occur with the review and approval of the Environmental Coordinator, and shall be verified by submittal of a letter from the appropriate agency or organization to the Environmental Coordinator. All replacement conditions and monitoring measures (e.g., number of trees, maintenance, etc.) shall apply.

4. To prevent or reduce the spread of disease from pine pitch canker, bark beetles or other diseases affecting the forest, the following measures shall be followed:

- a. Infected or contaminated material shall not be transported to areas that are free of the disease;
 - b. When cutting or pruning a diseased tree, tools shall be cleaned with a disinfectant before using them on uninfected branches or other trees;
 - c. Disease and insect buildup shall be avoided by promptly removing and disposing of dead pine material by either burning (where and when allowed), burying, tarping with clear plastic for six months, or chipping. If material is chipped, it should be left as a thin layer on site;
 - d. Plant material shall be covered or enclosed when it is taken off-site to avoid dispersal of material contaminated with bark beetles.
5. Prior to final building permit inspection, the applicant shall provide a letter to the County prepared by a qualified nurseryman or landscape contractor that the revegetation plan has been properly implemented; and
 6. All new plants shall be maintained until successfully established. This shall include caging from animals (e.g., deer, rodents), periodic weeding and adequate watering (e.g., drip-irrigation system). If possible, planting during the warmest, driest months (June through September) shall be avoided. In addition, standard planting procedures (e.g., planting tablets, initial deep watering) shall be used.
 7. Maintenance shall be often enough to keep weeds at least 3 feet away from each planting, provide adequate moisture to all plants, and ensure all other components (e.g., irrigation system, caging) are kept in good working order.
 8. The health and maintenance of replacement vegetation shall be monitored at least once a year from the date of final building permit inspection for a period of time no less than three years, or until the vegetation is successfully established, whichever comes later. Monitoring reports shall be prepared by an expert competent in landscape planting and maintenance of the Monterey pine forest, and reports shall be submitted to and approved by the County.
- G. Understory Vegetation Removal.** No understory vegetation shall be removed until a permit has been issued or unless an immediate hazardous condition exists. Understory vegetation removal to create, improve, or maintain adequate defensible space and Fire Hazard Fuel Reduction shall be the minimum necessary. Evidence used to determine whether understory vegetation has been removed without a permit

will include, but is not limited to, all photo documentation available. At the time of permit application, if there is evidence that the understory has been cleared without a permit or if there is evidence that the understory has been cleared outside of the approved 'project limit area,' 20 plants from the following list shall be planted on-site for every 1,000 square feet affected. At least four different species shall be used of approximately equal amounts. All plants shall be from container stock of one-gallon or less. All planting shall be subject to the "Replacement of Vegetation" requirements described above.

1. Acceptable Species. The following are considered acceptable for replacement:

Quercus agrifolia (Coast Live Oak) - no more than two seedlings per 1,000 square feet

Arctostaphylos tomentosa (Manzanita)

Heteromeles arbutifolia (Toyon)

Rhamnus californica (Coffeeberry)

Rubus ursinus (California Blackberry)

Symphoricarpos mollis (Creeping Snowberry)

Vaccinium ovatum (Evergreen Huckleberry)

Ribes menziesii (Gooseberry)

Lonicera hispidula (Honeysuckle)

H. Clustering of Development Required. Clustering shall be required for new land divisions or multi-family residential development projects within the Monterey Pine Forest Habitat areas. When feasible, new development shall be restricted to slopes less than 20 percent.

3. Flood Hazards (FH). New development shall comply with Coastal Plan Policies for Hazards and the Flood Hazard provisions of the Coastal Zone Land Use Ordinance, and shall be reviewed for its relation to the Cambria Flood Mitigation Project. Approval of new development shall be contingent upon a finding that the proposed development will not interfere with implementation of the Cambria Flood Mitigation Project. (Rec 7.19)(CDP)

4. Santa Rosa Creek (FH). The following standards affect all land use categories in and adjacent to Santa Rosa Creek, as shown on Figure 7-2.

A. Biological Viability. Proposed development, including grading, and water well extractions, shall maintain the ecological viability of Santa Rosa Creek (as determined by the County in consultation with the appropriate State or federal agency), including the riparian corridor, stream channel, wetlands, and accompanying marine habitat.

B. Channelization or Filling in Floodways. Except for minor fill for public serving activities or uses (e.g., trails, roads, stream bank restoration, County flood

mitigation projects, and utilities), channelization or fill in the undeveloped floodway (active channel) and floodway fringe (flood plain) of Santa Rosa Creek shall be prohibited (see Figure 7-2).

C. Creek Setbacks and Habitat Protection. All new development shall be set back a minimum of 100 feet from the upland edge of riparian vegetation. Setbacks of less than 100 feet are allowed in accordance with Section 23.07.174d.2 of the Coastal Zone Land Use Ordinance. Recreational trails shall be sited outside of areas with riparian vegetation.

D. Public Access. All new development shall provide an offer to dedicate an appropriate area adjacent to Santa Rosa Creek to accommodate the Cross Town Trail, Santa Rosa Creek Trail, or other planned public access improvements.

- 5. Historical Preservation (H).** New development shall be consistent with Coastal Plan Policies protecting special communities and small-scale neighborhoods by, among other means, preserving structures of historic significance and complying with CZLUO provisions for historic sites and the Secretary of Interior's standards for the treatment of historic structures. This shall include an evaluation of the historic significance of all potentially historic structures listed in Chapter 6 of this plan according to the procedures established by the Secretary of the Interior. (CDP)

Alterations, including additions to these structures should follow the Secretary of the Interior's Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings or the Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings (1995, Weeks and Grimmer) or equivalent. Implementation and adherence to the Standards should take into consideration the structure's integrity and setting as appropriate.

Local Coastal Plan (LCP)

- ~~**1. Shoreline Access - Fiscalini Ranch.** New development shall be required to incorporate means to ensure public access and recreational use on a permanent basis. Such assurance shall include an offer-to-dedicate or a deed restriction for the area west of the extension of Windsor Boulevard. The extent of dedication and improvements and appropriate agency for maintenance will be determined as part of the development plan. The level of public access must be consistent with the extent of the development approved. Improvements to this are shall include:~~
- ~~A. A minimum of two vertical pedestrian trails extending from the extension of Windsor Boulevard to the bluff edge. One trail to be located at the north and south of the extension of Windsor Boulevard. At each trail head, clustered public parking areas will be provided by the developer at the time of development on the west side of Highway 1.~~
 - ~~B. A bicycle path shall be constructed west of the extension of Windsor Boulevard or incorporated in the design of the road.~~
 - ~~C. A picnic area with restrooms, parking and trash receptacles.~~
 - ~~D. Lateral access and public recreation from the seaward side of Windsor Boulevard to the mean high tide line, consistent with public safety and sensitive habitat concerns shall be provided.~~

COMMUNITY WIDE: The following standards apply to all land within the Cambria Urban Reserve Line.

1.3. Reservation of Service Capacity. The Cambria Community Service District (CCSD) shall reserve available water and sewage treatment capacity on a yearly basis for the following priority uses:

- A. Visitor-Serving Uses.** To allow for continued growth of visitor-serving facilities, 20% percent of the water and sewer capacity shall be reserved for visitor-serving and commercial uses.
- B. Affordable Housing - Program Required.** The CCSD shall reserve sufficient water and sewer capacity to serve affordable housing. If consistent with other permits, an amount not to exceed 25 percent of the Visitor-Serving reservation may be allocated to a qualified affordable housing program.

Prior to issuance of any further water will-serve letters, the District shall propose to the County a program to accommodate a limited number for affordable housing units each year. The program shall be consistent with definitions of affordable housing in the County Housing Element. The exact number shall be determined based on unmet housing needs, and availability of water. Under this program and to meet the need of affordable housing units, the District may divert part of the water which otherwise would have been allocated to the Visitor-Serving, Commercial, or Multi-Family Residential water waiting list.

2. Limitation on Residential Construction. In accordance with the Environmental Protection Agency's concern for environmental protection (as expressed in the condition on the sewer treatment facility expansion permit), the maximum number of residential permits shall not exceed 125 per year. This shall remain in effect as long as the EPA requires this condition.

~~To implement this, the final building inspection shall be issued upon receipt of a water permit from the Cambria Community Services District. Such letters shall be issued by the district on the following basis:~~

- ~~a. — Seventy (70) percent of the 125 permits shall be reserved for single family residential uses.—~~
- ~~b. — Thirty (30) percent shall be reserved for multiple family residential uses.—~~

- c. ~~At the end of each quarter, those permits for single family or multiple family units which are not issued in one quarter may be reallocated to either single family or multiple family residential use based on demand.~~

3. Cambria CSD service extensions outside the Urban Services (USL) or Urban Reserve Lines (URL). To allow the CCSD to meet current commitments, while providing for a community greenbelt and avoiding growth inducement, the CCSD may only provide services outside of the USL or URL of Cambria under the following circumstances:

A. The development was established and was located within the District's boundary or already receiving service prior to the adoption of this plan update, or;

B. Prior to May 13, 1997:

1. The site of the proposed development was within the CCSD's boundary and had a commitment to being served according to the district's regulations; and

2. The site of the proposed development had basic infrastructure in place for the required service; and

3. The site of the proposed development was in conformance with the Local Coastal Program, including allowable uses, densities, and minimum parcel size; and

4. The proposed development would cluster building sites in close proximity to the URL/USL and shall provide for permanent open space protection according to the Cluster Division requirements of the CZLUO. (Mod108)

4. Desalinization Plants. Desalinization plants constructed to serve development within the service boundaries of the CCSD shall only be permitted if owned and operated by the CCSD. Private desalinization plants are prohibited. (Mod109)

5. New Residential Land Divisions. Projects creating new residential lots shall be required to permanently retire an equivalent legal building site located within the Cambria URL on a 1:1 basis. Proof of the required retirement shall be submitted prior to recordation of the final parcel or subdivision map. Retired lots shall have an open space or conservation easement recorded against the title of the property to prohibit development in perpetuity. Easements may be held by the County or the County may grant them to another public agency or qualified non-profit organization. (Mod109) & (Rec 2.17)

5. Identification of Additional Water Resources. All proposals for subdivision or development outside the Community Services District Assessment District, (other than a

~~single family residence on an existing lot) shall be required to identify an additional water resource or an alternative for use of the existing allocation by retiring water permits needed for lots within the existing community services district (except for existing contractual arrangements). This may be an appropriate alternative to be developed through a Transfer of Development Rights Program.~~

- 6. Cambria Community Services District Review.** Prior to application acceptance, land use and building permit applications shall include a written verification of water and sewer service from the Cambria Community Services District. A water and sewer service condition compliance letter from the Cambria Community Services District shall be provided to the Department of Planning and Building prior to final building inspection.
- 7. Cambria Fire Department Review.** All new development shall comply with applicable state and local Cambria fire codes. Prior to application acceptance, land use and building permit applications shall include a fire plan review from the Cambria Fire Department.
- 8. Site Review.** Based on the results of a site review, all projects determined to have the potential to adversely impact a sensitive resource may be subject to discretionary review. If discretionary review is triggered, a biologic assessment report consistent with Coastal Zone Land Use Ordinance section 23.07.170 may be required.
- 9. Erosion Control.** In addition to other applicable requirements of the Coastal Zone Land Use Ordinance, ~~the following shall also be met~~ all runoff from impervious surfaces such as roofs, driveways, walks, patios, and/or decks, shall be collected and ~~detained~~ retained on-site to the greatest extent possible. Run-off not able to be retained on-site shall be or passed through an effective erosion control device or drainage system approved by the Public Works Department. County Engineer.

~~Permanent Erosion control devices, shall be installed prior to, or concurrently with, on-site grading activities.~~

~~———— If grading is to occur between October 15 and April 15, a sedimentation and erosion control plan shall be submitted per Coastal Zone Land Use Ordinance Section 23.05.036.~~

~~———— Grading, filling, or site disturbance of existing soil and vegetation shall be limited to the minimum areas necessary.~~

~~———— Stockpiles and other disturbed soil shall be protected from rain and erosion by plastic sheets, or other covering.~~

~~———— All areas disturbed by grading shall be revegetated, with temporary or permanent erosion control devices in place.~~

10. Landscaping. All areas of the site disturbed by project construction shall be revegetated with native, drought and fire resistant species that are compatible with the habitat values of the surrounding forest.

A. All landscaping and construction practices shall work to maintain and regenerate the Monterey pine forest and if applicable, the riparian corridor. Plant materials should be used to mimic or enhance naturally occurring vegetation. Materials should be propagated from native stock taken from an area within a 50-mile radius of Cambria to ensure that the gene pool is not diluted for endemic species. This is particularly true for Monterey Pines and riparian plantings. (CDP)

B. Prohibited Plant Materials. Non-native, invasive, fire prone, and water intensive (e.g., turf grass) landscaping shall be prohibited on the entire site. A list of prohibited plants, such as Pampas grass and Scotch broom, is available from the Department of Planning and Building.

11. Exterior Lighting. Use only the minimum amount necessary to achieve essential illumination.

A. All light fixtures, including security lighting, shall be aimed and shielded so that the direct illumination shall be confined to the property boundaries source. Particular care is to be taken to assure that the direct illumination does not fall onto or across any public or private street or road. Motion sensing light fixtures shall be fully shielded and properly adjusted, according to the manufacturer's instructions, to turn off when detected motion ceases.

B. All light fixtures are required to be fully shielded.



Figure 7-3: Focus Light Downward

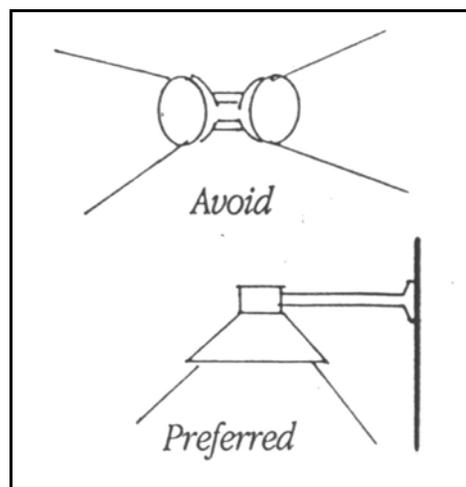


Figure 7-4: Light Positions

12. Santa Rosa Creek Frontage. (CDP)

- A.** Development on any site adjacent to Santa Rosa Creek shall be designed to face the creek as well as the street. Buildings on sites adjacent to the creek shall have activity centers and facades facing the creek that are designed to at least the same level of detail as the facades that face the street. (CDP)
- B.** On sites adjacent to Santa Rosa Creek, trash, recyclables and storage shall be in a visually concealed area such as an alcove in the building or an area screened by landscaping. (CDP)
- C.** Solid walls and fences are not allowed between buildings and the creek.

13. Site and Project Design Development Within View of Highway One. New development shall be screened from view from Highway One in accordance with the criteria below. Where an addition to an existing structure is proposed, the entire structure shall be subject to the criteria below.

- A.** **Landform Alterations.** Landform alterations (such as road grading, earth moving, and vegetation removal) shall be minimized. Where no feasible alternative exists, site disturbance shall be limited to areas that are least visible from Highway One.
- B.** **Location of Buildings.** Buildings and parking areas shall use appropriate landscaping (including native vegetation when feasible) to minimize views of the site from Highway One.
- C.** **Highway One Frontage.** Development on any site in the West Village that has a rear property line along or can be viewed from Highway One shall store trash, recyclables and other materials in a visually concealed area attached to the back of the building, with similar materials and colors as the building, in order to create a neater and cleaner look from Highway One. (This area could be an alcove in the building or an area surrounded by a fence or hedge.) (CDP)
- D.** **Vegetative Screening.** Vegetation shall be used to provide screening of development from Highway One. Vegetation which is used to screen landform alterations, buildings, or parking areas, shall be maintained for the life of the project.
- E.** **Night Lighting.** Night lighting shall be the minimum necessary for convenience and security, and shall be shielded in order to minimize pollution of night skies. Lighting shall not conflict with the character of the area.

F. Colors and Materials. Appropriate colors and materials shall be used to minimize structure visibility from Highway One.

~~**6. Setbacks - Residential Single- and Multi-Family.** Unless specified in other Cambria Urban Area Standards, the following special setbacks apply to the respective neighborhoods shown on Figure 7-14. Where no setback is listed, setbacks shall be determined by applicable sections of Coastal Zone Land Use Ordinance:~~

~~**A. Pine Knolls**~~

~~The minimum setbacks on all lots are: front - 10 feet, side - 3 feet, rear - 5 feet, except for Tracts 44 and 112, which are per Coastal Zone Land Use Ordinance:~~

~~**B. West Village**~~

~~The minimum setbacks for lots in any residential category shall be as follows:~~

~~Front: 10 feet~~

~~Side: 3 feet~~

~~Rear: 5 feet~~

~~**C. Happy Hill**~~

~~The minimum setback for small subdivisions are: front - 10 feet, side - 3 feet, rear - 5 feet, except the following which are per Coastal Zone Land Use Ordinance unless a greater setback is noted:~~

~~**D. Tract 358**~~

~~Side corner: 25 feet~~

~~**E. Tract 384**~~

~~Side corner: 25 feet (lots 2, 7, 10, 14)~~

~~**F. Tract 420**~~

~~As per Coastal Zone Land Use Ordinance~~

~~**G. Moonstone Beach**~~

~~Front: 10 feet~~

~~Side: 3 feet~~

~~Rear: 5 feet~~

~~H. Park Hill~~

~~The minimum setbacks on all lots are: front - 10 feet, side - 3 feet, rear - 5 feet, except the following which is per Coastal Zone Land Use Ordinance unless a greater setback is noted:~~

~~I. Tract 159 and other lots with Coastal Bluffs~~

~~Bluff (rear):~~

~~To be determined by geologic report to withstand bluff erosion and wave action for a period of 75 years, and shall not be less than 20 feet.~~

~~J. East/West Ranch~~

~~All proposed dwelling units are to be setback a minimum of 50 feet from Highway One.~~

~~K. East and West Lodge Hill~~

~~For subdivisions that created 25-foot wide lots (small lot tracts), minimum setbacks shall be: front - 10 feet, side - 3 feet, rear - 5 feet, except as provided by the following:~~

~~L. Small Lot Tracts~~

~~a. West Sherwood Drive:~~

~~Front: 5 feet~~

~~Bluff (rear): To be determined by geologic report to withstand bluff erosion and wave action for a period of 75 years and shall not be less than 25 feet.~~

~~b. Single Lots (25'): Front and rear setbacks must total 25 feet, with a minimum of 10 feet in the front and 10 feet in the rear yard unless adjusted pursuant to Coastal Zone Land Use Ordinance Section 23.04.108 a(2) (sloping lot adjustment). Side setbacks shall be a minimum of 3 feet, 5 feet on the street side of a corner lot.~~

~~c. Double Lots (50'): Front and rear setbacks shall total 25 feet with a minimum of 10 feet in the front and 10 feet in the rear unless adjusted pursuant to Coastal Zone Land Use Ordinance Section 23.04.108 a(2). Side yards shall be a minimum of 5 feet; 10 feet on the street side of a corner lot.~~

- ~~d~~ Except for blufftop lots, setbacks may be averaged to allow for flexibility in design and minor adjustments may be allowed (not to exceed 2 feet in the rear) to preserve trees on the site.
- ~~e~~ Front setbacks may be adjusted pursuant to CZLUO Section 23.04.108 a(2) for sloping lot adjustment.
- ~~f~~ A zero side yard setback may be permitted by development plan review where preservation of healthy trees is accomplished or where grading would be minimized, in accordance with Section 23.04.110 f of the Coastal Zone Land Use Ordinance.

~~M. Large Lot Tracts~~

~~The following tracts are per Coastal Zone Land Use Ordinance unless a greater setback is noted:~~

- ~~1. Tract 61~~
~~As per Coastal Zone Land Use Ordinance~~
- ~~2. Tract 97~~
~~As per Coastal Zone Land Use Ordinance~~
- ~~3. Tract 163~~
~~The minimum front setback on all lots is 20 feet, except as follows:~~
~~Front: 25 feet- (block 6, lots 7-11)~~
~~10 feet- (block 3, lots 15-31)~~
~~(block 4, lots 12-20)~~
~~(block 5, lots 2-8)~~
- ~~4. Tract 521~~
~~As per Coastal Zone Land Use Ordinance~~
- ~~5. Cambria Pine Estates 1~~
~~As per Coastal Zone Land Use Ordinance~~
- ~~6. Pineview Tract~~
~~As per Coastal Zone Land Use Ordinance.~~

(The following standard #14 is included for informational purposes only. The request for this amendment to the North Coast Area Plan is included in Exhibit LRP2004-00024:F.)

14. Fiscalini Ranch. The Ranch. The following standards 7 through 9 apply only to the area designated on Figure 5 7-5 as The Fiscalini Ranch.

A. Limitation on Use.

1. **Recreation Land Use Category.** Uses shall be limited to Outdoor Sports & Recreation, Passive Recreation, Crop Production & Grazing, Communications Facilities, Coastal Accessways, Public Assembly & Entertainment, Temporary Events, One Caretaker Residence, Residential Accessory Use, Fisheries & Game Preserves, Water Wells & Impoundments, and Pipelines & Transmission Lines. Utilities shall be installed underground rather than by the use of poles and overhead lines.

2. **Open Space Land Use Category.** Uses shall be limited to Outdoor Sports & Recreation, Passive Recreation, Crop Production & Grazing, Communications Facilities, Coastal Accessways, Temporary Events, One Caretaker Residence, Residential Accessory Use, Water Wells & Impoundments, and Pipelines & Transmission Lines.

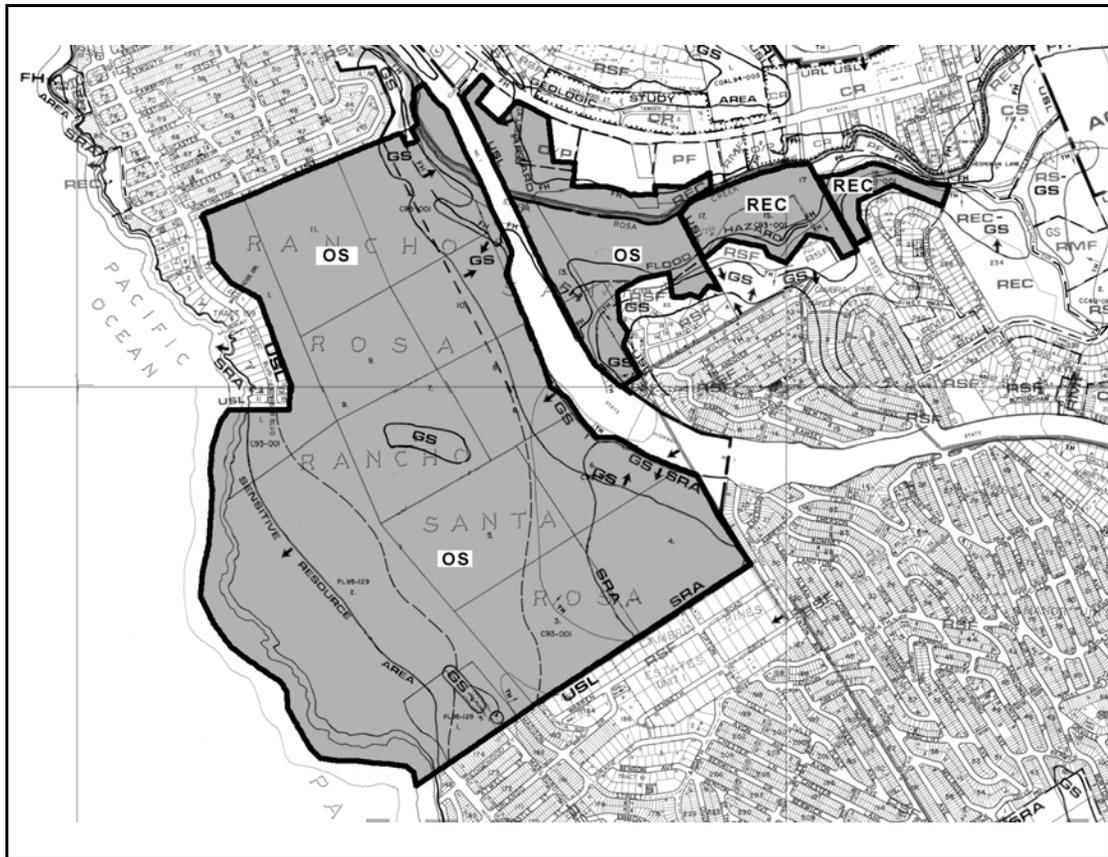


Figure 7-5: Fiscalini Ranch

- B. **Pedestrian and Bicycle Path - East Ranch Area.** A pedestrian and bicycle path shall be installed along Santa Rosa Creek at the time of development consistent with the environmentally sensitive habitat area policies of the Land Use Element.

- ~~7. **Permit Requirement - Communitywide.** Minor Use Permit review is required for development on slopes in excess of 20 percent, except where~~

~~a higher level of review is required by the Coastal Zone Land Use Ordinance or Planning Area Standard.~~

~~8. **Permit Requirement.** Any proposed development of the Fiscalini Ranch requires prior approval of a master Development Plan to identify regulations, conditions of approval and programs to be implemented with the project to achieve consistency with the general plan.~~

~~9. **Application Content.** Information submitted with the proposed master Development Plan for the Fiscalini Ranch is to include the following:~~

~~a. Specific locations for all proposed land uses (including all existing land uses proposed to be retained on the site), together with proposed height and bulk of structures, setbacks and other features of development. Buildings shall contain no more than two stories and not exceed 25. Structures shall be built to existing grade except where fill is needed for flood protection and then the amount of fill shall be the minimum needed to meet HUD standards. Development, except access roads and paths where no alternative route is feasible, shall be setback a minimum of 50 feet from Highway 1 and buffered by the use of landscape screening. The plan is also to identify any areas proposed to remain undeveloped, and whether such areas are subject to flooding, have slopes in excess of 20%, or are characterized by other hazards or conditions which limit development.~~

~~Except for "crossing bridges" or pedestrian bicycle paths where no feasible, less environmentally damaging alternative route exists, development shall not be allowed within the floodway, stream, riparian corridors, areas of archaeological resources, or undeveloped/unsubdivided pine forest. Where alternative routes do not exist for stream crossings, development impacts shall be mitigated to the maximum extent feasible. No development shall be allowed within wetland resources, over springs, geographically unstable areas, on slopes in excess of 20% or within the bluff top open space (required by Combining Designation Standard 1) that would require use of protective devices.~~

~~b. Wetland or marsh resources shall be protected with the minimum standard setback and any project shall be designed to protect and improve the existing drainage pattern that serves the resource.~~

~~c. The location and extent of existing and proposed streets, including their tentative widths and improvement standards and the proposed alignment, configuration and improvement standards. The proposed~~

alignments shall be the least environmentally damaging alternative, and shall minimize cutting and filling; shall not extend into sensitive habitat areas or buffers (except for the installation of utility lines, pipelines, drainage and flood control facilities, bridges across streams, or road approaches to bridges); shall minimize visual impacts on Highway 1, and shall not significantly adversely impact the road capacity or level of service on Highway 1.

- ~~d. Standards for population and building densities for the entire site, including lot sizes, types of construction, and provisions for water supply, sewage disposal, storm water drainage and solid waste disposal. These standards shall be set in coordination with a Transfer of Development Credits Program for the Lodge Hill area.~~

~~**10. Public Improvements.** The following improvements shall be minimum requirements to be incorporated in the design of the projects:~~

- ~~a. A buffer along Santa Rosa Creek providing a minimum setback of 50' from the inland extent of riparian vegetation with provision for access trails within the area.~~
- ~~b. Dedication of a civic/cultural facility site located to serve the community.~~
- ~~c. Dedication of an elementary school site within the project area or provision of in-lieu fees if an alternative site is not needed.~~
- ~~d. Retirement of water and sewer permits from steep and heavily forested 25 foot wide lots in Lodge Hill to preserve and protect the sensitive Monterey Pine forest habitat.~~

15. Cambria Commercial Design Plan Included by Reference. The Cambria Commercial Design Plan, certified by the California Coastal Commission on (date of certification to be inserted), and any amendments thereto, is hereby incorporated into this Land use Element as though it were fully set forth here. Development Standards contained in the Cambria Commercial Design Plan have been incorporated into the standards in the relevant sections below.

The Cambria Commercial Design Plan provides guidance for new development in Cambria's primary commercial districts, including the East Village, the West Village, Mid-Village, and Moonstone Beach. It contains goals, visions, and design guidelines that are flexible in their meaning to allow for interpretation. Applications for development shall include a statement explaining how the proposed project has met the intent of the goals, visions, and design guidelines.

~~———— Cambria Design Plan Included by Reference. The Cambria Design Plan, and any amendments thereto, is hereby incorporated into this Land Use Element as though it were fully set forth here. The Cambria Design Plan, dated May 9, 2002, was certified by the California Coastal Commission on May 9, 2002 and is on file in the Office of the Clerk of the Board of Supervisors. In the event of any conflict between the provisions of the North Coast Area Plan and the Design Plan, the Design Plan shall prevail, except that LCP provisions protecting coastal resources and public access and recreation opportunities shall control. All new development in the areas shown by Figure 4 shall comply with the development standards contained in the Design Plan as well as all other applicable LCP provisions. [Added 2002, Ord. 2912]~~

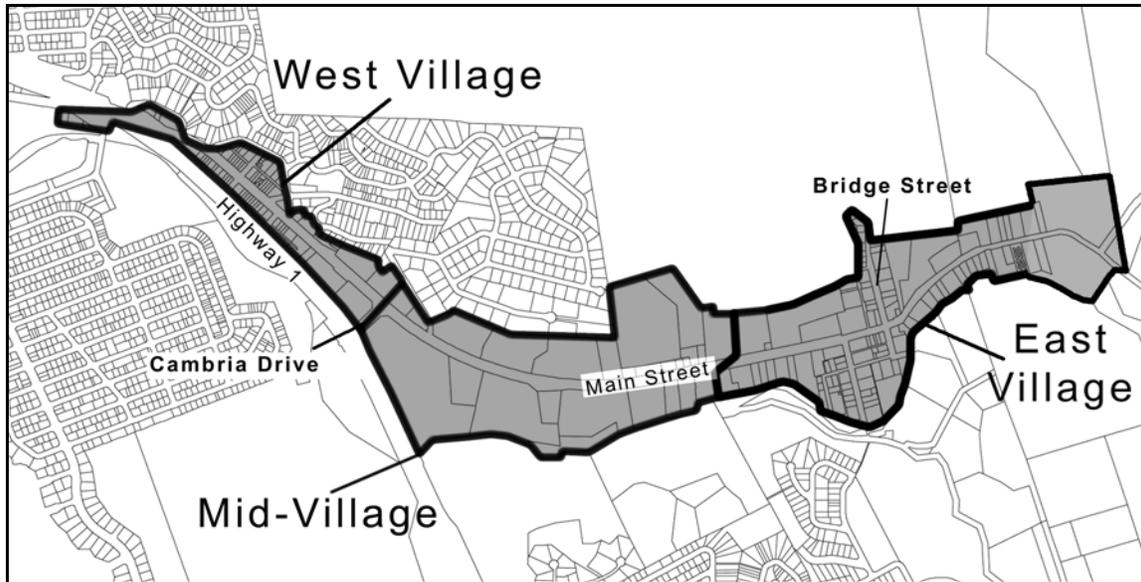


Figure 7-6: Cambria Commercial Districts Location Map

16. Commercial Districts. The following standards apply to the West Village, Mid-Village, and East Village areas as shown in Figure 7-6. (CDP)

A. Mixed Use Projects. To encourage housing, and in particular affordable housing projects, in the downtown commercial districts from East Village to Moonstone Beach, multi-family dwellings are allowed on the upper story or rear half of the lot. For the purpose of this standard, the front half of the lot is defined as the area located between the street line and the halfway line drawn through the midpoint of lot depth.

- 1. Principal Use.** Development of multi-family dwellings may be allowed prior to the development of commercial facilities, provided that the front half of the lot is reserved for future commercial uses.
- 2. Density, Floor Area and Open Area.** Density is limited to 15 units per acre, calculated using the entire acreage of the site. The maximum floor area and minimum open area requirements in CZLUO Chapter 23.04 shall not apply. Instead, the land use permit shall require an amount of private open space and common area for privacy, recreation, light and air that is appropriate for the nature of the project.
- 3. Visitor-Serving Areas (V).** Multi-family housing consistent with this section is allowable in ‘V’ areas provided it is designated for employee housing.

4. **Facilities for Children.** Multi-family projects for employees and affordable housing shall include facilities for children, such as enclosed play areas, appropriate for the nature of the project.

B. Signs. Signage shall be limited to reduce the potential for visual clutter and distraction. On-site signs are allowed according to applicable County policies, with the following allowances or limitations. Other policies in the *CZLUO*, area plan and Coastal Plan remain applicable and in effect. (CDP)

1. **Wall Signs.** Wall signs are limited to a maximum of 30 square feet for each business or tenant. (CDP)
2. **Suspended Signs.** Suspended signs may be used where no projecting signs are proposed. (CDP)
3. **Monument Signs.** Each development site shall have not more than one monument sign for each of its street frontages. Monument signs are limited to 32 square feet and shall not exceed 8 feet in width. Monument signs are limited to 3 feet in height when located within the setback areas and 4.5 feet in height when located outside the setback areas, including the base below the sign. (CDP)

Such signs shall be solid at the base in order to give them a substantial appearance, and the signs shall match the architecture of the building itself. (CDP)

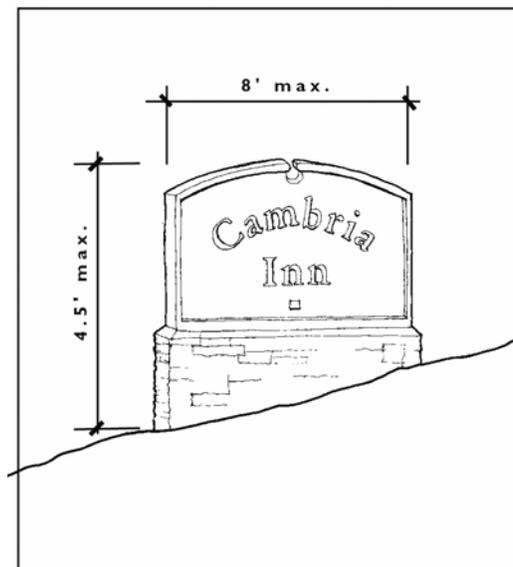


Figure 7-7: Monument Signs

4. **Projecting Signs.** Projecting signs may be utilized where no suspended signs are proposed. (CDP)
 5. **Marquee signs.** Marquee signs are limited to 30 square feet. (CDP)
 6. **Window Signs.** Window signs are not subject to the standards in Section 23.04.300 of the CZLUO if the total area of such signs does not exceed 15 percent of the total window area, and no more than one lighted sign per window pane. (CDP)
 7. **Sign Illumination.** Signs may only be externally illuminated. No neon lighting is allowed. (CDP)
- C. Public Restrooms Required.** All Development Plan applications for commercial, recreational or public facility projects that cumulatively involve more than 15,000 square feet of structures shall provide public restrooms located in a convenient and visible location.

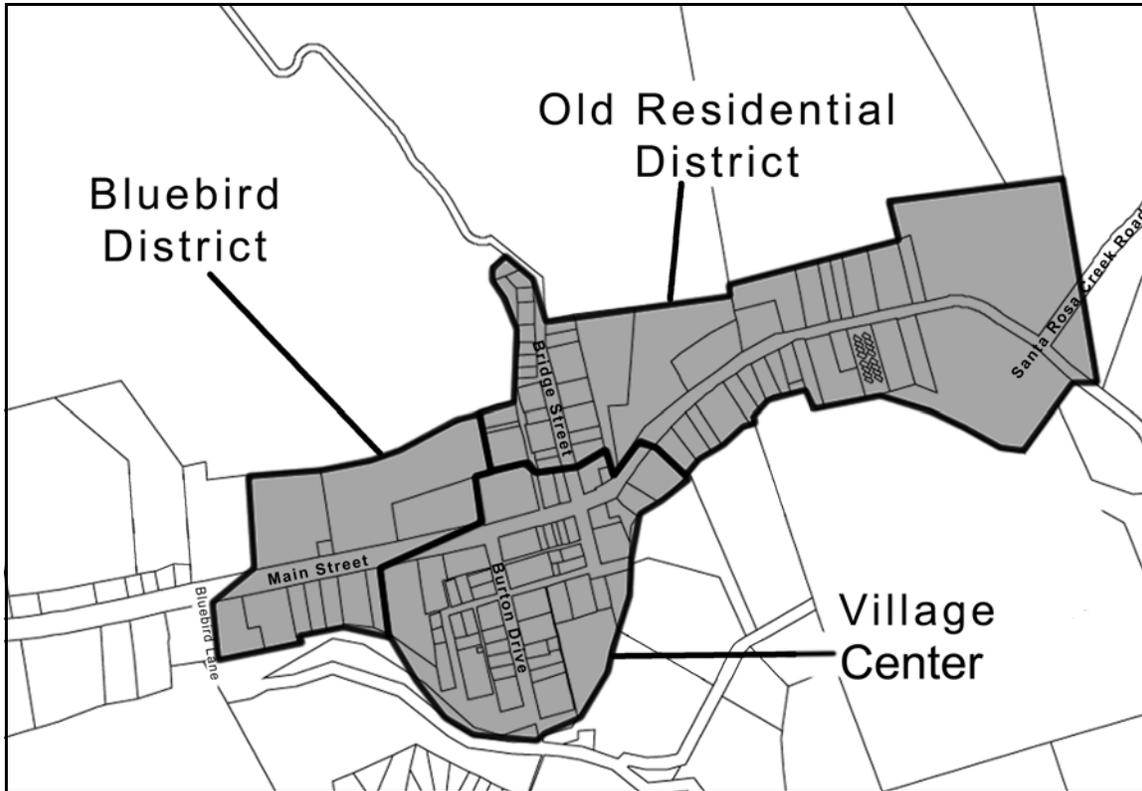


Figure 7-8: East Village Area Location Map

17. East-Village Area. The following standards apply to the East Village Area designated on Figure 7-8.

- A. Building Height.** The maximum allowable height in the East Village is 28 feet, except in the Old Residential District, where the maximum height is 22 feet. Buildings may exceed these maximum heights by up to 7 feet (for a total of 35 and 29 feet, respectively) if they have roofs with a pitch of 6:12 or greater. (CDP)

Building height is measured at every individual point on a site from the natural grade to the top of the building directly above that point. (CDP)

B. Front Setbacks.

- 1.** All new development and/or remodeling in the Village Center and Bluebird District shall be consistent with adjacent setbacks. New buildings in these areas shall have setbacks that represent the average of the front setbacks on the closest developed parcels on either side of the new building. As described below this regulation would have the following implications: (CDP)

- a. On Main Street between Burton Drive and the Santa Rosa Cemetery, buildings should be built to the property line. (CDP)
 - b. On Burton Drive, buildings should be built with a 10-foot setback, with 3 feet dedicated to a public sidewalk. (CDP)
 - c. On the south side of Main Street in the Bluebird District east of Redwood Center, buildings shall be built to the property line at the Redwood Center and east of it. (CDP)
 - d. On the south side of Main Street in the Bluebird District west of the Redwood Center, buildings should be built with 10-foot setbacks. (CDP)
 - e. No setbacks are shown for the north side of Main Street west of Burton Drive since any new development in this area should generally be on the bluffs above the street. (CDP)
2. All new development in the Old Residential District shall be built with a 15-foot minimum setback. (CDP)

C. Side Setbacks.

1. On Main Street between Burton Drive and Bridge Street, all new development and/or remodeling shall be built to the property line with zero-foot side setbacks. (CDP)
2. On Burton Drive, all new development and/or remodeling shall be built to the property line except on or adjacent to historic properties that are already constructed with side setbacks. On those parcels, side setbacks shall be the same as for the historic building in question. (CDP)
3. In the Bluebird and Old Residential Districts, all new development and/or additions shall have a minimum side setback of 10 feet. (CDP)

- D. Building Facades.** New development and/or exterior remodeling in the Village Center shall be articulated with one or more bays with a maximum width of 30 feet. Wider buildings shall be made up of several bays. (CDP)

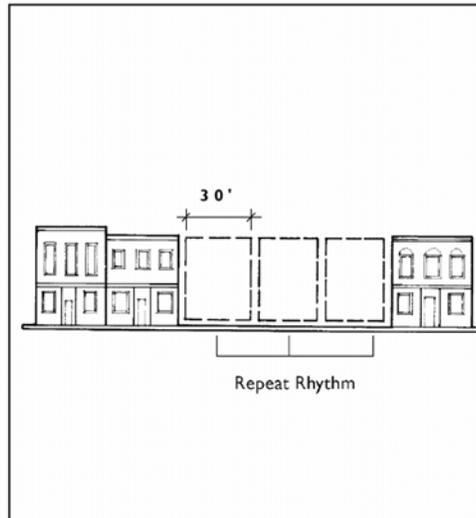


Figure 7-9: Building Facades

E. Pedestrian-Oriented Detailing.

- 1.** All building facades along public streets in the East Village should include pedestrian-scaled detailing such as vertical windows with sills, ornamental decorations, built-in benches, wall lamps, awnings, and door overhangs or recessed entries. (CDP)
- 2.** A minimum of 40 percent of the length of the facade of any building facing a street must be made up of windows and doors. (CDP)
- 3.** All facades, regardless of orientation, should include three-dimensional detailing, such as window moldings and reveals to cast shadows and create visual interest on the facade. (CDP)

F. Roofs.

- 1.** Mansard roofs are prohibited. Mansard roofs are defined as steeply sloping roofs where the top of the slopes do not form a gable but meet to create a flat roof top, as seen on Second Empire, Beaux Arts, and some Romanesque buildings. (CDP)



Figure 7-10: Mansard Roof

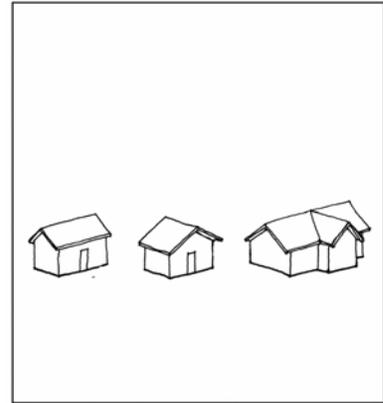


Figure 7-11: Gable Roofs

2. All structures north of Main Street on Bridge Street shall have gable roofs. (CDP)

G. Exterior Materials (CDP).

1. In order to enhance and blend with the historical character of the East Village, metal materials shall not be used on the exterior of buildings. (CDP)
2. All exterior wall changes shall be separated from one another by a change in plane of at least 4 inches, or be set off with architectural detail to create a visual distinction. (See Figure 7-12) (CDP)

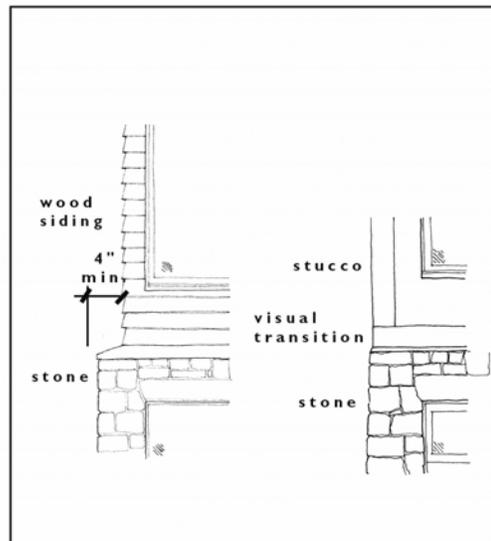


Figure 7-12: Exterior Materials

H. Limitation on Uses. (CDP)

- 1. In the Village Center and the Bluebird District, ground floor frontages shall be retail or other visitor-serving uses in order to create a sense of activity that is crucial for pedestrian-oriented areas.**

I. Parking (CDP).

- 1. All new on-site parking shall be located to the rear of buildings, unless such location is not possible on a specific site. (CDP) This must be supported by site-specific findings.**
- 2. If rear parking is not possible, parking shall be located to the side of a building, with the lot screened from the street by a fence, low wall or hedge. Side yard parking lots should be no wider than one double loaded bay, with a maximum width of 60 feet. (CDP)**
- 3. Parking lots shall be landscaped with at least one tree for every eight spaces, in addition to the shading requirement of the Coastal Zone Land Use Ordinance used for parking rather than building footprints. Where buildings abut both sides of a parking lot the parking lot shall not have a slope greater than 2 percent. (CDP)**

J. Curb Cuts (CDP).

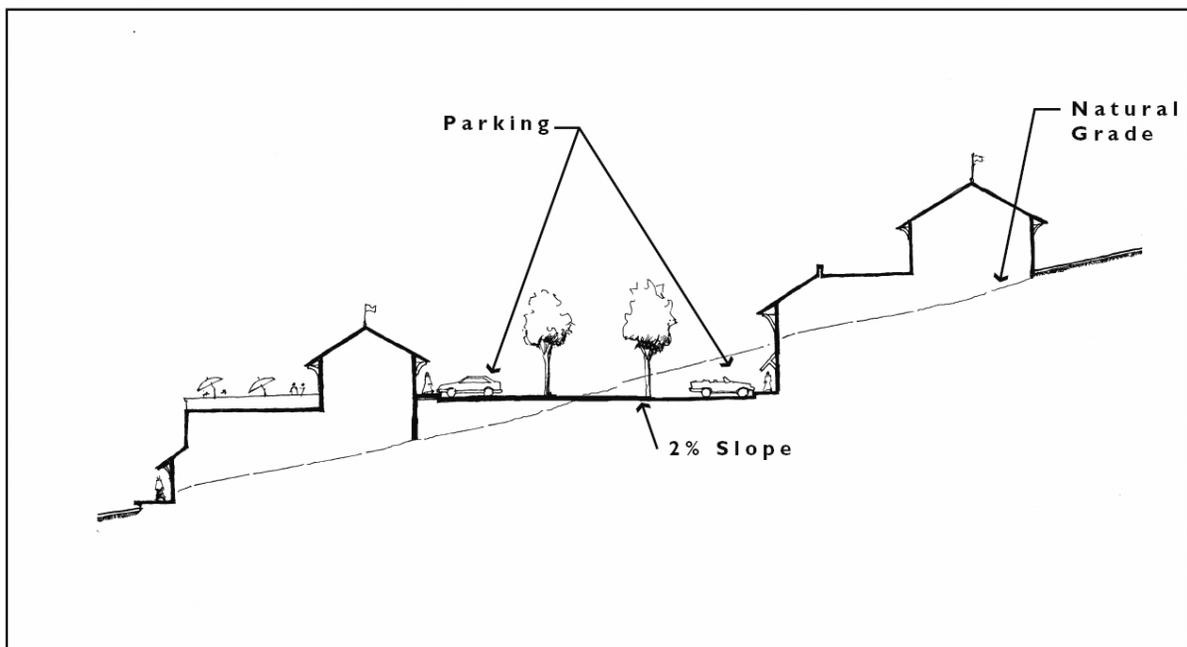


Figure 7-13: Parking on Sloping Sites

1. In the Village Center, no new curb cuts shall be allowed on Main Street.
2. In the Village Center, no more than one curb cut shall be allowed for each parcel. Lots that have auto access via alleys shall have no curb cuts from any public street.
3. Where curb cuts occur, the driveway shall be no more than 10 feet wide, unless two-way in and out access is required, in which case the driveway shall be no wider than 20 feet. (CDP)

K. Development on Sloping Sites. The hillsides are one of the dominant physical features in the East Village which provide enclosure on the northern edge of the Village. Preservation of the hillsides and careful, unobtrusive development is essential in maintaining an attractive backdrop for the East Village. For development on steeply sloping sites abutting the northern side of Main Street, the following development standards apply: (CDP)

1. Development and parking shall be located on the flatter portions of these sites on top of the bluffs away from Main Street, and not in graded areas at the toe of the hillside at the edge of Main Street. (CDP)
2. Site grading shall generally be limited to areas within and adjacent to the building footprint, parking areas and driveways. Where necessary due to unusual site conditions, or where necessary to blend graded areas with

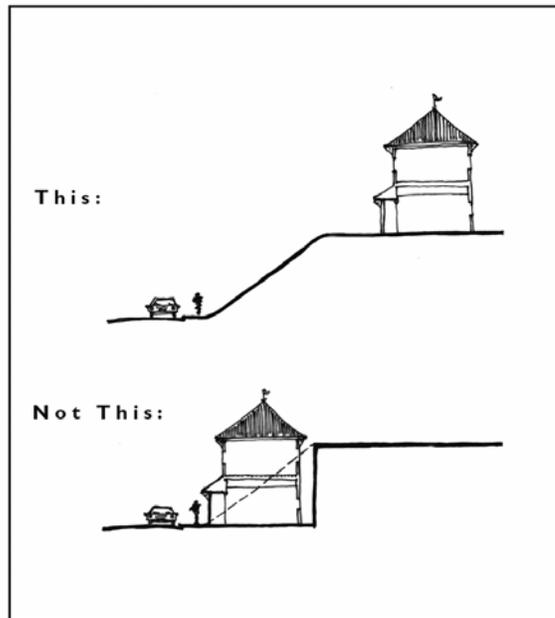


Figure 7-14: Building on Bluffs

adjacent natural contours grading may be outside of those areas. (CDP)

3. Building masses shall generally follow the contours. Where possible, large cuts and graded pads should be avoided. On sloping sites, buildings should have multiple levels and be dug into and step down the hill. All building in areas with natural slopes above 15 percent shall have stepped foundations or other similar techniques to minimize grading associated with large building pads. (CDP)

4. Cut slopes shall be no higher than adjacent building heights. (CDP)

L. **Drive-up Windows.** No drive-up windows are allowed in the East Village (CDP).

M. **Storage.** All storage shall be screened by a solid wall or fencing that matches the architectural materials and style of the building. (CDP)

18. **Mid-Village Area.** The following design standards apply to the area of Cambria designated as Mid-Village as shown in the Figure 7-15. These standards attempt to preserve and enhance the landscape features of Mid-Village, making them the most dominant feature in the surrounding natural and built environment. Appropriately scaled buildings would be the most dominant feature of the built environment with each building's necessities, such as parking and storage, carefully sited and designed to serve its building and blend into the surrounding environment. (CDP)

A. **Building Orientation.** This section is intended to ensure that buildings face Main Street, with their interior activity areas visible from the street. (CDP)

1. **Permeability and Activity.** The Main Street facade of all commercial/office buildings shall be the "front," detailed with windows and doors and the main pedestrian entry to each space. (CDP)

B. **Building Height.** In order to maintain the existing character in which the natural landscape, not the built environment, is the dominant feature in the environment, building height shall be limited as follows: (CDP)

1. **Allowed Height.** All buildings south of Main Street shall not exceed 25 feet. (CDP)

2. **Allowed Height on Up-Slope Sites.** All buildings north of Main Street shall not exceed 20 feet. (CDP)

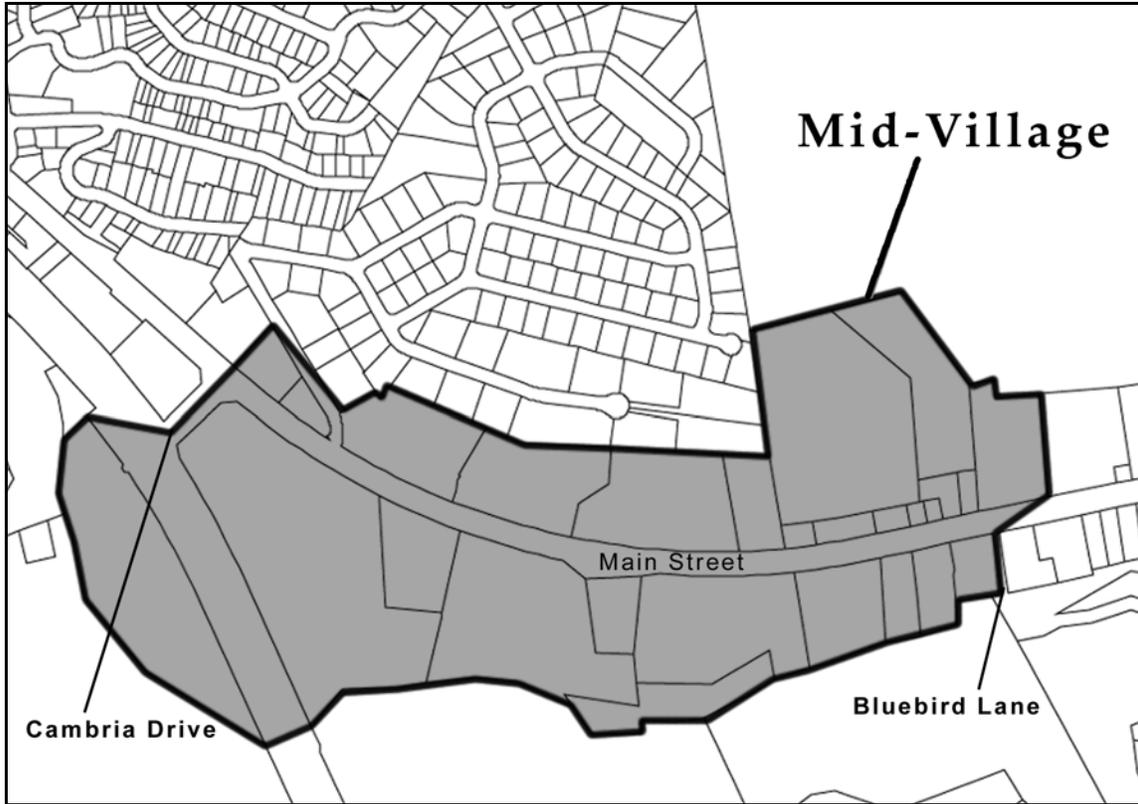


Figure 7-15 : Mid-Village Area Location Map

3. **Additional Height for Pitched Roofs.** As a means to encourage sloped roofs, the peak of a sloped roof may be up to 7 feet taller than the maximum building height of 20 or 25 feet (for a total of 27 or 32 feet), provided that the roof pitch is 6:12 or greater. (CDP)
4. **Additional Height for Non-Habitable Structures.** Non-habitable, architectural building elements, such as clock towers and steeples, may have a maximum height of 35 feet. (CDP)
5. **Measurement.** Building height is measured at every individual point on a site from the natural grade to the top of the building directly above that point. (CDP)

C. Setbacks. All new development and/or remodeling in Mid-Village shall be as follows (CDP):

1. **Front Setback for Down-Slope and Cross-Slope Sites.** A minimum 15-foot front setback is required for all parcels that are not up-slope sites in relationship to Main Street. Exception may be made for the Mid-State Bank

site in order to allow for buildings to be built to the sidewalk, if this proves feasible. (CDP)

2. Front Setback for Bluff-top Development Up-Slope Sites. All development on up-slope sites, in relationship to Main Street, shall be set back 15 feet beyond the top of the bluff. This distance is generally a minimum of 50 feet from the front parcel line. (CDP)

3. Side Setback. A minimum 10-foot side setback is required of all buildings in this district. The minimum setback is to be increased one-foot for each three feet of building height above 12 feet. (CDP)

4. Rear Setback. A minimum 20-foot rear setback is required for all buildings on the downslope side of Main Street. For parcels adjacent to Santa Rosa Creek this setback shall be offered for dedication for a public right-of-way. (CDP)

D. Roofs. In order to preserve Cambria’s charm as an historic and rural community, it is essential that roof lines are indicative of the shapes that were developed in the historic community. (CDP)

1. Minimum Roof Pitch. Any pitched roof shall have a minimum 3:12 pitch. (CDP)

E. Service Areas. Service areas shall be placed away from Main Street, Santa Rosa Creek and other public open space. (CDP)

F. Parking. Parking lots shall be carefully sited in order to preserve the rural character of Cambria. (CDP)

1. Siting. Parking shall be located behind buildings where possible. (CDP)

2. Highway One Buffer. A visual buffer shall be provided to mitigate views of parking areas from Highway One. Such buffers could include landscaping or additional commercial buildings. (CDP)

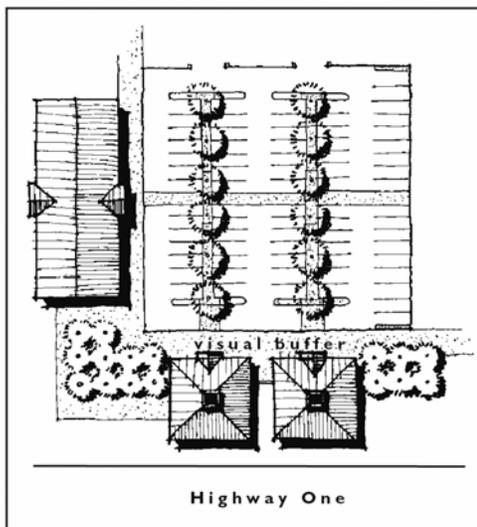


Figure 7-16: Highway One Visual Buffer

3. **Pedestrian Connections.** Where parking is located behind buildings, clearly delineated pedestrian connections to Main Street shall be provided. (CDP)

- G. **Drive-up Windows.** No new drive-up windows are allowed in the Mid-Village area. Existing drive-up windows may be relocated or reconfigured on a given site. (CDP)

- H. **Storage.** This standard places limitations on storage in order to visually conceal it and blend it with the surrounding physical environment. This is intended to minimize the potential for visual clutter and distraction from the buildings and surrounding landscape. (CDP)
 1. All storage shall be screened by a solid wall or fencing that matches the architectural materials and style of the building. (CDP)

- I. **Development on Sites with Greater than Ten Percent Slopes.** The following standards are required for development on slopes greater than ten percent in order to preserve Cambria's hillsides and pines, which are important to the visual quality of the community.
 1. **Grading Limitations.** Grading and buildings shall be designed to follow the natural slope by orienting and aligning with the existing contours. (CDP)

2. **Upper Story Front Setbacks.** On up-slope parcels, upper floors shall be setback a minimum of ten feet from the exterior wall directly below them. The upper-floor roof shall not extend more than two feet into the setback. (CDP)
3. **Sub-floor Areas.** The lowest floor shall not be more than six feet above finished grade. Areas between the lowest floor and finished grade shall be covered with finished walls or appropriate architectural screening and not be left open. Landscaping and/or building articulation shall be used to improve the appearance of the building and reduce apparent mass.(CDP)

11. ~~Cambria Village Square - Cookie Crock Market Area. The following Standards 9 through 18 apply to Cambria Village Square (Tentative Tract 1036, APN's 13-101-45, 46, and 49):~~

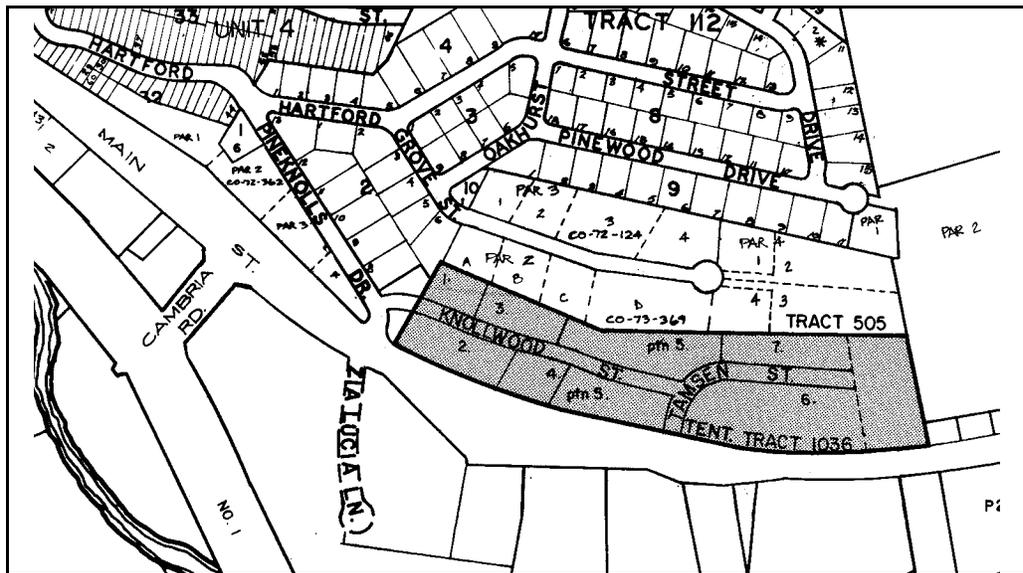


Figure 7-7: Cambria Village Square

~~**A. Use Areas and Development Intensities.** Residential Multi-Family, Commercial Retail, and Office and Professional Use areas and development intensities are limited to the following locations in Cambria Village Square:~~

1. ~~Residential multi-family. Low-density residential 15 units per acre on Parcels 1, 2, and 6. Allowable densities for portions of the site designated for residential development shall be calculated using only the portions of the site that have slopes of 20% or less, with the remaining area to be left in open space.~~
2. ~~Commercial and office/professional. Uses are to be located on Parcels 4, 5 and 6, with a maximum floor area of 72,000 sq. ft. and maintaining a~~

~~maximum commercial-to-office use area ratio of approximately 70% to 30%.~~

- ~~10. **Permit requirement - Development Plan.** Master Development Plan for the entire property for review and approval by the county prior to development of the property. The master development plan is to include the following information:~~
- ~~A. Site layout and development concepts for all proposed Multi-Family Residential, Commercial Retail, and Office and Professional use areas (including any existing structures proposed to be retained on the site), together with the proposed height and bulk of structures, setbacks, parking areas, landscape areas and other features of development.~~
- ~~B. Architectural Design. Architectural theme of development, including detailed elevations which incorporate design, siting, and scale elements consistent with early 20th Century structures.~~
- ~~C. Circulation. Circulation plan identifying internal on-site streets, pedestrian walkways and proposed linkages to existing local streets or arterials.~~
- ~~D. Phasing Plan. A phasing plan for implementation of the project.~~
- ~~E. Traffic Impacts. Mitigation of the project's traffic impacts on areawide circulation, including prohibition of vehicular access to individual parcels from Pineknolls Drive and Main Street. Vehicular access shall be from the internal collector streets of the development.~~
- ~~F. Visual Impact Measures. Measures to reduce any negative impacts of the project on area residents and motorists traveling on Highway One, including a detailed landscaping plan with provisions for siting parking behind structures where feasible and landscaping visible parking areas to minimize their appearance.~~
- ~~G. Sign Requirements. Sign shall use wood or wood appearing materials.~~
- ~~B. Permit Requirements after Master Development Plan. After approval of the Master Development Plan, submit a Development Plan application for each proposed phase of development, showing details of siting, grading, structure locations, circulation within each phase, and connection to the overall circulation system.~~
- ~~C. Height Limits. The maximum allowable height for all buildings is 25 feet.~~
- J. East Village. The following standards apply 18 through 26 only to the Mid State Bank site Recreation land use category in the East Village area, shown in Figure 7-8 7-17. in addition to the standard listed above. (Mod129)**

A. **Permit Requirement.** Development Plan approval is required prior to any permanent development in this area, in order to identify overall development of the property, circulation, uses and phases, to achieve consistency with the General Plan.

1. **Application Content.** The Applications for the land use permits Development Plan shall include the following information, in addition to the content requirements in the applicable Coastal Zone Land Use Ordinance Sections: for Development Plans

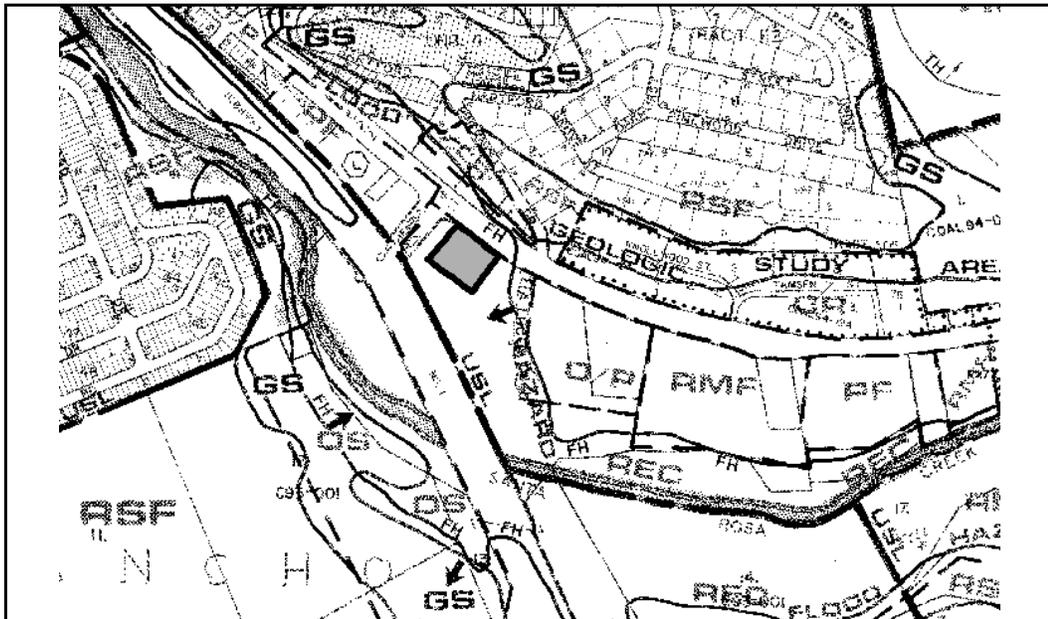


Figure 7-17: Mid State Bank Site

1. **Development.** A description of overall development of the property, circulation, proposed uses and development phases, demonstrating that proposed development will be consistent to achieve consistency with the General Plan.

f a. **Site planning.** Specific locations for all proposed land uses, together with the proposed height and bulk of structures, setbacks and other features of development.

2. **Open areas.** Any areas proposed to remain undeveloped, and whether such areas are subject to flooding, having slopes in excess of 20%, or are characterized by other hazards or conditions which limit development.

c. Visitor-serving priority uses and coastal recreation uses and areas identified as the principal use of the site;

~~3. **Recreational use.** Areas intended for open space and passive recreation with amenities such as walkways, bicycle paths, outdoor furniture, and similar improvements shown with typical drawings.~~

~~4. **Availability of services.** Information on the current and projected water supply and sewage treatment capacity is adequate to support the amount of proposed development.~~

~~f. The location and extent of existing and proposed streets, including their proposed alignment, configuration, tentative widths and improvement standards and any improvement standards and any off-site improvements:~~

~~b. **Views from Highway One.** A viewshed analysis shall be submitted addressing impacts of any new development on views from Main Street and Highway One. Landscaping areas to buffer views from Highway One, Cambria Drive and Main Street of access roads, development and parking drives and spaces.~~

~~2. **Development Standards.** Site planning and architecture shall reflect Cambria's historical character, and shall be oriented toward Main Street to the maximum extent feasible. A variety in of building designs, materials, and ornamentation shall be utilized used to avoid a uniform "shopping center" appearance. When feasible, support equipment, unattractive building elements, and parking areas shall be buffered for visual impacts, and placed out of view of Highway One. New development shall be sited in a manner which maintains the existing open views from Highway One across the meadows to Santa Rosa Creek. No development requiring the extension of water or sewer service shall be located outside the Cambria Community Services District (i.e., south of Santa Rosa Creek).~~

~~D. **Height Limitation.** Buildings shall contain no more than two stories and not exceed 25 feet, except that non-habitable, architectural building elements, deemed appropriate and essential in creating a consistent architectural character in the area (e.g., clock towers, steeple, etc.), shall be permitted to extend above the height limit (e.g., clock tower, steeple, etc.), but no higher than allowed by the height regulations pursuant to the Coastal Zone Land Use Ordinance.~~

~~E 2. **Flood Protection.** Structures shall be built to or below existing grade, except where fill or pier supports are needed for flood protection. ,and then In this case, the amount of fill and/or the height of piers shall be the minimum needed to place the ground floor one foot above the 100-year flood stage (As necessary to meet the requirements of required by Coastal Zone Land Use Ordinance Section 23.07.060.) No fill shall be placed in a location or manner which would impair the effective 100-year flood flow capacity of~~

Santa Rosa Creek. Fill for development along Main Street may be contoured along the Recreation/Commercial Retail boundary to provide a more attractive appearance.

F 3. Parking and Vehicle Access. ~~Setbacks.~~ Access roads and driveways shall be set back a minimum of 30 feet from Highway One and Cambria Road. Development, including parking driveways, shall be oriented toward Main Street to the maximum extent feasible. ~~parking shall be setback 50 feet from Highway One, and 50 feet from the edge of riparian vegetation or top of the bank of Santa Rosa Creek, whichever is greater. Exceptions from the 50-foot creek setback may be allowed as stated by Standards 21 and 22.~~

24. Circulation and Access. ~~Two-way vehicle access to the site shall be via a single intersection with Main Street at Pine Knolls Drive. A two-way collector street shall be extended from the intersection to a cul-de-sac outside the 50-foot creek setback. The plan line of this street shall be extended to the property line at Santa Rosa Creek.~~

~~The proposed alignment of access roads shall be the least environmentally damaging alternative, and shall minimize cutting and filling; shall not extend into sensitive habitat areas or buffers (except for the installation of utility lines, pipelines, drainage and flood control facilities, bridge across streams, or road approaches to bridges); shall minimize visual impacts on Highway One, and shall not create significant adverse impacts on the road capacity or level of service of Highway One. No driveways or access roads will be allowed to encroach onto Highway One from the site.~~

~~**G. Habitat and Creek Protection.** Except for "crossing bridges" and access roads or pedestrian bicycle paths, where no feasible, less environmentally damaging alternative route exists development shall not be allowed within the floodway, stream, riparian corridors, areas of archaeological resources, in geologically unstable areas, or on slopes in excess of 20%. Where alternative routes do not exist for stream crossings, development impacts shall be mitigated to the maximum extent feasible.~~

~~**H. Pedestrian and Bicycle Path.** A pedestrian and bicycle path is to be installed along Santa Rosa Creek at the time of development. located outside the riparian habitat and consistent with the environmentally sensitive habitat policies of the Land Use Element, . This path shall be designed to connect to the adjoining segment of the pathway which is required by the standards for the adjacent Residential Multi-Family land use designation on Santa Rosa Creek. The path shall be maintained, and shall remain open for public use. (Mod129)~~

- 19. West Village Area.** The following standards apply to the West Village shown in the Figure 7-18. (CDP) The standards in this section apply to all new development in order to maintain architectural character and pedestrian orientation of the West Village. (CDP)

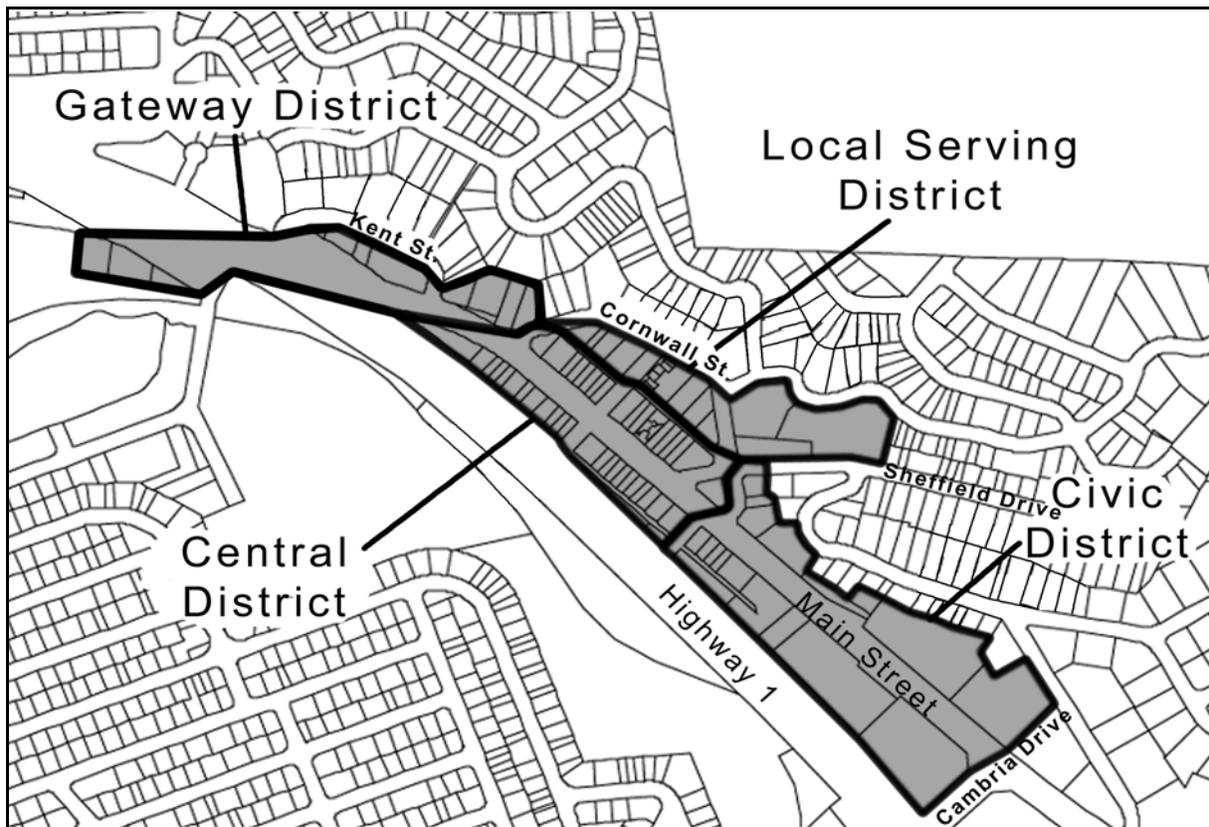


Figure 7-18: West Village Area Location Map

- A. Front Setbacks.** All new development and/or remodeling in the Central District shall be built to the sidewalk edge or up to 5 feet behind the sidewalk if the area between the building and sidewalk is landscaped. On the south side of Main Street, this generally means that buildings will be built to the property line. On the north side of the street, buildings will generally have to be setback about three feet from the property line, and this setback will be paved as sidewalk. (CDP)
- B. Side Setbacks.** All new development in the Central District shall be built to the property line with zero foot side setbacks. Exceptions may be made for a single driveway on an individual parcel in the northwest block face of Main Street (602 - 750 Main Street) in the Central District. (CDP)
- C. Fire Walls.** All new development and/or remodeling shall be designed in a manner that eliminates the need for fire walls above roofs, as currently exists on Main Street. In order to achieve this, interior fire walls with a “lay-down parapet” shall be used to avoid the external parapet wall in compliance with the Uniform Building Code. (CDP)
- D. Building Facades.** New development and/or remodeling in the Central District shall be articulated to include bays that match the typical 25-foot bays already found along Main Street. Building bays are recessed sections of the facade that occur between structural and/or ornamental building columns. (CDP)

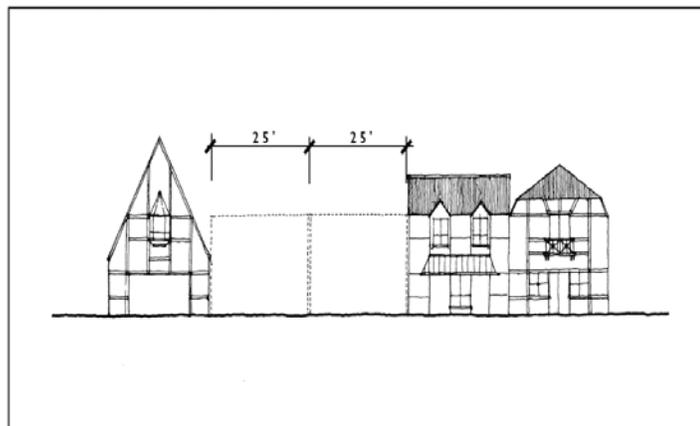


Figure 7-19: Building Facades

- E.** **Limitation on Use.** In the Central District ground floor frontages shall be retail or visitor-serving uses only. These uses create a sense of activity that is crucial for pedestrian-oriented areas. (CDP)
- F.** **Curb Cuts.** No curb cuts shall be allowed on Main Street in the Central District, except on the northwest block face (602 - 750 Main Street), where no more than one curb cut shall be allowed for each parcel. Where possible, sites should share vehicle access with their neighbors as a means to minimize the number of curb cuts on Main Street. Driveways shall be no more than 10 feet wide. Lots that have auto access via side streets and/or alleys shall have no curb cuts on Main Street. (CDP)
- G.** **Drive-up Windows.** No new drive-up windows such as those used for banks and fast food places are allowed. Existing drive-up windows may be reconfigured or relocated but not expanded. (CDP)

14 20. **Access Limitation.** Commercial development ~~is not to~~ shall not use access from Pine Knolls Drive.

15. 21. **Setbacks - Main Street at Pineknolls Drive.** Because of special site constraints, Parcel 2 of Parcel Map CO 72-362 located between Main Street and Pineknolls Drive is allowed a four (4) foot setback from Main Street.

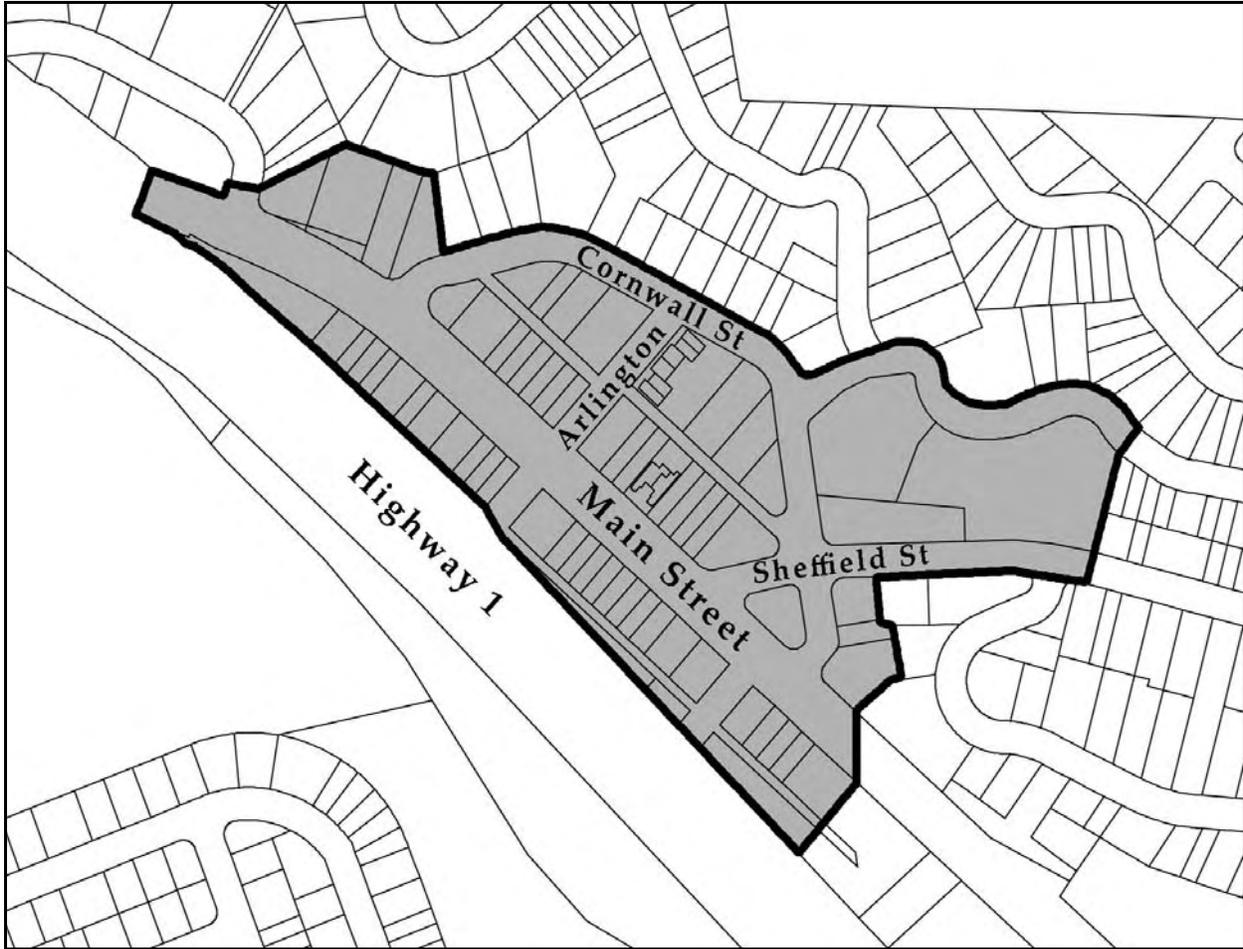


Figure 7-20: Cambria West Village Parking Assessment District

16 22. Parking Requirements - Cambria West Village Parking District. The required number of parking spaces specified in the Coastal Zone Land Use Ordinance shall be reduced by two parking spaces for each assessment (lot) parcel within the district boundary as shown on Figure 7-20.

East and West Village. The following standards apply only to the Commercial Retail category in the East and West Village:

1. ~~**Height Limitation.** The maximum allowable height for all buildings on Main Street, Bridge Street and Burton Avenue is 28 feet.~~
2. ~~**Application Content and Design Criteria.** Applications for any development in the downtown area shall include the following:~~

- a. ~~Detailed elevations which incorporate design siting, and scale elements consistent with the early 20th century structures which establish the special architectural character of the area. Proposals for the renovation or remodeling of early 20th century buildings shall respect the original character of the structures.~~
- b. ~~Detailed landscaping plan including provisions for siting parking behind structures where feasible and landscaping visible parking areas to minimize their appearance.~~
- c. ~~Sign shall use wood or wood-appearing materials.~~
- d. ~~Setbacks shall reflect the setbacks of structures within the block of the site proposed for development to allow for integrating new structures with the character of the residences or residences that have been converted to commercial use.~~

East Village. ~~The following Standards apply only to the East Village area (see Figure):~~

- 5. ~~**Limitation on Use - East Village.** All uses normally allowed in the Commercial Retail category by Coastal Table 'O' are allowed except: Auto and Mobile home Dealers; Transmission and Receiving Facilities, Storage Yards and Sales Lots and all uses within the Manufacturing and Processing use groups.~~

West Village. ~~Standards 2 and 3 apply only to the West Village area (see Figure):~~

- 3. ~~**Front Setback Requirements.** Front setback shall be zero feet unless a more detailed setback is specified below.~~
- 7. ~~**Limitation on Use - Rodeo Grounds Road.** Uses shall be limited to hotels and motels; bed and breakfast facilities; eating and drinking places (not including drive-in restaurants; fast food and refreshment stands); tourist-oriented gift shops; and art galleries. (This is a visitor-serving priority area).~~
- 8. ~~**Permit Requirement.** Development Plan approval is required for any development of the property south of Santa Rosa Creek, west of Burton Drive.~~

COMMERCIAL SERVICE: The following standards apply to all land within the Commercial Service land use category.

- 1. Limitation on Use.** All uses as set forth in Coastal Table 'O' - Allowable Uses are allowable except the following: Specialized Animal Facilities, Agricultural Processing, Petroleum Extraction, Hotels, Motels, Churches, and Recycling and Scrap.

~~Uses shall be limited to: Animal Raising and Keeping; Specialized Animal Facilities; Crop Production and Grazing; Farm Equipment and Supplies; Nursery Specialties (soil and non-soil dependent); Amusements and Recreation Services (limited to health and athletic clubs); Temporary Events; Apparel and Finished Products; Collection Stations (if entirely contained in a building); Electronic and Scientific Instruments; Food and Kindred Products; Furniture and Fixtures; Metal Industries-Fabricate; Printing and Publishing; Small Scale Manufacturing; Caretaker Residence; Home Occupations; Residential Accessory Uses; Temporary Dwelling; Building Materials and Hardware; Fuel and Ice Dealers; Furniture, Home Furnishings and Equipment; Mail Order and Vending; Outdoor Retail Sales; Auto and Vehicle Repair and Service; Business Support Services; Contract Construction Services; Laundries and Dry Cleaning; Offices; Temporary Offices; Public Safety Facilities; Consumer Repair Services; Accessory Storage; Storage Yards and Sales Lots; Temporary Construction Yards; Pipelines and Power Transmission; Public Utility Centers; Vehicle and Freight Terminals; Warehousing; and Wholesaling and Distribution, in accordance with Coastal Table O, Part I of the Land Use Element~~

- 2. Permit Requirement.** ~~Development Plan~~ A Minor Use Permit approval is required for all new development unless a Development Plan is otherwise required by the Coastal Zone Land Use Ordinance.

- 3. Design Standards.** New development shall comply with the following standards:

- A. Site Coverage.** The area of the site covered by structures, including any structural overhangs, is limited to 60 percent of the total site area.
- B. Building Design.** New buildings shall be designed to minimize visual impacts from Burton Drive and the surrounding area.

RECREATION: The following standards apply to all land within the Recreation land use category.

1. **Permit Requirement.** A Minor Use Permit ~~approval~~ is required for all new development unless a Development Plan is otherwise required by Chapters 23.03 or 23.08 of the Coastal Zone Land Use Ordinance.
2. **Location Criteria - RV Parks.** Recreational Vehicle Parks are prohibited ~~adjacent to~~ on sites that abut sites in any Residential Land Use category categories.
3. ~~Limitation on Use - Public holdings. Public holdings shall be limited to public recreation uses for day use.~~
4. **Limitation on Use.** Principal Permitted uses are limited to: Eating and Drinking places (not including drive-in restaurants, fast-food and refreshment stands); Food and Beverage Retail Sales (limited to tourist-oriented supplies); tourist-oriented General Merchandise Stores; Hotels and Motels; Passive Recreation, participant sports and active recreation, and uses from the Open Space category in Coastal Table 'O'. Non-Principally Permitted uses are limited to: Caretaker's Residence; Service Stations; Public Assembly and Entertainment (when accessory to a Hotel and Motel); Financial Services (only when expressly designed to serve visitor needs); Coastal Accessways; and Water Wells and Impoundment. (This is a visitor-serving priority area.)
4. **Moonstone Beach.** ~~Standards 5 through 13~~ The following S standards apply only to the Moonstone Beach Area, as shown in Figure 7-21. instead of the previous Standards 1 to 4.
 - A. **Limitation on Use.** Principally Permitted uses are limited to: Eating and Drinking Places (drinking places independent from restaurants are prohibited), Hotels and Motels, tourist-oriented Food and Beverage Retail Sales (with location not limited to road intersections), General Merchandise Stores (tourist-oriented specialty shops including gift and antique stores, ~~and cleaning and laundry agencies~~, not exceeding 2,500 square feet in area, including any attached living quarters and art galleries). Non-Principally Permitted uses are limited to: Caretaker Residences (when accessory to a Principally Permitted use and clearly subordinate to that use and occupying the rear portion or second story of the structure), Libraries and Museums (~~zoos are prohibited~~), Indoor Amusements & Recreation, Outdoor Recreation, Temporary Events, (when accessory to a Hotel and Motel), Public Safety Facilities, and Coastal Accessways. (This is a visitor-serving priority area.) (Mod130)

- 9 E. Height Limitation.** The maximum allowable height for all buildings between Highway One and the ocean is 15 feet from average natural grade. A reduced maximum height may be required if the new development would ~~entirely~~ block public views to and along the shoreline.
- 10 F. Landscaping Requirements.** Landscaping of the entire site, including side property lines, is required, using native plants and trees compatible with the coastal environment.
- 11 G. Road Improvements.** Road improvements for Moonstone Beach Drive ~~are not to~~ shall not include curbs, gutters, ~~and~~ or sidewalks.
- 12 H. Sign standards.** Signs are to be designed, located, and constructed to preserve views of the ocean from Highway One. They are to be constructed of native materials and to be externally lighted with shielded lights.
- 13 I. Site Coverage - View Corridors Required.** At least 50% percent of each site is to remain free of structures and landscaping that would block views of the shoreline and coastal terrace, in order to provide a view corridor ~~of the shoreline and coastal terrace~~ from Highway One. Development on contiguous lots ~~shall may~~ employ ~~θ~~ zero-foot side yard setbacks ~~where~~ when feasible to maximize view corridors.

5. Cambria Pines Lodge. ~~Standards 14 through 17~~ The following standards apply only to the Cambria Pines Lodge property, as shown in Figure 7-22.

- 14 A. Limitation on Use.** Principally Permitted uses are limited to Eating and Drinking Places (not including drive-in ~~restaurants~~, and fast-food restaurants, and refreshment stands), Food and Beverage Retail Sales (tourist-oriented), General Merchandise Stores (tourist-oriented), Hotels and Motels, and Bed and Breakfast Facilities. Non-Principally Permitted uses are limited to: ~~service stations~~, Indoor Amusements & Recreation, Outdoor Recreation, Temporary Events, (when accessory to a Hotel or Motel), Public Safety Facilities, a Caretaker Residences, Public Assembly and Entertainment (when accessory to a Hotel or Motel), and uses permitted by the Cultural, Education, and Recreation use group ~~uses~~ (excluding Libraries and Museums, ~~membership organizations~~, Schools and Social Service Organizations) in accordance with Coastal Table 'O', Part I of the Land Use Element. (This is a visitor-serving priority area.)

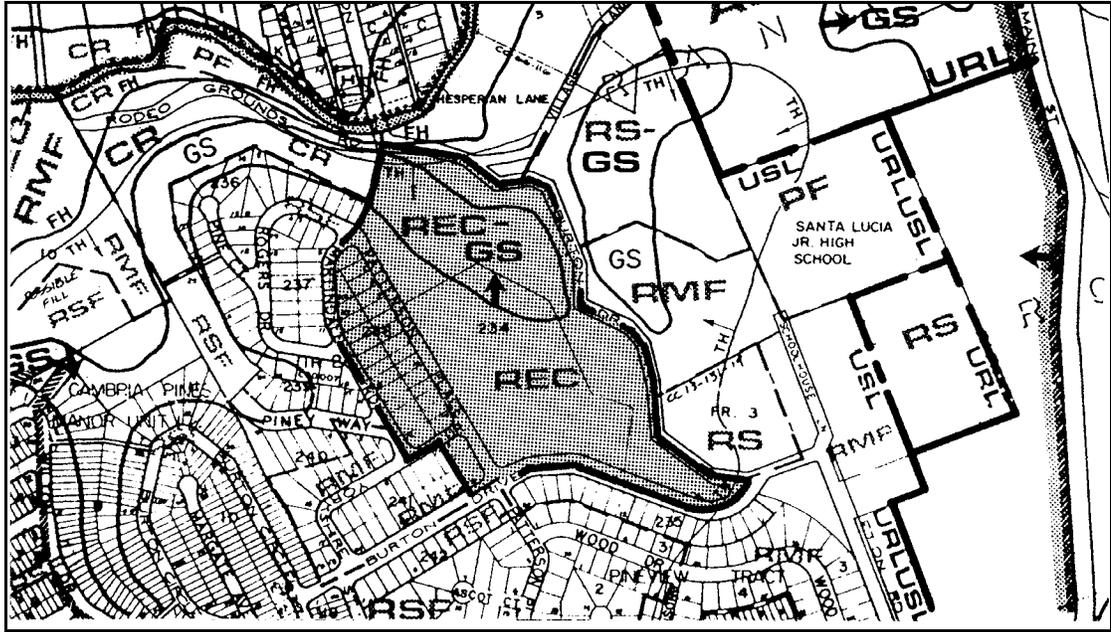


Figure 7-22: Cambria Pines Lodge

15.B. Limitation on Use - West of Patterson Way. For that area designated Recreation, but located outside the "V" overlay which is located west of Patterson Way, principally permitted uses are limited to Hotels and Motels, Bed and Breakfast Facilities, and Passive Recreation. Non-Principally permitted uses are limited to a Caretaker's Residence. ~~and Multi-Family Dwellings.~~

16.C. Permit Requirements. ~~A~~ Development Plan approval is required for all uses. The Development Plan is to integrate the residential uses with the lodge facility, and also determine the precise areas for visitor-serving uses.

A. Phasing plan. If the property development is to be phased over a period of time, the first Development Plan application is to include a project phasing plan.

17. Development Standards. ~~New development proposals are subject to the following standards:~~

~~A. Allowable densities for portions of the site proposed for residential development shall be calculated using only those areas of the site that have slopes of 20% or less, with the remaining area to be left in open space. No development is permitted on slopes greater than 20%, except for trails as may be appropriate.~~

~~B. Density on the site (as calculated by Standard A above) shall not exceed 26 units per acre.~~

East Village. Standards 18 through 26 apply only to the Recreation land use category in the East Village area shown on Figure 5, in addition to standards listed above.

~~18. **Permit Requirement.** Development Plan approval is required prior to any permanent development in this area, in order to identify overall development of the property, circulation, uses and phases to achieve consistency with the general plan.~~

~~19. **Application Content.** The application for the Development Plan shall include the following information, in addition to the content requirements in the applicable Land Use Ordinance Sections for Development Plans:~~

~~a. Specific locations for all proposed land uses together with proposed height and bulk structures, setbacks and other features of development;~~

~~b. Any areas proposed to remain undeveloped, and whether such areas are subject to flooding, have slopes in excess of 20%, or are characterized by other hazards or conditions which limit development;~~

~~c. Visitor-serving priority uses and coastal recreation uses and areas identified as the principal use of the site;~~

~~d. Areas intended for open space and passive recreation with amenities such as walkways, bicycle paths, outdoor furniture and similar improvements shown with typical drawings;~~

~~e. Information on the current and projected water supply and sewage treatment capacity adequate to support the amount of proposed development;~~

~~f. The location and extent of existing and proposed streets, including their proposed alignment, configuration, tentative widths and improvement standards and any improvement standards and any off-site improvements;~~

~~g. Landscaping areas to buffer views from Highway 1, Cambria Drive and Main Street of access roads, development and parking drives and spaces.~~

~~20. **Development Standards.** Site planning and architecture shall reflect Cambria's historical character. Variety in buildings, materials and ornamentation shall be utilized to avoid a uniform shopping center appearance. Support equipment and unattractive building elements shall be placed out of view of Highway 1. New development shall be sited in a manner which maintains the existing open views from Highway 1 across the meadows to Santa Rosa Creek. No development requiring the extension of water or sewer service shall be located outside the Cambria Community Services District (i.e., south of Santa Rosa Creek).~~

- ~~21. **Height Limitation.** Buildings shall contain no more than two stories and not exceed 25 feet, except that non-habitable, architectural building elements, deemed appropriate and essential in creating a consistent architectural character in the area, shall be permitted to extend above the height limit (e.g., clock tower, steeple, etc.), but no higher than allowed by the height regulations pursuant to the Coastal Zone Land Use Ordinance.~~
- ~~22. **Flood Protection.** Structures shall be built to or below existing grade except where fill or pier supports are needed for flood protection, and then the amount of fill/height of piers shall be the minimum needed to relegate the ground floor one foot above the 100-year flood stage as necessary to meet the requirements of Coastal Zone Land Use Ordinance Section 23.07.060. No fill shall be placed in a location or manner which would impair the effective 100-year flood flow capacity of Santa Rosa Creek.~~
- ~~23. **Setbacks.** Access roads shall be setback 30 feet from Highway 1 and Cambria Road. Development, parking drives and spaces shall be setback 50 feet from Highway 1 and 50 feet from the edge of riparian vegetation or top of the bank of Santa Rosa Creek, whichever is greater. Exceptions from the 50-foot creek setback may be allowed as stated by Standards 21 and 22.~~
- ~~24. **Circulation and Access.** Two-way vehicle access to the site shall be via a single intersection with Main Street at Pine Knolls Drive. A two-way collector street shall be extended from the intersection to a cul-de-sac outside the 50-foot creek setback. The plan line of this street shall be extended to the property line at Santa Rosa Creek.~~
- ~~— The proposed alignment of access roads shall be the least environmentally damaging alternative, and shall minimize cutting and filling; shall not extend into sensitive habitat areas or buffers (except for the installation of utility lines, pipelines, drainage and flood control facilities, bridge across streams, or road approaches to bridges); shall minimize visual impacts on Highway 1, and shall not create significant adverse impacts on the road capacity or level of service of Highway 1. No driveways or access roads will be allowed to encroach onto Highway 1 from the site.~~
- ~~25. **Habitat and Watershed Protection.** Except for "crossing bridges" and access roads or pedestrian bicycle paths where no feasible, less environmentally damaging alternative route exists, development shall not be allowed within the floodway, stream, riparian corridors, areas of archaeological resources, in geologically unstable areas or on slopes in excess of 20%. Where alternative routes do not exist for stream crossings, development impacts shall be mitigated to the maximum extent feasible.~~
- ~~26. **Pedestrian and Bicycle Path.** A pedestrian and bicycle path is to be installed along Santa Rosa Creek at the time of development, located outside the riparian habitat and consistent with the environmentally sensitive habitat policies of the Land Use Element. This path shall be designed to connect to the adjoining segment of the pathway which is required by the standards for the adjacent Residential Multi-Family land use designation on Santa Rosa Creek. The path shall be maintained and open for public use.~~

J6. Patrick House - Bed & Breakfast. The following standard applies only to the J. Patrick House.

A. Limitation on Use. Uses are limited to a Bed and Breakfast Facility, and uses allowable in the Residential Use Group according to Coastal Table 'O' (Allowable Use Chart), Part I of the Land Use Element.

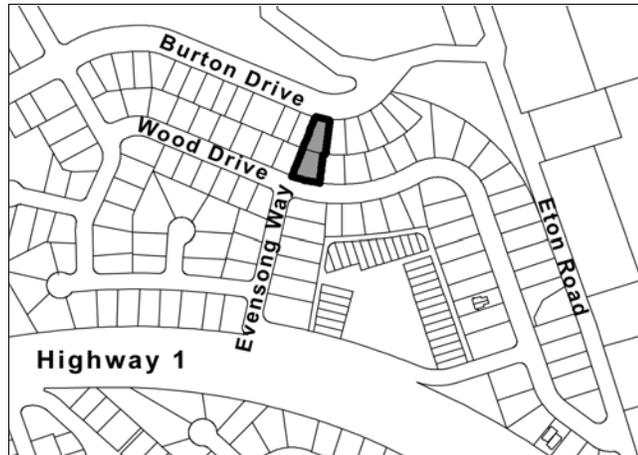


Figure 7-23: J. Patrick House Parcels

RESIDENTIAL MULTI-FAMILY: The following standards apply to all land within the Residential Multi-Family land use category.

~~1. **Density Limitations.** All Multi-family projects shall not exceed the following densities are subject to the following standards:~~

~~A. Projects on sites less than one acre shall not exceed 15 units per acre.~~

~~B. Projects on sites greater than one acre shall not exceed 26 units per acre.~~

2. **Height Limitation.** The maximum height for structures is 28 feet from average natural grade, ~~except when. Development visible from Highway One the height shall not exceed two stories, or 25 feet in height.~~

3. **Required Findings – Limitation on Use.** To maintain affordable housing opportunities, the following standards apply:

A. Single family dwellings shall not be allowed on lots that meet the minimum site area for multiple family dwellings as set forth in CZLUO Section 23.04.044 (Required Area).

B. Single family dwellings on lots that do not meet the minimum site area as set forth in CZLUO Section 23.04.044 (Required Area) may be allowed through development plan approval only where the Planning Commission first makes the finding that there is no feasible way for the property owner to acquire the minimum site area. Single-family dwellings in the Residential Multi-family land use category shall also conform with all standards applicable to the Residential Single-family land use category.

4. **First Baptist Church.** The following standard applies only to that parcel bounded by Green Street, Ardath Drive and Highway One on West Lodge Hill.

A. 3. Limitation on Use. Uses are limited to: Churches, Membership Organizations, and Schools - Preschool through Secondary. ; ~~Accessory Storage; Pipelines and Power transmission; and Public Utility Centers.~~

The Ranch. Standards 4 through 5 apply only to the Residential Multi-Family land use category on the Ranch, east of Highway 1 and south of Santa Rosa Creek (see Figure 5):

~~4. **Permit Requirements.** Development Plan approval is required for all uses.~~

~~5. Pedestrian and Bicycle Path. A pedestrian and bicycle path is to be installed along Santa Rosa Creek at the time of development located outside the riparian habitat and consistent with the environmentally sensitive habitat policies of the Land Use Element.~~

5. Tract 226. The following standards apply only to Tract 226 (see Figure 5 ~~7-24~~).

A. 5. Permit Requirement - Tract 226. Prior to ~~any additional~~ development or land division in Tract 226, a ~~Master Development Plan or Specific Plan~~ is to shall be approved for the entire tract, including each ownership within the tract. The Development Plan application is to include specific provisions for resolving the following issues:

~~A. 1.~~ **A. 1.** Establishment of an association of property owners to develop cooperative proposals for: ~~site~~ land uses, access, circulation and parking, development phasing, ~~improvement design, design improvements,~~ construction and maintenance ; definition and maintenance of common facilities.

~~B.~~ **B.** ~~Aggregation of contiguous lots and ownerships as necessary to provide adequate sites commercial development.~~

~~C. 2.~~ **C. 2.** ~~Definition of use; Types of allowable uses, improvement standards, landscaping proposals concepts, and timing of improvement of the common area~~ installation of improvements within the common areas.

~~D. 3.~~ **D. 3.** Definition of a unified architectural theme for site development.

B. Land Division. Any subdivision or resubdivision of Tract 226 shall not result in any additional lots. (Mod131)



Figure 24: Tract 226

RESIDENTIAL SINGLE-FAMILY: The following standards apply to all land within the Residential Single-Family land use category.

~~1. **Lateral Access Dedication - Park Hill.** New blufftop oceanfront development shall be required to offer to dedicate a lateral easement for public access extending from mean high tide to the toe of the bluff.~~

1. **Permit Requirement.** A Minor Use Permit approval is required for development on lots of 20% or greater slope and for development within Special Projects Areas. A Plot Plan is required approval for other lots unless a higher level of review is required by the Coastal Zone Land Use Ordinance. A Minor Use Permit approval is required where TDCs are used to increase Footprint or Gross Structural Area.

Lodge Hill

~~In addition to the previous standards for 25' lots, the following standards apply to Lodge Hill (see Figure 5):~~

~~7. **Permit Requirements and Application Content.** Minor Use Permit approval for development on lots of 25% or greater slope and for development within Special Project Areas. Plot Plan approval for other lots unless a higher level of review is required by the Coastal Zone Land Use Ordinance. Consideration of a bonus for additional footprint or gross structural area requires Minor Use Permit approval.~~

~~Applications shall include information on grading, drainage and erosion and sedimentation control in accordance with Chapter 23.05 of the Coastal Zone Land Use Ordinance.~~

2. **Secondary dwellings.** The maximum floor area of a secondary dwelling shall be 640 square feet. The secondary dwelling shall be permanently attached by a common wall to the primary dwelling or on the second floor of the primary dwelling's detached garage or located less than 50 feet from the primary residence, whichever causes the least environmental disturbance. Guesthouses are defined in and subject to the standards of Section 23.08.032 of the CZLUO.

3. **Application Content.** In addition to the applications requirements of the Coastal Zone Land Use Ordinance, Minor Use Permit applications shall include the following: (RDP)

A. **Contour Map.** Provide site contour information for the entire property at two-foot intervals. (RDP)

- B. Architectural Elevations.** Provide front, side, and rear elevations of all proposed structures. (RDP)
- C. Vegetation Location.** The site layout plan shall contain location of any shrubs and native vegetation. (RDP)
- D. Streetscape Plan.** A scale drawing showing the front exterior elevation (view) of the proposed project and the front elevations of adjacent buildings on the two lots to the left and the two lots to the right of the subject property. (RDP)
- E. Statement of Design Criteria Compliance.** Residential Single Family Standard 10 contains discretionary design criteria that are flexible in their meaning to allow for interpretation. The application shall include a statement explaining how the proposed project has met the intent of the design criteria.(RDP)
- F. Additional Contents - Oceanfront Lot Development.** Applications shall include plans with measurements for the form, mass, scale, and roofing and yard features (such as fencing). To the maximum extent feasible, new development shall be compatible with the character of the surrounding neighborhood and protect public views. (RDP)

~~3. **Lot Consolidation - subdivisions of 25-foot lots.** The following standards apply to Lodge Hill as well as all other areas (such as Park Hill) that are subdivided into 25-foot lots. In all areas such as Lodge Hill, Happy Hill and Park Hill that are subdivided into 25-foot lots, the following standards shall apply:~~

- ~~A. A single lot with a frontage of 25 feet constitutes a separate building site only if it is under separate ownership from adjacent lots (See Section 23.04.048 of the Coastal Zone Land Use Ordinance).~~
- ~~B. Single ownership of two or more adjoining vacant lots are subject to the requirements of Section 23.04.048 – Lot Consolidation of the Coastal Zone Land Use Ordinance.~~
- ~~C. **Pine Forest Preservation.** New construction shall be required to preserve the Cambria Pine Forest as follows:~~
 - ~~(1) No tree shall be removed unless it is within the structural line of an approved development in accordance with Section 23.05.060-064 of the Coastal Zone Land Use Ordinance.~~
 - ~~(2) Trees may only be removed if the County or a county approved consultant determines they are diseased or pose a hazard.~~

- ~~(3) Any tree(s) with a trunk diameter of 8 inches or greater removed from a development site are to be replaced on a two for one basis, to the approval of the Planning Department.~~
- ~~(4) If available, replacement trees shall be five gallon Monterey Pines, grown from seeds obtained from the Cambria stand.~~
- ~~(5) Construction practices to protect Monterey Pines from disturbance shall be implemented. Such practices shall include protecting tree trunks from construction equipment by wrapping with heavy materials (e.g., layers of burlap); protecting root systems through the design of the foundation and careful use and storage of construction equipment.~~
- ~~(6) Undeveloped area of each building site shall be maintained in native vegetation and natural character.~~

8. ~~Site Development Standards. New development shall satisfy the following standards:~~

4. Topographic Standards.

- A. Land Alteration Within View Corridors.** Land alteration shall be minimized on sites located within areas that are determined by the Planning Director to be public view corridors from collector or arterial roads (per Coastal Zone Land Use Ordinance Section 23.05.034d). These roads are identified on the Circulation map as Highway One, Main Street, Burton Drive, Eton Road, Ardath Drive, Pineridge Drive, Windsor Boulevard, Charing Lane, Weymouth Street, Buckley Drive, and Cambria Pines Road. (RDP)
- B. Minimize the Extent of Excavation and Fill on a Site.** Grading, filling or site disturbance of existing soil and vegetation shall be limited to the minimum areas necessary. Surfaces such as driveways, patios, and walkways shall be limited to the smallest functional size and shall utilize porous materials and design where feasible.
- C. Minimize the Visual Impacts of Retaining Walls as Seen from the Public Right-of-way and Neighboring Sites.** Multiple terraces are preferred, with terracing or revetments when feasible, rather than one tall retaining wall. The exterior height of retaining walls for driveways and other site development shall be no taller than six feet from finish grade, unless other solutions are necessary for fire or emergency access or are infeasible. Walls should be stepped or terraced to provide small-scale articulation. (RDP)

5. Height. The maximum height for all single family structures is 28 feet above average natural grade, except as required below follows. (RDP)

A. Lodge Hill. The maximum height for structures visible from Highway One † within the Lodge Hill area is 25 feet above average natural grade.

B. Marine Terrace. The maximum height for structures between Marlborough and Sherwood Drive is 22 feet above average natural grade.

C. Nottingham Drive, Windsor Boulevard and Sherwood Drive. The maximum height for structures between the ocean and ~~Sherwood Drive~~ the street is 15 feet as measured from the centerline of the fronting road, which is calculated as the average of five equidistant points on the centerline for the following areas:

1. Nottingham Drive. Cambria Pines Unit 5, those blocks west and southwest of Nottingham Drive.

2. Windsor Boulevard. Tract 159, Blocks 1, 2, and 3, that are west and southwest of Windsor Drive.

3. Sherwood Drive. The maximum height for structures shall be 15 feet.

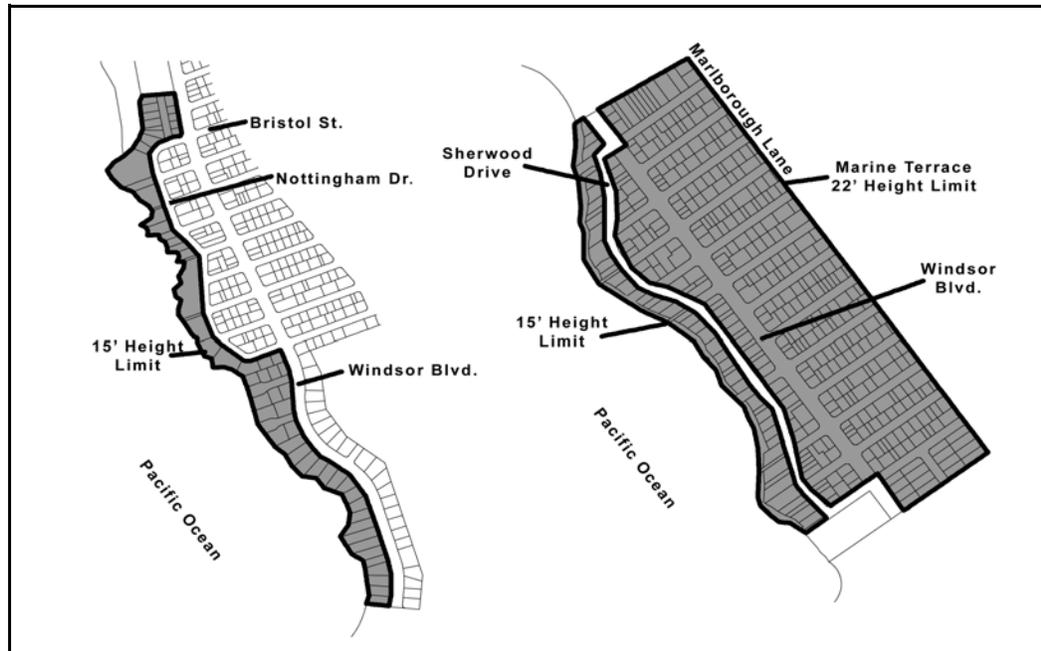


Figure 7-25: Windsor - Nottingham-Sherwood Height Limit Areas

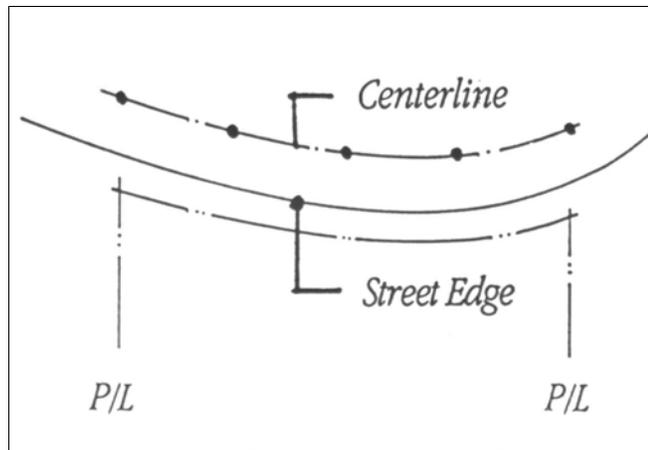


Figure 7-26: Measuring Height from Centerline

- D. Moonstone Beach. Height Limitation.** The maximum allowable height for all new buildings between Highway † One and the ocean is 15 feet from average natural grade.

5 6. Moonstone Beach. The following standards apply only to the Moonstone Beach Residential Single-Family land use category. (see Figure 5).

- A. Exterior Building Colors and Materials.** Native materials and textures ~~are to predominate in use~~ shall be used on the exterior of buildings. ~~Paint if used, is to be of Building colors shall be~~ subdued hues and tones harmonizing with the colors of the natural environment. Roofing should be ~~wood or wood-appearing materials.~~
- B. Road Improvements.** Road improvements for Moonstone Beach Drive are not to include curbs, gutters ~~and~~ or sidewalks.

~~The Ranch. Standard 5 applies only to Residential Single Family land use category in the area designated as the Ranch (see Figure 5):~~

- ~~5. — Permit Requirements. Development Plan approval is required prior to any development on the Ranch, with the plans detailing location of uses including single-family neighborhoods and clusters of multiple family units. The following additional details are to be incorporated into the plan:~~
- ~~— C. — Time schedule for development and any program for phasing.~~
- ~~— D. — Major and collector streets leading to Lodge Hill, Park Hill and the West and East Villages.~~
- ~~— E. — Recreation/resort oriented activities and passive recreation areas.~~

~~F. Measures to provide for preservation of the shoreline, steep hillsides, and larger stands of native pines in open space. Proposed means for integrating other open space areas into the design of the Ranch development to separate it from adjacent residential areas.~~

~~G. Maximum density shall be 2 units per acre.~~

7. Building Size. Development in the Single Family Residential category in areas subdivided into 25-foot lots (such as Lodge Hill, Happy Hill, Park Hill and Pine Knolls) shall comply with the footprint and gross structural area requirements shown on Table 7-1. (RDP)

~~9. Building Standards for Lodge Hill. The construction of residences on lots in Lodge Hill in the Residential Single Family category (see Figure 4 for area) shall be in accordance with the height, footprint and gross structural area requirements shown on Table G.~~

The standards of ~~Table G 7-1~~ do not apply to Tract 163, Tract 61, ~~Tract 358, Tract 384, Tract 420, Tract 44, Tract 112,~~ Cambria Pines Estates #1, and the two marine terrace blocks (Blocks 1 and 2, Tract 97) south of Lampton Street. Any parcel deemed by the county to be non-conforming because of its size is subject to standards of ~~Table G 7-1~~.

A. Use of Table 7-1. ~~Table G is used by first determining the number of legal subdivided lots that comprise the ownership (such as a single 25-foot, double or triple configuration) and selecting the appropriate category. Then select the correct type of lot (such as Special Project Area 1, Forested, or Steep Lot) using the definitions of these standards. This will yield the maximum allowable footprint and gross structural area. Table 7-1 is used by first determining the lot size based on square footage held as a single legal lot prior to construction permit issuance. Then select the correct type of lot (such as Special Project Area 1, Forested, or Steep Lot) using the definitions of these standards. This will yield the allowable footprint and gross structural area (GSA) for the base area lot size.~~

Where lot sizes do not conform exactly to the base area as shown in Table 7-1, footprint and GSA are to be adjusted proportionally. For example, for a 3,800 square foot single legal forested lot, the allowable footprint would be 1,308 square feet (1,200 x 1.09 = 1,308) and the allowable GSA would be 1,962 (1,800 x 1.09 = 1.962)

~~Table G Footnotes. Standards 1-3 below shall be used with Table G where interpreting lot sizes that do not conform exactly to base density or where a Footprint and Gross Structural Area bonus is requested.~~

~~1. Building sites greater than 5,250 square feet may be permitted additional Footprint and Gross Structural Area equal to the percent that the site is greater than 5,250 square feet.~~

- ~~2. Building sites 5,250 sq. ft. or less, the permitted maximum Footprint and GSA shall be adjusted as follows:~~
- ~~a. Single lot category - if the building site is greater than 1,750 square feet, the Footprint and GSA may be increased by the percent that the lot area is greater than 1,750 square feet.~~
 - ~~b. Double lot category - if the lots are greater than 3,500 square feet, the Footprint and GSA may be increased by the percent that the lot is greater than 3,500 square feet.~~
- ~~Where the square footage of the building site is less than the base area (1,750 square feet for single lot, and 3,500 square feet for double lot category), the permitted Footprint and GSA shall be decreased accordingly.~~

**Table 7-1
Standards for Building Sites**

A. LOT SIZE: 1,750 - 3, 499 SQUARE FEET (Base Area = 1,750 Sq. Ft.)		
TYPE OF LOT	FOOTPRINT (SQ. FEET) (Based on a 1,750 sq. foot lot)	GSA (SQ. FEET) (Based on a 1,750 sq. foot lot)
1. SPECIAL PROJECT AREA 1 (STEEP CANYON)		
0-25% SLOPE	600	900
25% PLUS	500	600
2. SPECIAL PROJECT AREA 2 (VISIBLE HILLSIDE)		
0-25% SLOPE	600	900
25% PLUS	500	700
3. FORESTED	600	900
4. STEEP LOTS (30% PLUS)	500	700
5. MARINE TERRACE	900	1,000
6. TYPICAL LOTS	700	900

B. LOT SIZE: 3,500 - 5,249 SQUARE FEET (Base Area = 3,500 Sq. Ft.)		
TYPE OF LOT	FOOTPRINT (SQ. FEET) (Based on a 3,500 sq. foot lot)	GSA (SQ. FEET) (Based on a 3,500 sq. foot lot)
1. SPECIAL PROJECT AREA 1 (STEEP CANYON)		
0-25% SLOPE	1,050	1,350
25% PLUS	900	1,000
2. SPECIAL PROJECT AREA 2 (VISIBLE HILLSIDE)		
0-25% SLOPE	1,100	1,400
25% PLUS	950	1,100
3. FORESTED	1,200	1,800
4. STEEP LOTS (30% PLUS)	950	1,100
5. MARINE TERRACE	1 STORY: 1,900 2 STORY: 1,650	1 STORY: 1,900 2 STORY: 2,000
6. TYPICAL LOTS	1 STORY: 1,900 2 STORY: 1,300	1 STORY: 1,900 2 STORY: 2,000

C. LOT SIZE: 5,250 SQUARE FEET AND LARGER (Base Area = 5,250 Sq. Ft.)		
TYPE OF LOT	FOOTPRINT (SQ. FEET) (Based on a 5,250 sq. foot lot)	GSA (SQ. FEET) (Based on a 1,750 sq. foot lot)
1. SPECIAL PROJECT AREA 1 (STEEP CANYON)		
0-25% SLOPE	1,400	1,800
25% PLUS	1,200	1,400
2. SPECIAL PROJECT AREA 2 (VISIBLE HILLSIDE)		
0-25% SLOPE	1,500	1,900
25% PLUS	1,300	1,500
3. FORESTED	1,600	2,400
4. STEEP LOTS (30% PLUS)	1,400	1,600
5. MARINE TERRACE	1 STORY: 2,200 2 STORY: 2,050	1 STORY: 2,200 2 STORY: 2,450
6. TYPICAL LOTS	1 STORY: 2,200 2 STORY: 1,700	1 STORY: 2,200 2 STORY: 2,600

TABLE G

STANDARDS FOR LODGE HILL LOTS

A. SINGLE LOT CATEGORY - 25' LOTS (1750 SQ.FT.)

TYPE OF LOT	MAX. HT.	FOOTPRINT	GROSS STRUCTURAL AREA
1. SPECIAL PROJECTS AREA 1 (Steep Canyon)			
a. 0-25% slope	25'*	500 sq.ft.	900 sq.ft.
b. 25% plus	25'*	400 sq.ft.	600 sq.ft.
2. SPECIAL PROJECTS AREA 2 (Visible Hillside)			
a. 0-25%	25'*	500 sq.ft.	900 sq.ft.
b. 25% plus	25'*	400 sq.ft.	700 sq.ft.
3. FORESTED	28'***	500 sq.ft.	900 sq.ft.
4. STEEP LOTS (30% plus)	28'***	400 sq.ft.	700 sq.ft.
5. MARINE TERRACE	22'	800 sq.ft.	1,000 sq.ft.
6. TYPICAL LOTS	28'***	600 sq.ft.	900 sq.ft.

3. ~~Footprint and GSA bonus. Where an applicant can clearly demonstrate that design and layout concessions have been made in order to save healthy trees, minimize site disruption, visual impact, minimize erosion, or selection of compatible building materials, and clearly goes beyond the basic requirements of these standards, the Planning Director, by Minor Use Permit review, may grant up to a 10 % increase of Footprint and GSA as indicated on Table G.~~

B. Definitions. The following definitions shall be used in the interpretation of Table 7-1G:

- 1. Footprint.** Means The area of the lot, expressed in square feet, covered by residential and accessory structures including any structural overhangs; ~~expressed in square feet, and.~~ Footprint includes living areas, garages and carports but it does not include open deck area, balconies, or eaves.
- 2. Gross Structural Area.** Means All interior areas, expressed in square feet of floor area, within the volume of the structure including it includes living areas, storage, garages and carports. Gross Structural Area is measured to the exterior limit of the building walls. Gross Structural Area does not include open exterior decks or interior mezzanines (as defined by the UBC) ~~tofts~~ added within the height limitation to gain additional square footage.
- 3. Slope.** The average slope of a lot ~~to~~ be determined by using one of the ~~slope determination~~ methods in Chapter 23.11 (Slope, Average) of the Coastal Zone Land Use Ordinance.
- 4. Special Projects Areas.** Refers to ~~sensitive~~ Areas delineated as such on the ~~Figures. 7 and 8~~ 7-27 and 7-28.
- 5. Forested Lot.** A lot containing one or more native Monterey Pine trees that are six inches or more in diameter 4.5 feet above ground, and not in a Special Project Area as shown on Figures 7-27 and 7-28 or within Marine Terrace area as defined in this section.
- 6. Marine Terrace.** The area located between Marlborough Lane and ~~Sherwood Drive~~ the coastal bluff in Lodge Hill, the area between Windsor Boulevard and the coastal bluff in Park Hill, and Tract 159 in Park Hill.
- 7. Step Lot.** A lot with an average slope of 30% percent or greater.
- 8. Typical Lot.** A lot that has an average slope less than 30% percent, contains no Monterey Pine trees, and is not located in the Marine Terrace as defined in this section or Special Projects Area as shown on Figures 7-27 and 7-28.

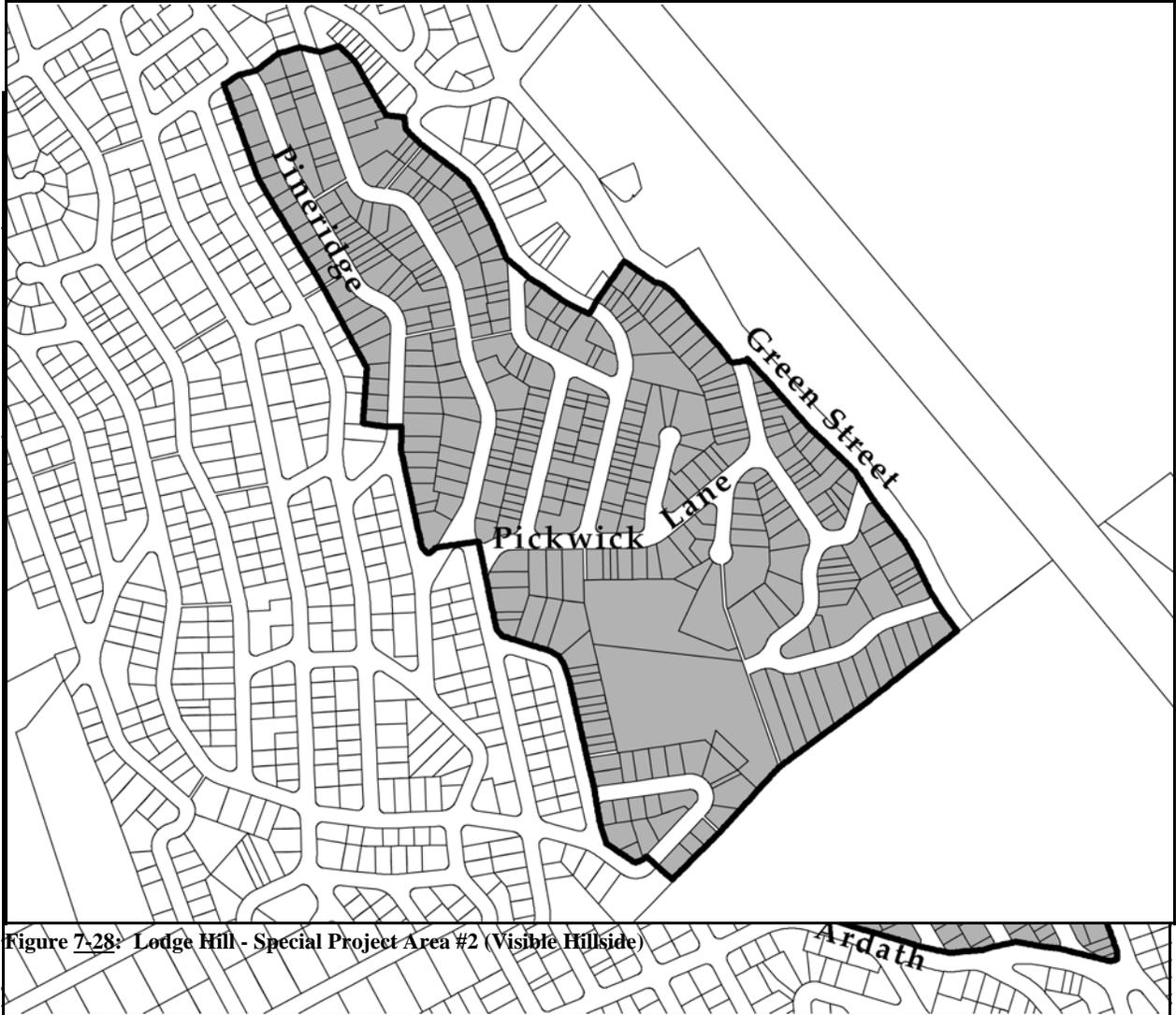


Figure 7-28: Lodge Hill - Special Project Area #2 (Visible Hillside)

Figure 7-27: Lodge Hill - Special Project Area #1 (Fern Steep Canyon)

- C. Transfer of Development Credits.** The Transfer of Development Credit Program (TDC's) may be used to transfer allowable footprint and GSA for lots within certain areas to more suitable sites within Cambria.

Note: TDC may not be available in all areas due to limits of participation of local land conservancies.

- 1. Eligible Purchasers of TDC'S.** Purchasers of TDC's may include the following:
 - a. Owners of small lots within Lodge Hill.** Through the transfer of development credits (TDC's), owners of property on Lodge Hill (specifically those areas identified as West Lodge Hill and East Lodge Hill) may be allowed an increase in the allowable footprint and gross structural area on their property.
 - b. Owners of other small lot properties within the Cambria Urban Reserve Line.** Properties with sewer or water service by the Cambria Community Services District (CCSD), may participate in the TDC Program (May include Park Hill and Happy Hill).
 - c. Other properties.** Owners of properties that have been required by planning area standards, conditions of development approval, or other provisions of the Local Coastal Program to offset impacts of development through purchase of TDC's may also participate. (Rec4.41)
- 2. Payment of Fees.** Under this program, a fee may be paid to the Land Conservancy of San Luis Obispo County or another TDC program administrator approved by the Director of Planning and Building. The fee must be adequate to allow for purchase of typical lots within the preservation (special study) areas, plus sewer assessments and administrative costs.
- 3. Resale Provisions.** With the concentration of purchases in specific preservation areas, the program may propose lot consolidation and eventual sale of portions of the preserved area to adjacent property owners. The preserved areas sold to adjacent property owners shall be consolidated with the adjacent property and guaranteed to maintain as open space through a recorded, permanent open-space easement granted to the county. Money collected from the sale of the preserved areas shall be used for the retirement of additional lots.

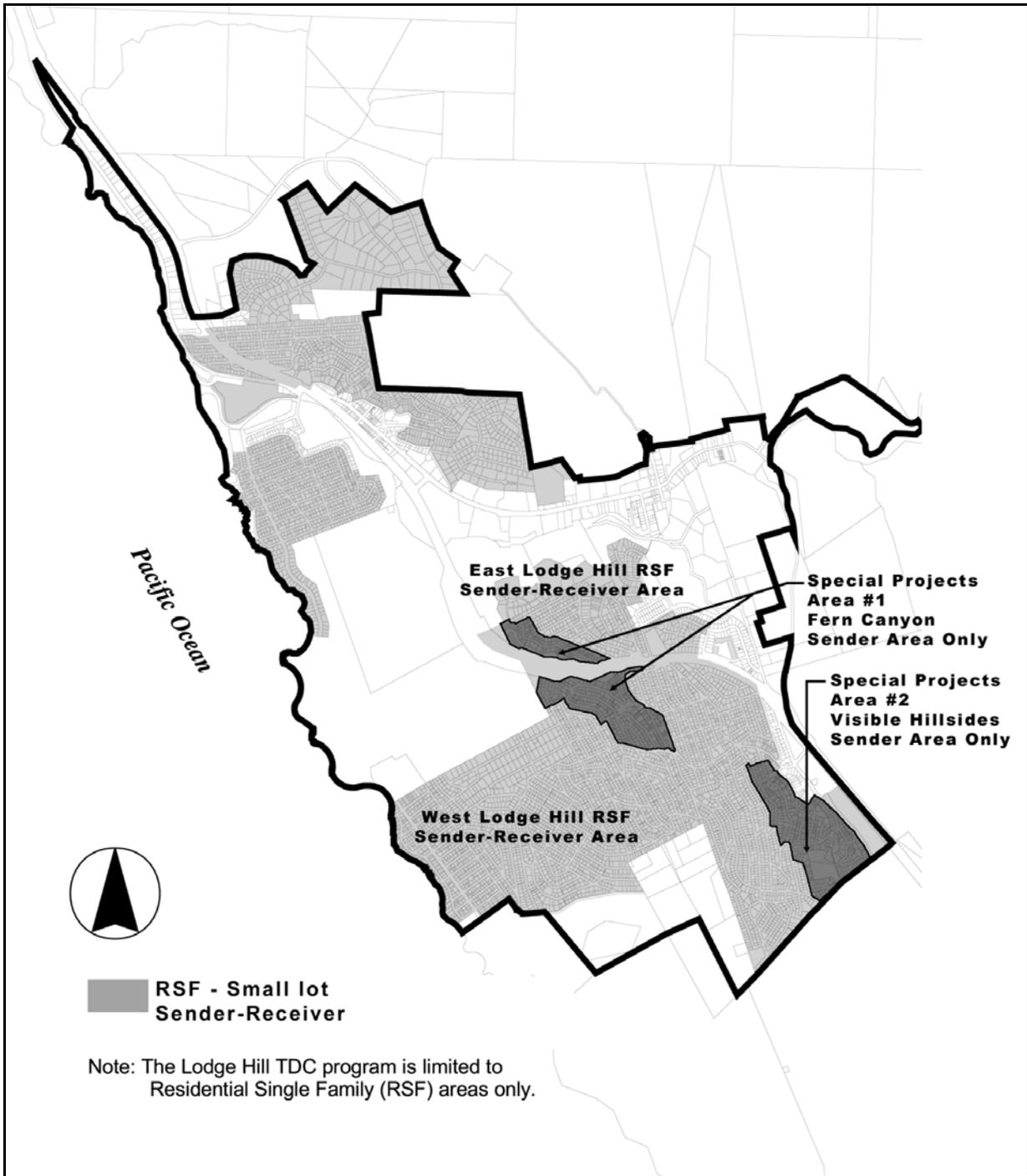


Figure 7-29: Transfer of Development Credits Program Area

4. Criteria for Using TDC's. For lots eligible to receive TDC's, the number of TDC's that can be used on any one house shall be evaluated against the following criteria: (Rec 2.18)

a. Site design. The use of TDC's shall not result in an appearance which would be incompatible with homes in the surrounding area.

b. Maximum TDC use. Total increase in Footprint or Gross Structural Area due to the purchase of TDCs shall not exceed the following: (RDP)

- 1,750-3,499 square foot lot size (base area=1,750 sq. ft.): 100 square feet
- 3,500-5,249 square foot lot size (base area=3,500 sq. ft.): 300 square feet
- 5,250 and larger (base area=5,250 square feet): 400 square feet

Where building sites are different than the base area in the appropriate category, a proportionally adjusted number of TDCs may be permitted.

c. Limitation on use of TDCs. TDCs shall not be used to increase building size on sites that contain wetlands, habitat for rare or endangered species, identified cultural resources, slopes in excess of 30 percent, or where, in the opinion of the Planning Director, excessive grading or tree removal is being proposed. (Rec 2.18)

d. Use of TDCs in Special Project Areas. Special Project Areas are intended to be sender areas, however, TDC's may be used on eligible projects in Special Project Area #2 (Visible Hillside) provided that the lots retired are also located in the same Special Project Area. (Rec 4.41)

D. Deck Size Limitations. Exterior decks shall be located to avoid removal of trees. Solid exterior decks shall be limited to 10% percent of the permitted Maximum Footprint, while decks of permeable construction (e.g., open wood slats) shall be limited to 30% percent of permitted Maximum Footprint.

~~12. Sherwood Drive - Setback and Height Requirements. The maximum height for structures between the ocean and Sherwood Drive shall be 15 feet as measured from the centerline of Sherwood Drive.~~

E. Lodge Hill Special Project Areas. There are two areas of Lodge Hill where with special resource protection ~~problems exist~~ issues. ~~Issues associated with Special Project Area # One is sensitive because of the rare pine forest, steep slopes, and potential to loose the forest habitat as small lots are developed.~~ are related to the

presence of the rare Monterey pine forest. As small lots are developed, the potential for loss of forest habitat is high. Issues associated with Special Project Area 2 Two is highly visible from Highway One because of its lack of vegetation and prominent location. are related to this area's visibility from Highway One, because of the area's prominent location and lack of screening vegetation. Both areas have steep slopes and high erosion potential. Minor Use Permit is required prior to development in these areas, and New development is subject to the following standards:

1. Building Materials. All development within the viewshed of Highway One shall be constructed ~~and maintained in~~ of natural-appearing materials, using and earthen or forest-toned colors (e.g. natural weathered wood). Reflective materials shall be minimized. The natural appearance of buildings, fences, and other development shall be maintained through the life of the structure.

2. Utility Locations. Where feasible, utilities should be located to minimize visual impacts, including undergrounding.

~~b. All development within the viewshed of Highway One shall be landscaped with native plant materials to complement and screen the development from Highway One.~~

~~d. A minimum of two Monterey or Cambria Pines shall be planted in the yard areas on lots in Special Projects Area 2 and maintained in a satisfactory condition.~~

F. Cambria Pines Estate Unit No. 1. Minimum parcel size shall be 15,000 square feet. No additional subdivisions shall be approved in this area.

G. Top of the World. (APN: #24-301-28, and 31). The presently unsubdivided parcel (APN: 24-301-13) may be subdivided into no more than two parcels. The resulting These two parcels shall not be further subdivided.

8. Setbacks. Unless specified in other Cambria Urban Area Standards, the following special setbacks apply to the respective neighborhoods shown on Figure 7-1. Where no setback is listed, setbacks shall be determined by applicable sections of Coastal Zone Land Use Ordinance.

A. Front and Rear Setbacks. For all lots less than one acre in size, front and rear setbacks must total 25 feet, with a minimum of 10 feet in the front and 10 feet in the rear, except as provided below:

1. All Lots with a Coastal Bluff. The bluff setback is to be determined by an engineering geology report, prepared by a California-licensed certified Engineering Geologist, to be adequate to withstand bluff erosion and wave action for a period of 75 years and in any case shall not be less than 25 feet.

2. **Sloping Lot Adjustment for Garages.** The front setback may be adjusted for garages as allowed by Coastal Zone Land Use Ordinance Section 23.04.108a(2).
3. **West side of Sherwood and Nottingham Drives.** The front setback for areas shown in Figures 7-30 and 7-31 is 5 feet.

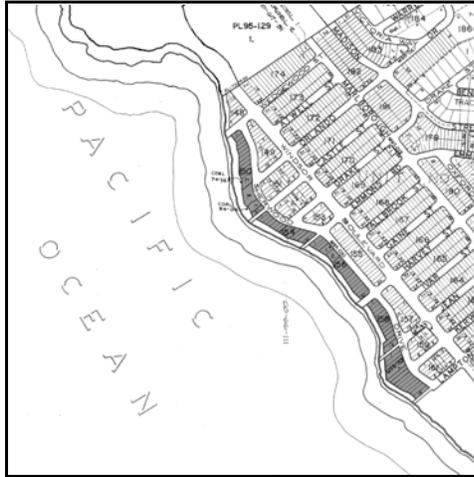


Figure 7-30: West of Sherwood Drive

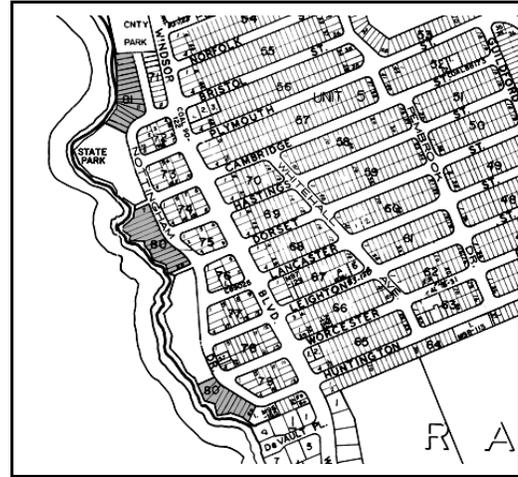


Figure 7-31: West of Nottingham Drive

4. **Tract 163.** The minimum front setback for those areas shown in Figure 7-32 is 20 feet, except as follows: (RDP)

<u>Block 3, lots 15 - 31:</u>	<u>10 feet</u>
<u>Block 4, lots 12 - 20:</u>	<u>10 feet</u>
<u>Block 5, lots 2 - 8:</u>	<u>10 feet</u>
<u>Block 6, lots 7 - 11:</u>	<u>25 feet</u>

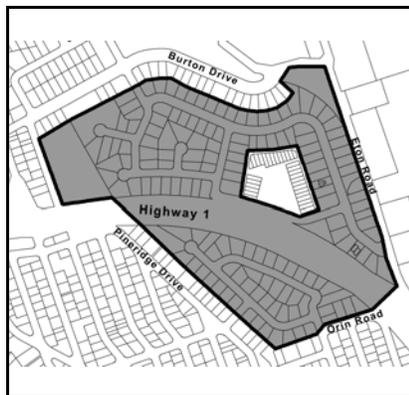


Figure 7-32: Tract 163

5. **Tracts having setbacks established by conditions of approval.** Building setbacks for Tracts 384, 420, 543 are established in final subdivision maps and take precedence over other setback standards. (RDP)

B. Side Setbacks. For all lots less than one acre in size, ground level floor side setbacks shall be in accordance with Table 7-2 below:

**Table 7-2
Standards for Side Setbacks**

Lot Width	Ground Level Floor Side Setbacks
Less than 50 feet	3 feet except 5 feet on street corner side
50 feet and wider	Combined 12 feet with 5 feet minimum on each side. Street corner lots combined 17 feet with 5 feet minimum on each side, and 10 feet minimum on the street corner side.

C. Building Face Height and Setback. On lots with an average slope of 15% or greater prior to grading, at least 50% of the length of the down-slope building face shall have a maximum height of 16 feet from finished grade with a minimum setback of six feet from that building face to the next higher story. Cornices, eaves, and rain gutters may project up to three feet into the required 6 foot setback. These requirements may be modified only if all of the following are met:

1. A Minor Use Permit is approved.
2. The maximum height of the building face is 28 feet as measured from finished grade.
3. The Review Authority determines that the apparent mass of the building is reduced through use of significant articulation - including three dimensional components - that meets the intent of Residential Single Family Design Criteria 10E. Building Design Standards.

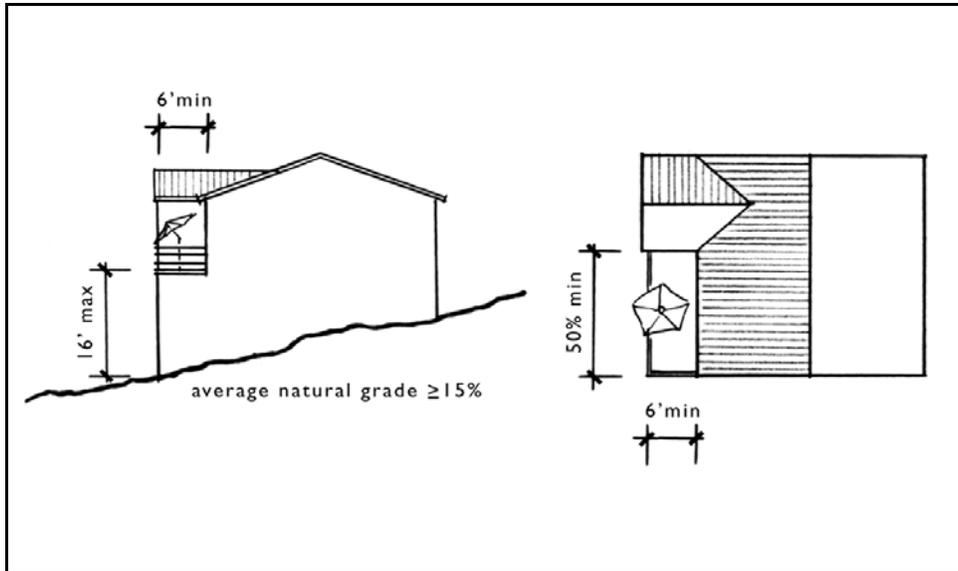


Figure 33: Building Face Height and Setback

9. Parking and Access Standards. The following standards set requirements for the design of parking areas for single-family residential development.

- A. Parking Requirements.** Two off-street spaces are required for each single-family dwelling. At least one space shall be covered (garage or carport), and the other space may be located within the front setback.
- B. Parking.** Where physical constraints of a building site preclude adequate on-site parking, alternatives such as parking bays located in conjunction with neighboring or nearby lots should be utilized, subject to approval of an adjustment (Coastal Zone Land Use Ordinance Section 23.01.044), and provided the space is not further than 100 feet from the building lot.

10. Design Criteria. This section contains discretionary design criteria that are flexible in their meaning to allow for interpretation. Applications for development shall include a statement explaining how the proposed project has met the intent of the criteria.

- A. Impermeable Surfaces.** Impermeable surfaces should be minimized in order to maintain a sense of open space and to serve specific purposes, such as retaining clusters of trees and the visual quality of public view corridors. (RDP)
 - 1. Structures, landscape screening and fences should be located to leave open space in larger areas rather than splitting it into small units, particularly as seen from the street. (RDP)**

2. The finished side of fencing should face the public street or be alternated in sections.

B. Parking Drives and Garages. Garages should not dominate the site or the design of the house. (RDP)

1. The mass of a garage should appear subordinate to the house, and the design of the garage should appear integrated into the building design. Garage door colors and materials should be the same or subordinate to the house design. (RDP)

C. Topography. The site design should follow the natural contours of the site where possible. (RDP)

1. Minimize the use of abrupt changes in grade on the site and between adjoining properties. A design that uses sloped, planted areas to create a smooth grade transition is preferred. (RDP)

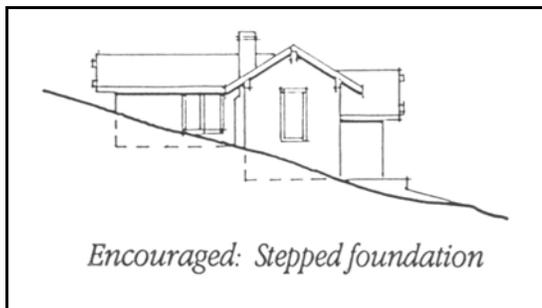


Figure 7-34: Stepped Foundation

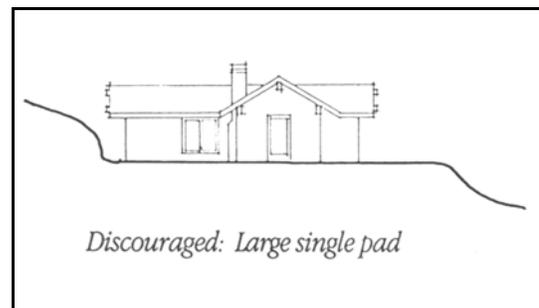


Figure 7-35: Large Single Pad Foundation

D. Drainage. Drainage systems should be designed to retain water on-site and encourage infiltration when feasible.

1. Natural drainage patterns should be retained and remediated if retention is infeasible on-site. (RDP)
2. On downhill sites, encourage drainage easements on lower properties so that drainage can be released on the street below. (RDP)
3. The portion of a driveway that is longer than 30 feet measured from the front edge of the garage should not exceed 10 feet in width, except as required by the Fire Department. Refer to CZLUO Section 23.05.104d for required minimum driveway widths.(RDP)

E. Building Design Standards.

1. Reduce apparent massing to achieve a small-scale appearance appropriate for the character of the neighborhood.
2. Design buildings to avoid removal of significant vegetation and blend structures into the natural setting to the maximum extent feasible.
3. Building facades should appear to be no more than two stories in height, as viewed from the public right-of-way. Presenting a one-story height to the street on downhill sites is encouraged. (RDP)
 - a. Two-story elements should be stepped in a series of successive increments. (RDP)
 - b. Tall exterior foundation walls and unused underfloor areas that add to the building mass should be avoided. (RDP)

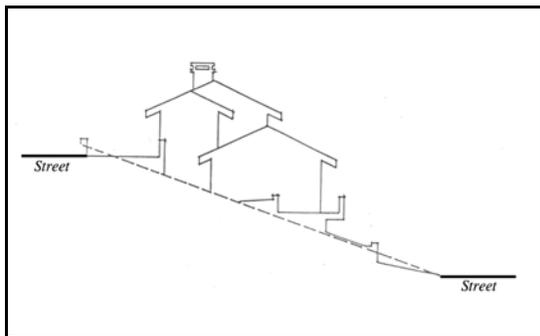


Figure 7-36: Stepped Two-story Elements



Figure 7-37: Unused Underfloor

4. **Building Forms, Materials and Details.** Building design should be visually compatible with the neighborhood while creating individual character. Building forms and ornamentation that are monumental or out of scale with the rest of the building should be avoided. (RDP)
5. **Use Natural-appearing Building Materials:**
 - a. In the forest setting, materials are preferred with patterns or texture, such as wood or wood-appearing siding, for example board and batten siding or shingles, as primary materials for exterior walls. Stucco, in conjunction with natural materials, may be considered if



Figure 7-38: Encouraged: Natural Materials



Figure 7-39: Use of Stucco

it supports the design. Fire resistant materials may be required. (RDP)

- b.** Colors that blend with background forest colors and earth tones are preferred choices to blend with the natural setting. Bright colors should be reserved to key accent features.

6. Roofs. (RDP)

- a.** Changes in elevation and direction in roof planes should be designed to express important elements of the residence. (RDP)
- b.** Roofs and details such as eaves should be consistent with and express



Figure 7-40: Roof Defining the Style

the architectural style of the building.(RDP)

7. **Architectural Details.** Details such as chimneys, overhangs, windows, doors (including garage doors), dormers, porches, entries and decks can be used to help reduce massing and scale. However, making any of these elements oversized can exaggerate the building's scale and should be

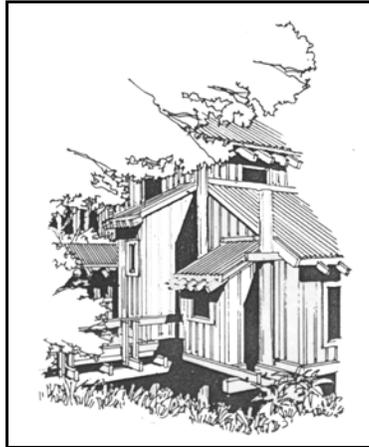


Figure 7-41: Creative style with integrated details

avoided. (RDP)

RESIDENTIAL SUBURBAN: The following standards apply to all land within the Residential Suburban land use category.

1. **Limitation on Use.** All allowable uses as shown on Coastal Table 'O' are permitted, except Mobile Home Developments.
- ~~2. **Site planning - West Lodge Hill.** New land divisions are to include design provisions for clustering of residential units to preserve the groves of pine trees.~~
2. **Commercial Nursery.** The commercial nursery at Burton Drive and Schoolhouse Lane is an allowable use in accordance with Coastal Table 'O' provided that a rural, rather than commercial, ambience is maintained through site arrangement, building design, sales displays and signing design.
3. **Residential Design.** Development shall conform to Residential Single Family Standard #10 - Design Criteria.
- ~~4. **Land Divisions.** The 51-acre parcel located at the southeastern edge of Lodge Hill shall not be further subdivided.~~

PUBLIC FACILITIES: The following standards apply only to lands within the Public Facilities land use category.

- 1. Site Specific Standards/Cambria Elementary School Site.** Standards 1 through 13 apply. The following standard applies only to the land shown in Figure 7-40.

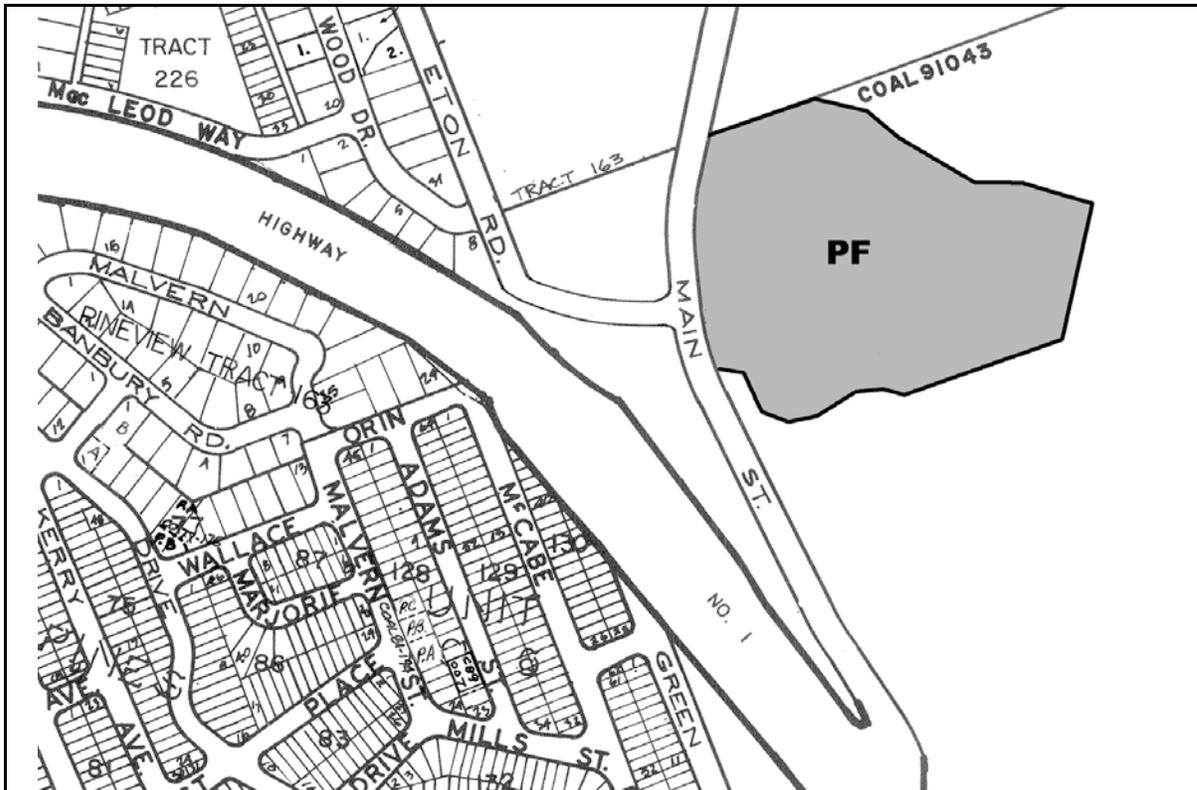


Figure 7-42: Cambria Elementary School Site

- A. Limitation on Use.** Allowable uses shall be limited to: Schools - pre to secondary, and agricultural uses. Portions of the site that will not be developed as part of an approved school facility shall be used only for agriculture, open space, or habitat restoration and enhancement. No subdivision other than that necessary to create the school site, or adjustment of lot lines that would result in an increase in the size of the Public Facilities parcel, shall be allowed. The size of the school site shall be the minimum necessary to accommodate the required school facilities, and in no case shall exceed 20 acres. All new development shall be consistent with approved Development Plan D020150D or subsequent Development Plan.

Note: The following is proposed for deletion because the school is expected to be completed prior to adoption of this plan update. Refer to the development plan conditions of approval.

~~The installation of public sewer and water utilities may be allowed on the Public Facilities site only where necessary to serve school uses that have received all necessary development approvals, provided that all pipelines are the minimum size necessary to serve the approved development and the deed for the property is permanently restricted in a manner that prohibits tie-in to the utility lines. The installation of utilities may not occur until development of approved school facilities has commenced and the deed restriction has been recorded. In addition, prior to the commencement of school construction, the school district shall record a one-foot wide easement around the perimeter of the site, to a third party, acceptable to the Executive Director of the Coastal Commission, that creates a utility prohibit zone. This zone shall prohibit the extension of utilities across it, except in the single location required to provide services to the school.~~

- ~~2. **Setbacks.** School facilities shall be set back from all wetland areas on the site in accordance with Section 23.07.172 of the Coastal Zone Land Use Ordinance. As part of the coastal development permit review required for the creation of the public lot and the development of school facilities adjustments to the parcel and or school design shall be made as necessary to provide setbacks that are the most protective of significant coastal resources.~~

~~The following agricultural buffer on the site, and a “right to farm” statement shall be provided with any development or land division:~~

- ~~A. **Compatibility of Agricultural Uses.** School development shall be compatible with the continuance of agricultural uses on surrounding parcels by providing agricultural buffers designed to prevent conflicts between school use and surrounding agricultural operations. Buffers shall be entirely located on the school site, incorporate vegetative or other physical barriers, and be as wide as necessary to prevent land use conflicts. Buffers shall be no less than 50 feet wide along the adjoining property lines except in limited instances where the Review Authority determines that a lesser setback would effectively prevent conflicts with agriculture.~~

- ~~No structures used for human habitation shall be constructed in the buffer area. Uses allowed in the buffers shall be limited to student agricultural activities, septic systems, and any habitat improvements as may be specified in a habitat restoration plan. The buffer area shall be permanently protected and restricted by easement or dedication. Buffer plantings or any other required barriers shall be maintained in perpetuity.~~

- ~~B. **Right to Farm Statement.** Prior to the approval of school development on the site, the applicant shall record a deed restriction certifying that the owner(s) or lessees of the property acknowledge and agree: (a) that the property described herein is adjacent to land utilized or designated for agricultural purposes; (b) that students, faculty, and all other users of the property may be subject to inconvenience or~~

~~discomfort or adverse effects arising from adjacent agricultural operations including, but not limited to, dust, smoke, noise, odors, fumes, grazing, insects, application of chemical herbicides, insecticides, and fertilizers, and operation of machinery; (c) users of the property accept such inconveniences and/or discomforts from normal, necessary farm operations as an integral part of occupying property adjacent to agricultural uses; (d) to assume the risks of inconveniences and/or discomforts from such normal, necessary agricultural use in connection with this permitted development; and (e) to indemnify and hold harmless the owners, lessees, and agricultural operators of adjacent agricultural lands against any and all liability, claims, demands, damages, costs (including costs and fees incurred in defense of such claims), expenses, and amounts paid in settlement arising from any issues that are related to normal, necessary farm operations and their impact to users of the property.~~

~~3. Design Standards. The following design requirements apply to any new development of school facilities and to the creation of the Public Facilities parcel:~~

~~A. The size and configuration of the Public Facilities parcels shall preserve the maximum amount of agricultural land contained on the existing 479-acre agricultural parcel, and shall not exceed 20 acres in size. This shall be achieved by clustering school facilities as much as possible.~~

~~B. Where feasible, new structures shall be hidden from Highway One; otherwise such development shall be screened through planting and permanent upkeep of appropriate tree species, in accordance with the landscaping standards below.~~

~~C. The buildings shall be placed on the site with minimal use of cut and fill while meeting state design criteria and disabilities act considerations. The impervious surface coverage shall be limited to the minimum necessary to accommodate a public school of a size documented as needed by the school district to serve existing and projected student populations and to meet State School Sizing Criteria. In no case shall this exceed 6 acres.~~

~~D. All structures shall be limited to one story and shall avoid silhouetting above the ridgeline when viewed from public places to the maximum degree feasible.~~

~~E. The buildings architecture shall use best design standards to be consistent with the character of the community of Cambria and compatible with the rural agricultural character of the surrounding rolling hill landscape. Compatible design shall be achieved through the use of: utilitarian design features; roofs pitches above horizontal; low-slung buildings separated by open spaces to break up visual massing; large building facades broken up by varied rooflines, offsets, and building projections that provide shadow patterns; and large structures broken down into smaller building elements (rather than long continuous forms). Large box-like designs, large unbroken rooflines, and/or flat surfaces lacking architectural treatment~~

shall not be allowed. All require fencing shall be rustic split rail fencing of rough-hewn and unpainted wood timbers (e.g., cedar) with the exception that alternative fence designs may be considered where necessary to provide effective agricultural buffers and designed to be compatible with the site's surroundings.

- ~~F. Any ancillary structures on the buildings such as air conditioning and electrical features shall be shielded from view, with screens that are part of the architectural design.~~
 - ~~G. Architectural elevations showing exterior finish materials, colors, and heights above the existing natural ground surface shall be submitted with any application. Colors of the buildings and building materials shall minimize the structure massing of new development by reducing the contrast between the purposed development and the surrounding environment. Colors shall be muted to soften the appearance of the structures and to reduce visibility from scenic Highway One, and shall be compatible with the natural colors of the surrounding environment, including vegetation, rock outcrops, etc. Darker, non-reflective, earth tone colors shall be selected for walls chimneys, etc. and darker green, gray, slate blue, or brown colors for the roof structures.~~
 - ~~H. The design of the school facilities shall include a refuse containment and maintenance component that provides fully enclosed or animal-proof garbage containers; specifically designated eating areas; and provisions built into maintenance contracts requiring that all eating areas anywhere on campus be swept clean on a daily basis.~~
 - ~~I. All linear utilities (including but not limited to electrical power, telephone and cable television service connections) shall be placed underground. Accessory utilities (e.g., utility meters, electrical panels, and transformers) shall be placed underground as practicable and safe.~~
4. Lighting. A lighting plan showing the height, location, and intensity of all exterior lighting shall be submitted with any application for development and shall, at a minimum, comply with the following standards:
- ~~A. All light fixtures shall be shielded so that neither the lamp nor the related reflective interior surface is visible. All lights poles, fixtures, and hoods shall be dark colored. All exterior light sources shall be low-level and adjusted so that light is directed away from neighboring areas. The height of freestanding outdoor light fixtures shall be limited to the height of the tallest permitted building on the site, and in no case any taller than 20 feet. Any security lighting shall be shielded so as not to create glare when viewed from neighboring areas. Light poles and fixtures shall not be obtrusive to travelers along Highway One. There shall be no exterior night lighting, other than the minimum lighting necessary for pedestrian and vehicular safety purposes.~~

- ~~5. Landscaping. A landscape plan meeting the requirements of Section 23.04.180 et seq. of the Coastal Zone Land Use Ordinance, and prepared by a qualified individual acceptable to the Department of Planning and Building, shall be submitted with any application for development. The landscape plan shall, at a minimum, include the following:~~
- ~~A. Vegetation that will provide 75 percent screening of new development after five years, including ancillary structures such as trash collection areas and maintenance structures when viewed from public view corridors such as: Highway One, north of Main looking east; Ardath Drive and Green Street intersection looking east; intersection of Main and Highway One looking east; and Highway One, south of Main and Highway One looking north. This requirement shall be certified by the individual who prepared the plan. The landscape plans shall show clusters of trees and the use of shrubs with trees that vary in height such that the appearance of a stockade ringing the project will be reduced. Landscaping around the playground areas shall transition into the surrounding grazing land. Parking areas shall include landscaped berms or other measures to ensure that parked cars are screened as much as possible from view of travelers along Highway One and other public view corridors. The overall landscape design shall evoke the sense of rolling rural area by limiting large trees to those required to screen the development, and by transitioning from the developed area to the surrounding grassland habitat using a mix of native shrubs and grasses.~~
 - ~~B. Landscape screening along Highway One to screen views of any development to northbound and southbound travelers, including additional screening (shrubs and ground cover) planted prior to any site disturbance along the frontage of Highway One between Highway One and Main Street to screen views from the scenic highway. Existing trees planted by the Land Conservancy and additional screening shall be shown on the landscape plan. The trees and screening materials shall blend in with the existing trees along Highway One, shall be consistent with community of Cambria tree selections and shall be compatible with the area's natural surroundings.~~
 - ~~C. The use of native plant species and non-invasive ornamental species that are drought-tolerant and/or have low irrigation requirements, are fire resistant and are tolerant to the use of recycled water. Development of the landscaping plan shall be coordinated with the Cambria Fire Department to avoid fire hazards.~~
 - ~~D. A detailed irrigation plan that provides an adequate and permanent source of water to maintain the landscaping. The irrigation plan shall provide maximum water conservation by using drip irrigation where feasible; designing the system to avoid runoff, overspray, low lead drainage, or other similar conditions where water flows onto adjacent property, non-irrigated areas, walks, roadways or structures; and utilizing collected runoff and reclaimed water in accordance with Standards 5f below. Subsurface irrigation of any play fields should be investigated as a method to reduce evaporation losses and allow for the use of fields during irrigation.~~

- ~~E. An aggressive tree planting and landscape plan using species endemic to the area, in coordination with the APCD to specifically address the use of deciduous and evergreen trees, planted so that they shade structures in summer, decrease indoor temperatures, and reduce energy demands for air conditioning and fossil fuel emission.~~
- ~~F. Compliance with the County of San Luis Obispo and the Cambria Community Services District requirements concerning the installation and use of reclaimed water systems for the landscape irrigation such as the installation of rain water cisterns to collect and re-use runoff. The cisterns and piping shall be appropriately sized to be used as reservoirs for reclaimed water from the purple pipe system after runoff water has been used. To avoid potential cross connection, the irrigation system must be separate from all potable service and have a separate meter. When a recycled water system is available and prior to use, the plumbing system shall be tested to ensure that there is no cross connections between irrigation and potable piping. Additional precautions include separate potable plumbing to drinking fountains in playing fields.~~
- ~~G. A grounds maintenance plan that minimizes the use of pesticides, herbicides, and fertilizers, and protects against adverse impacts associated with them. Pesticides and herbicides shall only be used if there is a documented problem and not on a regular preventative schedule, and shall not be applied if rain is expected. Non-chemical fertilizers are preferred. The least toxic alternatives, and the minimum necessary for the problem, shall be used in any case. The landscaping and grounds maintenance plan shall include nutrient control parameters.~~
- ~~H. As a condition to approval of any development, a qualified individual shall be retained to monitor the new landscaping for no less than five years, and to maintain the site free of weeds and invasive non-natives (such as acacia, pampas grass, and scotch broom), to ensure that it meets the goals of screening. A status report shall be submitted to the Department of Planning and Building annually. Any necessary remedial measures identified in the status reports shall be completed within 60 days of the completion of the report. All plantings must be maintained in good growing conditions throughout the life of the project, and wherever necessary, shall be replaced with new plant materials to ensure continued compliance with the plans.~~
- ~~6. Cultural/Historic Resources. In the event that cultural or historic material is discovered during construction activities, all construction in the affected area shall cease until the find is evaluated by a qualified archeologist/historian approved by the Department of Planning and Building and the requirements of Section 23.05.140 of the Coastal Zone Land Use Ordinance have been satisfied.~~
- ~~7. Grading, Drainage and Erosion Control Plans. Grading, drainage and erosion control plans meeting the requirements of Section 23.05.020 et. seq. of the Coastal Zone Land Use~~

Ordinance shall be submitted with any application for development. The plans shall, at a minimum, include the following:

- ~~A. A drainage and erosion control plan (including submittal of drainage calculations) and storm water pollution prevention plan (SWPPP) for review and approval by the County Department of Planning and Building in consultation with the Resource Conservation District, County Public Works Department, and the Regional Water Quality Control Board. Any dewatering system, drainage diversion or other temporary construction measures designed to reduce runoff and sedimentation from leaving the site shall be included in the submittal.~~
- ~~B. Appropriate erosion control measures during construction, including limiting construction activities to dry weather to avoid increased surface water runoff and erosion on-site. The plans shall specifically identify all nearby storm drain inlets and natural drainage swales, and shall protect them from construction related runoff and sediment with sand bag barriers, filter fabric screens, straw bale filters, block and gravel filters, drop-inlet sediment traps, etc. Appropriate erosion control devices (i.e. hay bales, silt fences or equivalent apparatus) shall be installed around the perimeter of each construction zone and all areas experiencing disturbances of the ground surface. All stockpiled materials and disturbed ground surfaces shall be covered on a daily basis. Monitoring of the erosion control devices shall be done on a daily basis by construction personnel, and periodically by the biological monitor, to ensure proper function.~~

~~The plans shall make it clear that: (a) dry cleanup methods are preferred whenever possible and that if wet cleanup is necessary, all runoff will be collected to settle out sediments prior to discharge from the site; all de-watering operations must require filtration mechanisms; (b) off-site equipment wash areas that provide containment and filtration of debris and wastewater are preferred whenever possible; if equipment must be washed on-site, the use of soaps, solvents, degreasers, or steam cleaning equipment should be allowed; in any event, wash water shall not be allowed to enter storm drains or any natural drainage; c) concrete rinsates shall be collected and shall not be allowed into storm drains or natural drainage areas; (d) good construction housekeeping shall be required (e.g., clean up all leaks, drips, and other spills immediately; refuel vehicles and heavy equipment off-site and/or in one designated location; keep materials covered and out of the rain (including covering exposed piles of soil and wastes); dispose of all wastes properly, place trash receptacles on site for that purpose, and cover open trash receptacles during wet weather); and (e) all erosion and sediment controls shall be in place prior to the commencement of grading and/or construction as well as at the end of each day.~~
- ~~C. Minimizing of grading to create cut and fill slopes that are not obtrusive to travel along Highway One.~~

- ~~D. Delineation of the vertical height of all cut and fill slopes with the border of cut slopes and fills rounded off to a minimum radius of five feet. All cut or fill areas shall vary in height to look natural (undulate) and in no case shall exceed a slope of 2:1.~~
- ~~E. Location of soil stockpiles in areas that do not have potential to experience significant runoff during the rainy season.~~
- ~~F. Revegetation of all disturbed and barren areas immediately following completion of construction-related activities with appropriate native vegetation to reduce the risk of erosion from the site. Areas experiencing temporary disturbance should be replanted with native species that are characteristic of habitats of the project area.~~
- ~~G. Provisions for a Certified Engineering Geologist to inspect the cut slopes at the completion of rough grading to ensure that no unforeseen conditions are present. If slope instabilities are present that pose a hazard to persons or structures, the project Engineering Geologist shall provide recommendations to eliminate the identified hazards.~~
- ~~H. A post-construction drainage plan designed to capture and filter typical site runoff to remove typical pollutants and avoid any adverse discharge to adjacent wetland areas. Runoff from all surfaces subject to vehicular traffic shall be filtered through an engineered filtration system specifically designed to remove vehicular contaminants (vegetative or other media filter devices effective at removing and/or mitigation contaminants such as petroleum hydrocarbons, heavy metals, and other particulate; or, engineered filtration systems specifically designed to remove vehicular contaminants). Filtered runoff shall be reused for landscape irrigation, or shall be discharged in a manner that maintains pre-construction drainage patterns, supports wetland restoration purposes and/or recharges groundwater basins, without causing erosion or sedimentation. All detention and filtration systems shall be designed to filter and/or treat the volume of runoff produced from each and every storm event up to and including the 85th percentile 24-hour runoff event, prior to its discharge to a storm water conveyance system. Post-development peak runoff rates and volumes shall be maintained at levels similar to pre-development conditions. Opportunities for directing runoff into pervious areas on-site for infiltration and/or percolation of rainfall through grassy swales or vegetative filter strips shall be maximized where geotechnical concerns would not otherwise prohibit such use. All outside storage areas and lading areas shall be graded and paved and either: (1) surrounded by a low containment berm; or (2) covered. All such areas shall be: (1) equipped with storm drain valves which can be closed in the case of a spill; or (2) equipped with a wash down outlet to the sanitary sewer. All restaurants and/or food services uses shall include a plumbed wash-down area (either inside or out) connected to the sanitary sewer.~~

~~I. Provisions for site maintenance, including a program for sweeping and/or vacuuming parking lot areas, driveways, and other vehicular traffic areas at regular intervals and at least once prior to October 15th of each year. Any oily spots shall be cleaned with appropriate absorbent materials. All debris, trash, and soiled absorbent materials shall be disposed of in a proper manner. If wet cleanup of any of these areas is absolutely necessary, all debris shall first be removed by sweeping and/or vacuuming, all storm drains inlets shall be sealed, and wash water pumped to a holding tank to be disposed of into the sanitary sewer system.~~

~~All drainage facilities shall be permanently operated and maintained. At a minimum:~~

~~1. All traps/separators and/or filters shall be inspected to determine if they need to be cleaned out or repaired at the following minimum frequencies: (1) prior to October 15th each year; (2) prior to April 15th each year; and (3) during each month that it rains between November 1st and April 1st. Clean-out and repairs (if necessary) shall be done as part of these inspections. At a minimum, all traps/separators and/or filters must be cleaned prior to the onset of the storm season, no later than October 15th of each year;~~

~~2. Debris and other water pollutants removed from drainage devices during clean-out shall be contained and disposed of in a proper manner; and~~

~~3. All inspection, maintenance and clean-out activities shall be documented in an annual report submitted to the Planning and Building Department no later than June 30th of each year.~~

~~8. Sewer/Water Facilities. Any development shall provide water supply and sewage disposal systems designed as follows:~~

~~A. Water efficient plumbing features, including all Cambria Community Services District standards for plumbing fixtures.~~

~~B. All water and sewer lines shall be designed and installed in accordance with the requirements of the County of San Luis Obispo and the Cambria Community Services District. The water and sewer line will be the minimum size necessary to accommodate the permitted use; they shall be designed and built without extra connection points (i.e., stub-outs) not necessary for the permitted use; and, that a permanent restriction against the extension of water and sewer service beyond site shall be recorded on the deed for the property. Fire flows must be considered when designing these pipelines, which shall be installed only in conjunction with actual construction of the development that they are to serve. Plans for water and sewer infrastructure shall identify the location and size of all water and wastewater pipelines, as well as, calculations indicating the amount of water needed and wastewater generated from the development, and the commensurate sizing of the utility lines.~~

- ~~C. In the event of a stage 1 or greater water supply condition, Cambria Community Services District-requested standby water conservation programs shall be implemented.~~
- ~~9. Wetland Protection Plan. The application for school development shall include measures to ensure that adjacent wetland habitats shall be protected. This shall included, but may not be limited to, the drainage, erosion control, and water quality protection measures required by standard 7.~~
- ~~A monitoring plan shall be approved by the County Department of Planning and Building and shall be implemented by the School District to ensure that adverse impacts to adjacent wetlands are effectively avoided. The monitoring plan shall include, goals, responsibilities, authorities, and procedures for verifying compliance with environmental mitigation; lines of communication and reporting methods; daily and weekly reporting of compliance; construction crew training regarding environmental sensitivities; authority to stop work; and action to be taken in the event of non-compliance.~~
- ~~10. Drainage basin and drainage swales. Any proposed drainage basins and/or drainage swales that convey runoff shall be designed to act as wetland habitat. Drainage basins shall be designed to have gently sloping sides to allow establishment of riparian and wetland vegetation along the banks, and the outlet shall be placed at a height that will retain some water in the basin after storm flows pass. Swales shall be revegetated with native wetland species appropriates to the area, such as juncus. Drainage swale and basin design shall take this into account when determining the size of the swales and basins, and by designing access routes for maintenance that will minimize disruption of wetland habitat.~~
- ~~11. Construction vehicle activities. Any development shall include an appropriately marked staging areas. Staging, re-fueling, and maintenance of vehicles will be performed only in those marked areas. Cleaning and refueling of equipment and vehicles will be avoided during rainy conditions in order to reduce the potential for inadvertent release of fuel or other contaminants from construction areas to aquatic habitats.~~
- ~~12. Traffic and Circulation. Roadway, pedestrian, bicycle and transit related improvements shall be required as part of the discretionary approval review process and shall be consistent with county standards and shall be reviewed and approved by the County Public Works Department. An encroachment permit for any approved alteration or work within the right-of-way is required. Realignment of Main Street may require obtaining an encroachment permit from Caltrans. Improvements shall be shown on any application submittal and shall include at a minimum:~~
- ~~A. Realignment of Main Street to accommodate road improvements. This may require moving the existing road to the west approximately 20 to 30 feet in order to improve Main Street alignment and accommodate the entrance driveway as to width and vertical profile.~~

- ~~— B. — A left-turn lane into the driveway~~
- ~~— C. — Eight foot shoulder on Main Street to accommodate right turns and bicycles.~~
- ~~— D. — Emergency access connection to the private road opposite Ardath Drive.~~
- ~~— E. — A safe access route along Main Street designated for students.~~
- ~~— F. — Any driveway entrances shall be designed to accommodate the turning radius for busses, delivery trucks, fire trucks and garbage trucks. The vertical profile of the driveway must accommodate busses and other vehicles with longer rear overhang such that these vehicles do not “bottom out” when entering or exiting the driveway.~~
- ~~— G. — Site enhancements to promote pedestrian, bicycle, and transit accessibility to the site’s design.~~
- ~~— H. — County of San Luis Obispo traffic fees established for Cambria to offset cumulative impacts to Highway 1 as required by County Code Title 13.01.010-060, and allocation fees as determined by the County Engineer, shall be paid prior to commencement of any construction.~~

~~13. — Air Quality. During construction, the following Best Available Control Technology for diesel fueled construction equipment and dust control measures shall be implemented where feasible. As a condition to development, the Department of Planning and Building shall be notified, by letter, of the status of the following measures, and shall clearly state why any measures not taken are infeasible.~~

~~— Diesel-fueled construction equipment~~

- ~~— A. — Use of CARB motor vehicle diesel fuel. All off-road and portable diesel power equipment shall be fueled exclusively with CARB certified diesel.~~
- ~~— B. — Electrify equipment where possible.~~
- ~~— C. — Maintain equipment in tune per manufacturer’s specifications, except as other wise required above.~~
- ~~— D. — Install catalytic converters on gasoline-powered equipment.~~
- ~~— E. — To the extent feasible use Compressed Natural Gas (CNG) or propane on site mobile equipment instead of diesel-powered equipment.~~

~~— Dust control~~

- ~~— A. — Reduce the amount of disturbed area where possible.~~

- ~~B. Use water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency will be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible.~~
- ~~C. All dirt stockpile areas should be sprayed daily as needed.~~
- ~~D. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following the completion of any soil disturbing activities.~~
- ~~E. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with fast-germinating native grass seeds and watered until vegetation is established.~~
- ~~F. All disturbed soil areas not subject to revegetation shall be stabilized with approved chemical soil binders, jute, netting, or other methods approved in advance by the APCD.~~
- ~~G. All roadways, driveways, sidewalks, etc., should be paved as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.~~
- ~~H. Vehicle speeds for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.~~

C. SAN SIMEON ACRES VILLAGE STANDARDS

The following standards apply to development within the San Simeon Acres Village Reserve Line to the land use categories or specific areas listed:



Figure 7-43: San Simeon Acres Village Location Map

COMBINING DESIGNATIONS: The following standards apply to lands in the Local Coastal Plan (LCP) combining designation.

- 1. Marine Habitat (SRA) - Projects with Point-Source Discharges.** The richness, sensitivity, and unspoiled character of the marine habitats in San Simeon Acres demand particularly rigorous measures to ensure the protection of these special resources. Accordingly, no surface point-source discharges into the marine environment are allowed, except as follows: (Mod68)

Exceptions:

- A. San Simeon Acres Community Services District.** Any capacity expansions needed to serve permitted growth within the existing San Simeon Acres Community Services District (SSCSD) service areas, provided that any new outfall is consistent with the Monterey Bay National Marine Sanctuary (MBNMS) provisions.

B. Passthrough Discharges. Aquaculture seawater passthrough discharges, provided that:

1. Discharge is consistent with MBNMS, EPA, RWQCB and California Department of Fish & Game (CDFG) Regulations.
2. The discharge point is located south of San Simeon Point.
3. The discharge method will not result in a eutrophic concentration of nutrients, and will not result in adverse impacts to wild abalone populations or other native marine organisms.

C. Seawater Passthrough Devices. Seawater passthrough discharges for public aquaria, and for scientific research facilities, provided that:

1. Discharge is consistent with MBNMS, EPA, and RWQCB Regulations.
2. The discharge method will not result in adverse impacts to kelp beds or other native marine organisms.

D. Water Quality Enhancement. Discharges to streams, for the purpose of hydrologic replenishment and/or stream water quality enhancement, provided that:

1. Discharge is consistent with NMFS, U.S. Fish & Wildlife Service (USFWS), EPA, RWQCB, and CDFG Regulations.
2. The discharged waters will be of appropriate temperature and quality so as not to disrupt the steelhead run, nor the in-stream habitat for any other sensitive species including, but not limited to, the red-legged frog and tidewater goby nor will impact adjacent agriculture. (Mod68)

COMMUNITYWIDE: The following standard applies to land within the San Simeon Village reserve line which are not limited to a single land use category.

1. Service Capacity. The San Simeon Acres Community Services District (SSCSD) shall reserve available water and sewage treatment capacity on a yearly basis for the following priority uses:

A. Visitor Serving Uses. 75 percent of available water and sewer capacity.

B. Affordable Housing - Program Required. Of the remaining 25 percent of capacity, the SSCSD shall reserve sufficient water and sewer capacity to serve affordable housing. If consistent with other permits, the amount of water and sewer capacity required to serve affordable housing units may be re-allocated from the Visitor-Serving category.

Prior to issuance of any further water allocation letters, the District shall propose to the County a program to accommodate a limited number for affordable housing units each year. The exact number shall be determined based on unmet housing needs and availability of water. Under this program and to meet the need for affordable housing units, the District may divert part of the water which otherwise would have been allocated to the Visitor-Serving water waiting list.

C. Water Use. Within three years of adoption of this Plan, an instream flow management plan for Pico Creek shall be completed by the SSCSD and approved by the County. The plan shall identify a specific amount of new development, withdrawals for which will not adversely affect riparian and wetland habitat or agricultural activities. If three years after the adoption of this Plan the study has not yet been approved by the County, no further development or land division which relies on water from Pico Creek shall be approved. (Mod 133)

~~1. **Priority for Services.** Eighty percent (80%) of the development in San Simeon Acres (including up to 75% of the water use of the community) shall be reserved for visitor-serving uses. When the community's water use reaches 140 acre feet a year, no new development or land division shall be approved until the following have been completed:~~

~~a. A study to determine the safe yield from the Pico Creek groundwater basin. And either:~~

~~Approval from the County Health Department and/or the State Water Resources Control Board to increase San Simeon Acres water allocation from the Pico Creek ground-water basin consistent with the findings of the safe-yield study. The increased water allocation shall not decrease the biological productivity of the Pico Creek stream environment or adjacent riparian habitat, or~~

~~Discovery, development and production of additional water resources from an alternative water supply source consistent with the resource protection policies of the Local Coastal Program.~~

~~2. **Underground Utilities.** Overhead utility lines are to be placed underground on with new development:~~

3 2. Setbacks From Bluff. The bluff setback is to be determined by an engineering geology report, prepared by a California-licensed certified Engineering Geologist, to be adequate to withstand bluff erosion and wave action for a period of 75 years and in any case shall not be

~~less than 25 feet. Setbacks from the bluff edge is to be a minimum of 25 feet unless a geologic report indicates that a greater setback is necessary.~~

4 3. Shoreline Access in New Visitor-Serving Developments. Visitor-serving development including ~~public assembly and entertainment (when accessory to a hotel or motel) eating and drinking places, food and beverage retail sales, general merchandise stores, service stations, bed and breakfast facilities and hotels and motels (as specified in the Limitation on Use Standards for Commercial Retail)~~ located between the first public road and the sea shoreline shall be required to provide the following public access improvements:

A. Access from the Road to the Beach. A vertical access easement for public pedestrian use open to the public.

B. Parking Areas. Areas available for public parking.

~~c. Lateral access dedications from the toe of the bluff to the mean high tide line.~~

4. Shoreline Access in Residential Development. One additional vertical access shall be required to serve the public and new residential development on Balboa Avenue. Lateral access dedications from the toe of the bluff to the mean high tide line shall be required for any new development on the bluff top west of Balboa Avenue.

5. Traffic Mitigation - Highway One. Proposed development shall be reviewed to identify any potential adverse impacts to coastal resources, including any potential impacts to levels of service on Highway One. Inadequate road capacity may be grounds for denial unless mitigation measures are incorporated to ensure that adequate levels of service can be provided. An increase in traffic that detracts from the rural, scenic nature of Highway One shall not be permitted. The acceptable level of service (LOS) for Highway One is LOS D within the San Simeon Acres Village Reserve Line.

6. Design Considerations. Measures to increase scenic quality from Highway One shall be encouraged through the design review process. Appropriate measures could include installation of sidewalks, street furniture, street trees, and decorative street lights. Figures 7-44 and 7-45 illustrate how these considerations may be implemented.

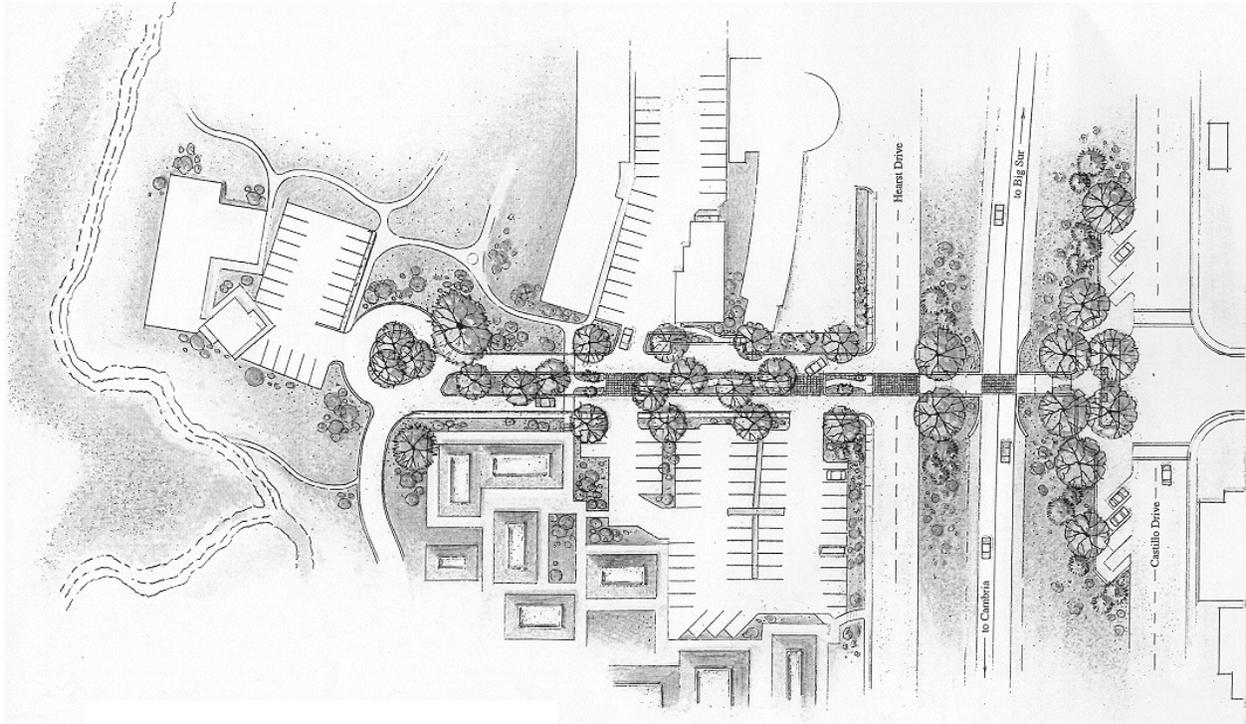


Figure 7-44: San Simeon Acres - Highway One Crossing Plan

- 7. Urban Design Compatibility with Natural Features.** New development shall respect the natural setting of its location. Building materials, exterior colors, and architectural features shall be carefully chosen to blend with the surrounding landscape. Structures and natural surroundings shall complement each other, and the impact of development on the site shall be minimized. Unique and attractive features of the landscape, including, but not limited to, unusual landforms, scenic vistas, sensitive habitats, and native vegetation shall be preserved and protected.
- 8. Building Scale.** New development shall be at a scale and size that reflects the existing features of the site and vicinity. Scale is the relationship of the size of the structure to its surrounding features, both natural and man-made. Development shall not be too massive for the lot size, shall not block light and air for smaller neighboring homes or structures, and shall not diminish the architectural harmony and character of the community.
- 9. Compatibility with Existing Structures.** All development shall be located and designed to minimize the impacts of noise, light, glare, privacy loss, and odors on adjacent areas. Traditional building styles of early coastal buildings shall be encouraged. Roof lines



Figure 7-45: San Simeon Acres Crossing Perspective

and building exteriors shall be compatible with buildings in the surrounding area.

10. Commercial Design.

- A. Design Compatibility.** Commercial development shall reflect the character of the area by incorporating distinctive design elements that are compatible with the surrounding community.
- B. Parking Location.** On-site parking shall be located at the rear of commercial structures where possible.
- C. Pedestrian Visibility.** Commercial structures shall be designed to maximize the visibility of shops and store fronts from the pedestrian eye level.

11. Commercial Signs. Signs shall be limited to reduce the potential for visual clutter and distraction. Existing signs that do not meet the following standards may be continued as set forth in Section 23.09.032 - Nonconforming Signs.

- A. New signs shall be monument style, low profile, and shall not compete visually with existing signs in the area.**
- B. Signs shall be subtle, unobtrusive, energy efficient, weather resistant, and reflect the existing community character. Signs shall be at a scale and size oriented toward pedestrian traffic in areas where pedestrians are present.**

12. Mixed Use Projects. To encourage employee housing in commercial areas, multi-family dwellings are permitted on the upper story or on the rear half of a lot developed with commercial development. (Mod 134). For purposes of this standard, the front half of the lot is defined as the area located between the front property line and a line drawn parallel to the street through the midpoint of lot depth.

- A. Density.** Density shall be limited to 26 units per acres, calculated using the area of the rear half of the lot. The maximum floor area and minimum open area requirements in CZLUO Chapter 23.04 shall not apply. Instead, the land use permits shall require an amount of private open space and common area for privacy, recreation, light, and air that is appropriate for the nature of the project.
- B. Visitor-serving Areas (V).** Multi-family housing consistent with other standards in this section may be allowed in 'V' areas, provided it is designed for employee housing, and that the County finds and determines that the proposed housing will not displace existing or potential future visitor-serving uses. (Mod 134)

COMMERCIAL RETAIL: The following standards apply only to lands within the Commercial Retail land use category.

1. **Limitation on Use.** Principally Permitted Uses are limited to: tourist-oriented uses including Eating and Drinking Facilities ~~places~~ (not including drive-in restaurants, fast-food and refreshment stands); Food and Beverage Retail Sales (limited to tourist-oriented supplies); General Merchandise Stores (limited to tourist-oriented shops); and Motels and Hotels. Non-principal permitted uses are limited to: Public Assembly and Entertainment (when accessory to a hotel and motel); Caretaker's Residence; Service Stations; Bed and Breakfast Facilities; Water Wells and Impoundments; and Coastal Accessways. (This is a visitor-serving priority area.)
2. **Height Limitation.** The maximum allowable height for all buildings ~~is commercial retail uses shall not exceed~~ 25 feet. Grading on bluff-top parcels shall be limited to that necessary for the construction of the building foundation and minor grading for parking lots. Grading shall not be allowed for the creation of an additional underground floor or parking spaces.
3. **Density - West of Highway 1.** The density for hotel and motel projects shall be calculated excluding the portions of the property which lie seaward ~~oceanward~~ of the edge ~~top~~ of the bluff.
6. ~~**Limitation on Use (On East Side of Avonne Ave.).** Principal permitted uses are limited to: hotel and motel. Non-principal permitted uses are limited to: residential multiple-family, and single-family residential. (This is a visitor-serving priority area.)~~

RESIDENTIAL MULTI-FAMILY: The following standards apply only to lands within the Residential Multi-Family land use category.

1. **Density - Multi-Family Projects.** The maximum allowable density for multi-family development shall ~~residential uses is to be~~ 26 dwelling units per acre. For the purpose of calculating density, the portions of the property seaward ~~lying oceanward of the top~~ of the edge of the bluff are ~~shall not to be included for the purpose of calculating density.~~
2. **Height Limitation.** The maximum allowable height for all structures shall be 25 feet from average natural grade except ~~that on land~~ those structures located west of Highway 1. For structures located west of Highway One, the maximum height shall be 20 feet from average natural grade except for bluff top lots, which shall be no higher than 14 feet above the centerline of Balboa Avenue.

CHAPTER 8: COASTAL ACCESS



A. Purpose and Organization

Protecting and maximizing public shoreline access is a fundamental goal of the Coastal Act. To facilitate achievement of this goal, Coastal Act section 30500 requires that all Local Coastal Programs (LCPs) include a specific public access component. This chapter provides a comprehensive reference to County goals, policies, standards, and ordinances pertinent to coastal access.

B. Public Access Goals

General Goal No. 17 for Cambria and San Simeon Acres, described in Chapter 1 and repeated below, outlines several objectives for providing public access to the shoreline, without overburdening natural resources or infringing on constitutionally protected private property rights. The goal envisions the creation of a coastal trail and regional bike path system enabling residents and visitors to enjoy these segments of the California coastline.

General Goal No. 17 (from Chapter 1):

Public Access to the Shoreline. Provide for public access, consistent with the need to protect natural resource areas from overuse, by:

- A.** Maximizing public access to and along the coast through the following:
 - 1.** Developing all feasible vertical and lateral pedestrian access easements to and along the shoreline, consistent with other public access goals of this plan;
 - 2.** Developing a Coastal Trail through the Communities;
 - 3.** Developing all other feasible pedestrian circulation systems in the coastal zone, consistent with other public access goals of this plan;
 - 4.** Providing a bike path system for the Planning Area;
 - 5.** Providing conspicuous signage for all public access easements;
- B.** Preventing interference with the public's right of access to the sea, whether acquired through use or legislative authorization, including, but not limited to, the use of dry sand and rocky coastal beaches to the first line of terrestrial vegetation.
- C.** Requiring new development between the nearest public roadway and the shoreline and along the coast to provide public access consistent with sound resource management and consistent with public safety, military security needs, and the protection of fragile coastal resources.
- D.** Carefully balancing the public's right of access to the sea with constitutionally protected private property rights. (Mod 6)

C. Overview of Existing and Potential Coastal Access

The North Coast segment of San Luis Obispo County provides a wide variety of public access opportunities, including one state beach, and a variety of formal and informal accessways to and along the coast. Existing and potential accessways are discussed below and generally shown on Figures 8-1 and 8-2. An inventory of offers-to-dedicate lateral and vertical access to the beach is included in Appendix A. San Luis Obispo County Parks, in coordination with the Department of Planning and Building, maintains this inventory. Please contact County Parks for the most current coastal access inventory. The exact location of a continuous coastal trail will evolve over time as potential segments are identified on public lands, and segments across private lands are voluntarily offered for dedication, sometimes in conjunction with development projects.

1. Cambria

Shamel County Park north to Leffingwell Landing

This contiguous sandy area is wide open for public use. This area is clearly visible from Moonstone Beach Drive, and there are numerous opportunities to view the beach and ocean. Parking, trails, and boardwalks are also available, as are restrooms at Santa Rosa Creek and Leffingwell Landing. There is also a boat launch at Leffingwell Landing. The adjacent inland uses are primarily visitor-serving motels. Overall, this area provides excellent access because the beach and coast are easily accessible for the whole length via Moonstone Drive, there is good visual access, and it is adjacent to many commercial visitor-serving uses (motels).

Shamel County Park

This County facility provides a developed lawn area with picnic tables, barbecues, restrooms, parking, and direct beach access. There is a large sandy beach contiguous with the State Park north from Shamel Park to Moonstone Beach and Leffingwell Landing.

Park Hill

Park Hill is primarily a residential area. The shoreline is generally accessible via cul-de-sac roads, but the coast is generally not visible from Highway One. The beaches here are small and seasonal, with little overall carrying capacity. Three southern street ends (Murray, Bryan, and DeVault) provide good visual overlooks, but no stairs. At the northern end of Park Hill a bluff top park containing pocket beaches has been improved with trails. This property is owned by State Parks, and extends from Worcester to just past Cambridge. A bluff top trail ends at the top of a 25-foot bluff, and access to Shamel Beach requires scrambling down the bluff face, raising concerns about safety and erosion. A stairway would be a useful improvement here.

East/West Ranch

Lateral access along the West Ranch is currently provided, with entrances at the south and north from the end of Windsor Boulevard. An established trail system provides access to upland areas of the ranch. There is also potential for linking Cambria's East Village to the coastline via the Cross Town Trail.

Lodge Hill

Because of the geographic configuration of this area, the many access options in Lodge Hill primarily serve neighborhood residents. Generally, the public is not drawn to this area, in part because the shoreline is not visible from Highway One (although access is signed on the Highway), and in part because access to the coast involves a circuitous route through local streets. The beaches here are small and seasonal, with very little capacity for public use. Of the six streets that end at or near the coast in Lodge Hill, three are currently developed with stairways. The three accessways are fairly well-spaced laterally, and one of the three, at Lampton County Park, has been improved with a parking lot, trails, a bike rack, and a stairway. The other stairways are located at Harvey and Wedgewood.

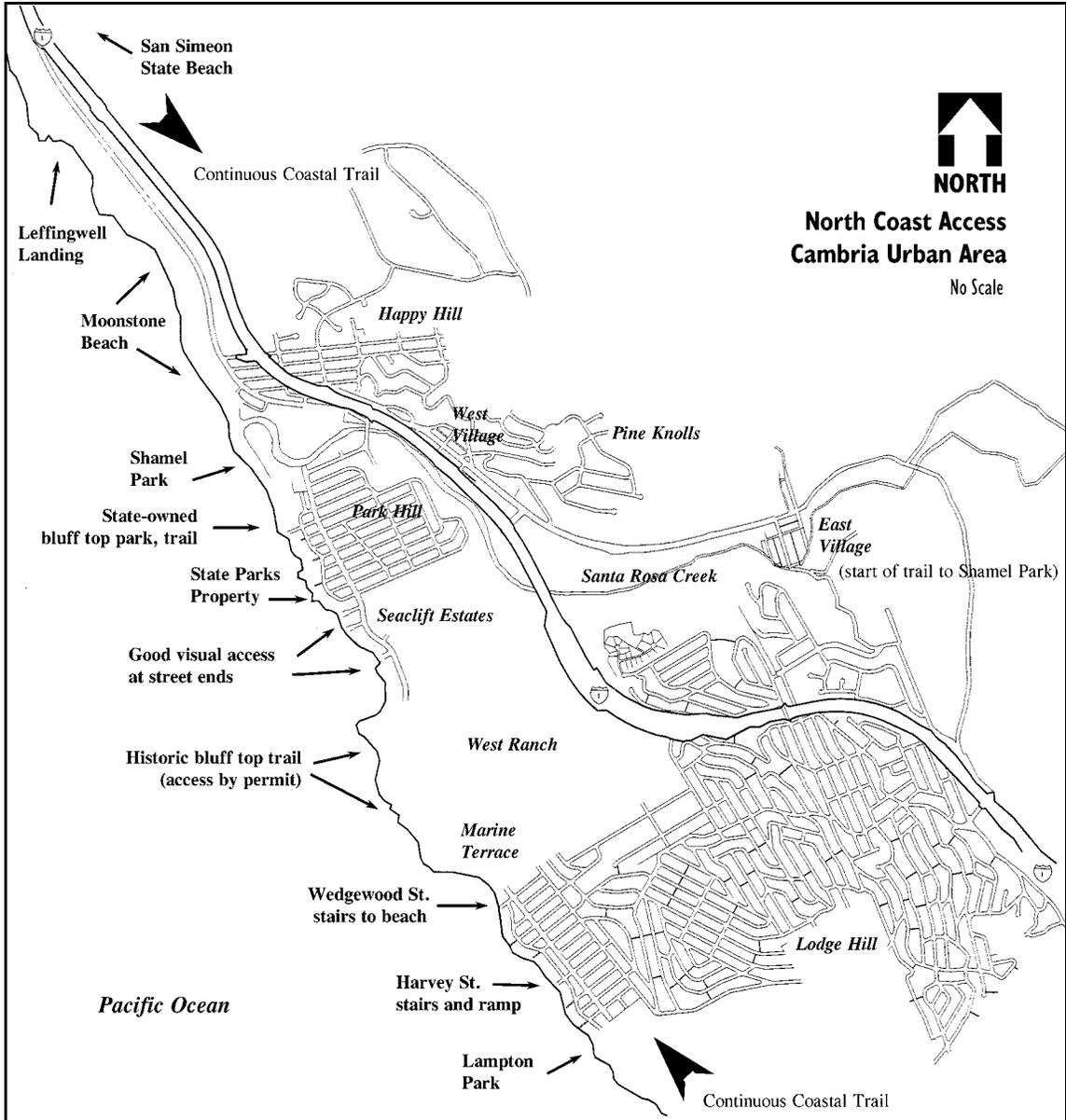


Figure 8-1 - Coastal Access - Cambria

2. **San Simeon Acres**

Access in San Simeon Acres is good – the highlight is a broad beach with excellent access. The main accessway to the beach is at the end of Pico Avenue, where parking for approximately ten cars and a stairway is located. There is a second accessway south of the Cavalier Inn that needs a conspicuous sign. There are also two vertical offers-to-dedicate at the southern end of the beach which should be analyzed for future development opportunities.

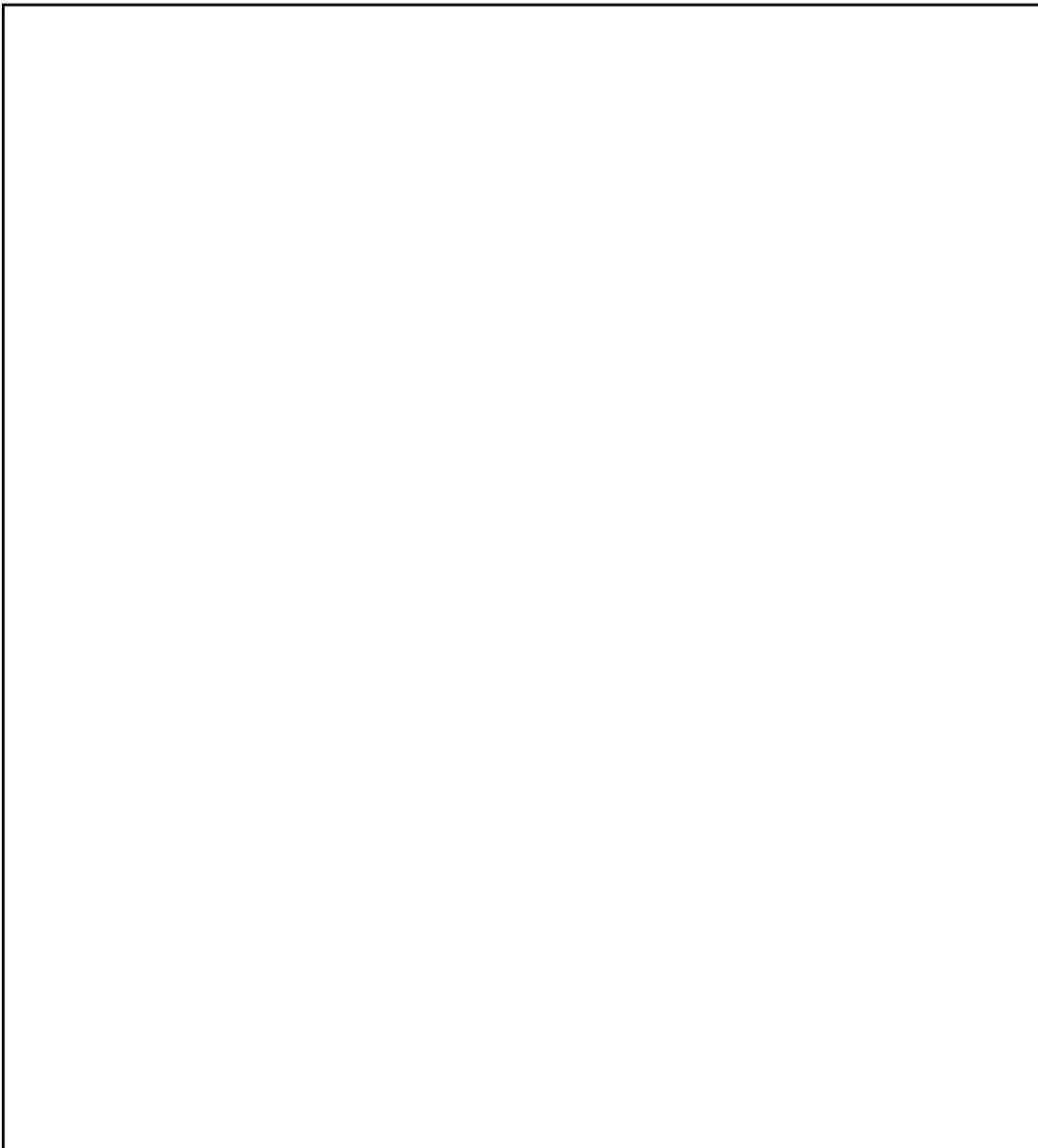


Figure 8-2 - Coastal Access -San Simeon Acres

D. Access Opportunities and Concepts.

With increasing demand for access to the coast, additional public access easements will be necessary. Enhancement of existing access corridors will also be necessary to serve new user groups, or to provide better locations for accessways.

This Plan outlines several specific County programs for opening and managing existing and future accessways, including a Coastal Access Implementation Plan described in Chapter 6: Combining Designations. The following table outlines some of the more significant additions and enhancements envisioned in the coastal access program for Cambria and San Simeon Acres.

Table 8-1
Coastal Access Enhancement Objectives

<u>Area</u>	<u>Improvement</u>	<u>Responsible Agency</u>
<u>Areawide</u>	<u>Urban Coastal Trail & Highway One Bikeway, 2 small scale camping or hostel facilities</u>	<u>Cal Trans, State Dept of Parks and Recreation, County</u>
<u>NW San Simeon Acres</u>	<u>Roadside park or overlook</u>	<u>County or San Simeon Acres Community Services District (SSACSD)</u>
<u>San Simeon Acres</u>	<u>Pedestrian network linking East & West sides, safer Highway crossing, and link to the beaches</u>	<u>County, SSACSD, or private land owners</u>
<u>Cambria: East Village to Shamel Park</u>	<u>Pedestrian and bicycle trail - "Crosstown Trail"</u>	<u>County, Cambria CSD, private land owners</u>
<u>Cambria: West Lodge Hill</u>	<u>Signs and walkways at end of Castle Street, Emmons Road, and Jean Street</u>	<u>County or other responsible agency</u>

1. Access Opportunity: The Coastal Trail & Bikeway.

A coastal trail and bikeway in Cambria and San Simeon Acres (as segments of the California Coastal Trail) could provide significant access opportunities not currently available to the public. As envisioned in this Plan, the coastal trail and bikeway would require support from the public as well as government agencies, in the form of landowner cooperation, significant funding, and more detailed planning. Agencies or organizations willing to accept offers-to-dedicate and manage segments of the trails must also be found.

The coastal trail and bikeway are related since they would share the same purpose and some of the same alignment, but would serve different users. Because the trail and bikeway would be so closely related, there is an opportunity to share management and improvement costs among agencies holding different segments in the public trust. Shared

improvements might include right-of-way improvements, signage, overnight facilities such as campgrounds or hostels, and management facilities. Shared ongoing maintenance activities, equipment, and security might also be a possibility.

The following section outlines in general terms a vision for the development of the coastal trail and bikeway. Prior to implementation, more detailed plans and environmental studies may be required.

A. The Coastal Trail

- 1. Goals of Coastal Trail Development.** The goal of the California Coastal Trail is to provide one continuous trail along the entire coast of the State of California. Regardless of whether or not the state-wide goal is met, however, segments in San Simeon Acres and Cambria could provide spectacular recreation opportunities not currently available to the public.

The County envisions integrating the coastal trail with existing trails, bikeways, parks, and existing and potential support facilities. In order to accomplish this, the coastal trail may include secondary trail loops.

- 2. The Users.** The coastal trail would primarily serve hikers, bikers, and equestrians. The use of secondary trails may be limited because of concerns about safety or compatibility with site-specific coastal resources. For example, segments of the coastal trail located close to wetlands or marine mammal habitat may be limited to hikers.
- 3. Precise Planning.** The coastal trail will require considerable cooperation and commitment from a variety of stakeholders. Once general support has been secured and potential funding sources have been identified, more detailed plans will need to be developed. The following sections outline some areas needing further study:
 - a. Environmental Constraints.** A detailed environmental constraints study should be for the area between Highway One and the ocean. The study should identify and locate all sensitive resources, including environmentally sensitive habitat areas, prime soils, archeological and cultural resources, visual resources, and areas of geological hazards, such as steep slopes.
 - b. Design of the Coastal Trail Alignment.** Using the results of the environmental constraints study, a trail alignment and cross section should be developed that integrates the **coastal** trail with existing facilities and other access easements, and that discusses feasible alternatives. The public should be involved as much as

possible in the design process. It is especially important that landowners, representatives of relevant government agencies, and other crucial stakeholders be involved in the process from the beginning.

c. Public Comment. Once the trail design and alternatives have been drafted, they should be presented to the general public for comments. The public comment period should determine the preferred alternative, and resolve any outstanding design issues. The outcome of this process should guide purchase programs.

d. Final Design and Purchase. A document describing the final design should be crafted, based on the preferred alternative. Upon completion of the final design, public and/or private agencies can be found to purchase the necessary land, and an agency can be approached about trail maintenance. Permitting can also go forward.

e. Construction and Maintenance. Final construction and formal opening of the coastal trail should follow the permitting phase. Maintenance of the coastal trail will be performed by the agency or organization which has accepted that responsibility.

4. Support Facilities. There are a number of facilities that will be necessary to support the coastal trail.

a. Overnight accommodations. Primary overnight facilities should be located in Cambria or San Simeon Acres.

b. Staging Areas. Several staging areas will be necessary to support vehicle parking, equestrian needs, and maintenance facilities.

5. Trail Location & Alignment. While some of the coastal trail may be located on the Highway One right-of-way, such alignment is not ideal. Secondary trails will have to be purchased and developed if access to beaches, bluff over-looks, and other recreational areas is to be provided. Trails should avoid dangerous bluffs, streams, wetlands, environmentally sensitive habitat areas, and agricultural lands.

6. **Trail Design.** To provide maximum opportunities for access, the Trail should be designed as a multi-use trail. However, in some circumstances restrictions on certain types of use might be appropriate due to topography, environmental constraints, or the special needs of certain user groups. On properties with conservation easements, with the consent of the easement holder, trails should follow existing ranch roads or be located in areas that have already been disturbed.
7. **Management and Maintenance.** Management and maintenance should be consolidated in one agency, such as the State Department of Parks and Recreation or County Parks. In some cases, trail use or overnight camping may be require a permit from the managing agency.

B. Highway One Bikeway. Portions of Highway One have already been improved as a Class II Bikeway. The vision of the coastal trail and bikeway described above maintains this alignment, but advocates upgrades to Class I Bikeways where feasible and adds improved secondary routes through scenic and special areas, such as Moonstone Beach and Cambria’s downtown and overnight facilities. Support facilities for the Bikeway could be consolidated with those serving the Coastal Trail.

1. **Bikeway Purpose and Goals.** The purpose of the Bikeway is to enhance existing opportunities for bicycle recreation in the Planning Area. Like the coastal trail, the Bikeway should eventually constitute one continuous bikeway along the North Coast, with several Class I secondary routes providing access to special recreation areas. The goal is to integrate the bikeway with existing trails, bike paths, parks, and existing and potential support facilities.
2. **The Users.** The bikeway would primarily serve bicycle riders. The use of secondary trails may be limited because of concerns about safety or compatibility with site-specific coastal resources.
3. **The Planning Process.** The bikeway, like the coastal trail, will require considerable cooperation and commitment from a variety of stakeholders. Once general support has been secured and potential funding sources have been identified, more detailed plans will need to be developed. The planning process should emphasize providing enhancements to existing routes, adding new routes to areas of interest, and combining support facilities with the coastal trail.

E. Management Objectives and Programs

Management of coastal access areas is complicated by the many different types of access facilities and the number of responsible agencies involved. Accessways include undeveloped beaches, bluff top trails, beach stairs, highway pull-outs and vista points, parks, and overlooks. In the Planning Area, some accessways are managed by private land owners. Others may be managed by public agencies such as: Cambria Community Services District, San Simeon Acres Community Services District, California Coastal Conservancy, CalTrans, County Department of General Services Parks Division, and the State Department of Parks and Recreation. Nonprofit organizations, such as the Land Conservancy of San Luis Obispo County, may also accept and manage coastal accessways.

The following management objectives are meant to support existing access and provide for new access opportunities in the Planning Area:

- 1. Existing Facilities.** Management organizations should continue to fund and support ongoing maintenance of existing facilities. Agencies should avoid losing land resources such as offers-of-dedication and other potential assets. Support facilities should be expanded to meet growing public demand and to meet the needs of new user groups.
- 2. Potential Access.** Management organizations should continue to look for ways to maximize coastal access by pursuing grants, donations, or other funding opportunities. Coordinating plans for existing facilities with plans for new facilities should provide a comprehensive and cost-effective approach. Agencies involved in development permit review should require access dedications according to adopted regulations.
- 3. Public Support.** The public should be involved as much as possible in providing input and resolving access issues. It is especially important that landowners, representatives of appropriate government agencies, and other crucial stakeholders be included from the beginning in relevant discussions.
- 4. Maintenance and Restoration.** Access easements must be maintained in order to provide for public safety and protection of sensitive coastal resources. Existing accessways that are significantly degraded because of overuse, or lack of funding to provide maintenance, should be rehabilitated, re-routed, or (if necessary) closed.
- 5. County of San Luis Obispo.** Where the County is the responsible agency, the Parks Manager, working in the Department of General Services, should continue to:
 - A.** Accept outstanding offers-to-dedicate vertical and lateral coastal access easements.
 - B.** Design and install access facilities such as parking lots, stairs, trash receptacles, restrooms, and signs.

- C.** Maintain existing facilities.
- D.** Manage the coastal access inventory.
- E.** Evaluate coastal development proposals for coastal access potential.

In addition, the County is responsible for reviewing new development for consistency with the coastal access requirements in the Local Coastal Program. New development may be required to provide additional access in accordance with the Local Coastal Program and the California Coastal Act.

F. Coastal Access Policies, Standards, and Ordinances

Because the entire Planning Area is within the coastal zone, land use and development within this area is subject to provisions of the County's Local Coastal Program, which are contained in four documents: *Land Use Element and Local Coastal Plan - Framework for Planning*; *Land Use Element and Local Coastal Plan - Coastal Plan Polices*; *North Coast Area Plan*; and *Coastal Zone Land Use Ordinance*. These documents work together to implement the Local Coastal Program. They are available from the San Luis Obispo County Department of Planning and Building. The basis for the Local Coastal Program and a brief description of each component are provided below. The following is included for reference purposes only as they are adopted separately and may be changed from time to time.

The California Coastal Act

Policies in the California Coastal Act of 1976 guide the conservation and development of California's 1,100 mile coastline with the goal of protecting California's coastal resources and providing for their wise use. The Act establishes the California Coastal Commission as a permanent State coastal management and regulatory agency and requires each of the state's coastal cities and counties to adopt a long-term management plan, known as a Local Coastal Program. Each Local Coastal Program consists of a land use plan, zoning ordinances and other implementing actions. In enacting the Coastal Act, the legislature declared that a basic goal of the state for the coastal zone is to:

Maximize public access to and along the coast and maximize recreational opportunities in the coastal zone consistent with sound resource conservation principles and constitutionally protected rights of private property owners. (Section 30001.5)

Chapter 3, Article 2 (Sections 30210 through 30214) of the Coastal Act contains six policy sections addressing public access and development between the first public road and the shoreline. The full text of applicable public access policies from the Coastal Act is included in Chapter 2 of the County's *Coastal Plan Policies* document.

Framework for Planning

Land Use Element and Local Coastal Program

This document serves as the Land Use and Circulation Element of the General Plan for the County's coastal zone. It provides a framework for County decisions on land use, development, and circulation. General Goal No. 12.c encourages better access to the coast through the acquisition and development of coastal accessways, trails, and parks, in appropriate locations. *Framework for Planning* identifies areas within the coastal zone where shoreline access is important. Coastal Access is a use to be encouraged in all land use categories.

Framework for Planning - Allowable Uses

Coastal Accessways are a principally permitted use in all land use categories in the Coastal Zone. Principally permitted uses are both allowable and encouraged.

Framework for Planning - Definitions

Framework for Planning provides the following definition of Coastal Accessways as a land use category:

Coastal Accessways [C3]

Land areas, pathways and improvements that may be used for access to the shoreline or other coastal resource such as a stream. They may include pathways, trails, overlooks and may be improved or unimproved. Typical improvements may include parking, lighting, structural improvements such as retaining walls, stairs, signs, picnic tables and restrooms.

North Coast Area Plan

Land Use Element and Local Coastal Program

An area plan contains both policies and implementation measures focused on a specific geographic area and/or community. The North Coast Area Plan describes County land use and circulation goals, policies, programs, and standards for the North Coast Planning Area. In the event that a policy or ordinance elsewhere in the Local Coastal Program conflicts with an area plan standard, the area plan standard shall prevail. This Community Plan is intended to replace and update those sections of the current North Coast Area Plan relating to the urban and village areas of Cambria and San Simeon Acres.

Coastal Plan Policies

Land Use Element and Local Coastal Plan

This document states the policy commitment of the County to implement the mandates of the Coastal Act. Coastal Plan Policies apply to all four coastal zone areas in San Luis Obispo County - the North Coast, Estero, San Luis Bay, and South County. Chapter 2 of the Coastal Plan Policies document outlines shoreline access policies and issues related to shoreline access for each of the four coastal planning areas in the county.

Coastal Zone Land Use Ordinance (CZLUO)

This document implements Land Use Element and Local Coastal Plan policies and contains procedures for review and evaluation of proposed land uses and land divisions, similar to a zoning ordinance. While the *Framework for Planning* identifies where specific uses may be established, the CZLUO determines how such uses may be developed by specifying performance criteria which proposed uses must satisfy in order to receive approval. Section 23.04.420 of the Coastal Zone Land Use Ordinance outlines requirements for protection and provision of coastal access. The ordinance specifies the type of required access, procedures for acquisition, and the

type and extent of required improvements.

APPENDIX



Sections

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A. <u>Coastal Access Inventory</u>	A-1

Other Background Reports

B. <u>Community Issues & Comments, August 1996</u>	Separate Report
C. <u>Revised Plan Alternatives, August 1996</u>	Separate Report
D. <u>Population Characteristics, March 1995</u>	Separate Report
E. <u>Land Use Survey & Buildout Estimates, November 1994</u>	Separate Report
F. <u>Fiscal Analysis, June 1996</u>	Separate Report
G. <u>Economic Analysis - RHA Associates, January 1997</u>	Separate Report
H. <u>Monterey Pine Forest - Pitch Canker Action Plan</u>	Separate Report
I. <u>Final Environmental Impact Report, NCAP, March 1996</u>	Separate Report
J. <u>Project Description, NCAP, June 2000</u>	Separate Report

APPENDIX A: COASTAL ACCESS INVENTORY

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A-1 Coastal Access Inventory	A-3

This appendix provides background information, but is not formally adopted as part of this plan. San Luis Obispo County Parks, in coordination with the Department of Planning and Building maintains this inventory. Please contact County Parks for the most current information.

A. Introduction

Cambria and San Simeon Acres offers a wide variety of public access opportunities, including highly accessible beaches and formal and informal accessways.

The following section contains a detailed inventory and maps of existing accessways and offers-to-dedicate lateral and vertical access to the coast. The inventory includes maps and photographs of individual sites. A data base table lists access sites by address, assessor's parcel number, a description of the access, and key dates for relevant legal documents offering and establishing access.

The coastal access inventory provides the baseline information necessary to master plan county wide coastal access and prioritize development of selected accessways. All of this information should be evaluated and updated periodically.

B. Coastal Access Inventory

Review Guide

Introduction.

The following is a guide for the reader for understanding the maps, photos and symbols. Each site is shown with a map, and if possible, a photo.

How to read the symbols.

This draft contains specific language and symbols that:

- **Maps** are from the County Assessors system, with notes added about a specific level of access. The APN is a specific parcel with a unique 6 digit number such as "013-181-004". This is a book - page - parcel number organization.

Areas where an access or other feature is indicated are shown as shaded. Such as (//////). In some cases, North arrows may not line up consistently.

- **Photos** are either taken from digitized aerials, or shot on site with a conventional camera. Areas where an access or other feature is indicated are shown either as a circle or an arrow. ()

Table A-1
Coastal Access Inventory

**Cambria and San Simeon Acres
Coastal Access Inventory**

Inventory Page 3

#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
San Simeon Acres									
17	013-031-28, 41	San Simeon Acres CSD	199-09	SSIM	Lateral Access at Balboa Ave.	Located on parcels used by SSACSD for sewage treatment facility, San Simeon Acres	Deed restriction	Coastal Commission Accepted	Mean high tide to toe of bluff
18	013-391-004	Cavalier Acres, Inc.	4-81-242	SSIM	Lateral Access	West end of Cavalier Inn expansion, San Simeon Acres	Offer to Dedicate	Accepted	Mean high tide to toe of bluff
19	013-402-001 thru 006	Midland Pacific Bldg Corp.	4-86-236	SSIM	Lateral Access	9229 Balboa Ave., San Simeon Acres	Offer to Dedicate	Accepted	
20	013-402-001 thru 003	Western Cal. Investments	4-82-380	SSIM	Lateral Access	9115 Balboa Ave., San Simeon Acres	Offer to Dedicate	Accepted	Mean high tide to toe of bluff
21	013-402-007 thru 011	Cohen, Meyer	4-82-566	SSIM	Lateral/Vertical Access	9051, 9053, 9055 Balboa Ave., San Simeon Acres	Offer to Dedicate vertical and lateral access.	Accepted	Vertical access is landscaped and fenced
22	013-403-008 thru 012	Sessa, Robert S & Carol A	418-28	SSIM	Lateral Access	9221, 9223, 9225, 9227 Balboa Ave., San Simeon Acres	Offer to Dedicate	Accepted	Mean high tide to toe of bluff
23	N/A	N/A	181-81	SSIM	Coastal Access Signs	HWY 1, approaching Pico Lane from No. and So., San Simeon Acres	Coastal Access Signs	Maintained by Cal Trans	
24	N/A	N/A	298-80	SSIM	Coastal Access Signs	Pico Lane access way, San Simeon Acres	Coastal Access Signs	Maintained by SLO County	
25	013-391-007	Cavalier Acres, Inc.	4-81-242	SSIM	Vertical Access	San Simeon at Cliff Dr., Cavalier Inn expansion site, San Simeon Acres	Deed Restriction, vertical access path with sign	Accepted	

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
26	013-403-024	Stinson/Alvarez	4-84-248	SSIM	Vertical Access	9231 Balboa Ave., San Simeon Acres	Offer to Dedicate 5' wide vertical access along side weekly/monthly rental complex	Not accepted	
27	013-403-013 thru 017	Sansone ET AL/ Miller	4-85-175	SSIM	Vertical/Lateral Access	9211, 9213, 9215, 9217 Balboa Ave., San Simeon Acres	Offer to Dedicate	Lateral accepted, Vertical not accepted	Vertical access is landscaped and open
28	013-031-047	**	4-81-242	SSIM	Lateral Access	250 San Simeon	Offer to Dedicate	Accepted	**Incomplete Data Adjacent to vertical access
29	013-051-017	**	D920040P	SSIM	Lateral Access	8125 Lone Palm Dr., San Simeon	Offer to Dedicate	Not accepted	**Incomplete Data
30	011-231-005	**	AL 91-137	SSIM	Lateral Access	Rho Piedra Blanca	Offer to Dedicate, 25' dry sandy beach	Not accepted	**Incomplete Data
31	N/A	**	304-80	SSIM	Stairway and viewing platform	West end of Pico Lane, San Simeon Acres	Stairway and viewing platform	SSACSD	**Incomplete Data

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
Cambria									
32	013-325-008	**	4-83-109	CAMB	Lateral Access	4655 Windsor Blvd., Cambria	Toe to seawall to mean high tide	Accepted	**Incomplete Data
33	022-222-024, 025	**	4-85-235	CAMB	Lateral Access	5275, 5279 Nottingham Dr., Cambria	Adjacent to State Park property, link to Shamel Park vertical access way	Accepted	**Incomplete Data. Mean high tide to toe of bluff
34	022-301-007	Sultze, Eugene	4-84-61	CAMB	Lateral Access	5015 Nottingham Dr., Cambria	Toe of seawall to mean high tide, 53' wide	Accepted	
35	022-162-010	**	D920038P	CAMB	Lateral Access	5411 Nottingham Dr., Cambria	Offer to Dedicate	Not accepted	**Incomplete Data
36	022-162-019	**	198-04	CAMB	Lateral Access	5451 Nottingham Dr., Cambria	Offer to Dedicate Westerly property line to toe of bluff	Accepted	**Incomplete Data
37	013-321-007	Edwards, Paul R	409-13	CAMB	Lateral Access	210 De Vault, Cambria	Offer to Dedicate	Accepted	
38	013-325-007	**	D950095P	CAMB	Lateral Access	4675 Windsor Blvd., Cambria	Offer to Dedicate	Not accepted	**Incomplete Data
39	013-325-006	**	D950050P	CAMB	Lateral Access	4699 Windsor Blvd., Cambria	Offer to Dedicate	Not accepted	**Incomplete Data
40	013-322-005	**	189-18	CAMB	Lateral Access	4855 Windsor Blvd., Cambria	Offer To Dedicate	Accepted	**Incomplete Data

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
41	013-325-002	LaBarbera, Barry & Jeann	4-82-218	CAMB	Lateral Access	4779 Windsor Blvd., Cambria	Offer to Dedicate	Accepted	
42	013-325-003	Halas, John V	405-01	CAMB	Lateral Access	4757 Windsor Blvd., Cambria	Offer to Dedicate	Accepted	
43	022-162-011	Ruffino, Steve & Sharon	4-84-264	CAMB	Lateral Access	5421 Nottingham Dr., Cambria	Offer to Dedicate	Accepted	
44	022-222-024,025	Bogardus	4-85-023	CAMB	Lateral Access	5251 Nottingham Dr., Cambria	Offer to Dedicate	Not accepted	
45	022-222-023	Lapins, Aivars & Gail	409-19	CAMB	Lateral Access	5225 Nottingham Dr., Cambria	Offer to Dedicate	Accepted	
46	023-011-007	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Portion of beach, Windsor Blvd. Between Fiscalini Ranch and Wedgewood.	Irregular ocean front parcel, lateral access across many parcels	Maintained by SLO County	
47	023-012-001	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Portion on beach, Sherwood Drive between Wedgewood and Castle	Ocean front parcel, lateral access across many parcels	Maintained by SLO County	
48	023-012-010	Dingman, Loretta	4-85-006	CAMB	Lateral Access	2689 Sherwood Drive, Cambria	Offer to Dedicate	Accepted	
49	023-041-020	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Along Sherwood between Castle and Emmons Rd.	Ocean front parcel, lateral access across many parcels	Maintained by SLO County	
50	023-041-021	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Portion of beach, Sherwood Drive, between Emmons Rd. and Harvey Street	Ocean front parcel, lateral access across many parcels	Maintained by SLO County	

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
51	023-041-036	Grossman, Helen	4-85-054	CAMB	Lateral Access	2311 Sherwood Drive, Cambria	Offer to Dedicate	Accepted	
52	023-041-042	Anderson, D	4-85-353	CAMB	Lateral Access	2155 Sherwood Drive, Cambria	Offer to Dedicate	Accepted	
53	023-082-020	Cambria Development Co.	Not thru permit	CAMB	Lampton Park	Sherwood Drive and Lampton Street, Cambria	Undeveloped County Park, approx. 1.79 acres	Maintained by SLO County	
54	023-081-011	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Portion of beach, Sherwood Drive between Harvey St. and Jean St.	Ocean front parcel, lateral access across many parcels	Maintained by SLO County	
55	023-082-011	Cambria Development Co.	Not thru permit	CAMB	Lateral Access	Portion of beach, Sherwood Drive between Jean St. and Lampton	Ocean front parcel, lateral access extends across many parcels	Maintained by SLO County	
56	023-082-019	N/A	Chapter 8 tax sale	CAMB	Vertical Access	Adjacent to Lampton St. Right of Way, seaward of Sherwood Drive	Delinquent property tax, acquired for right of way 11/80. Phone, electric, and septic utilities on site	Maintained by SLO County	
57	013-381-003	N/A	4-82-385	CAMB	Leffingwell Landing Creek, San Simeon State Beach	North end of Moonstone Beach Drive (lot 3 of parcel)	Developed day use area, informal lateral and vertical trails, picnic, bathrooms, parking	Maintained by State Parks	
58	022-101-001	Tyler, John	Not thru permit	CAMB	Vertical and Lateral access	Shamel County Park, Windsor Blvd at Nottingham Drive	2.02 acres, restroom and pool	Maintained by SLO County	
59	022-161-001	Olivarez, Ralph & Eloisa	Not thru permit	CAMB	Parking	Shamel Park	Parking for Shamel Park. Site: .13 acres, parcel is contiguous with Shamel Park	Maintained by SLO County	

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
60	022-162-001	Hricz, William John	Not thru permit	CAMB	Shamel Park, Ocean front	Shamel Park	.26 acres, acquired to expand Shamel Park	Maintained by SLO County	
61	022-162-014	N/A	Not thru permit	CAMB	San Simeon State Beach	Bluffs west of Nottingham Drive	Overlooks and bluff top trails, rocky beach	Maintained by State Parks	Also APN 22-221-2, 22-281-2, 22-301-1
62	022-361-068	N/A	Not thru permit	CAMB	Leffingwell Landing, San Simeon State Beach	Between San Simeon and Santa Rosa Creeks, along Moonstone Beach Drive	Bluff top lateral trails and overlooks, informal vertical(s) to sandy beach below. Developed stairs at southern end of Moonstone Beach Drive.	Maintained by State Parks	Also APN 22-371-12, 22-371-13, 22-381-10
63	N/A	N/A	298-80	CAMB	Coastal Access Sign	Moonstone Drive and HWY 1 at north end of Cambria	Coastal access sign for San Simeon State Beach	Maintained by SLO County	
64	N/A	N/A	XS-82-19	CAMB	Vertical Access	End of Wedgewood	Street end developed with access stairs, parking area	Maintained by SLO County	
65	N/A	N/A	Not thru permit	CAMB	Street End	End of Castle Street, Lodge Hill	Undeveloped street end	Maintained by SLO County	
66	N/A	N/A	Not thru permit	CAMB	Street End	End of Emmons, Lodge Hill	Undeveloped street end	Maintained by SLO County / Accepted	
67	N/A	N/A	XS-82-19	CAMB	Street End	End of Harvey, Lodge Hill	Street end developed with handicap access ramp, parking area, bicycle racks, stairs	Maintained by SLO County	
68	N/A	N/A	Not thru permit	CAMB	Street End	End of Jean Street, Lodge Hill	Undeveloped street end	Maintained by SLO County	

**Cambria and San Simeon Acres
Coastal Access Inventory**

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#	APN	CDP Applicant	Permit #	Area	Type of Access	Location	Description	Status	Note
69	N/A	N/A	Not thru permit	CAMB	Street End	End of Lampton Street, Lodge Hill	Undeveloped street end	Maintained by SLO County	
70	N/A	N/A	Not thru permit	CAMB	Vertical Access	Plymouth Street end, Park Hill	Undeveloped street end	Maintained by SLO County	
71	N/A	N/A	Not thru permit	CAMB	Vertical Access	Cambridge Street end, Park Hill	Undeveloped street end	Maintained by SLO County	
72	N/A	N/A	Not thru permit	CAMB	Vertical Access	Dorset Street end, Park Hill	Undeveloped street end	Maintained by SLO County	
73	N/A	N/A	Not thru permit	CAMB	Vertical Access	Leighton Street end, Park Hill	Undeveloped street end	Maintained by SLO County	
74	N/A	N/A	Not thru permit	CAMB	Vertical Access	De Vault Street end, Tract 159, Cambria	Undeveloped street end, informal vertical trail down steep bluff	Maintained by SLO County	
75	N/A	N/A	Not thru permit	CAMB	Vertical Access	Bryan Street end, Tract 159, Cambria	Undeveloped street end, vertical steep bluff	Maintained by SLO County	
76	N/A	N/A	Not thru permit	CAMB	Vertical Access	Murray Street end, Tract 159, Cambria	Undeveloped street end, very steep bluff	Maintained by SLO County	
77	Multiple	N/A	Not thru permit	CAMB	Vertical & Horizontal Access	West Ranch	Bluff & Interior trails	Maintained by CCSD	

Updated: July 2004

G:\Virtual Project Files\General Plan Amendments\2004-2005FY\LRP2004-00024 CAMBRIA_SAN SIMEON ACRES UPDATE\Draft Plans\PC Rec Draft\Revised Chapters\Appendix A-BOS.wpd

Coastal Access Maps

The maps of existing accessways and offers-to-dedicate lateral and vertical access to the coast are not available electronically. Please see a hard copy of the April 2006 Board of Supervisor's Approved Draft, available at the Department of Planning and Building, for maps and photographs of individual sites.

San Luis Obispo County Parks has also prepared an update to this inventory. Please contact County Parks for the most current information.

CAMBRIA COMMERCIAL DESIGN PLAN

~~MAY 9, 2002~~

DEPARTMENT OF PLANNING AND BUILDING
COUNTY OF SAN LUIS OBISPO
SAN LUIS OBISPO, CALIFORNIA 93408

ADOPTED MAY 9, 2002
AMENDED (INSERT DATE OF AMENDMENT)

County of San Luis Obispo

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Adopted

by the Board of Supervisors
August 22, 2000 - Resolution 2000-349

Certified

by the California Coastal Commission
May 9, 2002

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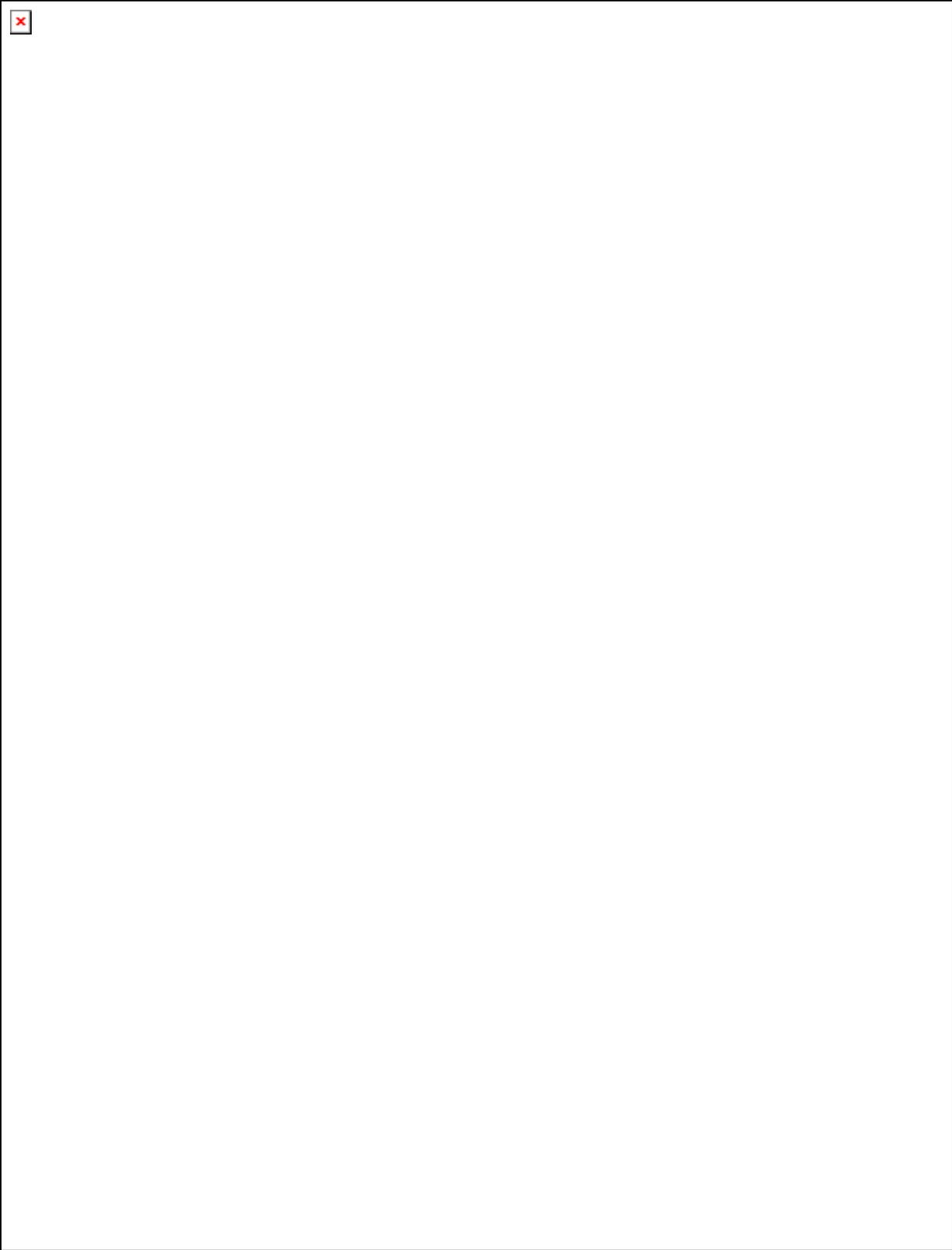
I. INTRODUCTION

This document is a Commercial Design Plan, prepared and adopted by the County of San Luis Obispo, for the community of Cambria. It gives guidance for new development in Cambria’s primary commercial districts, including the East Village, the West Village, Mid-Village and Moonstone Beach, shown on page 2.

This document includes the following nine chapters:

- Chapter 1 is this brief introduction.
- Chapter 2 explains the Design Plan’s vision and goals, and the key issues addressed by the Plan.
- Chapter 3 describes Cambria’s physical setting and the planning process that resulted in the preparation of this Plan.
- ~~———— Chapter 4 addresses area-wide design concerns.~~
- Chapter 5 addresses new development in the East Village.
- Chapter 6 addresses new development in the West Village.
- Chapter 7 addresses new development in Mid-Village.
- Chapter 8 addresses new development in Moonstone Beach.
- Chapter 9 describes actions that are needed to implement this Plan.

~~The planning process for this Plan is described in Appendix A. Other appendices describe the Monterey pines, pine pitch canker, policy and planning context, and historic district policies and incentives.~~



II. VISION, GOALS & KEY ISSUES

This chapter outlines the Design Plan’s vision and goals for Cambria, and the key issues addressed by the Plan.

The Cambria Design Plan is intended to maintain and enhance the architectural and natural identity of Cambria, while also allowing appropriate and innovative commercial development that reflects existing architectural patterns. The purpose of the Plan is to provide concrete suggestions that consider the three important themes in Cambria:

- Significant natural features.
- The people.
- Cambria’s unique variety of architecture.

A. The Vision For Cambria

Cambria desires to maintain its atmosphere as a rural village surrounded by a green frame of Monterey pine trees.

The charming and historic East and West Villages will continue to function as the commercial and cultural hubs for the area and attract visitors traveling along Highway One. In the East Village, shoppers will stroll along Burton Drive and Main Street, stopping to eat at one of many well-known restaurants. Weddings and picnics will occur on the grounds of the new Historical Park and Museum, while people will also visit the historic Chinese Temple and bike along the Cross Town Trail through Santa Rosa Creek Park.

In the West Village, people will eat at cafe tables in the new Arlington Courtyard and shop at the galleries and stores in the buildings along Main Street. At night, before the entertainment starts at the Pewter Plough Playhouse, patrons would gather for drinks and conversation in the Playhouse Courtyard. On summer nights, outdoor cafes and picturesque street lamps would dot the streetscape and enliven the Village. In the Civic District, residents would be able to visit a the newly expanded library, the Vet’s Hall, the Allied Arts Facility, and the Adult Recreation Center and the Farmer’s Market at the Pinedorado.

Mid-Village will remain the auto-oriented local-serving commercial district for the community, as well as the corridor that connects the East and West Villages. In this area, the clusters of pine trees along Main Street will be reinforced to connect the East and West Villages with a corridor of native greenery. The steep topography of the Mid-Village area will also be respected in new development.

Moonstone Beach will maintain its orientation towards the Pacific Ocean, with cafes and motels facing Moonstone Beach Drive and the ocean. People will jog and walk along Moonstone Beach Drive, stopping to watch the sun set behind Cambria Rock.

The entire community will be surrounded by preserved and enhanced stands of local Monterey pine trees, which create the visual frame that sets Cambria’s design tone. In some areas, this “green frame” will be reinforced with new pine trees lining the hillside adjacent to Main Street. More traditional street trees would create a slightly more formal “village” feeling along Main Street in the East and West Villages.

Cambria will continue much as it is today: a town with a strong and unique community base that is reflected in the commercial areas, in cultural events, and in a pleasant mix of historic and modern architecture.

B. Plan Goals

This section lists the goals for the Design Plan that were defined at the initiation of the planning process:

- To assist in the creation of new commercial development through the creation of design guidelines that blend new development with the built context. The design guidelines are to utilize the existing vocabulary of scale, siting, architectural patterns and building elements.
- To develop an open space and circulation scheme for downtown Cambria that creates a pedestrian-friendly environment and allows for safe and functional circulation and parking.
- To develop a streetscape design including street sections, sidewalk designs, street lighting, signage and street furniture for the downtown.
- To reinforce Cambria’s dramatic and natural setting of hills, ocean and pines.

More specific goals for each of the four districts covered in this Plan are contained in Chapters 5 through 8.

C. Key Issues and Opportunities

This Design Plan addresses the following key issues that will confront Cambria’s commercial areas as the town continues to grow:

Identity of the East and West Villages

The two villages are physically separated from each other by a large swath of auto-oriented uses and a small ridge; they also differ in character and origin. By distinguishing the identity of these villages further while enhancing their cultural and historical heritage, Cambria can become two destinations rather than one.

Natural Features

The rolling topography, pines, creek and views of the ocean are the main ingredients that make up the identity of Cambria. These features need to be consciously acknowledged and enhanced in elements of the Design Plan such as in the gateways, edges, creek frontage in the East Village and Mid-Village, and views of the ocean and Scott Rock.

Gateways

Gateways create the first impression of a place and provide a transition zone from the natural surroundings into the built environment. In Cambria, there are four important gateways: at the east and west ends of Main Street, at Cambria Drive, and along Burton Street entering the East Village. These gateways should be emphasized by utilizing important existing characteristics at each of these gateways to enhance them and create more distinct transitions from rural and natural surroundings into Cambria.

View Corridors

Views of the Pacific Ocean and Scott Rock in and around Cambria in particular need to be preserved as continued development occurs. For example, Moonstone Beach is currently quite visible from Highway One, and these views need to be preserved as additional seaside development occurs. Similarly, views from Main Street toward Scott Rock at the eastern gateway to Cambria must also be preserved.

Pedestrian Amenities

Pedestrian-oriented commercial uses abound in both the East and West Villages; however pedestrian elements such as seating, lighting, and public open space are lacking in each of these areas. Designs for sidewalks and streetscape elements will provide additional identity to each of the villages while creating a more pleasant pedestrian environment.

Public Open Space

Build on the existing activity areas and create focal points for the community by developing public open spaces in these areas of the villages.

Parking Facilities

Downtown Cambria is inundated with vehicles from tourists and residents alike. Because of the steep topography and the parcelization of the unorganized parking lots in the East Village, parked cars litter the landscape. A comprehensive parking scheme for the East Village could provide more adequate and attractive parking.

Commercial/Office Design Style

Cambria's distinct architectural identity is created by small-scale buildings whose diverse yet compatible styles range from the mid-1800's to today. In order to allow for innovative new development that reflects the existing architectural character without mimicking it, design guidelines should be developed that provide a vocabulary of patterns and materials while engendering creative architecture.

III. EXISTING CONDITIONS

This chapter describes existing conditions in Cambria relative to natural resources, urban design, historic resources, circulation and parking, and development policy. It identifies significant characteristics that create the charming, rural character that currently typifies Cambria.

A. Physical Setting

Regional Location

As illustrated in Figure 1, the unincorporated community of Cambria is located in northern San Luis Obispo County, next to the Pacific Ocean and along Highway One.

The community is nestled in the picturesque landscape of the rolling foothills of the Coast Ranges. As a result of its location, Cambria's climate is dominated by ocean weather with frostless winters and cool summers with frequent fog and wind.

Cambria is located north of the intersection of Highway One and Highway 46. It is one of a series of small unincorporated communities¹ that dot the coast of San Luis Obispo County and are separated from the majority of development in the County by the green buffer of the Coast Ranges. The community of Cayucos, which is Cambria's closest southern neighbor, is 7½ miles to the south, while San Simeon, home to Hearst Castle, is 5 miles to the north. Because of its location and size, Cambria serves as the southern gateway to the beautiful Highway One/Big Sur Coast. Cambria's most cosmopolitan neighbor, the City of San Luis Obispo, is more than thirty miles from the community of Cambria.

¹ *North Coast Area Plan Update: Population Characteristics, Background Report* of March 1995 estimates that the population of Cambria was 6,500 in 1995.



Layout of the Community

Cambria consists of approximately one square mile of developed land. It is composed of five separate commercial districts and nine residential neighborhoods, which are illustrated in Figure 2. This Design Plan addresses four of the community's five commercial districts.

The commercial districts are located in the interior Santa Rosa Valley and along Moonstone Beach. These districts are all located in the flatter areas of Cambria and adjacent to important circulation routes: Main Street (old Highway One) and Highway One. Cambria's downtown contains three of the five commercial districts: the historic East and West Villages and the intervening Mid-Village area. The East and West Villages act as the community hubs and are also popular with tourists. These two villages have traditional retail and office uses, while Mid-Village is primarily auto-oriented with a mix of commercial and institutional uses. All three of these areas provide the local residents with day-to-day necessities and other commercial needs.

Moonstone Beach, which is one of the primary tourist destinations in Cambria, lies in the narrow sliver between Highway One and the Pacific Ocean. It is the most northern district in the community and is laid out on the east side of Moonstone Beach Drive where restaurants and lodging, the primary uses in this area, take advantage of the ocean views.

The fifth major commercial district in Cambria, Tin City, is a primarily light industrial area composed of concrete tilt-up and steel industrial type structures nestled to the south of Santa Rosa Creek near the East Village. Tin City is visually obscured from the community by the riparian vegetation of the creek and the surrounding hillside and pine forest. Because it is both visually unobtrusive and more utilitarian than the other commercial districts in Cambria, Tin City is not addressed in this Design Plan.

These five commercial areas are surrounded by nine primary residential neighborhoods, which are situated on the surrounding hills and ocean bluffs of Cambria. Each of the nine neighborhoods has its own character defined by typical house and lot sizes, the slopes in the neighborhood, views, microclimate, and neighborhood age. Currently, single-family homes range in size from large executive homes on one-acre parcels to small cottages on 25-foot wide lots. In some neighborhoods, large homes are built on small lots and loom over neighboring residences and the commercial villages.

An additional 418 acres of undeveloped property, known as Fiscalini Ranch, occupies the center of the community and is a regionally significant property.

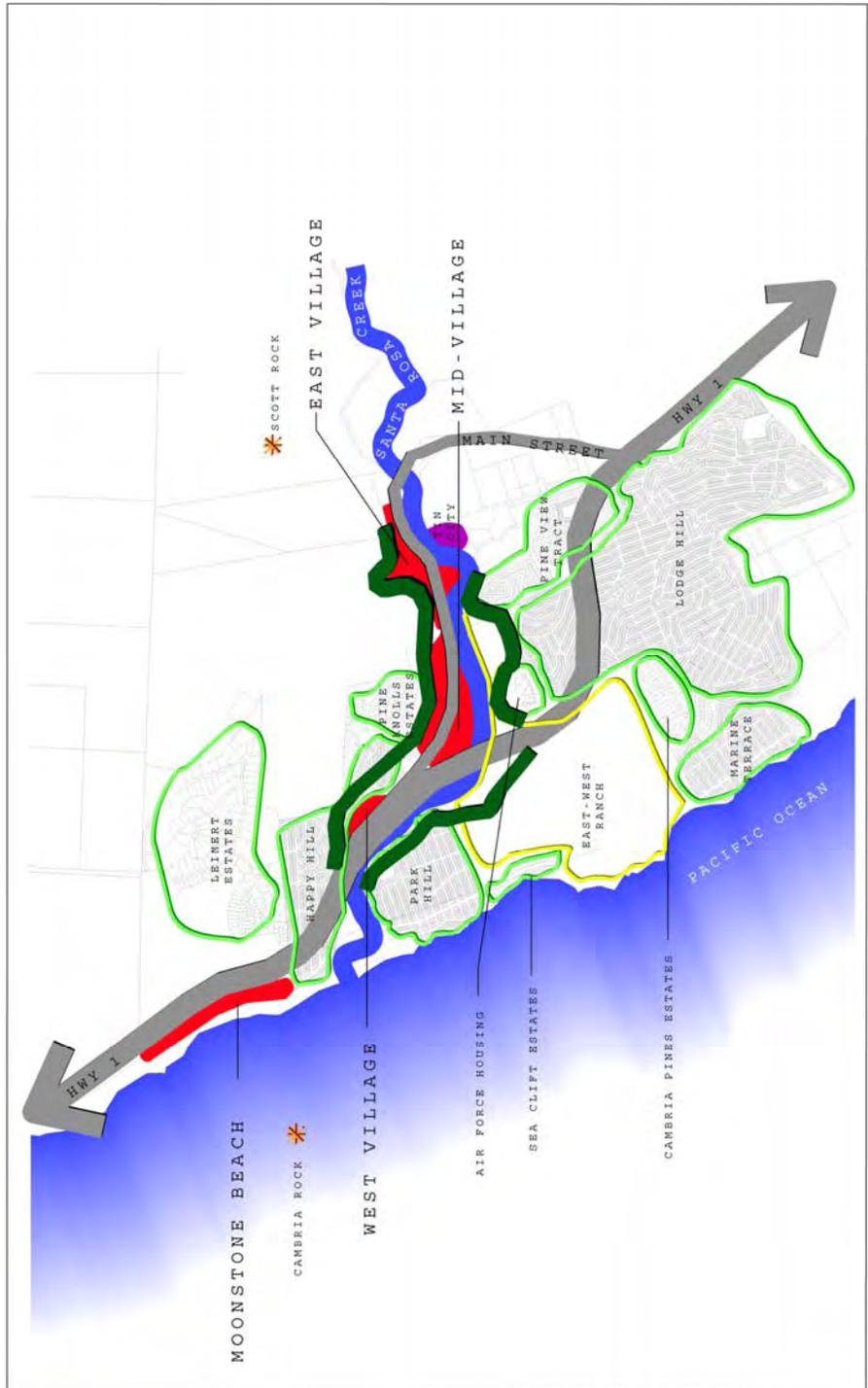


FIGURE 2
 CARICATURES & DISTRICTS
 OF CAMBRIA
 CAMBRIA DESIGN PLAN

- LEGEND
- COMMERCIAL DISTRICT
 - LIGHT INDUSTRIAL
 - RESIDENTIAL NEIGHBORHOOD
 - PROPOSED DEVELOPMENT
 - ★ LANDMARKS
 - ROADWAYS
 - WATERWAYS
 - MONTEREY PINE FRANE

B. Natural Resources

This section identifies and discusses important natural features that typify the community of Cambria and are important to its continued identity as a rural town set in the dramatic central California coastal landscape. Each of these features is important unto itself; however the combination of these features, such as the pines on the hills next to the ocean, are what make Cambria a unique place.

Monterey Pine Forest

Cambria is built in one of the state's three native Monterey pine forests, with nearly 2,300 acres in and around the community covered with Monterey pine trees. Much of the pine forest is in agricultural land surrounding the community and in a state park preserve.

The forest, which frames the villages and covers the neighborhoods on the hills, is an integral part of the community. The forest defines Cambria's visual character, provides shade and erosion control, and creates a green belt and green frame around the community.²

Today, the pine forest is threatened by two separate forces. First, Cambria is continuing to develop, resulting in the loss of some trees for home sites, views, and grading. Under the combining designation of the Monterey Pine Forest habitat in the North Coast Area Plan, the County provides guidance on siting of new development in relationship to the trees, guidelines for vegetation removal, and tree replacement thresholds. Under this designation, for each tree that is removed two must replace it. In addition to this combining designation, the County created a lot retirement program in the North Coast Area Plan also. The program allows for transfer of development rights in return for retiring lots from designated areas within Cambria. The designated areas are generally steep hillsides, which are heavily forested.

The other significant impact to the forest is a fungus known as the pine pitch canker, which has been killing the Monterey Pines throughout California and which threatens to destroy the Monterey Pine Forest in Cambria. According to the N.Y. Times, "scientists fear that 80 percent of the giant Monterey pines...could be gone in the next 30 years."³

Although many community members fear that Cambria will lose its entire pine forest in the next years, other evidence indicates that not all trees will be lost and the community can take steps to preserve the forest. Plant pathologists and horticulturists believe that 10% of the Monterey Pines are resistant to the canker without any outside assistance. In addition, a series of recent measures and academic research has offered hope for the forest's long-term survival:

- Use of expanded County Tree Removal and Replacement Rules
- Formation of the Cambria Forest Committee
- Implementation ~~Initiation~~ of the ~~Proposed~~ *Forest Management Plan*

² Topping, Ken, *Ken Topping's Notes on the Forest, Design Plan Steering Committee*, August 25, 1998.

³ "Lowly Fungus Poses Dire Threat to the Majestic Monterey Pines," *N.Y. Times*, June 10, 1998.

- Allocation of \$2.1 million by the State of California for Research on Pine Pitch Canker
- Development of a Pine Pitch Resistant Stock

~~Further information on these programs is provided for in the Implementation Chapter and background information related to Monterey pines and pine pitch canker are included in Appendix B of this report.~~

Topography

Another unique and notable feature of Cambria is the three-dimensional landscape of the surrounding hills, the Santa Rosa Creek Valley and the bluffs along the Pacific Ocean. This landscape creates a palette for development that does not encourage suburban sprawl and provides many nooks and crannies for special places with different character throughout the community. The two Villages lie in the flat areas of the Santa Rosa Creek Valley, while the neighborhoods encompass the villages on the surrounding hills and marine terraces. Each of the neighborhood locations has a different slope aspect, different topography and different amount of forest. Each of these characteristics creates a different ambience in the neighborhoods, creating what locals call different personalities for each of the neighborhoods.

Pacific Ocean

The Pacific Ocean is the western boundary of Cambria. Moonstone Beach, Park Hill, Seacliff Estates, Fiscalini Ranch and Marine Terrace are all adjacent to the ocean and are oriented towards it. Public access and views of the ocean exist along Moonstone Beach Drive, at San Simeon Beach, at Shamel Park and at many small public access points in the neighborhoods. A direct connection to the ocean defines much of Cambria.

Santa Rosa Creek

Santa Rosa Creek runs east to west through Cambria, beginning in the Lucia Range and ending in the Pacific Ocean. Much of the early development in Cambria was created on the flood terraces and flat areas around the creek. East Village was originally oriented towards Main and Bridge Streets, which connected the creek to the village. However, after the Great Fire and with the advent of the auto and Main Street, all development in the East Village, Mid-Village and West Village oriented itself towards the vehicular traffic on Main Street. The overall effect was a turning away from the creek.

In addition to turning its back on the creek, the development on the creek terraces within flood-prone areas has made an already problematic flood and runoff situation worse. Four recorded floods have covered the area of the West Village since the first settlers of Cambria began to develop this area. New development has covered permeable soils and has constricted the flow of the creek during heavy storm runoff periods. The largest contributor to flood conditions is Highway One, which acts as a dam and pushes water, in times of heavy storm run off, towards the West Village.

Cambria Rock

Cambria Rock, which sits in the Pacific Ocean just off shore near Weymouth Street in Moonstone Beach, is a natural landmark to the local residents. The view of this rock can be seen along Moonstone Beach Drive and at Moonstone Beach.

Scott Rock

Scott Rock, the other significant landmark rock in the community, is perched on top of the northeastern hill next to Cambria, just above the Coast Union High School. The rock is visible from the entrance into the East Village from Main Street near the intersection of Highway One.

C. Urban Design

This section describes current urban design patterns in Cambria in general and in the four commercial districts in particular. It includes analysis of streetscape conditions, landmarks, gateways and edges. Additional information on current existing streetscapes and gateway conditions for the villages and Moonstone Beach are described in each of their respective chapters.

Landmarks

Visually important landmarks that help people find their bearings in Cambria include Santa Rosa Creek and Highway One. Both the highway and the creek criss-cross the community, with the creek essentially running from east to west and Highway One running north and south.

Gateways

Gateways are the areas that mark the entry to a community or a district. They help to define community identity and physically demarcate the boundaries of a place. They create a visitor's first impression of a community. The community of Cambria has four major gateways that are defined by Highway One's intersections with the northern and southern ends of Main Street, Cambria Drive and Burton Drive.

It is important that the visual relationship and physical connections between Cambria and Highway One be addressed due to the physical relationship between Highway One and the community and because the flow of motorists off of Highway One have a significant impact on Cambria. This Design Plan seeks to enhance the identity of the community along this corridor by retaining views of the Pacific Ocean and enhancing or blocking views of the backs of commercial buildings. It also seeks to demarcate gateways and connections into the commercial districts designs that enhance the identity of the community.

Edges

The visual edges of the community are sculpted by the natural features, original subdivisions and zoning regulations. The most prominent edge is the Pacific Ocean, which defines Cambria's western edge.

The Urban Reserve Line (URL) is a policy boundary that determines the boundaries of urban development in unincorporated San Luis Obispo County. It separates urban and rural land uses and is based on the projected growth through 2016. In Cambria, most of the URL runs along parcel lines and the Highway One right-of-way; however, the URL and the Cambria Community Services District⁴ boundary has been set in relation to the original subdivisions. Nearly all of the development in the community occurs within the Cambria Community Services District boundary. Only very limited development is allowed by the County outside the Cambria Community Services District boundary.

⁴ The Cambria Community Services District provides water, wastewater, solid waste, recycling, fire, parks, street lighting, transit, and other services.

The northern and southern boundaries are less clear than the other edges of Cambria because topography in these areas does not constrict growth and allows new development to spread in these directions.

The edges of the commercial districts are clear in most areas due to the steep hillsides, forests, and creek backdrops. The older character of the downtown is also defined by the absence of large parking lots on the periphery. However, the edges between some of the commercial districts lack some clarity. Mid-Village, which developed after and between the East and West Villages, currently blurs into both of the villages to the east and west. This plan seeks to clarify the boundaries between Mid-Village and the adjacent East and West Villages.

Development Patterns

Development patterns in each of the four commercial districts vary according to the era of development, the history of ownership, the topography of the land and other development issues.

East Village

The development patterns in the East Village are the most varied of those of the four commercial districts addressed in this plan. In the East Village, Main Street generally runs parallel to Santa Rosa Creek and is one of two collector streets. The other collector, Burton Drive, runs perpendicular to and ends at Main Street, thus creating a “T”- pattern of collector streets. Burton Drive and Main Street handle the main vehicular and pedestrian movements within the Village, with pedestrian and vehicular circulation radiating off of these two streets onto the local streets. Both Burton Drive and Main Street are narrow, with two traffic lanes and two parallel parking lanes. These narrow rights-of-way slow traffic down and thus create a more pedestrian friendly atmosphere.

The lot and building patterns in this community reflect an era of development from the 1880's and early 1900's. Parcels have irregular sizes and shapes, with a preponderance of long, narrow lots in the commercial center and square lots in more predominantly residential areas. On Main Street in the Village Center, buildings are built with zero setbacks and lot coverage approaching 100 percent, while buildings in the other areas reflect residential development patterns with front, side and rear setbacks.

West Village

Originally platted as part of the 12,184-acre Santa Rosa Ranch in 1841, the West Village underwent a series of transformations at the mercy of various business transactions and the development of Highway One. In 1927, when the Cambria Pines Lodge was built, a large tract of land called Cambria Pines was subdivided with a business section that was called the Flats, now known as the West Village. The Flats, given its name because of its topographical setting, is tucked between Santa Rosa Creek and the hills to the north.

Main Street runs through the commercial center of the West Village and connects the village to the other villages and Highway One. The other roadways in the West Village respond to the importance of Main Street, the connection to Highway One and the constraints of the topography. Essentially, two loop roads exist on the north side of Main Street and between the surrounding hillsides; these are Kent and Cornwall Streets. Alleys run parallel to Main Street

behind the businesses that are on Main between the Cornwall Street loop, while Arlington Street bisects the Cornwall loop. Sheffield Street, which connects the West Village to the surrounding neighborhoods and Cambria Drive, which connects the West Village to Highway One, are the only other streets in the West Village.

Main Street in the West Village is wide, with a moderately wide sidewalk on its north side. The street's width provides ample space for diagonal parking but also encourages faster traffic speeds than are considered safe for pedestrian crossings. The other streets in the West Village generally have ample rights-of-way; however, the streets are developed with two narrow lanes, no parking and no formal pedestrian paths or sidewalks due to topographical constraints.

The platting in the West Village generally consists of long rectangular lots, oriented with the narrow dimension adjoining the street face. The lots are typically 25 feet wide by 75 feet deep.

Buildings in the West Village are typically built to the lot line or sidewalk. The buildings also are generally built with zero side setbacks and are generally two stories tall. Most of the buildings have retail uses on the ground floor, with a mix of retail, office and residential uses on the second floor.

Mid-Village

Mid-Village is a linear strip of businesses residences lining Main Street between the East and West Villages. The majority of parcels in this district are larger than parcels in the East and West Villages, because this portion of Cambria originally consisted of agricultural parcels which were subdivided and/or developed much later than the two Villages. The parcels range in size from approximately 14 acres down to .08 acres (50' x 75'). Most of the parcels line Main Street, however a few are located off of the street on the up-hill slope. Buildings in this area are generally built off of the street and are oriented towards parking lots. In most cases, parking lots are in front of the building and/or are visible from Main Street.

Moonstone Beach

Moonstone Beach is comprised of a linear, one-sided commercial strip that is at the top of a bluff along the Pacific Ocean. The strip, Moonstone Beach Drive, is dotted with motels, hotels, and restaurants that are located on the eastern side of Moonstone Beach Drive and are oriented for the views of the Pacific Ocean.

Lot sizes in Moonstone Beach are 100-foot by 200-foot commercial lots. Buildings are typically built with a significant front setback on Moonstone Beach Drive and the other streets. Because of the tremendous views of the Pacific Ocean many of the buildings are built at the maximum height limit and block views of the Pacific Ocean from Highway One.

D. Historic Resources

There are over forty historic structures in Cambria, all of which contribute to the overall character of the community. Many of these historic structures are located in the East Village, with a few the in Mid-Village and the West Village. These buildings are listed in Tables 1 and 2 and are shown in Figures 3 and 4. These structures were constructed between 1870 and 1940, and they reflect Victorian, craftsman, and standard commercial buildings from Cambria's past.

Most of the information available regarding these historic resources is found in informal studies conducted by the Chamber of Commerce and the Cambria Historical Society. Some additional information on specific properties is contained in the North Coast Area Plan. The California Environmental Quality Act (CEQA) uses a specific definition of historic resources that is different than the design plan. The structures on this list have not been analyzed in relation to the CEQA definition of historic resources. Such a determination should be made through the inventory recommended in the implementation chapter.

TABLE 1 EAST VILLAGE HISTORIC RESOURCES⁵

Current Name of Property (Known Historic Name and/or Original Owner)	Location	Year Built	Historic Combining Designation
Duncan House (Grant-Lull General Merchandise & Lull House)	2581 Main St.	1865	√
Olallieberry Inn (Manderscheid House)	2476 Main St.	1870's	√
Hilger House (Leffingwell House)	2420 Main St.	1880's*	√
The Cambrian	2442 Main St.	Unknown	
Old Santa Rosa Chapel (Old Santa Rosa Chapel)	2353 Main St.	1870	√
Cambria Community Church (First Presbyterian Church)	4313 Bridge St.	1874	√
The Tea Cozy (Thorndyke House)	4286 Bridge St.	1880	√
Lyons House (Lyons House)	4281 Bridge St.	1922	√
Simpson-Heller Building (Bank of Cambria)	2255 Main St.	1930	
Camozzi's Saloon	2262 Main St.	Unknown	√
Soto's Market	2244 Main St.	Unknown	√
Sound Studio (Bucket of Blood Saloon)	NE corner of Bridge & Center	1900's*	√
White House (Forrester House)	2276 Center St.	1868	√
Art Expressions (Maggetti House)	2261 Center St.	1890's*	√
Joss House (Chinese Temple)	2264 Center St.	1920 ⁶	√
Bianchini House (Guthrie House)	4120 Burton Dr.	1882	√
Carroll's Blacksmith Shop	4121 Burton Dr.	1900's ⁷	√
Heart's Ease (Proctor House)	4101 Burton Dr.	1870's*	√
Robin's Restaurant (Souza House)	4095 Burton Dr.	1920's*	√
Squibb House (Darke House)	4063 Burton Dr.	1877	√

⁵ Sources that provided information for this table include: *North Coast Area Plan, Public Hearing Draft, Tour Historic downtown Cambria* prepared by the Chamber of Commerce, *A Sunday Stroll Down Memory Lane*, prepared by the Cambria Historical Society, and telephone conversations with Mr. Wilford Lyons, representatives of the Old Santa Rosa Chapel, Camozzi's Saloon, Soto's Market, Coldwell Banker, Robin's Restaurant, Squibb House and Bramble's Restaurant.

⁶ House was joined together in 1920. Front portion was built prior to 1890. Right portion was built in 1899.

⁷ This building is in the process of being torn down and is being replaced with an exact replica.

TABLE 1 EAST VILLAGE HISTORIC RESOURCES - CONTINUED

Current Name of Property (Known Historic Name and/or Original Owner)	Location	Year Built	Historic Combining Designation
The Shop Next Door (Paterson Carpentry Shop)	4063 Burton Dr.	1889	√
Bramble's Restaurant (Mora House)	4005 Burton Dr.	1880's*	√
Burton Drive Inn (Rigdon Residence)	4036 Burton Dr.	N/A ⁸	√
Cambria Bike Outfitters ⁹ (Dickie House)	2150 Center St.	Unknown	√
Bluebird Motel (Lull House)	1880 Main St.	1880	√

*Exact date is unknown.

⁸ This is the site of the Rigdon Residence. The original Rigdon Residence is no longer on the site. Rufus Rigdon, one of the town's settlers occupied the original house on this site. His son, Elmer Rigdon, became a California State Senator and in 1917 won approval for the construction of the coast highway between San Simeon and Monterey.

⁹ This house was moved from its original location at the southeast corner of Burton Drive and Center Street. In addition the original house was incorporated into a new structure.

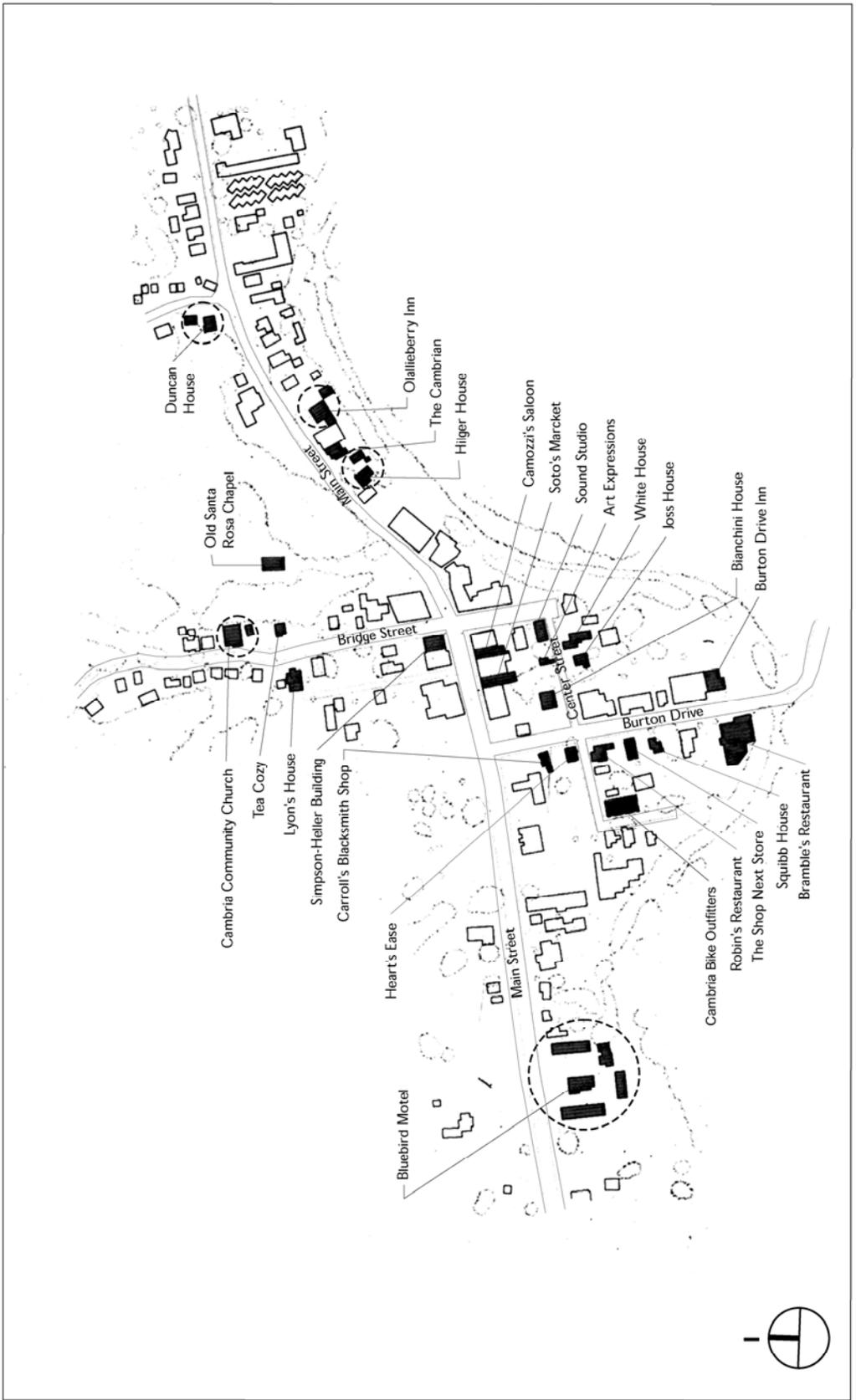


FIGURE 3

TABLE 2 CAMBRIA HISTORIC RESOURCES OUTSIDE THE EAST VILLAGE¹⁰

Current Name of Property (Known Historic Name and/or Original Owner)	Location	Year Built	Historic Combining Designation
Mid-Village			
Cambria Grammar School	1350 Main St.	1940	
West Village			
Veteran’s Memorial Building (Veteran’s’ Memorial Building)	1000 Main St.	1949	
Piedras Blancas Lighthouse Lens ¹¹	1000 Main St.	1874	
Schoolhouse Gallery (Santa Rosa Schoolhouse)	880 Main St.	1881 ¹²	
Texaco (Cambria General Store)	850 Main St.	1930’s	
Lion’s Club (Cambria Jail)	850 Main St.	Unknown	
Pewter Plough Playhouse (Southern Pacific Milling Co.)	824 Main St.	1939 ¹³	
Cambria Country Store (The Pines Bakery)	784 Main St.	1932	
Hillcrest Inn (Sunset View Motor Lodge)	800 Hillcrest Dr.	1930’s*	
New Moon/Exotic Nature (The Cambrian)	783 Main St.	1932*	
Soldier Factory (R.A. Creath’s Electric Supply)	789 Main St.	1920’s*	
Cambria Chamber of Commerce (Cambria Chamber of Commerce)	767 Main St.	1932	
Caren’s Corner (Benson Hotel)	755 Main St.	1920’s*	
Home Arts (O’Neils Coffee Shop)	723 & 719 Main St.	1937	
Old Stone Station (Van Scoy’s Red Lion Gas Station)	715-713 Main St.	1931	
Masonic Hall	614 Main St.	Unknown	
Happy Hill Residential Neighborhood			
Nitwit Ridge (Art Beale House)	881 Hillcrest Dr.	Unknown	√

¹⁰ Sources that provided information for this table include: *North Coast Area Plan, Public Hearing Draft, Tour Historic Downtown Cambria* prepared by the Camber of Commerce, and *A Sunday Stroll Down Memory Lane*, prepared by the Cambria Historical Society.

¹¹ This lens was removed from the Piedras Blancas Lighthouse in the 1940’s by the US Coast Guard. It was saved and installed by Cambrian citizens at its current location.

¹² This structure has been moved from it’s original location next to Santa Rosa Creek.

¹³ Structure rebuilt in 1951 and is part of the current Pewter Playhouse building.

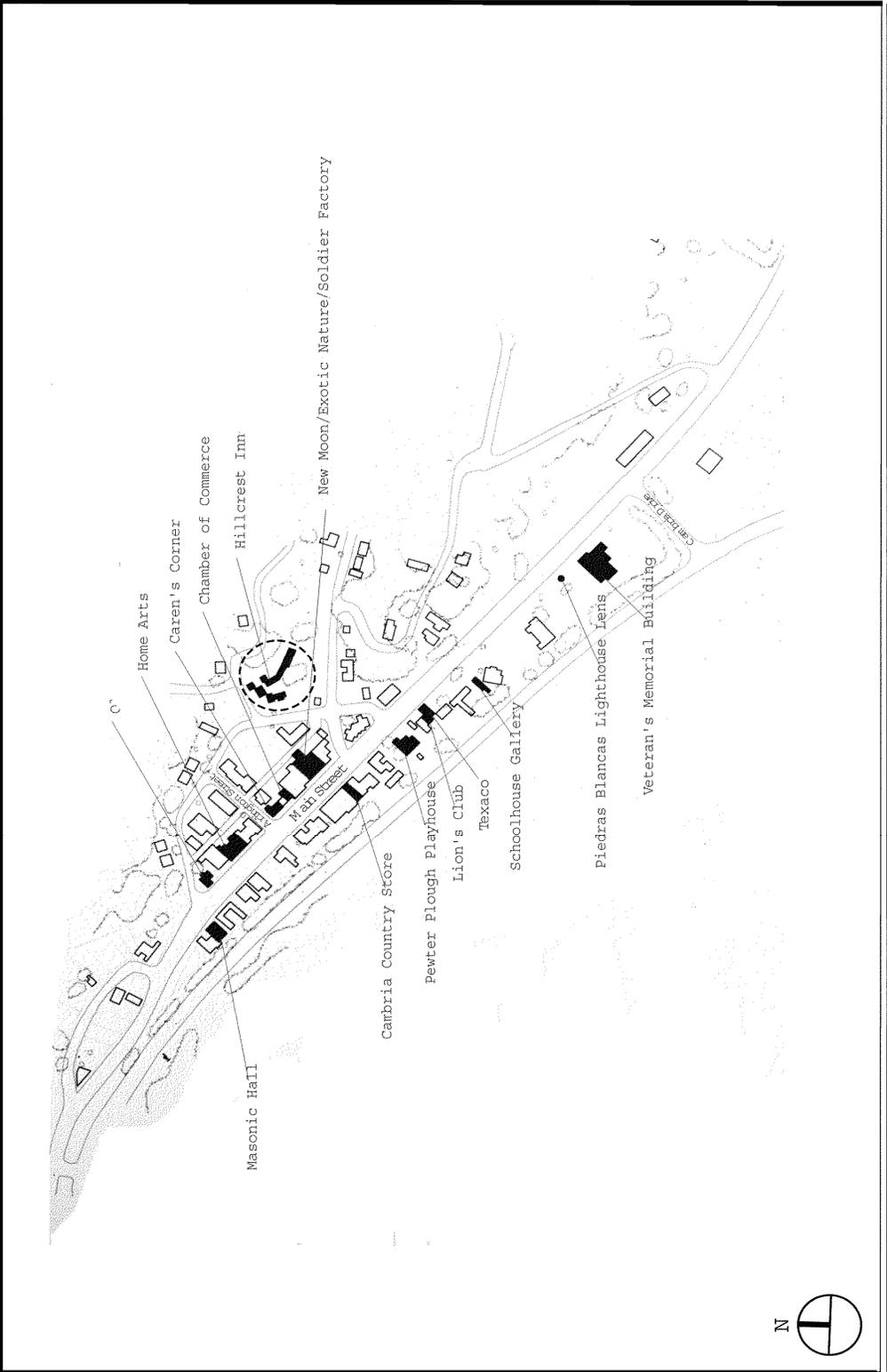


FIGURE 4

WEST VILLAGE HISTORIC RESOURCES

CAMBRIA DESIGN PLAN

E. Circulation and Parking

This section describes the existing parking and vehicular, bicycle and pedestrian circulation conditions in Cambria. Specific information on parking in the East and West Villages is provided for in their respective chapters.

Circulation

The information below is based on the North Coast Circulation Study written for the County in 1992. Field observations were executed in the East and West Villages at the beginning of the planning process in order to supplement and update the information provided in the Circulation Study.

Vehicular Circulation

Local vehicular circulation in Cambria is concentrated on Highway One, Main Street, Burton Drive, Windsor Boulevard, and Moonstone Beach. According to the North Coast Circulation Study, Highway One accommodates a medium volume of through circulation and quick stop offs in the West or East Villages, with approximately 6,400 daily trips and an Level of Service of E¹⁴ on summer weekends. Most vehicular traffic utilizes the primary collector of Main Street to travel east or west, with Burton Drive, Windsor Boulevard, Ardath Drive, Pine Knolls Drive, Charring Lane, Weymouth Street and Buckley Drive serving as the other collectors.

The majority of vehicular traffic is concentrated in the East and West Villages, with Mid-Village receiving a high volume of through traffic. According to the North Coast Circulation Study, traffic is highest in the East and West Village centers, with 10,000 vehicle trips a day and an LOS of D on summer weekends. The study states that traffic levels increased steadily between 1970 and 1990, and it is reasonable to believe that they have increased since 1992 as well.

Pedestrian Circulation

The Central District of the West Village and the Village Center of the East Village are the two primary high pedestrian traffic areas in Cambria. Sidewalks in these areas facilitate walking, while gravel shoulders in the Mid-Village and Moonstone Beach discourage pedestrian circulation.

Bicycle Circulation

According to the North Coast Circulation Study, there is not a great concentration of bicycle traffic within the community of Cambria. There are a large number of seniors and retirees in the population, and the hilly terrain makes the area difficult for bicycle travel. However, some local residents have shown an interest in improving Cambria for bicycling, particularly in the flatter areas along Santa Rosa Creek.

¹⁴ Level of Service is a scale traffic engineers utilize to measure the traffic flow of a street or intersection. The levels range from a grade of A, which is the highest and best Level of Service, to a grade of F, which is the lowest and worst Level of Service.

The Cambria Community Services District has been working to plan and construct a 1.75 mile Cross-Town Trail connecting the Park Hill neighborhood with Burton Drive in the East Village. As illustrated in Figure 5, the trail is proposed to start at Shamel Park at the west end of town. Between Shamel Park and the Windsor Bridge the trail would consist of two striped bikeways and a pedestrian trail on the north side of Windsor Boulevard. The trail would cross the bridge and follow the edge of Santa Rosa Creek, where the trail would consist of an 8-foot wide multi-use trail with 2-foot wide shoulders. At the intersection of Cambria Drive and Highway One the trail would leave the creek area and cross Highway One. The trail would continue along Cambria Drive up to Main Street where the trail would be integrated into the Cambria Main Street Enhancement Plan. On Main Street the trail would meander along the downslope side of Main Street with an average width of 8 feet. At Bluebird Lane, which is a 20-foot right-of-way and paper street, the trail would angle off Main Street and onto a pedestrian bridge that would connect the trail with Rodeo Grounds Road. The trail would follow the alignment of Rodeo Grounds Road past Burton Drive and through Tin City. In Tin City the trail would cross over the pedestrian bridge to the East Village Historical Park, which is described later in the East Village Chapter. The trail would continue east of the bridge however the future alignment east of the bridge is yet to be determined. Another trail along Santa Rosa Creek could extend from Cambria Drive to the creek and along it to the pedestrian bridge at Bluebird Lane.

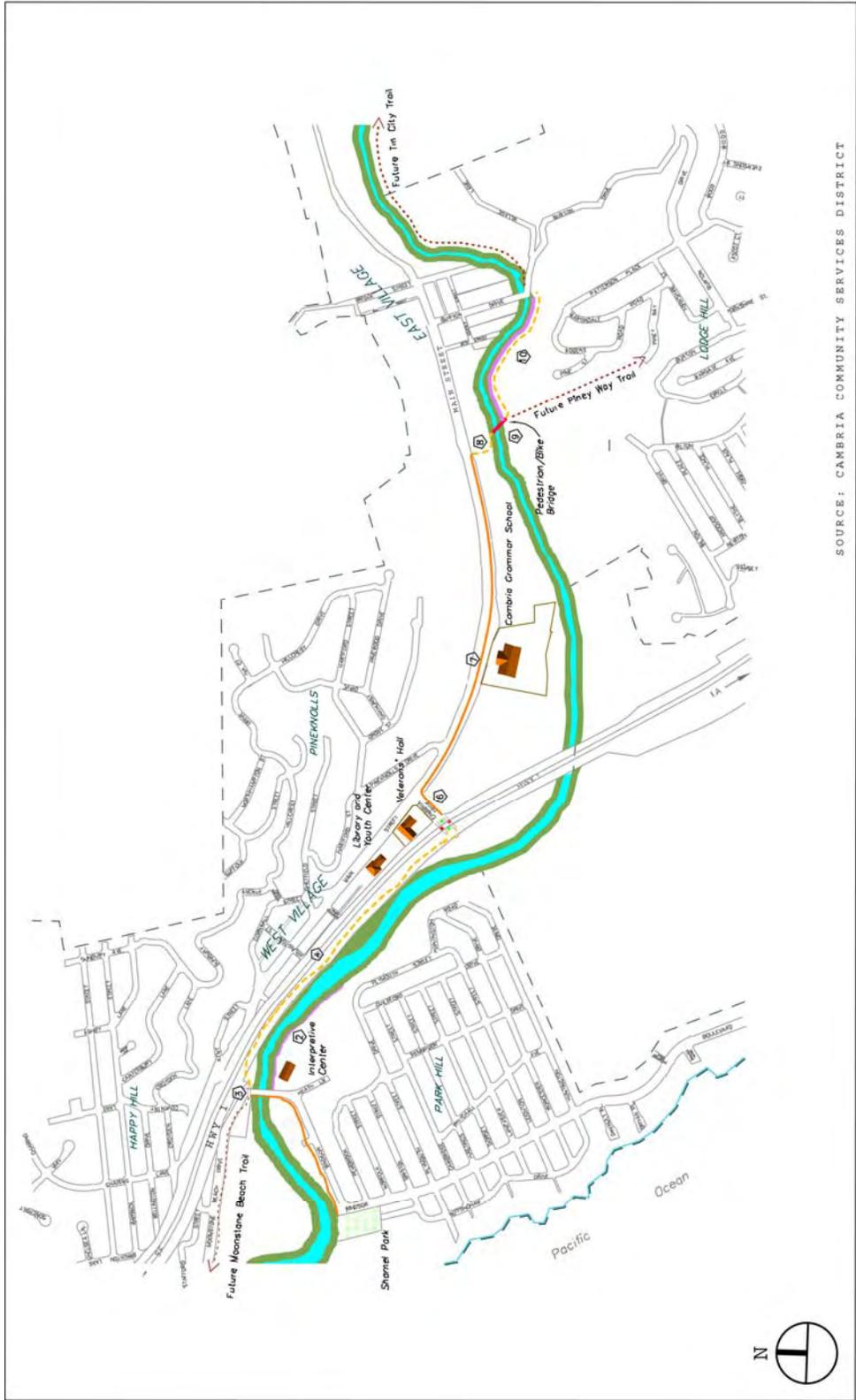


FIGURE 5
 CAMBRIA CROSS TOWN TRAIL
 CAMBRIA DESIGN PLAN

Parking

The existing parking supply in the East and West Villages is generally lower than the current demand. On summer weekends, visitors fill both legal and informal on-street parking spaces in the East Village and the West Village, and they often also utilize the off-street parking lots, which typically fill only after on-street spaces are all occupied. On summer weekends, the western gateway to the East Village is often littered with cars and RV's parked between the hillside and the roadside. The lack of parking spaces in the Villages creates vehicular congestion in the village centers as people drive around searching for parking. More specific information on parking in the East and West Villages is provided in their respective chapters.

F. Policy and Planning Context

The policy and planning context in San Luis Obispo County and Cambria is complex due to the number of agencies with jurisdiction and their concerns. The Cambria Design Plan works in conjunction with several County planning documents, including the County General Plan, the North Coast Area Plan, the County-wide Design Guidelines Manual, the County's Trails Plan and Bicycle Plan and with the Cambria Community Services District's Parks, Recreation and Open Space Master Plan. An overview of these policy documents and their relationship to the Design Plan is provided in Appendix C: Policy & Planning Context.

IV. AREA-WIDE DESIGN STANDARDS

This Chapter provides the design standards applicable to all new development within the Design Plan Area. In addition to complying with these requirements, new development shall conform to all applicable LCP provisions (e.g., Coastal Plan Policies, Coastal Zone Land Use Ordinances, and North Coast Area Plan standards), including but not limited to those provisions cross-referenced by the Design Plan.

Drainage Requirements (See Section 23.05.050e2 of the CZLUO)

All new development shall provide Best Management Practices (BMPs) to address polluted runoff. BMPs shall be sized and developed to meet the requirements of the California Storm Water Best Management Practices Handbook. Such measures shall include, but not be limited to: minimizing the use of impervious surfaces (e.g. install pervious driveways and walkways); directing runoff from roofs and drives to vegetative strips before it leaves the site; and/or managing runoff on sites (e.g. percolation basin). The installation of vegetated roadside drainage swales shall be encouraged and, if used, calculated into BMP requirements. The combined set of BMPs shall be designed to treat and infiltrate storm water runoff up to and including the 85th percentile storm event.

Commercial development shall use best management practices (BMPs) to control and prevent pollutants from entering the storm drain system. BMPs shall be chosen and sized to meet the guidance of the California Storm Water Best Management Practices Handbook (Industrial/Commercial). Such measures shall include both source control and treatment control practices that ensure contaminants do not leave the site. Storm water runoff from commercial sites shall be filtered through BMPs that treat storm water runoff up to and including the 85th percentile storm event. Restaurant and other commercial cleaning practices that can impact water quality (such as floor mat rinsing and vehicle cleaning) by introducing chemicals to storm drain systems (detergents, oils and grease and corrosive chemicals) shall provide designated areas that collect and dispose of this runoff through the sanitary sewer system. Street sweeping and cleaning shall use best management practices outlined in the above referenced handbook or the Model Urban Runoff Program¹ to keep contaminants and cleaning products from entering the storm drain system.

Santa Rosa Creek Setbacks and Habitat Protection (This standard has been revised and moved to Cambria Combining Designation Standard 10. D. Creek Setbacks and Habitat Protection)

All new development shall be setback a minimum of 100 feet from the upland edge of riparian vegetation. Setbacks of less than 100 feet are allowed in accordance with Section 23.07.174d.2 of the Coastal Zone Land Use Ordinance. Recreational trails shall be sited outside of areas with riparian vegetation.

¹ The “Model Urban Runoff Program: A How to Guide for Developing Urban Runoff Programs for Small Municipalities” was prepared in July 1998 by the Cities of Monterey and Santa Cruz, the California Coastal Commission, the Monterey Bay National Marine Sanctuary, The Association of Monterey Bay Area Governments, Woodward-Clyde consultants, and the Central Coast Regional Water Quality Control Board.

~~Flood Hazards~~ *(This standard has been revised and moved to Cambria Combining Designation Standard 10. B. Flood Hazard)*

~~New development shall comply with Coastal Plan Policies for Hazards and the Flood Hazard provisions of the Coastal Zone Land Use Ordinance, and shall be reviewed for its relation to the Cambria Flood Mitigation Project. Approval of new development shall be contingent upon a finding that the project will not interfere with implementation of the Flood Mitigation Plan.~~

~~Historical Preservation~~ *(This standard has been revised and moved to Cambria Communitywide Standard 12. A. Historic Preservation)*

~~New development shall be consistent with Coastal Plan Policies protecting special communities and small-scale neighborhoods by, among other means, preserving structures of historic significance and complying with CZLUO provisions for historic sites and the Secretary of Interior's standards for the treatment of historic structures. This shall include an evaluation of the historic significance of all potentially historic structures listed in Chapter 3 of the Design Plan according to the procedures established by the Secretary of the Interior, prior to permitting development that would impact these structures. Where historical buildings cannot be preserved for structural reasons, or where they are destroyed by fire, neglect, or other cause, the design of the replacement structure shall replicate the pre-existing historic structure. In any case where new development may impact a structure of potential historic significance, the project shall be referred to the State Historic Preservation Office (SHPO), and any comments or recommendations provided by SHPO shall be fully considered and evaluated as a part of the development review process.~~

V. EAST VILLAGE

This chapter focuses on the East Village. It includes a list of goals; illustrative designs for streetscape improvements, gateways, open space, circulation and parking; and design standards and guidelines.

A. Goals for the East Village

This Design Plan seeks to fulfill the following goals for the East Village:

- Enhance the identity of the East Village, its historical and natural resources, districts and its boundary.
- Improve connections to Santa Rosa Creek.
- Knit together the existing East Village retail areas on Main Street and Burton Drive.
- Provide for additional cultural and open space facilities in the East Village.
- Retain, improve and add to the parking supply in the East Village.
- Improve the streetscape of Main Street through plantings, paving, street lighting and other furnishings.
- Encourage development to blend with the existing built context.
- Reflect local slopes, geology and hydrology in planning and design.
- Protect and enhance the historic resources of the East Village.

B. Design Districts

For the purpose of this chapter, the East Village has been divided into three design districts as illustrated in Figure 7. These districts have different physical characteristics, land use patterns and design features. Somewhat different design treatments are proposed in each of the three districts.

The three districts are:

- **Village Center.** This district, at the center of the East Village, is the Village's central shopping and cultural area. It is filled with antique stores, boutiques, galleries, restaurants and local serving retail. This circular shaped district is bordered by Main Street and Santa Rosa Creek, and is framed by wooded hills on the north and south. This district contains most of the historical buildings in the East Village, with the associated small and irregularly shaped parcels and narrow street system.

- **Bluebird District.** This district acts as the western gateway into the East Village along Main Street. This area, which is between the paper street of Bluebird Lane and Burton Drive, is a one-sided strip bordered to the north by the steeply sloping, largely undeveloped hillside. The southern down-slope side of the street includes the Redwood Center and the historic Bluebird Motel. These two large developments act as two bookends to the district and to the smaller business buildings between them.
- **Old Residential District.** This district acts as the eastern gateway into the East Village. It includes the northeast section of the East Village along Bridge and Main Streets. The land use pattern in this district is formed by the old residences that were built at the edges of the East Village. In addition to the old residences in this district, there are new residences, old residences converted into small business buildings, and small motels and bed and breakfasts.

C. Gateway

Eastern Gateway

The Eastern Gateway to East Village is one of three important gateways to the community, as shown in Figure 7. The Eastern Gateway occurs along Main Street as it drops from Highway One into the entrance to town.

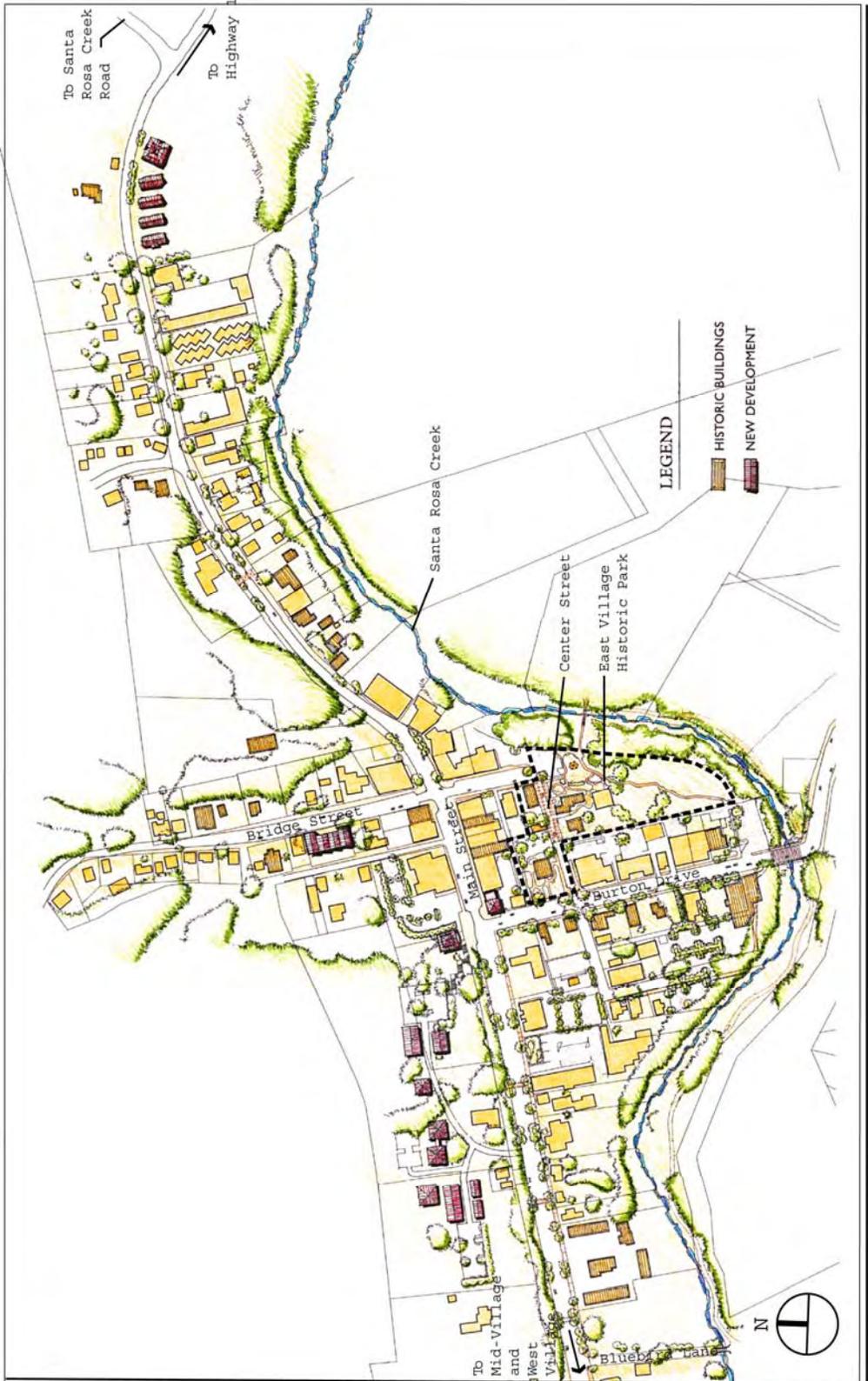


FIGURE 6

ILLUSTRATIVE P
OF THE EAST VILLAGE
CAMBRIA DESIGN

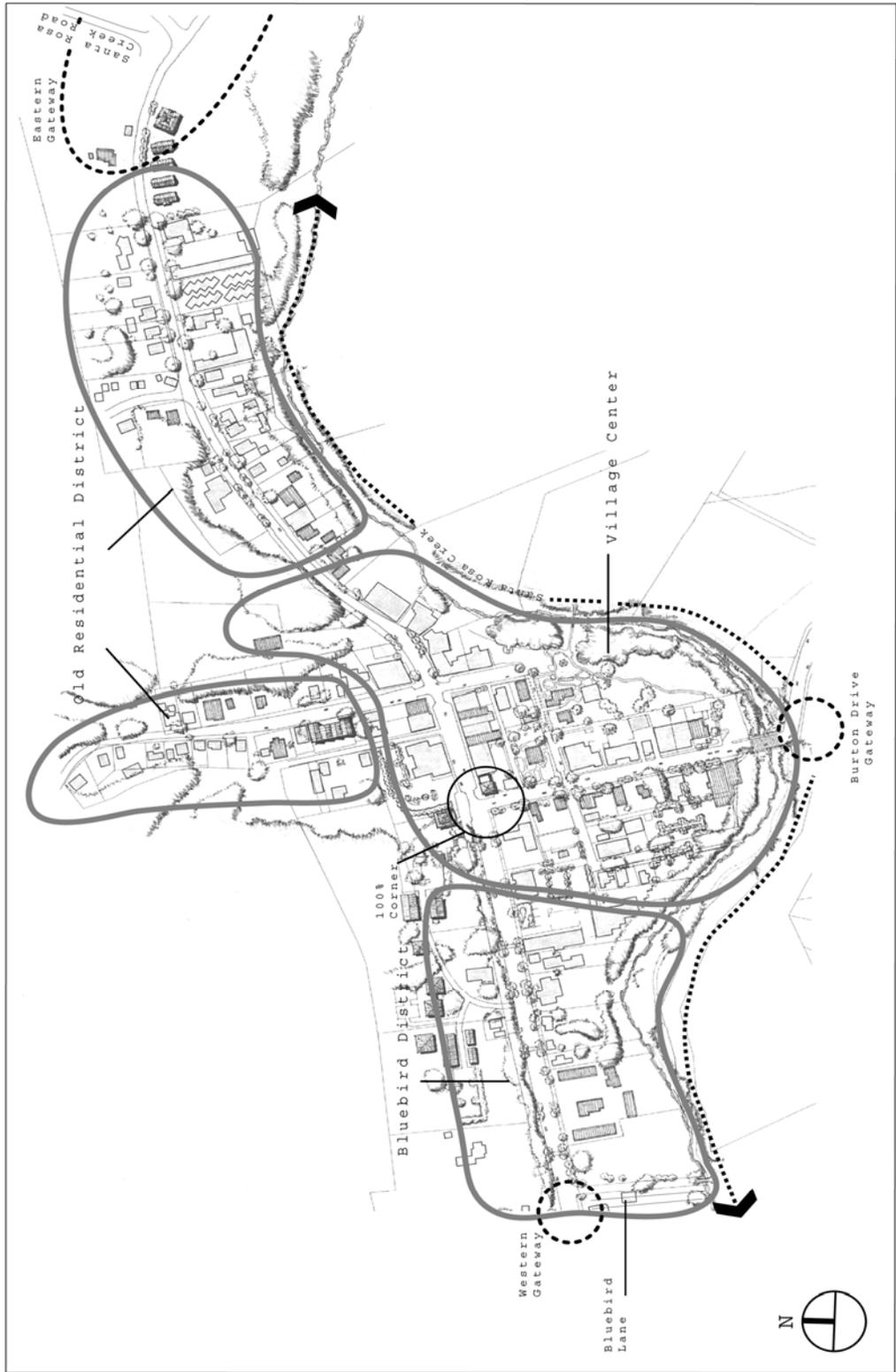


FIGURE 7
 DISTRICTS & GATEWAYS
 OF EAST VILLAGE
 CAMBRIA DESIGN PLAN

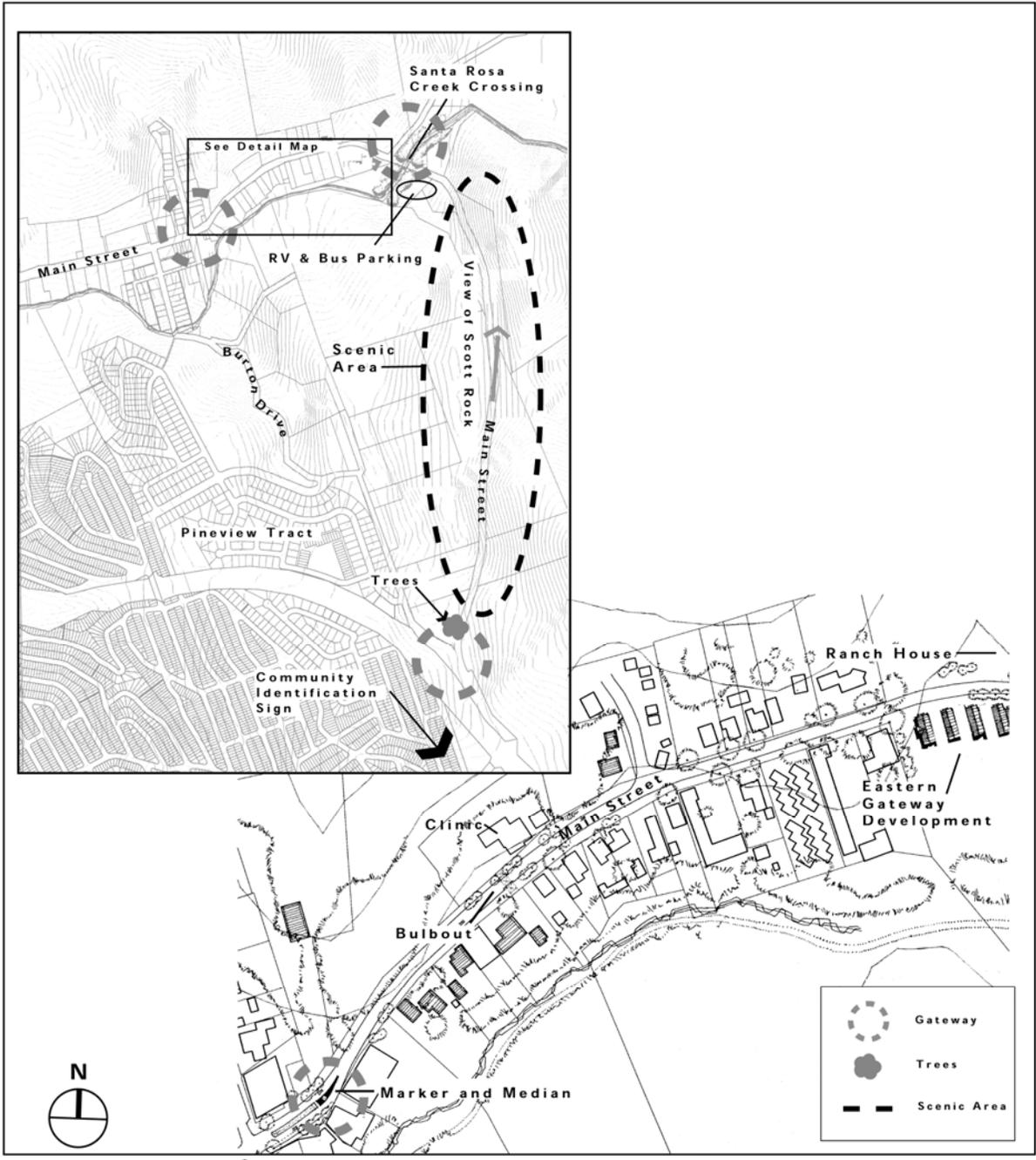


FIGURE 8

EASTERN GATEWAY

CAMBRIA DESIGN PLAN

As depicted in Figure 8, this lengthy road segment provides visitors approaching from the south with their first sense of Cambria. Undeveloped land dotted with cattle and lined with fences and eucalyptus trees provides the foreground scenery, while Scott Rock and the houses of the Pineview Tract are seen in the distance. As Main Street descends, it is surrounded by hillsides to the east and north. These hillsides terminate abruptly at Santa Rosa Creek. Next to the creek is an embellished gate to ranch land, and on the Village side of the creek is an old ranch house that represents the first structure in the East Village. Maintaining and enhancing this series of features would make this gateway even more special, and could occur through the following design interventions:

- A community identification sign would be located south of the Highway One and Main Street intersection to emphasize this intersection as the main gateway into Cambria for travelers’ arriving from the south, as shown in Figure 8. Such a sign could be located on an existing billboard or on a new sign. Markers such as those shown below in Figure 9 should evoke the central California coast and act as a physical logo to the community.



Figure 9: Community Identification Markers

- A cluster of specimen trees would be located at this intersection in the Highway One right-of-way. The selected tree species should be endemic to Cambria, and might include coast live oaks (*Quercus agrifolia*), bay laurel (*Umbellularia californica*) and big leaf maple (*Acer macrophyllum*).
- The segment of Main Street between Highway One and Santa Rosa Creek would be maintained in its current agricultural state. This should be possible through existing zoning.
- The Santa Rosa Creek crossing would be accentuated through the planting of additional riparian landscaping with species that are endemic to the site such as white alder (*Alnus rhombifolia*), California sycamore (*Platanus racemosa*), and western cottonwood (*Populus fremontii*). Cottonwood should not be used next to the road except when below the top of bank elevation in the creek channel.

This landscaping would visually enhance the gateway by providing a vertical element, which would announce the creek and provide a visual screen. In addition, it would enhance the existing riparian habitat and thus help reinforce Cambria’s natural setting.

This concept could include new plantings on the flood terrace of the Eastern Gateway development site that is discussed in more detail below in Section J.4 of this chapter. In addition, the design of this area should provide adequate space for the Santa Rosa Creek Trail and should be consistent with the County's Trails Plan.

- The first house located west of Santa Rosa Creek Road should be maintained and enhanced due to its historic character and significant location at the Eastern gateway to the village of Cambria. Property development should emulate and complement the street fronting locations and design elements of this and other houses along the Main Street corridor toward the village to the West, as well as farther up Santa Rosa Creek Road to the east. Development guidelines for this site are given in more detail in Section J.3 of this chapter.
- As a means to slow traffic entering the Village Center, curbs, gutters and sidewalks, parking spaces and a pedestrian bulb-out with a wide area of textured paving would be installed on Main Street west of the clinic.

The following existing policies would also help to maintain this gateway:

- Maintain the agricultural land along the roadway.
- Maintain the view of Scott Rock.
- Encourage private property owners to plant trees and shrubs at the intersection of Highway One and Main Street.

D. Streetscape

While the East Village is charming, its streetscape could be improved without diminishing the rural character or quaintness of the community. For example, a creative system of pathways, sidewalks, street tree plantings and street furniture can be established. The proposed improvements are in keeping with the following general principles:

- Street trees are suggested in some areas to add identity and character. Due to the narrow rights-of-way in many areas, however, only selected streets and portions of streets are proposed for street trees.
- A continuous system of pathways and sidewalks is proposed within the Village Center and along Main Street. Continuous pathways and sidewalks encourage walking and window shopping. Wider sidewalks can create more comfortable pedestrian spaces and allow for amenities like street trees and seating. Sidewalks are intended to be wide enough to accommodate pedestrians, wheelchairs, small personal shopping carts and baby carriages.
- “Bulbouts” are proposed to widen the sidewalks into the on-street parking lane at the intersections of Center Street/Burton Drive and Main Street/ Bridge Street, and on Main Street between Burton Drive and Bluebird Lane. These bulbouts would help narrow the apparent width of the street, allowing for an easier pedestrian crossing. Bulbouts could also accommodate outdoor cafe seating and street furniture.
- New pedestrian-scaled lighting fixtures would be installed to add to the historic charm of the East Village. These fixtures should be limited to those necessary for public safety, be of a design and size that maintains the present character of Cambria, and only provide light that is as natural looking as possible.
- There would be a variety of seating in the Village Center, which would include built-in seating integrated into building facades (such as that at Linn’s), wide planter walls, and benches designed through a community competition. This amenity could only be placed where sidewalk widths are eight feet or greater.
- New trash receptacles would be provided in the Village Center. The streetscape improvements proposed in this Plan would work within the existing rights-of-way of each of the streets. The improvements of each street are discussed individually below.
- One to two bike racks would be installed in the Village Center, in addition to those required for new development. These racks would be installed in the bulb-outs or in other pedestrian oriented areas where space is provided.

Main Street

This section discusses treatments for Main Street in three areas: in the Bluebird District, in the Village Center, and in the Old Residential District.

Bluebird District

Main Street’s right-of-way in the western end of the East Village is 68 feet wide. As illustrated in Figure 10a, this right-of-way currently accommodates two very wide traffic lanes with diagonal parking on the southern side of the street. A large portion of the right-of-way is made up of a steeply sloping hillside.

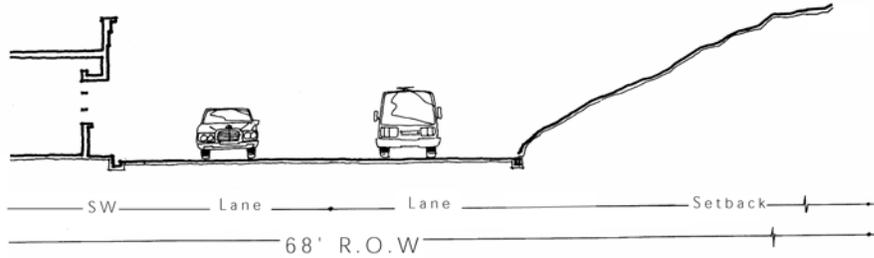


Figure 10a: Main Street in Bluebird District - Existing Conditions

As shown in Figure 10b, this portion of Main Street is proposed to be planted with Monterey pine (*Pinus radiata*), from local disease resistant stock, on the undeveloped uphill slopes. Erosion control, such as retaining walls, would be integrated with the Monterey pines to help secure this unstable hillside. The street itself and its down-slope side would be planted with native trees typical of the Monterey Pine forest understory. These trees and the pines would create a naturalistic corridor outside of the Village Center.

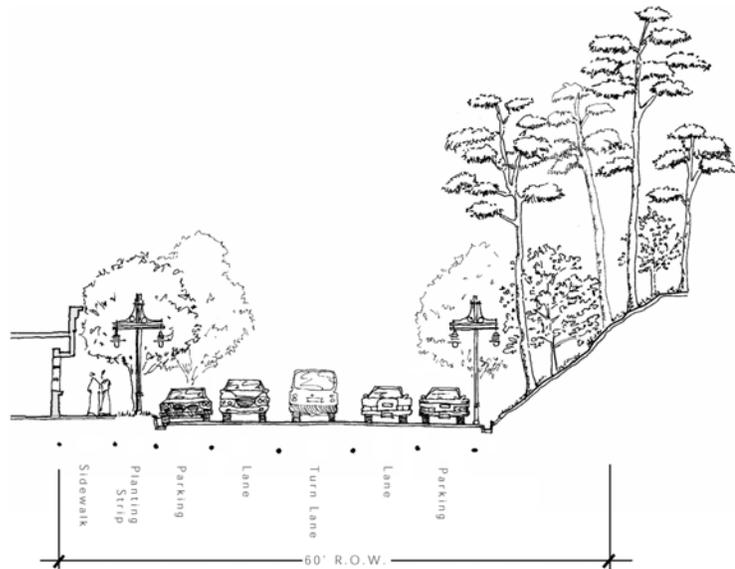


Figure 10b: Main Street in Bluebird District - Proposed Design

If cutting the slope is necessary to improve the road, no more than a three-to-one slope ratio is recommended. Where the retaining wall needs to be greater than four feet in height from natural grade to top of wall, the wall should be split and stepped down the slope, as illustrated in Figure 39 in the Mid-Village Chapter. The new streetscape would include:

- An 8-foot wide sidewalk on the south side of the street, one parallel parking lane, two traffic lanes, and a center turn lane.
- Landscaped bulb-outs at intervals in the parking lanes, with trees.
- The landscaped bulbouts and street trees in the planting strip on the southern side of the street would be tanbark oak (*Lithocarpus densiflorus*), bay laurel (*Umbellularia californica*), toyon (*Heteromeles arbutifolia*), coast live oak (*Quercus agrifolia*), or other trees that are approved by the Department of Planning and Building or any other species that can be documented as a native within Cambria.
- Pedestrian scaled street lighting, limited to those necessary for public safety, would line both sides of Main Street.
- Benches and trash receptacles would be placed at convenient locations along the sidewalk.

Village Center

The block and a half of Main Street between Burton Drive and the Santa Rosa Cemetery contains some of the most intensive village development in Cambria. The existing streetscape, which consists of two sidewalks, two parallel parking lanes, and two traffic lanes, fits within a relatively narrow 60-foot right-of-way, as shown in Figure 11a. Relatively tall one-story and two-story buildings built to their lot line enclose this lively street segment.

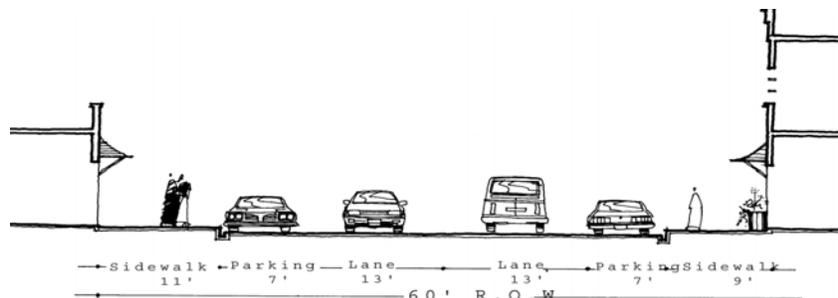


Figure 11a: Main Street in the Village Center - Existing Conditions

The improvements to this section of Main Street are shown in Figure 11b and would consist of:

- The plan proposes to widen the sidewalk on the north side of Main Street from nine feet to eleven feet to provide room for pedestrians and pedestrian amenities on this sunny side of the street.
- Pedestrian scaled street lamps, limited to those necessary for public safety, seating and trash receptacles.
- Installation of bulbouts with clearly marked crosswalks at the intersection of Main Street with Bridge Street. These bulbouts would be in the Bridge Street right-of-way only.

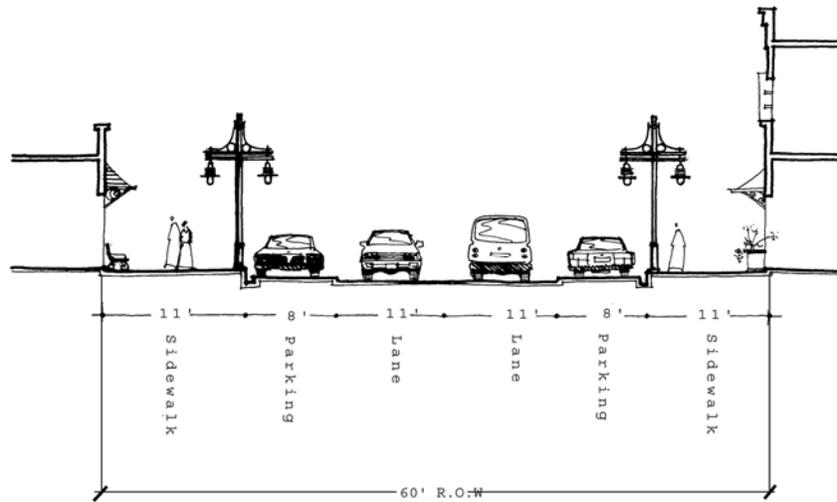


Figure 11b: Main Street in the Village Center - Proposed Design

Old Residential District

In the Old Residential District, Main Street has a 50-foot wide right-of-way. As illustrated in Figure 12a, the right-of-way contains two travel lanes, one parallel parking lane, inconsistent sidewalks on the south side of the street, and a drainage channel on the north side of the street. Older houses in this district, which are approximately twenty feet from the paved roadway, are generally built on the lot line.

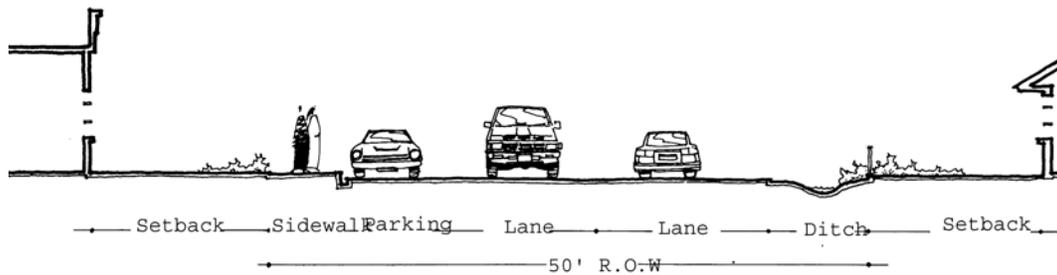


Figure 12a: Main Street in Old Residential District - Existing Conditions

To maintain the district's quiet, rural character, it is important to adjust the current width of the street in this area, which currently allows for excessively fast traffic. Improvements to this section of Main Street, shown in Figure 12b, would include:

- An 8-foot sidewalk on the south side of Main Street, separated from the street by a 2-foot planting strip.
- Pedestrian scaled streetlights, limited to those necessary for public safety, on the south side of Main Street.

- 11-foot travel lanes and parallel parking on both sides.
- Improvement of the drainage channel on the north side of Main Street by installing an underground pipe.
- Plantings of large specimen trees at the street edge to enclose the street and to strengthen the pattern of significant trees along Main Street such as in front of Olallieberry Inn. Since paving currently extends to the edge of the right-of-way, these plantings would have to occur on private property with the cooperation of individual owners.

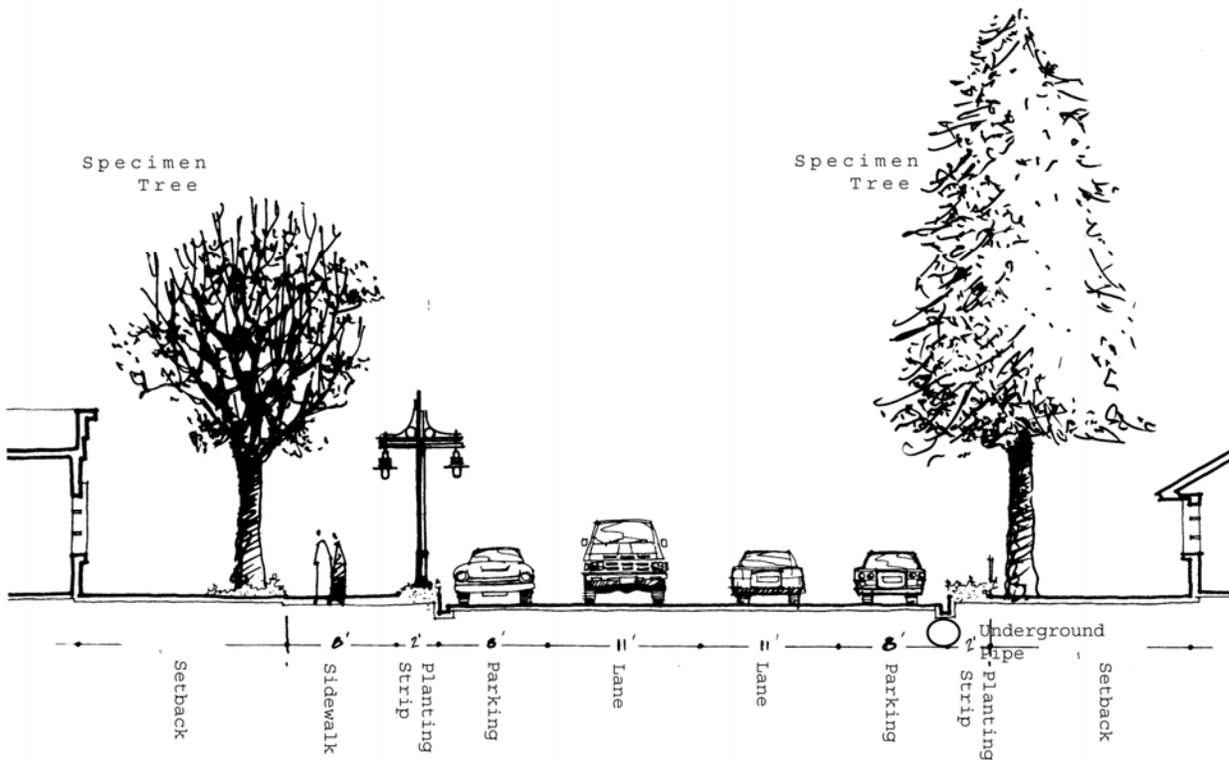


Figure 12b: Main Street in Old Residential District – Proposed Development

Burton Drive

The two block length of Burton Drive in the Village Center is the second busiest street within the East Village. As with Main Street, Burton Drive contains two sidewalks, two parallel parking lanes and two traffic lanes, but within a 52-foot right-of-way. As illustrated in Figure 13a, this street generally has very narrow sidewalks except where private property owners have paved the front setback area of their parcel.

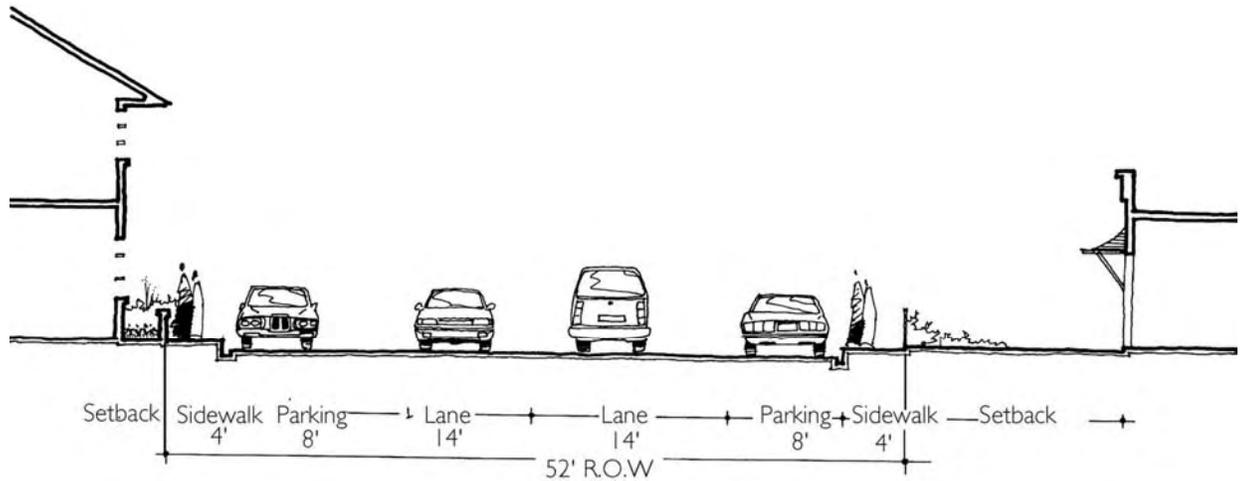


Figure 13a: Burton Drive – Existing Conditions

The improvements to Burton Drive, as shown in Figure 13b, would consist of:

- Widened sidewalks with a width of 8 feet on the west side of Burton and 6 foot wide sidewalks on the east side of Burton.
- Installation of street trees at strategic locations on the west side of the street to create a sense of enclosure. The east side of the street does not require trees since it is already well-defined with two-story buildings. The new trees should be evergreen and their roots should not disrupt the sidewalk. Such trees could include Catalina ironwood (*Lyonothamnus f. asplenifolius*), coast live oak (*Quercus agrifolia*), Brisbane box (*Tristania conferta*), or any other trees that are approved by the Department of Planning and Building.
- Pedestrian-scaled street lamps, limited to those necessary for public safety, seating and trash receptacles.

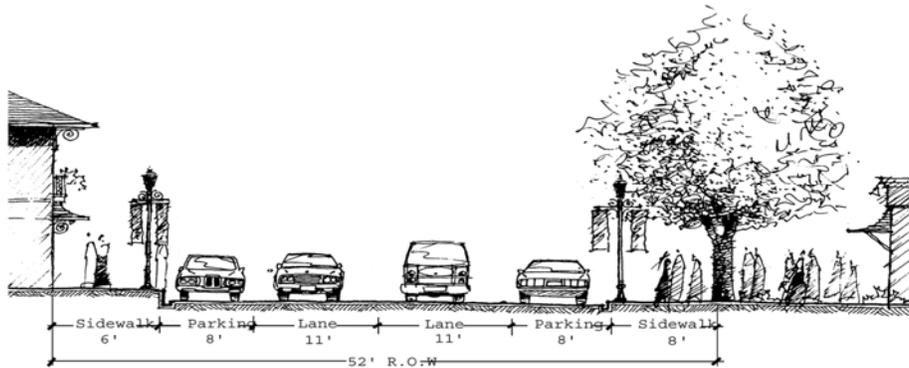


Figure 13b: Burton Drive – Proposed Development

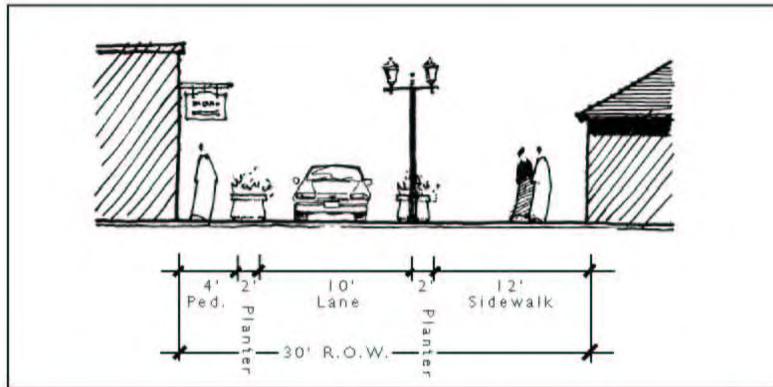
Center Street

Center Street is the narrowest street in the East Village, with a 30-foot right-of-way. It runs parallel to Main Street and acts as a connection from Bridge Street to West Street. Between Bridge Street and Burton Drive, Center Street currently has one parallel parking lane, one traffic lane, and one very narrow sidewalk which switches sides at mid-block. Between Burton and West Street, Center Street acts as a two way street with two traffic lanes with no on-street parking and one very narrow sidewalk on the southern side of the street.

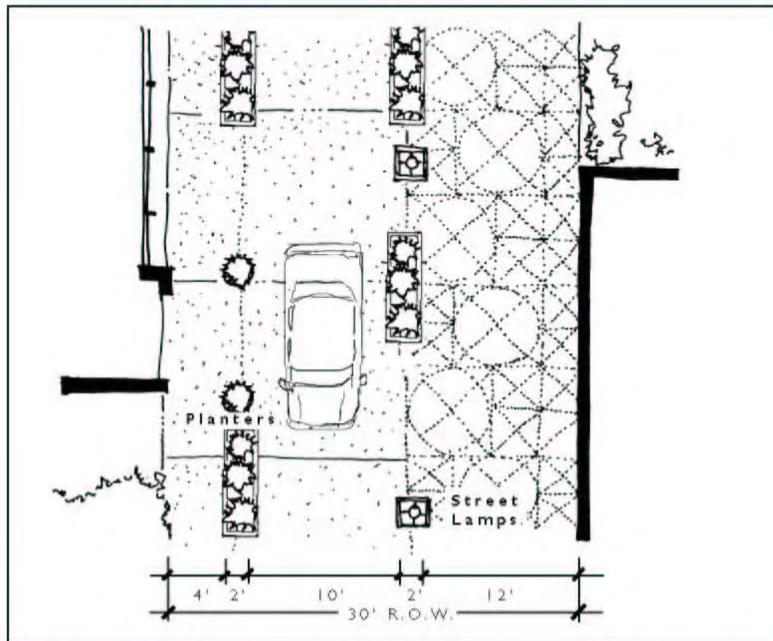
From Bridge Street to Hesperian Lane

Between Bridge Street and Hesperian Lane, Center Street would become the central spine of the Cambria Historical Center/Park ~~East Village Historical Park~~, which is described in Section E, below. In this area, the following improvements to the street are proposed, as depicted in Figure 14:

- The entire 30-foot right-of-way would be paved as one plane.
- Large planters, which would be 2 feet wide, would separate two pedestrian areas from a vehicle lane. This design would slow most vehicular traffic, creating a pedestrian-oriented development.
- Center Street would include a one-way, 10-foot travel lane.
- The remaining 16 feet of the right-of-way would be used for pedestrian circulation. There would be a 4-foot wide sidewalk on the north side of the street and a 12-foot wide walkway on the south side of the street.
- The seven existing parallel parking spaces and four perpendicular spaces on Center Street would be eliminated and replaced as diagonal parking on the CCSD lot and off of Hesperian Lane.
- The twelve-foot wide pedestrian walkway would act as a secondary vehicle path in order to provide two-way emergency vehicle access through this portion of Center Street.



a: Proposed Section



b: Proposed Plan

FIGURE 14

**CENTER STREET FROM
BRIDGE STREET TO HESPERIAN LANE**

CAMBRIA DESIGN PLAN

Center Street from Hesperian Lane to West Street

Improvements to the remainder of Center Street, from Hesperian Lane to West Street, include:

- A 6-foot wide sidewalk on the south side of the street.
- On the north side of the street, there would be no need for a sidewalk along the Guthrie-Bianchini parcel, since this Plan proposes construction of a public open space on this parcel surrounding the Guthrie-Bianchini House. There would be no sidewalk on the north side of Center Street between Burton Drive and West Street in order to preserve the small garden currently growing there. Pedestrians in this area could use the south side of the street.
- Pedestrian scaled street lamps, limited to those necessary for public safety, would be installed on the southern sidewalk.

Bridge Street

This section discusses treatments for three segments of Bridge Street. Two of these segments, on the south and north sides of Main Street, are in the Village Center. A third segment of Bridge Street is one-block north of Main Street in the Old Residential District.

Village Center south of Main Street

South of Main Street, Bridge Street has one wide travel lane, one diagonal parking lane, one parallel parking lane and two narrow sidewalks within a 60-foot right-of-way. The proposed improvements to Bridge Street south of Main Street are described below and illustrated in Figure 15:

- Widening the western sidewalk to eight feet, six inches. This would require narrowing the traffic lane and the diagonal parking lane to eighteen feet six inches and fifteen feet respectively. The loss of four parking spaces on Bridge Street would be offset in new shared parking lots discussed in Section G, below.
- Installation of pedestrian-scaled lamps, limited to those necessary for public safety, seating and trash receptacles.
- Installation of street trees. Recommended trees are white alder (*Alnus rhombifolia*), California sycamore (*Platanus racemosa*) and western cottonwood (*Populus fremontii*), which would reinforce the riparian image of Santa Rosa Creek Park at the end of the street.
- Improving the alley entrance and access for easier service deliveries.

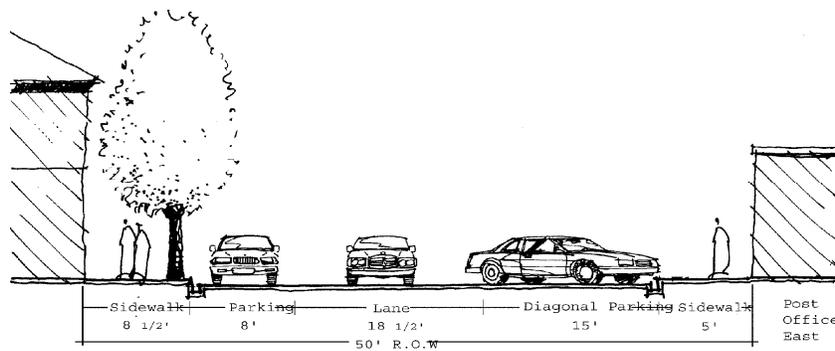


Figure 15: Bridge Street South of Main Street - Proposed Development

Village Center North of Main Street

The first 100 feet of Bridge Street north of Main Street has two traffic lanes, two parallel parking lanes, and two sidewalks. Beyond this 100-foot segment, Bridge Street is paved for two traffic lanes, and has no designated parking or sidewalks.

Improvements to the segment of Bridge Street north of Main Street, in the Village Center, would be intended to make this street the same as Bridge Street south of Main Street and would consist of:

- Improving more of the roadway to provide parallel parking spaces.
- Continuous 8-foot sidewalks on both sides of the street.
- Pedestrian scaled street lamps, limited to those necessary for public safety, and street trees.

Old Residential District

The northern remainder of Bridge Street (beyond 100') is in the Old Residential District. In this section, Bridge Street has a 30-foot right-of-way and is lined with small houses many of which are used for businesses. The right-of-way contains two narrow traffic lanes, no demarcated parking, and no sidewalks.

Improvements to this section of Bridge Street should include:

- Demarcation of parallel parking in an expanded gravel shoulder with a wood or rock edging.
- Continuous marked pedestrian pathways or sidewalks that meet ADA regulations but are not as formal as the concrete sidewalks in the Village Center. These pathways could be made of brick, cobblestone, or some other textured and non-monolithic material.

Hesperian Lane

The alley parallel to Burton Drive is known as Hesperian Lane. To improve circulation and add parking to the project area, this alley would be improved, slightly widened and 12 diagonal parking spaces would be provided off of the alley.

E. Cambria Historical Center/Park East Village Historical Park

Currently the East Village lacks a central focal point. The Village turns its back on Santa Rosa Creek, and important historical buildings have fallen into disrepair.

A conceptual plan prepared by the Parks, Recreation and Open Space (PROS) Commission of the Cambria Community Services District envisions creation of a Cambria Historical Center/Park comprised of the Guthrie-Bianchini House and the CCSD, Warren, and Greenspace parcels. The Cambria Historical Center/Park an East Village Historic Park, which would include cultural facilities in restored historical buildings, parkland and a nature walk along Santa Rosa Creek. As illustrated in Figure 16, the Cambria Historical Center/Park East Village Historic Park would encompass the parcels of the Guthrie-Bianchini House on Burton Drive, the CCSD parcel at the end of Bridge Street, the Joss House, the Bucket of Blood Saloon, and the reconfigured Center Street described above.



Figure 16: Cambria Historical Center/Park East Village Historical Park

Guthrie-Bianchini Historical Museum and Gardens

The Guthrie-Bianchini House, which was listed on the National Register of Historic Place in January of 1980, has been unoccupied since 1970 due to a disagreement between the heirs. As shown in the photo in Figure 17a, the house has fallen into disrepair. Despite its condition, this house is located in a key position in the East Village at the center of the most intensively-used portion of Burton Drive. It therefore constitutes an opportunity for a cultural facility and open space that could be a focus for the community.



Figure 17a: Guthrie-Bianchini House

The Cambria Historical Society is currently in the process of acquiring this property to improve the structure and surrounding gardens and provide a home for the historical museum of Cambria. As depicted on Figure 16, a new garden and historical museum would provide people with public open space and a sense of roots at the heart of the East Village.

Under this Plan, the house would be restored and utilized as a historical museum for the community. This museum would display important cultural and natural events, construction of Highway One, important figures to the community, historic photos and other historic relics that were significant in the development of the community of Cambria and its environs.

The grounds surrounding the house would create a center-of-town open space, a dignified setting for the Historical Museum and a community facility for weddings and events similar to the Jack House in San Luis Obispo. The grounds would feature heritage gardens, additional trees, benches, walkways, a transit stop at the street, picnic facilities and some parking spaces at the rear.

White House

The White House, located on the south side of Center Street, would accommodate shops that would provide pedestrian activity along Center Street and in the park.

Joss House

The Joss House, which housed the oldest Chinese temple in Southern California, would be restored and would contain information panels and displays on the history of Chinese in the Central Coast area.



Figure 17b: Joss and White House

Bucket of Blood Saloon

~~The Bucket of Blood Saloon is named for the Saturday night fights that would occur between patrons when this building housed the Rip and Riley’s restaurant. The building is currently utilized as a recording studio and would remain in this capacity under the plan for the East Village Historical Park. However, if the current tenant vacates the premises, the new use should be a business that would provide activity in the area, is open to the public, and allows the building to be more pedestrian friendly from the street. As part of the East Village Historical Park this building would undergo a cosmetic exterior renovation.~~



Figure 17c: Bucket of Blood Saloon

Nature Walk and Park

The land currently housing the abandoned CCSD offices, together with the rear portions of the White House and Joss House properties, would become a 3-acre open space area with a nature walk.

The Nature Walk, which would be located along the north side of Santa Rosa Creek and behind the Joss and White houses, would be an informal park and path that would provide people with access to the creek and a small taste of nature within the East Village. It would be smaller in scale but similar to San Luis Obispo’s creek walk, and would provide people with an area to sit and picnic outside of the

corridor of riparian vegetation along the creek. A series of overlooks would be in this walk as well as visual and physical connections to public streets or activity areas, so that it is a publicly defensible space.

The Nature Walk would need to be designed and installed so that it would have no significant impact on existing riparian vegetation. A preliminary analysis shows an alignment that is approximately 50 feet from the creek bank.

In addition to the Nature Walk, the park would have some other activity areas and additional parking. A gazebo would be located in the park at the end of Bridge Street to visually anchor the end of the street and to provide a stage for entertainment and gatherings. A pedestrian bridge, which would connect Tin City to the East Village, would be located on the Nature Walk and just east of the gazebo or in its vicinity. The bridge should be located in an area that minimizes environmental impacts. A patio gallery would be in the park behind the White house. Approximately ten parking places would be provided near the gazebo at the end of Bridge Street.

F. Circulation

This section discusses changes to vehicular, bicycle and pedestrian circulation created by the Plan.

Vehicular Circulation

Vehicular circulation in the East Village would be largely unchanged under this Design Plan. Most traffic would continue to use Main Street, Burton Drive, Bridge Street and Center Street.

The alley parallel to Main Street would be improved to improve access for trucks, to provide easy access for autos traveling east/west and improve aesthetics with paving and trees. West Street would be converted into a parking lot while still providing access to adjacent properties. This would not change circulation in the East Village, since West Street currently only provides parking lot access. Pedestrian access along this street would be maintained.

Pedestrians and Bicycle Circulation

Pedestrian and bicycle circulation in the East Village would be improved through implementation of the Cross Town Trail, which has been planned by community members over the past few years. In the East Village, the trail would follow the alignment described below:

- At the entry to the East Village on Main Street, the Cross Town Trail would turn off of Main Street, and run down Bluebird Lane towards Santa Rosa Creek.
- The trail and bikeway would cross Santa Rosa Creek on a bridge and connect with Rodeo Grounds Road, which continues to Burton Drive.
- East of Burton Drive, the trail would continue on the south side of Santa Rosa Creek on the edge of Tin City.
- The trail would connect with the Nature Walk at the pedestrian bridge in the Cambria Historical Center/Park ~~East Village Historical Park~~.

- East of the pedestrian bridge the trail alignment is yet to be determined. However, the long term goal of the Cross Town Trail is to connect the Pacific Ocean, the most western point of Cambria with Coast Union High School, the most eastern point of Cambria.

This Plan also foresees several other pedestrian and bicycle improvements in the East Village. They include:

- A Nature Walk on the east of Burton Drive and north of Santa Rosa Creek, as described in the Cambria Historical Center/Park ~~East Village Historic Park~~ section in this chapter.
- A pedestrian and bicycle bridge would connect from the new Santa Rosa Creek Park on the CCSD site to Tin City.

G. Parking

According to a count conducted by the consultant, there are currently 418 off-street parking spaces and 119 official on-street parking spaces within the East Village. Under this Design Plan, the total parking supply would increase from 537 to 558 spaces, for a gain of 21 spaces. Table 3 illustrates the additions and subtractions to on- and off-street parking proposed by this Plan. The table does not list every parking facility in the East Village, but only those in which changes would occur.

Some parking lots in the East Village are laid out inefficiently, and some adjacent parking lots on individual parcels could be improved by combining the parking lots across parcel lines. With these changes, it would be possible to increase the number of parking spaces on some existing lots through repaving and re-striping. In addition, some lots are currently used by businesses that have different parking demands at different times of day. Continuing to encourage the sharing of these lots could allow for more consistent use of the parking spaces over the course of the day.

Specifically, this plan foresees major improvements to the parking lots on both sides of West Street, which could be reconfigured to include the West Street right-of-way itself, which only provides access to the parking lot. This would result in the addition of 11 parking spaces over the existing total supply in the area of 95 spaces.

Several other parking areas would also undergo minor changes:

- **Cambria Historical Center/Park, ~~East Village Historic Park~~.** In the area of the Cambria Historical Center/Park ~~East Village Historic Park~~, which is discussed in Section E, the on- and off-street parking would be reconfigured. The proposed plan for Center Street between Bridge Street and Hesperian Lane would create a loss of 11 on-street spaces; however, 12 on-street spaces would be gained with the addition of parking spaces off of Hesperian Lane. Off-street changes would include reconfiguring the CCSD lot and adding four spaces to the Guthrie-Bianchini lot for a total of 14 new off-street spaces.
- **Bank of America and Cambria Bakery.** The sites of the Bank of America and the Cambria Bakery are both proposed for small amounts of new development under this Plan, as a means to create additional definition at the corner of Burton Drive and Main Street. This new

development is described in more detail in Section J, below. This new development would result in the loss of approximately four parking spaces in the Cambria Bakery lot, and the addition of approximately one space in the Bank of America lot. The existing lot currently has 38 spaces with three driveway access points. To create space for 39 parking spaces and a new building the parking lot was more efficiently sited with two driveway access points and individual parking space layout.

TABLE 3 EAST VILLAGE PARKING CHANGES¹

Location	Existing	Proposed	Gain (+) or Loss (-)
East Village Shared Lot ²	95	106	+11
East Village Historic Park ³	10	14	+4
Bank of America	38	42	+4
	22	18	-4
Cambria Bakery			
Total Off-Street Charges	165	180	+15
Main Street (Village Center)	23	27	+4
Center Street (Bridge & Burton)	15	5	-10
Hesperian Lane	0	12	+12
Total On-Street Charges	38	44	+6
Total for All Parking	203	224	+21

H. — Development Standards

This section includes development standards that are requirements in the East Village. While the subsequent section includes design guidelines that are intended to be advisory only, the standards in this section are so important to the continued success of the East Village that compliance with them is mandatory for all new development.

Building Height

The maximum allowable height in the East Village is 28 feet, except in the Old Residential District, where the maximum height is 22 feet. Buildings may exceed these maximum heights by up to 7 feet (for a total of 35 and 29 feet, respectively) if they have roofs with a pitch of 6:12 or greater.

¹ This table only includes locations where parking is proposed to be added, reduced or redesigned. It does not show all parking in the East Village.

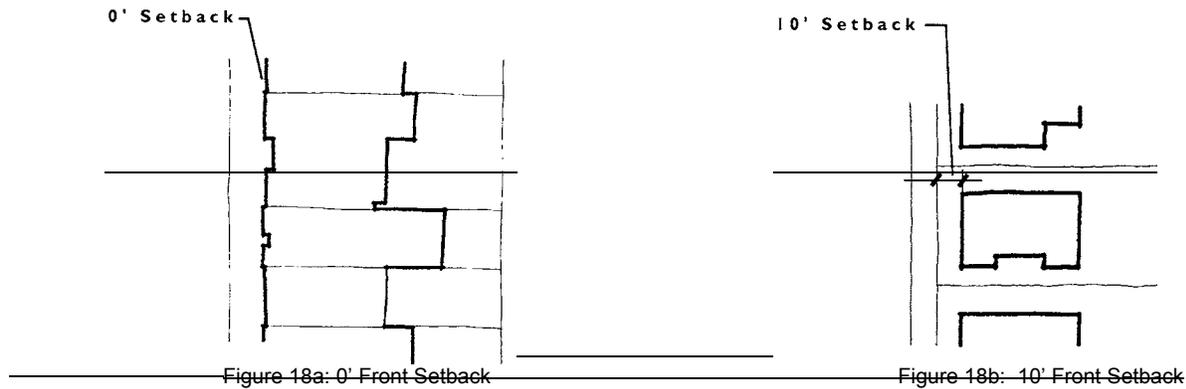
² This shared parking lot includes the Brambles parking lot, the office parking lot on West Street and a portion of West Street.

³ This lot includes existing parking on the CCSD, Guthrie-Bianchini and Joss House parcels.

Building height is measured at every individual point on a site from the existing grade to the top of the building directly above that point.

Front Setbacks

All new development and/or remodeling in the Village Center and Bluebird District shall be consistent with adjacent setbacks. New buildings in these areas shall have setbacks that represent the average of the front setbacks on the closest developed parcels on either side of the new building. As described below and shown in Figure 18a & b this regulation would have the following implications:



- On Main Street between Burton Drive and the Santa Rosa Cemetery, buildings should be built to the property line.
- On Burton Drive, buildings should be built with a 10-foot setback, with 3 feet dedicated to a public sidewalk.
- On the south side of Main Street in the Bluebird District east of Redwood Center, buildings shall be built to the property line at the Redwood Center and east of it.
- On the south side of Main Street in the Bluebird District west of the Redwood Center, buildings should be built with 10-foot setbacks.
- No setbacks are shown for the north side of Main Street west of Burton Drive since any new development in this area should generally be on the bluffs above the street, as described in Section J.6, below.

All new development in the Old Residential District shall be built with a 15-foot minimum setback.

Side Setbacks

- On Main Street between Burton Drive and Bridge Street, all new development and/or remodeling shall be built to the property line with zero-foot side setbacks. (see Figure 18c)

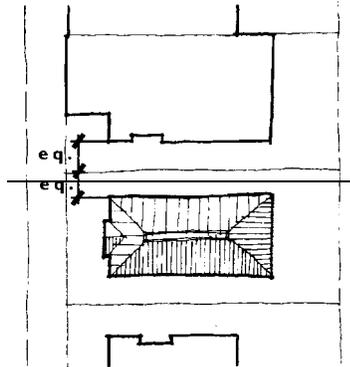


Figure 18c: Equal Setbacks Adjacent to Historic Buildings

- On Burton Drive, all new development and/or remodeling shall be built to the property line except on or adjacent to historic properties that are already constructed with side setbacks. On those parcels, side setbacks shall be the same as for the historic building in question.
- In the Bluebird and Old Residential Districts, all new development and/or additions shall have a minimum side setback of 10 feet.

Building Facades

New development and/or exterior remodeling in the Village Center shall be articulated with one or more bays with a maximum width of 30 feet. Wider buildings shall be made up of several bays. (see Figure 18d)

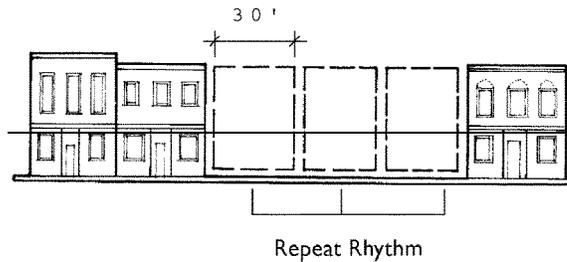


Figure 18d: Building Rhythm

Pedestrian-Oriented Detailing

All building faces along public streets in the East Village should include pedestrian-scaled detailing such as vertical windows with sills, ornamental decorations, built-in benches, wall lamps, awnings, and door overhangs or recessed entries. (see Figure 19a)

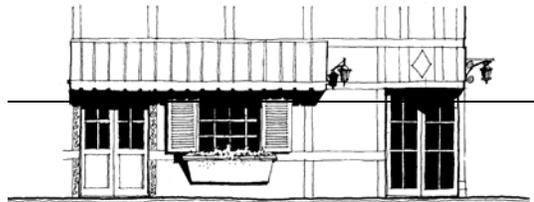


Figure 19a: Pedestrian Scale Detailing

Any building facing a street must have windows and doors. A minimum of 40% of the length of the building facades must be made up of such openings:

All facades, regardless of orientation, should include three dimensional detailing, such as window moldings and reveals to cast shadows and create visual interest on the facade. (see Figure 19b)

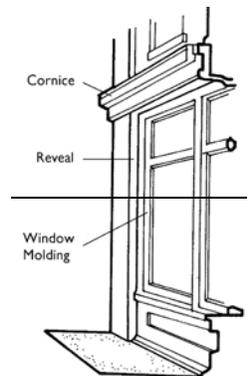


Figure 19b: Window Definition

Roofs

Mansard roofs are not allowed. Mansard roofs are steeply sloping roofs where the top of the slopes do not form a gable but meet to create a flat rooftop. Mansard roofs can be seen on Second Empire, Beaux Arts, and some Romanesque buildings. (see Figure 19c)

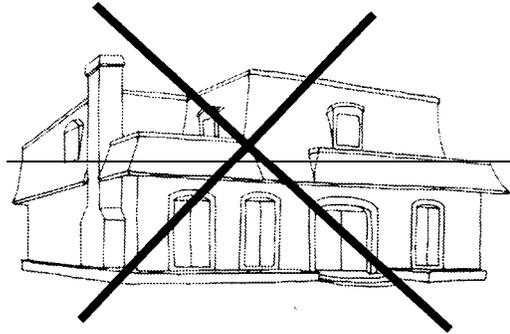


Figure 19c: Mansard Roof

All structures north of Main Street on Bridge Street shall have gable roofs. (see Figure 19d)

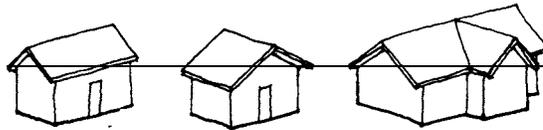


Figure 19d: Gabled and Hipped Roofs

Exterior Materials

Metal materials on the exterior of buildings are discouraged in order to enhance and blend with the historical character of the East Village. (see Figure 20a)

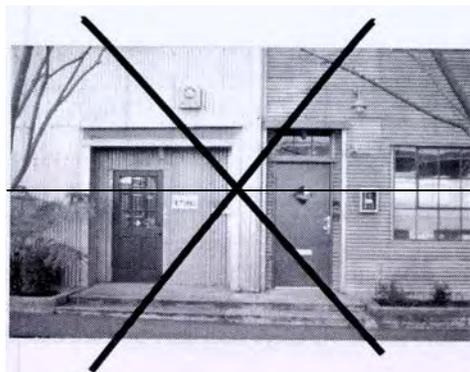


Figure 20a: Metal Exterior

All exterior wall changes shall be separated from one another by a change in plane of at least 4 inches, or should be set off with architectural detail to create a visual distinction. (see Figure 20b)

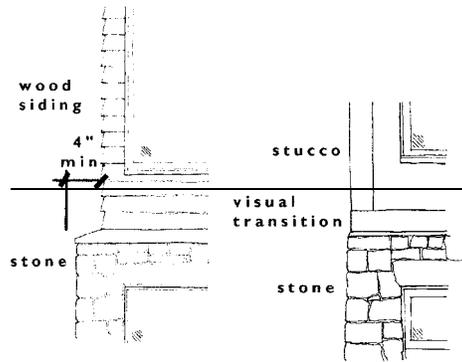


Figure 20b: Building Material

Uses

In the Village Center, ground floor frontages shall be retail or other visitor-serving uses only. These uses create a sense of activity that is crucial for pedestrian-oriented areas. Upper floor uses shall be lodging, offices or shops in the Village Center.

In the Bluebird District, ground floor frontage shall include retail or lodging. Upper floors shall be lodging, offices, shops or residences.

In the Old Residential District, uses on all floors may include lodging, residences, and offices.

Parking

All new parking on private property shall be located to the rear of buildings, unless the County finds that such a location is impossible on a specific site. (see Figure 20e)

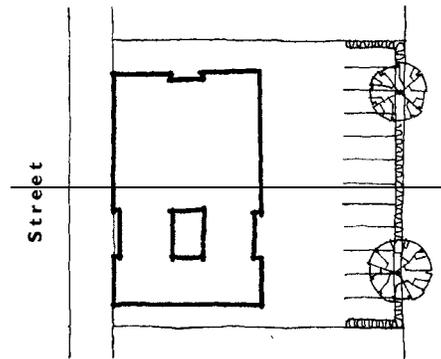


Figure 20c: Rearyard parking

If rear parking is not possible, parking shall be located to the side of a building, with the lot screened from the street by a fence, low wall or hedge. Side yard parking lots should be no wider than one double loaded bay, with a maximum width of 60 feet. (See Figure 20d)

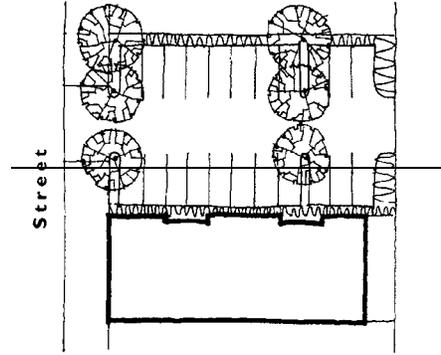


Figure 20d: Sideyard Parking

Parking lots shall be landscaped with at least one tree for every eight spaces, in addition to the shading requirement of the Coastal Zone Land Use Ordinance section 23.04.168f, which requires shading of 60% of the parking lot surface area.

Curb Cuts

In the Village Center, no new curb cuts shall be allowed on Main Street.

In the Village Center, no more than one curb cut shall be allowed for each parcel. Lots that have auto access via alleys shall have no curb cuts from any public street.

Where curb cuts occur, the driveway shall be no more than 10 feet wide, unless two-way in and out access is required, in which case the driveway shall be no wider than 20 feet. (see Figure 21a)

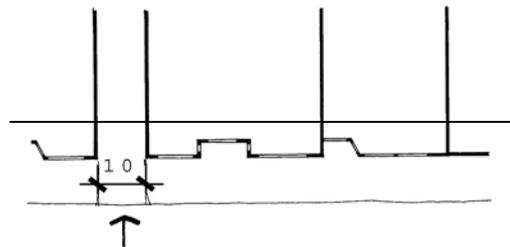


Figure 21a: Curb Cuts

Development on Sloping Sites

The hillsides are one of the dominant physical features in the East Village, which provide enclosure on the northern edge of the Village. Preservation of the hillsides and careful, unobtrusive development is essential in maintaining an attractive backdrop for the East Village. For development on steeply sloping sites abutting the northern side of Main Street, the following development standards apply:

Development shall be built on the flatter portions of these sites on top of the bluffs away from Main Street, and not in graded areas at the toe of the hillside at the edge of Main Street. (see Figure 21b)

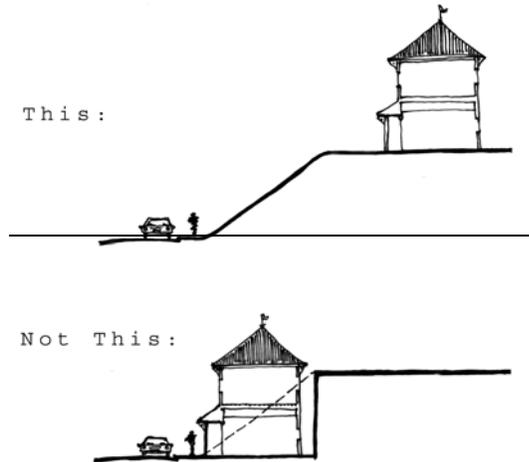


Figure 21b: Building on Bluff

For parcels that are zoned for commercial uses, the flatter areas of the site shall be utilized for parking rather than building footprints. Where buildings abut both sides of a parking lot, the difference in ground floor elevations, where feasible, should not be such to cause the parking lot between them to have a slope greater than 2%. (see Figure 21c)

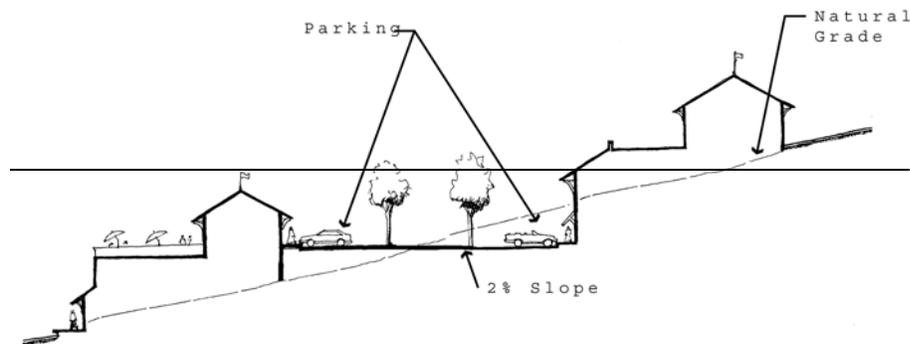


Figure 21c: Parking Locations on Sloped Sites

Site grading shall generally be limited to areas within and adjacent to the building footprint, parking areas and driveways, and where necessary due to unusual site conditions, or where necessary to blend graded areas with adjacent natural contours. [County Design Guidelines, page 102.]

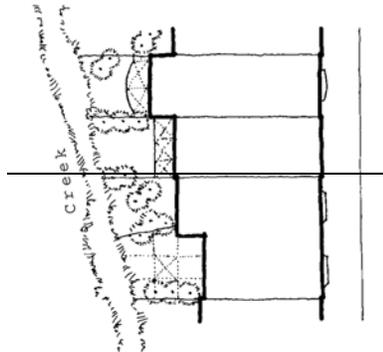
Building masses shall generally follow the contours. Where possible, large cuts and graded pads should be avoided. On sloping sites, buildings should have multiple levels and be dug into and step down the hill. All building in areas with natural slopes above 15% shall have stepped

~~foundations or other similar techniques to minimize grading associated with large building pads. [County Design Guidelines, page 102.]~~

~~————— Cut slopes shall be no higher than adjacent building heights.~~

~~***Santa Rosa Creek Frontage***~~

~~Development on any site adjacent to Santa Rosa Creek shall be designed to face the creek as well as the street. This means that buildings on sites adjacent to the creek shall have activity centers facing the creek, and facades facing the creek that are designed to the same level of detail as street-facing facades. (see Figure 22a)~~



~~————— Figure 22a: Activities Facing Creek~~

~~The creek side of any building must have openings including windows and doors. A minimum of 40% length of the building facades facing the creek must be made up of openings.~~

~~On sites adjacent to Santa Rosa Creek, trash, recyclables and storage shall not be placed in areas between buildings and the creek.~~

~~In the Old Residential District, Village Center and Bluebird Districts, all buildings shall be constructed consistent with the setback requirements identified by Chapter 4 (Area Wide Design Standards).~~

~~***Signs***~~

~~Signage shall be limited to reduce the potential for visual clutter and distraction. On-site signs are allowed according to applicable county policies, with the following allowances or limitations. Other policies in the CZLUO, area plan and Coastal Plan remain applicable and in effect.~~

~~————— **Wall signs.** Wall signs are limited to a maximum of 30 square feet for each business or tenant. Such signs shall be painted directly onto the building or be composed of individually applied letters. (see Figure 22b)~~

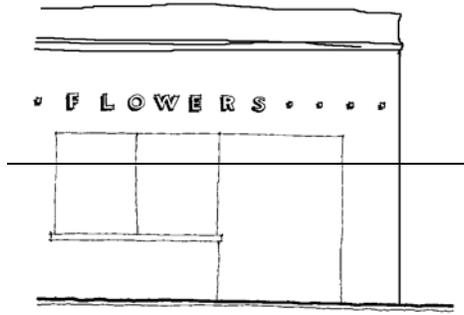


Figure 22b: Wall Signs

Suspended Signs. Suspended signs may be utilized if no projecting signs are utilized. [CZLUO allows both a suspended and projecting sign.] (see Figure 22e)

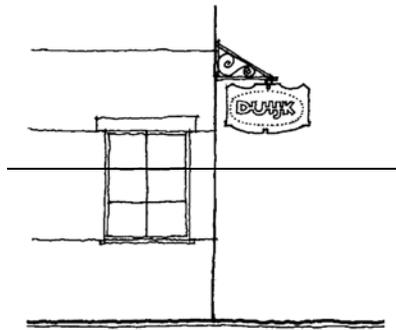


Figure 22c: Suspended Signs

Monument Signs. Each development site shall have not more than one monument sign for each of its street frontages. The monument signs are limited to 32 square feet, and shall not exceed 4.5 feet in height outside setback areas, including the base below the sign, and 8 feet in width. Such signs shall be solid at the base in order to give them a substantial appearance, and the signs shall match the architecture of the building itself. [CZLUO allows up to 60 square feet and a height of 3 feet within the setback areas.] (see Figure 22d)

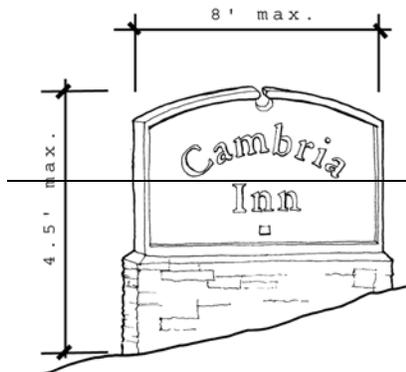


Figure 22d: Monument Signs

Projecting Signs. Projecting signs may be utilized if no suspended signs are utilized. [CZLUO allows both a suspended and projecting sign.] (see Figure 23a)

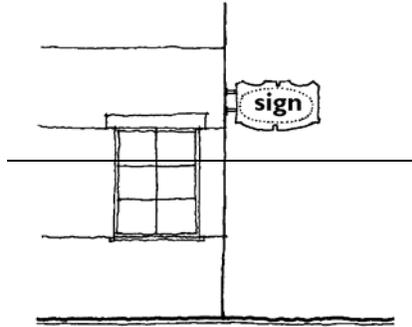


Figure 23a: Projecting Signs

Marquee signs. Marquee signs are limited to 30 square feet. [CZLUO allows up to 40 square feet.] (see Figure 23b)

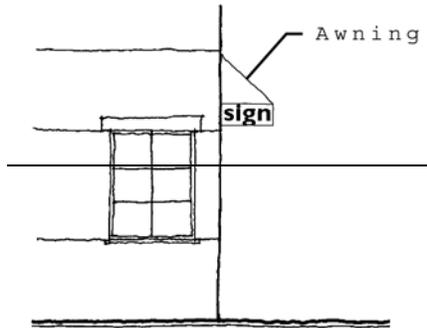


Figure 23b: Marquee Signs

Directional Signs. Also subject to land use permit requirements are signs that identify the direction of generic kinds of uses, such as “restaurants, lodging, bank” and so on, are limited to a height of five feet and area of 10 square feet. Such signs may be pole-mounted or monument design.

Window Signs. Window signs are exempt from the sign requirement of the CZLUO if the total area of such signs does not exceed 15% of the total window area, and no more than one lighted sign per windowpane.

Sign Illumination. Signs may only be externally illuminated. No neon is allowed in the East Village.

Drive-up Windows

No drive-up windows are allowed in the East Village.

Storage

All storage shall be screened by a solid wall or fencing that matches the architectural materials and style of the building. (see Figure 23c)

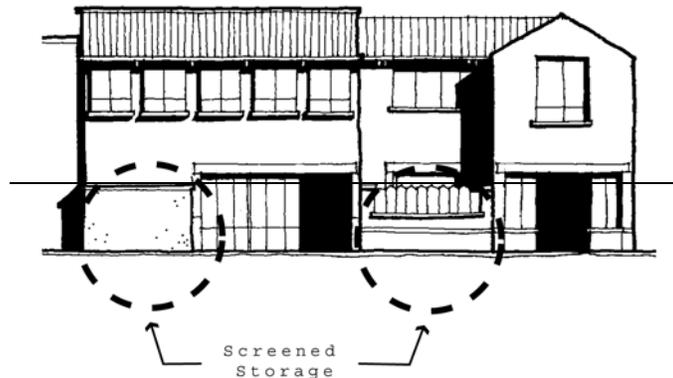


Figure 23c: Screened Storage

Landscaping

All landscaping and construction practices shall work to maintain and regenerate the Monterey pine forest and the Santa Rosa Creek riparian corridor. To this end, the following regulation shall be followed:

- Plant materials used to mimic or enhance naturally occurring vegetation should be propagated from native stock taken from an area within a 50-mile radius of Cambria to ensure that the gene pool is not diluted for endemic species. This is particularly true for Monterey pines and riparian plantings.

I. Design Guidelines

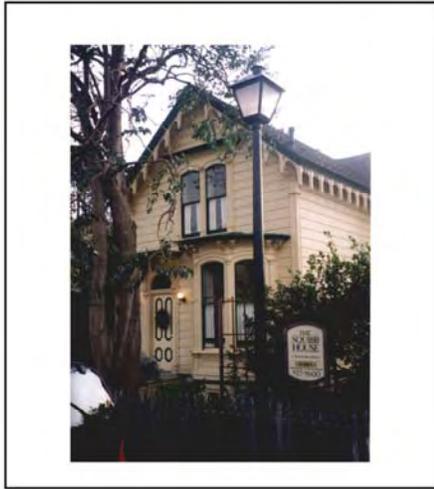
This section contains design guidelines that provide flexible policy direction for new developments in the East Village. Although compliance with these guidelines is not mandatory, this section is intended to give property owners, developers and architects a clear sense of the design that the community hopes to achieve. In other words, alternative design features that are determined to be “equal to or better” to the design guidelines described below may be approved.

Historical Precedents

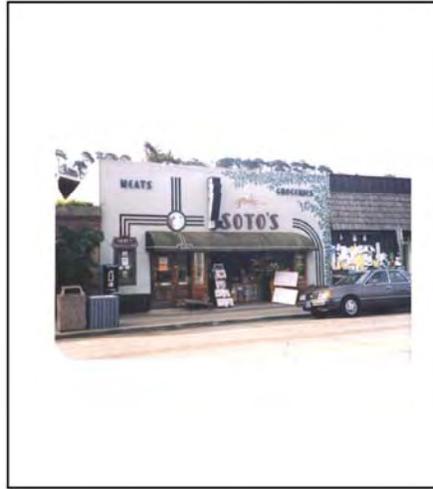
To assist in complying with North Coast Area Plan Standard 2a, the following design guidelines should be followed:

- Appropriate styles in the Village Center should include Victorian and brown shingle/craftsman. Appropriate building materials, which reflect this style, are wood and shingle siding, wood mullioned windows, and wood or asphalt shingles. (see Figure 24)

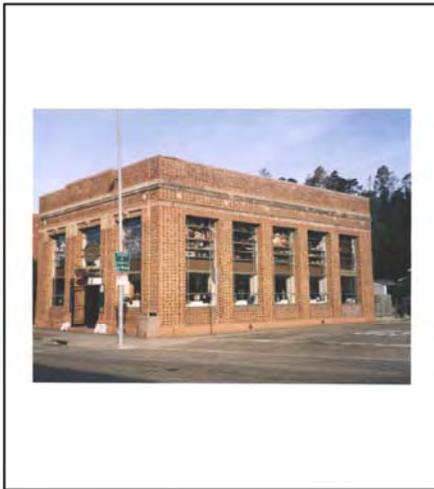
- On Main Street in the Village Center only, masonry buildings and simple cement plaster buildings with streamlined detailing, similar to the Simpson-Heller Building and Soto's Market, are also appropriate. Building materials that are reminiscent of these styles are brick masonry, tiled and stuccoed walls, large mullioned windows, glass and wood framed doors and parapet roofs. (see Figure 24)
- In the Bluebird and Old Residential District, new development and remodeling should be designed to follow the visual cues of Victorian residences, with pitched roofs, wood siding and milled wood trim. All new development should retain the residential scale and historic appearance of the Bluebird and Old Residential District. (see Figure 24)



I: Village Center



I: Village Center



I: Village Center



I: Old Residential District

FIGURE 24

DESIGN GUIDELINES

CAMBRIA DESIGN PLAN

Roofs

Both parapet roofs and exposed sloping roofs are allowed on Main Street in the Village Center. (see Figure 25a & b)

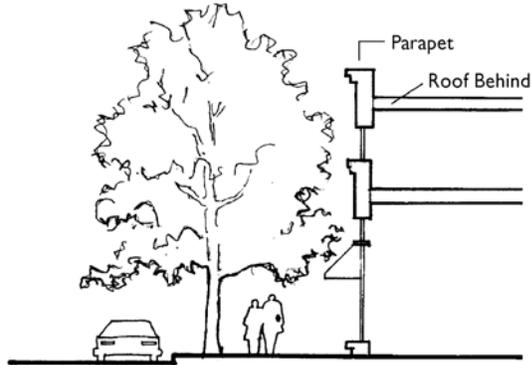


Figure 25a: Parapet Roofs (section)



Figure 25b: Parapet Roofs (Elevation)

In the remainder of the Village Center and the Bluebird and Old Residential Districts, all roofs should be exposed and sloped.

Exposed, sloped roofs should be shingled with wood appearing material or interlocking concrete shingles in soft tones of grey and brown.

Window Patterns

Ground floors of buildings in the Village Center should include large amounts of glass and other openings to promote visual permeability. (see Figure 25c)

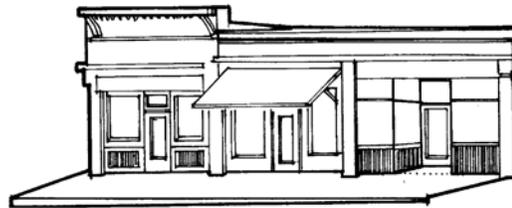


Figure 25c: Visual Permeability

Upper floors should have smaller window openings punched into solid walls. (see Figure 25d)

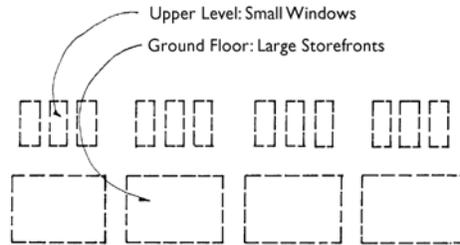


Figure 25d: Small Windows in Upper Floors

Windows should have clear glass.

Window frames should be painted or stained wood.

Doors

Building entries should occur within each building bay. (see Figure 26a)

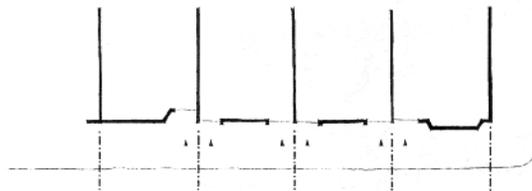


Figure 26a: Building Entries

Openings for doors should not be more than 6 feet wide by 8 feet tall. An exception are the doors at the existing Soto's Market, which are wider. (see Figure 26b)

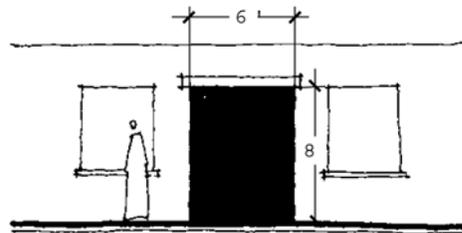


Figure 26b: Maximum Door Size

Building entries should be accentuated by moldings, lighting, overhangs or recessed alcoves.

Landscaping

Private landscaping in the East Village should be drought tolerant due to the community’s limited access to water.

In the Old Residential District, the Bluebird District and along the east side of Burton Drive, landscaping on private property should be reminiscent of the old residences, with more plant material elements than hardscape elements and without turf.

In the remainder of the Village Center, private landscaping should be contained in built-in or movable planters that match the architectural style of the building.

Sloped parcels on the up slope side of Main Street should landscape the hillsides to stabilize the slope and to create a visual buffer between the development up slope and Main Street below.

Parking

The paved area of any parking site should be no more than necessary to efficiently accommodate the required number of parking spaces. (see Figure 26c)

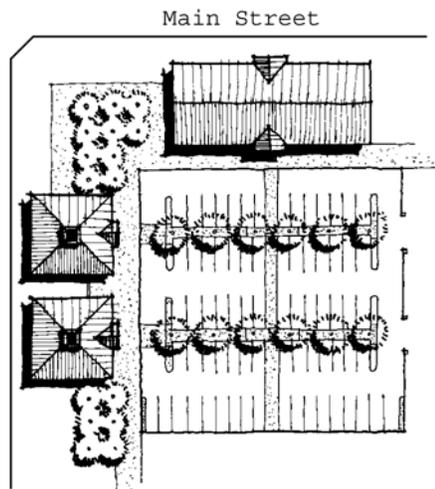


Figure 26c: Minimize Paved Area of Parking

Parking surfaces should be permeable wherever possible. Permeable surfaces and on site drainage will reduce the need for storm drainage infrastructure and allow storm water purification through the soil, reducing pollution of ground water. (see Figure 26d)

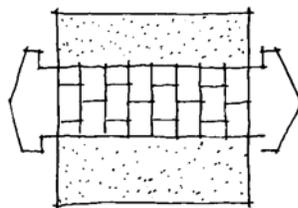


Figure 26d: Permeable Parking Surfaces

Clearly delineated pedestrian pathways should be provided within large parking lots. (see Figure 26e)

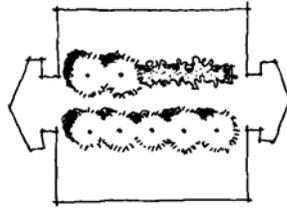


Figure 26e: Pedestrian Pathways

Parking lots should be linked by alleys or off-street connections where ever possible.

J. Key Private Developments

New development on several privately-held parcels will help enhance a pedestrian oriented East Village. This section suggests how future private development might occur.

Main Street/Burton Drive Intersection

The parking lots serving the Bank of America and Cambria Bakery represent two of the four parcels of property adjacent to the East Village's "100% corner" at the intersection of Main Street and Burton Drive, and a third parcel adjacent to the intersection is vacant. This pattern of development creates a lack of definition for this important corner.

In order to better define this corner, construction of two new retail buildings would be appropriate on both of these sites as illustrated in Figure 27. These buildings should face Main Street and leave adequate room for parking behind as illustrated in Figure 27a. The additional parking requirements for each of these buildings could be met off site in one of the three new and improved shared parking lots.

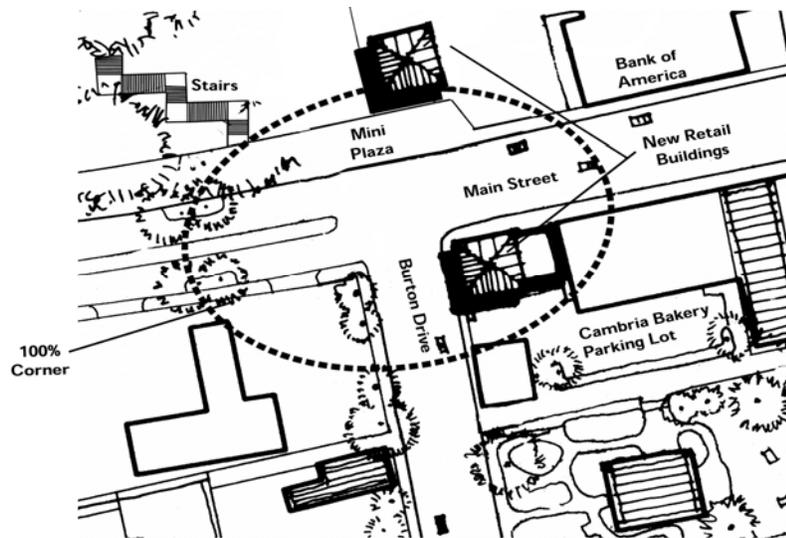


Figure 27a: Main Street/Burton Drive Intersection

Vacant Lot on Bridge Street

The vacant parcel on Bridge Street just north of Main Street represents the transition zone from commercial development (and the Village Center) to single family homes (and the Old Residential District). It is a logical location for a new medium-density residential development or lodging.

This new development should front Bridge Street, while auto access and back yards could front onto Wall Street. It should be scaled and designed to reflect surrounding residential development as depicted in Figure 27b.

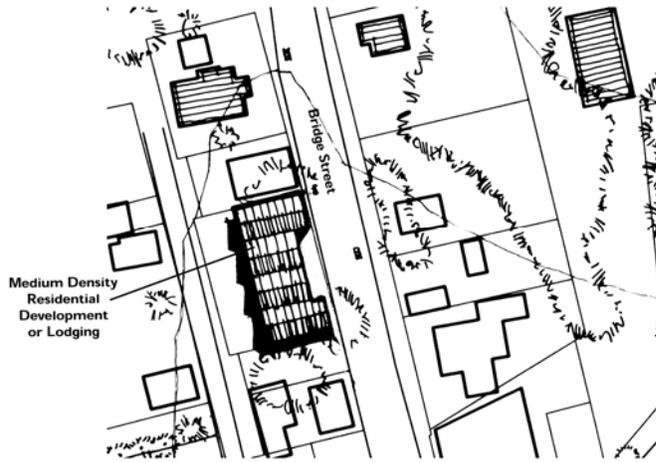


Figure 27b: Vacant Lot on Bridge Street

Williams Ranch House Parcel

This 7.2 acre parcel is the prominent location of the first of a line of charming older homes that are seen when entering the village of Cambria from the Eastern gateway. The hillsides in back of the ranch house have varied slope aspects that are visually prominent from Main Street in both directions.

New development on this strategic site should preserve and enhance the rural country charm of this property, and act as a transition from the rural character of East Village to the agricultural character of Santa Rosa Creek Road. The existing house should be preserved and enhanced if feasible, or replaced with a house including its front porch and roof lines, with additions kept behind the existing front elevation, to reflect its design elements, details, proportions, and relative location. New residences should be included along Main Street, continuing the procession of houses from East village, utilizing historical design elements, proportion, and detail to convey an old fashioned, turn-of-the-century appearance.

Hillside development should protect the natural aspect of the area by minimizing the number of houses on the hills, with residential farmhouse character, utilizing subdued colors, and agricultural, barn-like character, utilizing natural exposed wood exteriors to the rear. Structures should be tightly clustered in period fashion, to minimize the impact of development, and emphasize the open field character of the property. The flatter hillside areas should be selected for structures, which should have stepped foundations and building profiles or a low-profile design following existing contours to reflect the

topography, and complement the specific site characteristics of each individual location. Landscaping should be strategically located to minimize the prominence of structures seen from public roads.

Eastern Gateway

The parcel south of Main Street at the edge of the East Village is the first developed site that people see as they enter Cambria from the east or south. Located in the Old Residential District, it is within five minutes walking distance of the Village Center. It is currently zoned for medium density residential development. This site is currently under consideration for a senior housing facility.

It is important that any building on this site enhance the eastern gateway by respecting the riparian corridor of Santa Rosa Creek and the historic residential development nearby. As depicted in Figure 28, the building footprint should be broken up into small sizes that replicate the pattern of the district and development should occur along the road and not near the creek. The buildings should be built close to Main Street to match the setback of the adjacent building and to emphasize the edge of development of the village. Parking should be placed behind the building. The riparian corridor should be enhanced with plant species that are discussed in the Gateway section at the beginning of the chapter.

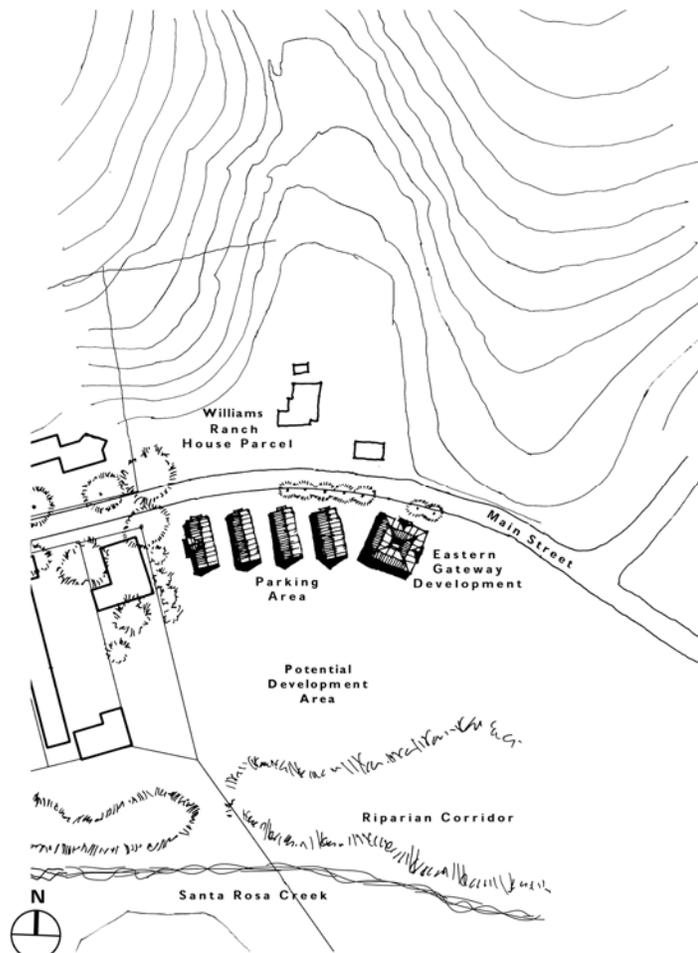


Figure 28: Private Parcel

Lot Next to Santa Rosa Cemetery

This L-shaped, undeveloped lot allows the green frame of the pine forest to extend into the village and create a transition zone between the Old Residential Neighborhood and the Village Center. The open character of the site along Main Street is very important to the character of the East Village. This parcel, which is currently designated for commercial/retail use, should be redesignated as multi-family residential in order to create a transition from the Village Center district to the multi- and single-family uses of the Old Residential District.

As illustrated in Figure 29a, any development on this site should respect the topography and the forest, thus all significant trees and Monterey Pines should be preserved. Such development should be located only on the upper portion of the site, with access from behind the Duncan House if possible.

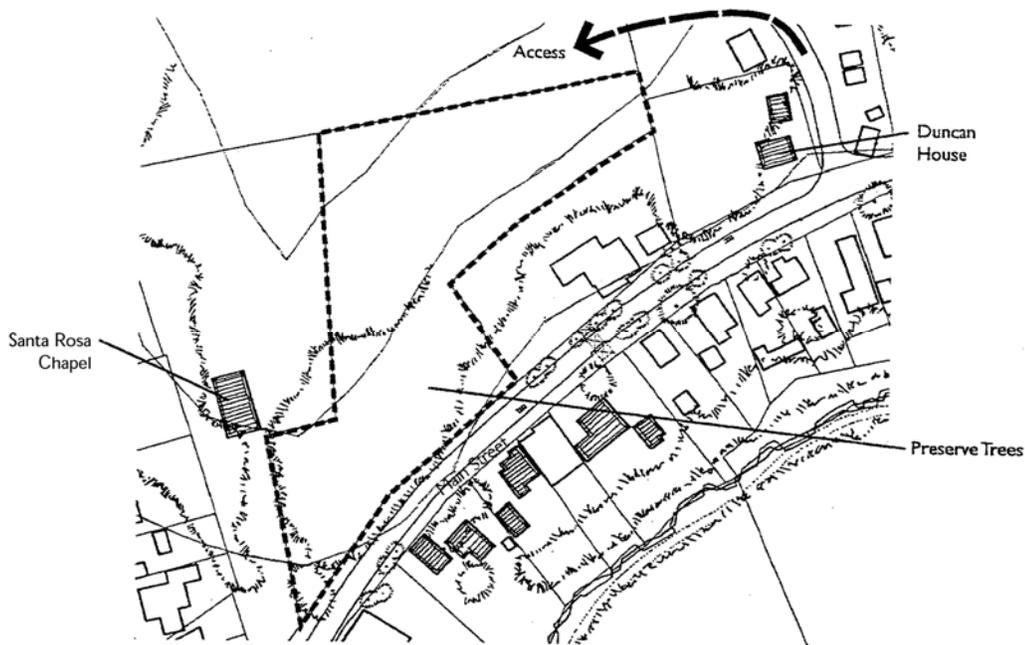


Figure 29a: Lot next to Santa Rosa Cemetery

Up-Hill Lots North of Main Street

Several parcels zoned for commercial use are located above Main Street between Burton Drive and Bluebird Lane. Any development on these sites should respect the topography and the forest, and should be concentrated on flatter areas at the tops of the bluffs, removed from Main Street. As illustrated in Figure 29b, a shared driveway could provide access from Main Street to the flatter sites, which include the “Secret Meadow” and “Old School Site”. These parcels are generally devoid of trees, and their slopes should be re-forested to create tree cover over the sites and provide a visual buffer between the development and Main Street, thus maintaining the “green frame” backdrop to the East Village. Site development should respect the topography with minimal additional flat-pad grading.

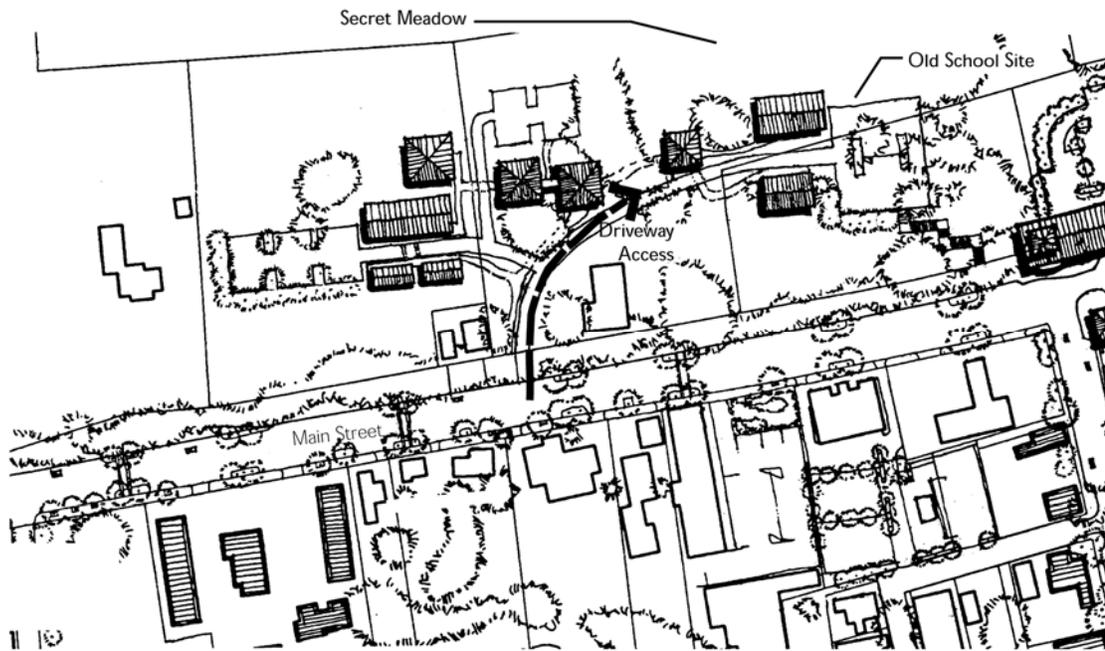


Figure 29b: Up-Hill Lots North of Main Street

VI. MID-VILLAGE

This chapter focuses on the properties fronting Main Street between the East and West villages. As depicted in Figure 30, the Mid-Village lies on the crest of the hill separating the two villages and is characterized by a mix of uses, large parcel sizes, and a lack of building presence on Main Street. This area deserves special attention in this Design Plan because of its prominent location between the two community centers of Cambria and because of its lack of definition. This chapter includes a list of goals for Mid-Village, descriptions and illustrations of proposed streetscape improvements, development Standards and design guidelines for private development, a discussion of design development along Santa Rosa Creek, and ideas for development on key sites.

A. Goals for Mid-Village

This Design Plan seeks to fulfill the following goals for Mid-Village:

- Enhance the aesthetic qualities of Mid-Village.
- Encourage buildings to have a street presence along Main Street where topographically feasible.
- Improve the streetscape of Main Street through plantings and paving.
- Improve safety for pedestrians, bicyclists and autos.
- Ensure high quality in the design of new structures along Main Street.
- Improve the relationship of the Mid-Village's built environment to Santa Rosa Creek and maintain a no build zone around the creek.
- Reflect slopes in planning and design.
- Enhance the visual relationship between Main Street and Highway One.

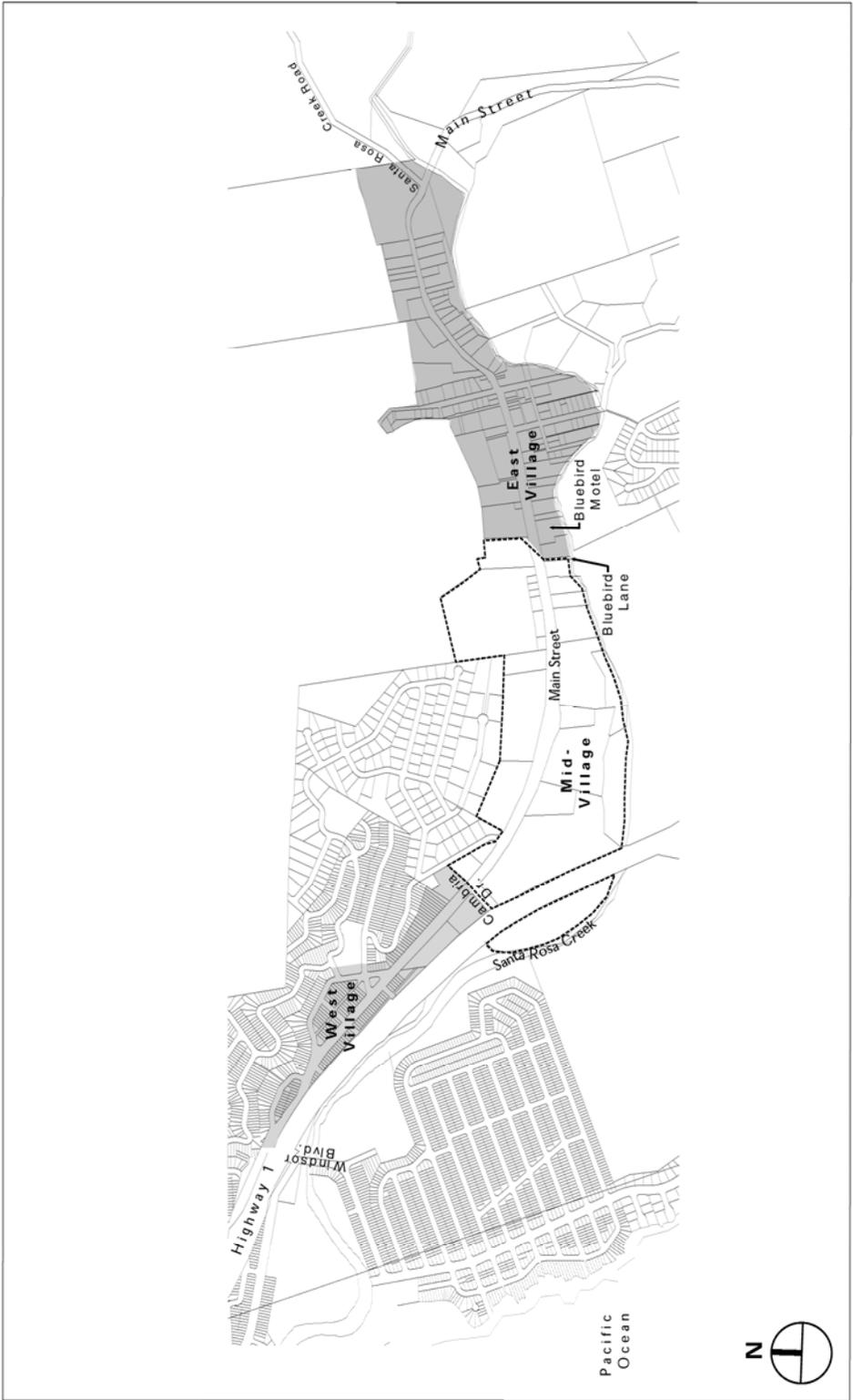


FIGURE 30
 LOCATION OF MID-VILLAGE
 CAMBRIA DESIGN PLAN

B. Main Street Streetscape

The strip of Main Street between the East and West Villages is a wide swath of asphalt with inconsistent sidewalks and no street lighting, furnishings or plantings. A portion of the section of Main Street in the Mid-Village is two lanes, the remainder of the section currently is made up of two traffic lanes with a left-hand turn lane between the lanes.

As illustrated in Figures 31b and 31c, the street would be reconfigured to consist of a travel lane with a bikeway in each direction and a multi-use trail on the down-slope side of the street. An informal clustering of Monterey pines would be planted on the hillsides on the north side of the street, while portions of the down-slopes south of Main Street would be planted with native trees, creating a naturalistic corridor between the villages in this lush, hilly and rural streetscape setting.

Figure 31b shows the proposed configuration of the portion of Main Street with a shared left-turn lane, which would be developed with a landscaped median, while Figure 31c shows the configuration where no left-turn lane occurs.

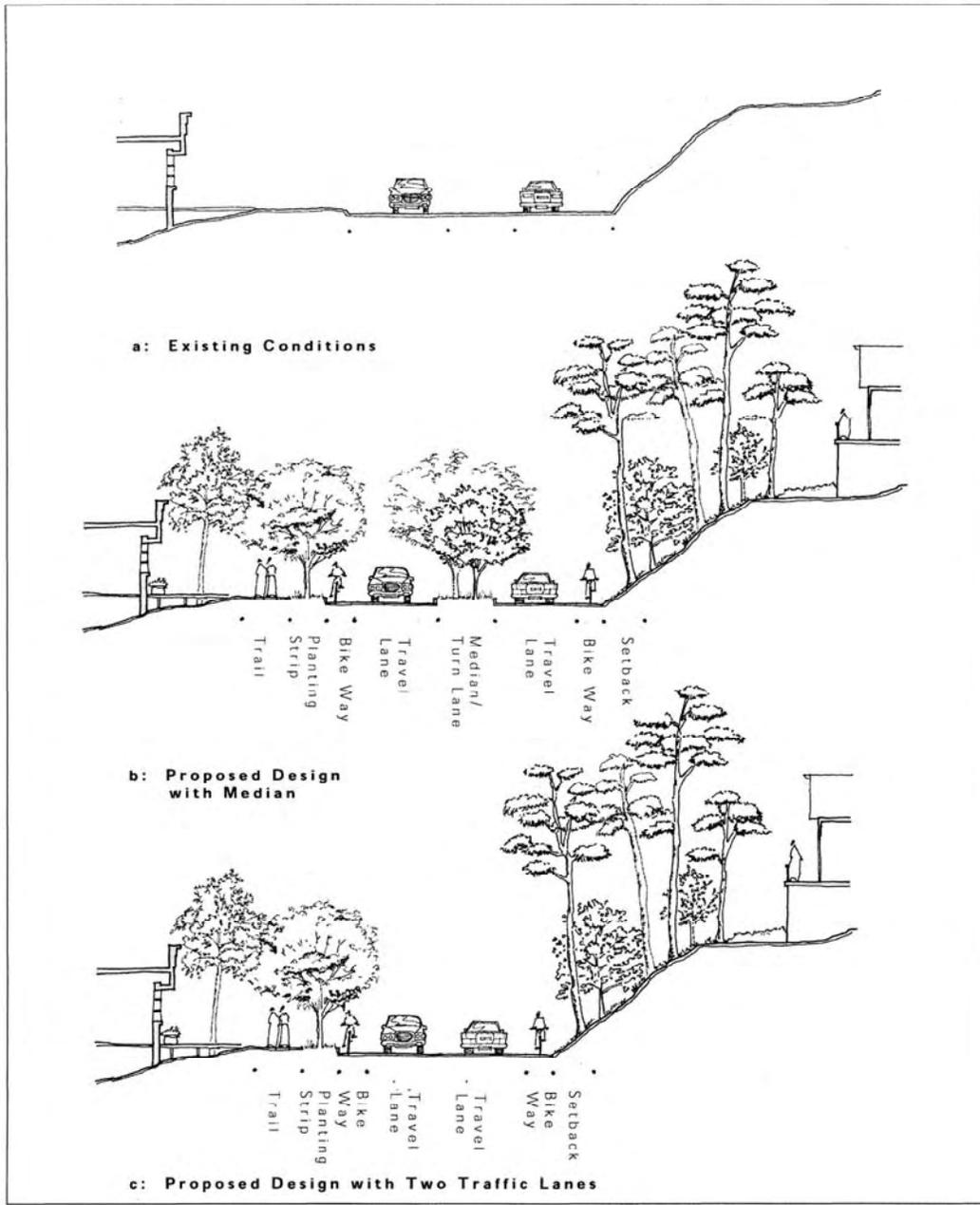


FIGURE 31

MAIN STREET IN THE MID-VILLAGE

CAMBRIA DESIGN PLAN

C. Development Standards

This section includes development standards that are requirements in Mid-Village. While the subsequent section includes design guidelines that are intended to be advisory only, the Standards in this section are so important to the success of this area between the two villages that compliance with them is mandatory for all new development.

These Standards attempt to preserve and enhance the landscape features of Mid-Village, making them the most dominant feature in the surrounding natural and built environment. Appropriately scaled buildings would be the most dominant feature of the built environment with each building's necessities, such as parking and storage, carefully sited and designed to serve its building and blend into the surrounding environment.

Building Orientation

This section is intended to ensure that buildings face Main Street with their interior activity areas visible from the street.

Permeability and Activity. The Main Street facade of all commercial/office buildings shall be the "front," detailed with windows and doors and the main pedestrian entry to each space. (see Figure 32a)



Figure 32a: Building Orientation

Building Height

Building heights in the Mid-Village area should be limited in order to maintain the existing rural character in which the natural landscape, not the built environment, is the dominant feature in the environment.

~~Allowed Height.~~ All buildings in this district, except buildings on the up-slope parcels, in relationship to Main Street, shall not exceed 25 feet. (see Figure 32b)

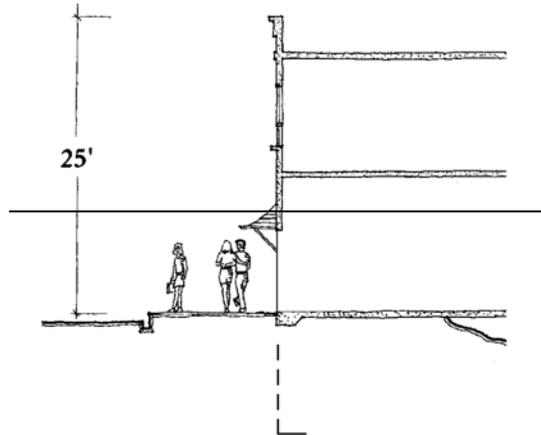


Figure 32b: Allowed Height

~~Allowed Height on Up-Slope Sites.~~ All buildings on the up-slope parcels, in relationship to Main Street, shall not exceed 20 feet. [This standard supersedes and replaces those in the Planning Area Standards for height of commercial retail buildings on Main Street in the North Coast Area Plan.] (see Figure 32c)

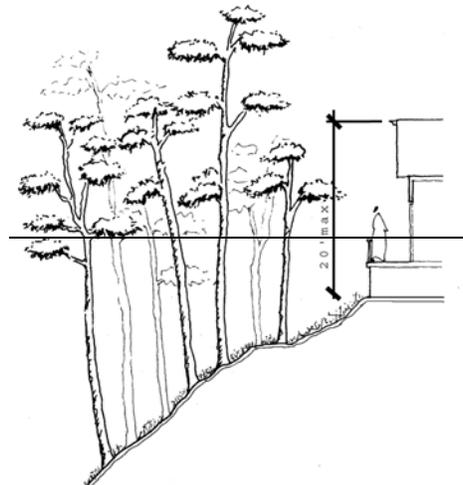
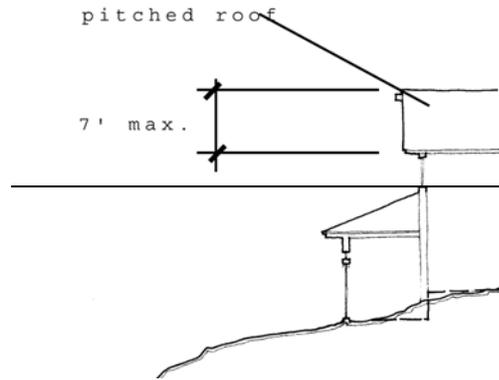


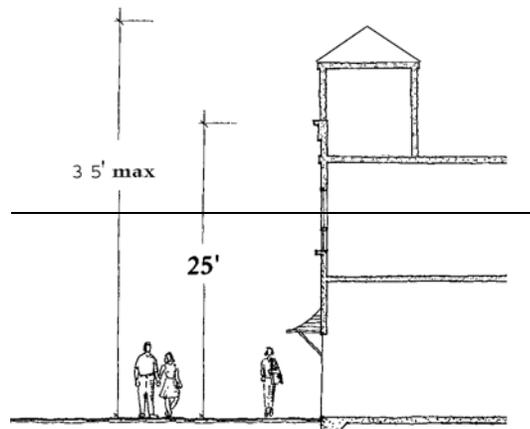
Figure 32c: Allowed Height on Up-Slope Sites

~~**Additional Height for Pitched Roofs.** As a means to encourage sloped roofs, the peak of a sloped roof may be up to 7 feet taller than the maximum building height of 20 or 25 feet (for a total of 27 or 32 feet), provided that the slope is 6:12 or greater. (see Figure 32d)~~



~~Figure 32d: Additional Height for Pitched Roof~~

~~**Additional Height for Non-Habitable Structures.** This height restriction does not include non-habitable, architectural building elements such as clock towers and steeples. These elements shall not exceed 35 feet. (see Figure 33a)~~



~~Figure 33a: Additional Height for Non Habitable Structures~~

~~Measurement.~~ Building height is measured at every individual point on a site from the natural grade to the top of the building directly above that point. (see Figure 33b)

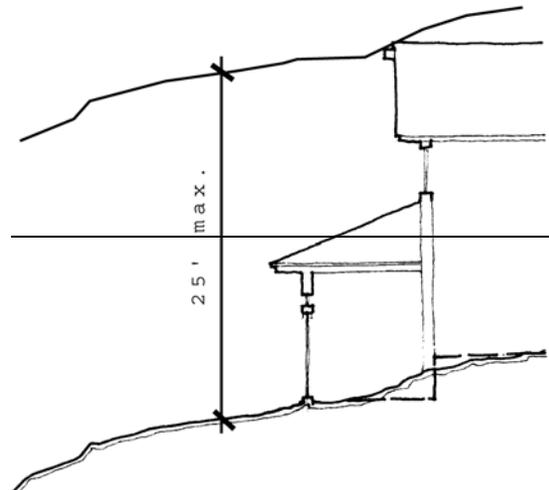


Figure 33b: Measurement

Setbacks

All new development and/or remodeling in Mid-Village shall be consistent with adjacent setbacks.

~~Front Setback for Down-Slope and Cross-Slope Sites.~~ A minimum 15-foot front setback is required for all parcels that are not up-slope sites in relationship to Main Street. Exception may be made for the Mid-State Bank site in order to allow for buildings to be built to the sidewalk, if this proves feasible. [The CZLUO requires a 10-foot setback in all commercial buildings not located in the Central Business District.]

~~Front Setback for Bluff-top Development Up-Slope Sites.~~ All development on up-slope sites, in relationship to Main Street, shall be setback 15 feet behind the face of the bluff. This distance is generally a minimum of 50 feet from the front parcel line. [The CZLUO requires a zero setback for all commercial buildings located in the Central Business District.] (see Figure 33c)

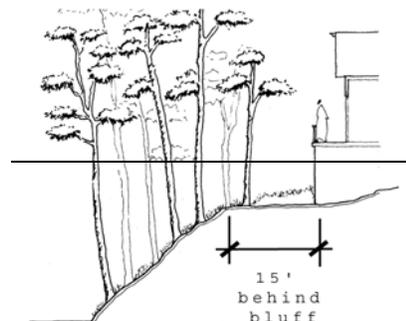


Figure 33c: Front Setback for Bluff-Top Development Up-Slope Sites

~~Side.~~ A minimum 10-foot side setback is required of all buildings in this district. The minimum setback is to be increased one foot for each three feet of building height above 12 feet. [The CZLUO allows zero setback.]

~~Rear.~~ A minimum 20-foot rear setback is required for all buildings on the down-slope side of Main Street. For parcels adjacent to Santa Rosa Creek this setback shall be offered for dedication for a public right-of-way. [The CZLUO requires zero setback for commercial and ten-foot setback for Office and Professional.] (see Figure 33d)

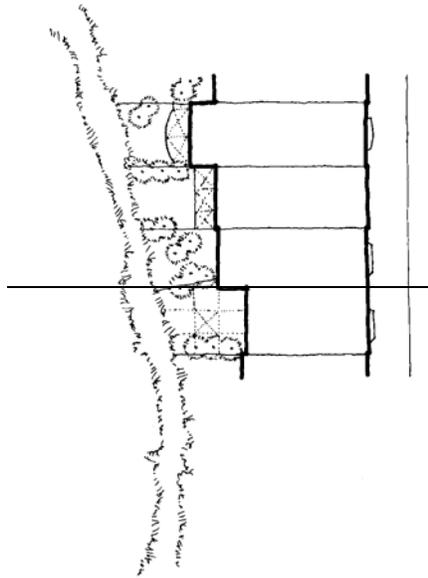


Figure 33d: Rear

Roofs

In order to preserve Cambria's charm as an historic and rural community, it is essential that roof lines are indicative of the vernacular shapes that were developed in the historic community.

~~Minimum Roof Pitch.~~ Any pitched roof shall have a minimum 3:12 pitch.

Service areas

~~Service areas shall be placed away from Main Street, Santa Rosa Creek and other public open space.~~

Parking

~~Parking lots shall be carefully sited in order to preserve the rural character of Cambria.~~

~~Siting.~~ Parking shall be located behind buildings where possible. (see Figure 34a)

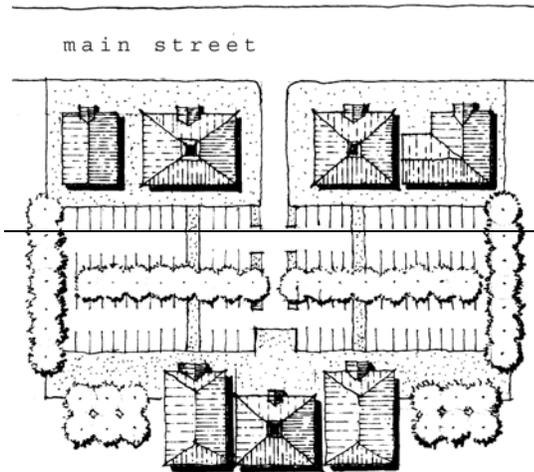


Figure 34a: Siting

Highway One Buffer. A visual buffer shall be provided to mitigate views of parking areas from Highway One. Such buffers could include landscaping or additional commercial buildings. (see Figure 34b)

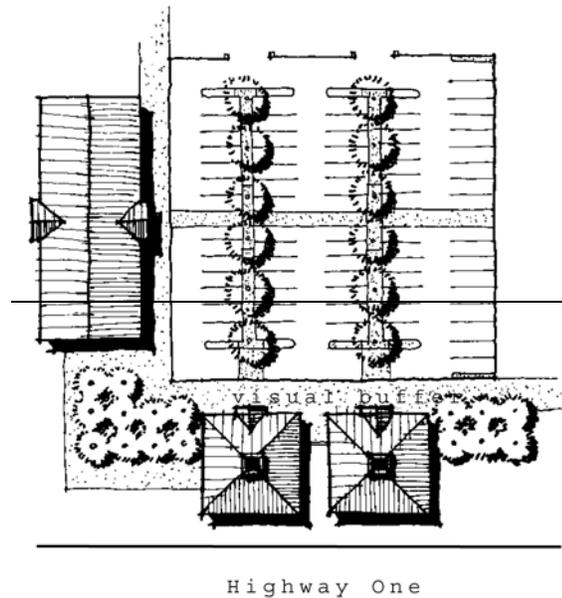


Figure 34b: Highway One Buffer

Pedestrian Connections. Where parking is located behind buildings, clearly delineated pedestrian connections to Main Street shall be provided.

Santa Rosa Creek Frontage

Development on any site adjacent to Santa Rosa Creek shall be designed to face the creek as well as the street. This means that buildings on sites adjacent to the creek, and facades facing the creek shall be designed to the same level of detail as street-facing facades. (see Figure 34c). All buildings shall be setback from the edge of the riparian vegetation consistent with the requirements identified by Chapter 4 (Area Wide Design Standards).

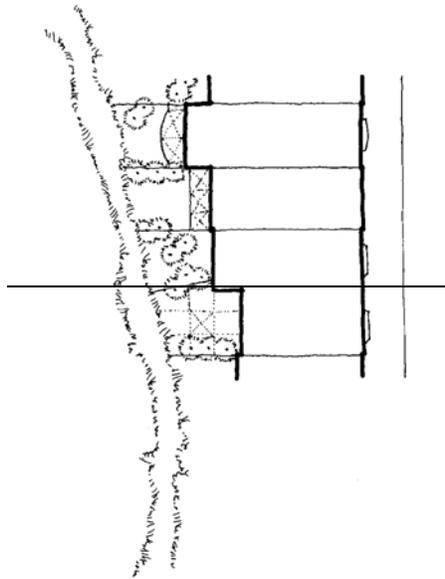


Figure 34c: Santa Rosa Creek Frontage

Signs

Signage shall be limited to reduce the potential for visual clutter and distraction. On-site signs are allowed according to applicable county policies, with the following allowances or limitations. Other policies in the CZLUO, area plan and Coastal Plan remain applicable and in effect.

Wall signs. Wall signs are limited to a maximum of 30 square feet for each business or tenant. Such signs shall be painted directly onto the building or be composed of individually applied letters. [CZLUO limits to 15% of the wall area, up to 80 square feet] (see Figure 34d)

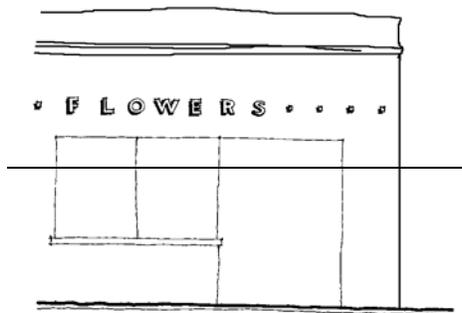


Figure 34d: Wall Signs

Suspended Signs. Suspended signs may be utilized if no projecting signs are utilized. [CZLUO allows both a suspended and projecting sign.] (see Figure 35a)

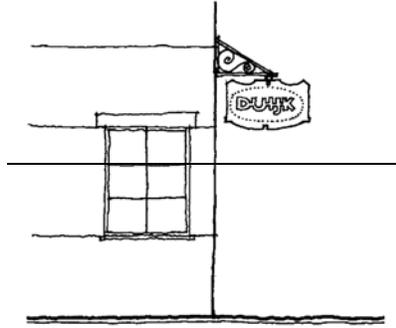


Figure 35a: Suspended Signs

Monument Signs. Each development site shall have not more than one monument sign for each of its street frontages. The monument signs are limited to 32 square feet, and shall not exceed 4.5 feet in height outside setback areas, including the base below the sign, and 8 feet in width. Such signs shall be solid at the base in order to give them a substantial appearance, and the signs shall match the architecture of the building itself. [CZLUO allows up to 60 square feet and a height of 3 feet within the setback areas.] (see Figure 35b)

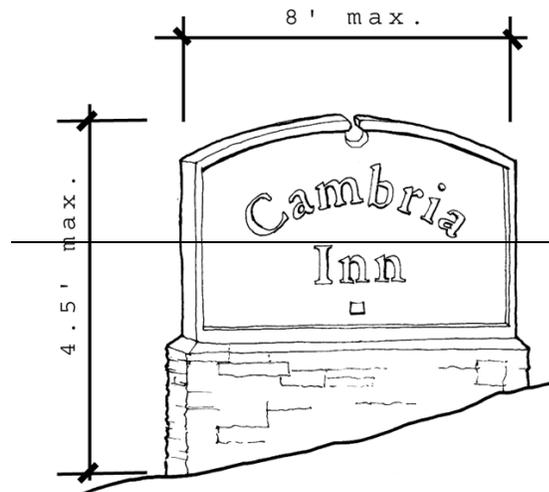


Figure 35b: Monument Signs

Projecting Signs. Projecting signs may be utilized if no suspended signs are utilized. [CZLUO allows both a suspended and projecting sign.] (see Figure 35e)

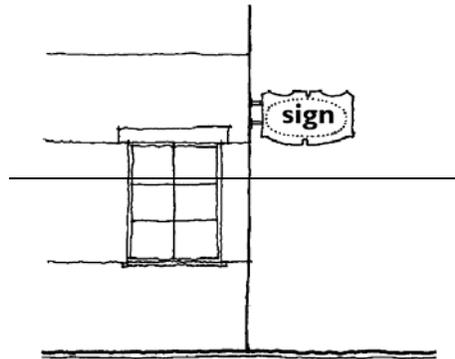


Figure 35c: Projecting Signs

Marquee signs. Marquee signs are limited to 30 square feet. [CZLUO allows up to 40 square feet.] (see Figure 35d)

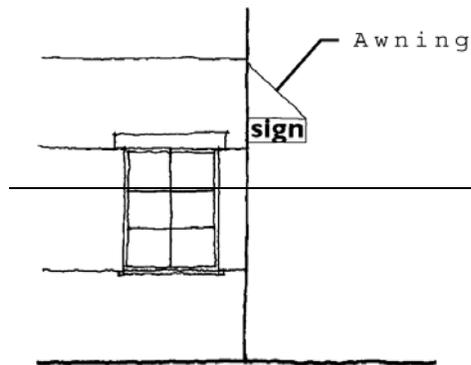


Figure 35d: Marquee Sign

Directional Signs. Also subject to land use permit requirements are signs that identify the direction of generic kinds of uses, such as “restaurants, lodging, bank” and so on, are limited to a height of five feet and area of 10 square feet. Such signs may be pole-mounted or monument design.

Window Signs. Window signs are exempt from the sign requirement of the CZLUO if the total area of such signs does not exceed 15% of the total window area, and no more than one lighted sign per windowpane.

Sign Illumination. Signs may only be externally illuminated. No neon is allowed in the Mid-Village.

Drive-up Windows

No new drive-up windows are allowed in the Mid-Village area. Existing drive-up windows may be relocated or reconfigured on a given site.

Storage

This standard places limitations on storage in order to visually conceal it and blend it with the surrounding physical environment. This is intended to minimize the potential for visual clutter and distraction from the buildings and surrounding landscape.

———— All storage shall be screened by a solid wall or fencing that matches the architectural materials and style of the building. (see Figure 36a)

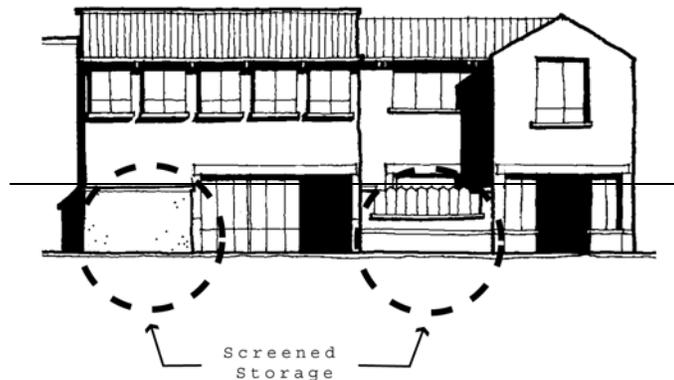


Figure 36a: Storage

Development on Sloping Sites

Cambria is typified by hillsides and pines, which are important to the visual quality of the community. Therefore, this section addresses development on sloping sites.

Grading Limitations. Grading and buildings shall be designed to follow the natural slope by orienting and aligning with the existing contours.

Second Story Setbacks. On up-slope parcels, upper floors shall be setback a minimum of ten feet from the exterior wall directly below them. The upper floor roof shall not extend more than two feet into the setback. (see Figure 36b)

———— **Sub-floor Areas.** The greatest distance between the lowest floor and finished grade shall not exceed six feet. Such areas shall be covered with finished walls or appropriate architectural screening and not be left open. The addition of landscaping can further improve the appearance of the building and reduce apparent mass. (see Figure 36b)

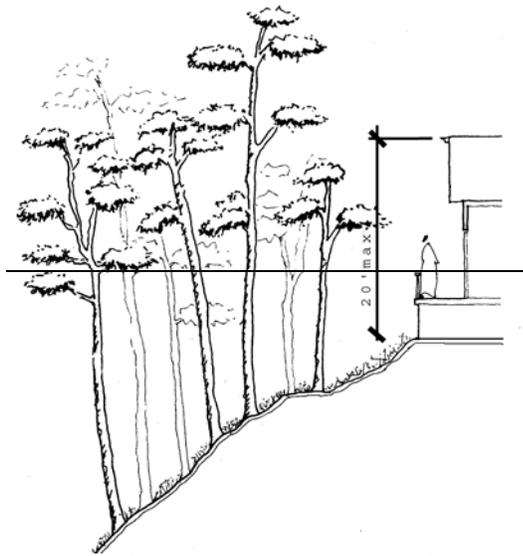


Figure 36b: Second Story Setback and Sub-Floor Areas

Landscaping

All landscaping and construction practices shall work to maintain and regenerate the Monterey pine forest and the Santa Rosa Creek riparian corridor. To this end, the following regulation shall be followed:

- Plant materials used to mimic or enhance naturally occurring vegetation should be propagated from native stock taken from an area within a 50-mile radius of Cambria to ensure that the gene pool is not diluted for endemic species. This particularly true for Monterey pines and riparian plantings.

D. Design Guidelines

This section contains design guidelines that provide flexible policy direction for new developments in the Mid-Village area. Alternative design features that are determined to be “equal to or better” may also be approved, since this section is intended to give property owners, developers and architects a clear sense of the design that the community hopes to achieve. Compliance with these guidelines is not mandatory, but is strongly encouraged.

Building Massing

Buildings in Mid-Village should be limited in perceived volume so that they do not dominate the landscape and create a suburban big-box environment.

Facades. Building facades should be articulated with changes in plane, materials and design increments of not more than 20 feet.
(see Figure 37a)

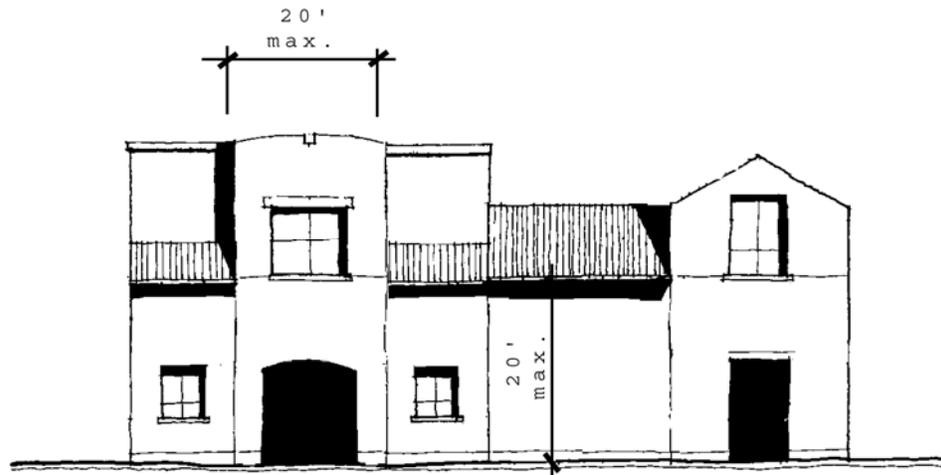


Figure 37a: Building Massing

Height Variation. Building mass should be articulated with changes in building height in horizontal increments of not more than 30 feet.

Gross Structural Area. Where buildings are more than one-story, the second floor should be no greater than 60% of the building footprint, except on buildings fronting Main Street where down slopes are so steep that the second story of the building is the ground floor on Main Street, in which case the second floor may be 100% of the building footprint. (see Figure 37b)

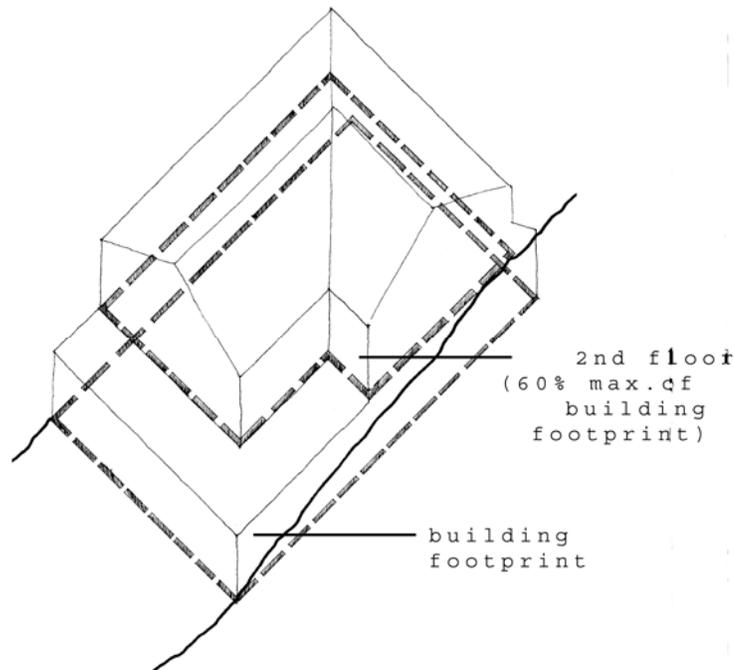


Figure 37b: Gross Structural Area

Building Articulation

In order to preserve Cambria’s charm as a rural community, it is essential that building facades have some detailing that enhances this character and lessens the potential for “big-box” retail type facades.

Facades. All facades should include significant three-dimensional components, such as canopies, awnings, cornices or eaves, to cast shadows and create visual interest to passing motorists and pedestrians. (see Figure 37c)

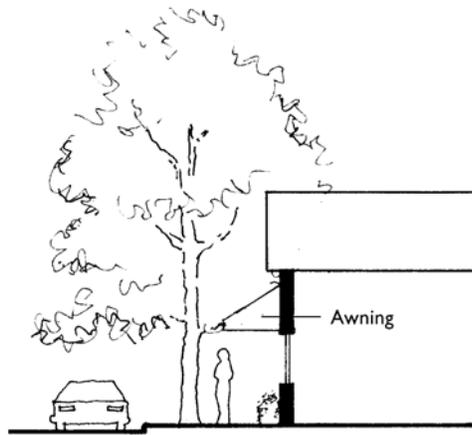


Figure 37c: Facades

Roofs

Roof shape is an important way that buildings fit into a district. These guidelines are intended to ensure that buildings in Mid-Village are developed with appropriate roof shapes.

Roof shapes. Simple pitched roofs such as gables or hips are preferred over flat roofs on commercial buildings. [Similar to County Design Guidelines, page 35.] (see Figure 38a)

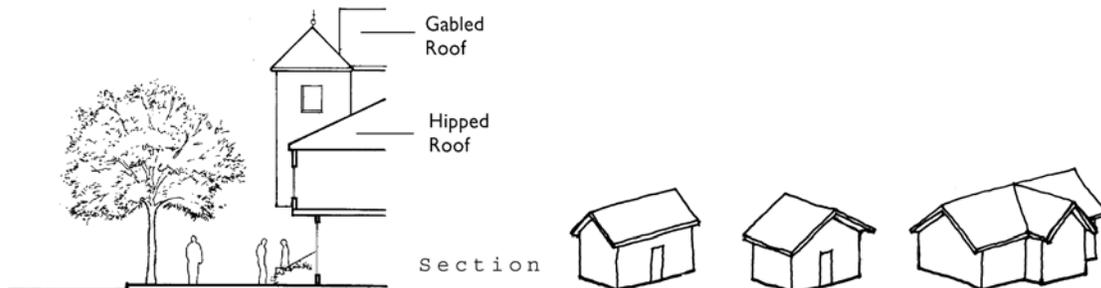


Figure 38a: Gabled and Hipped Roofs

Parapet Roofs. Where buildings have flat roofs, parapets should be articulated with changes in elevation, roofline cornices and recesses to create visual interest and accentuate important components of the building facade. [Similar to County Design Guidelines, page 35.] (see Figure 38b)

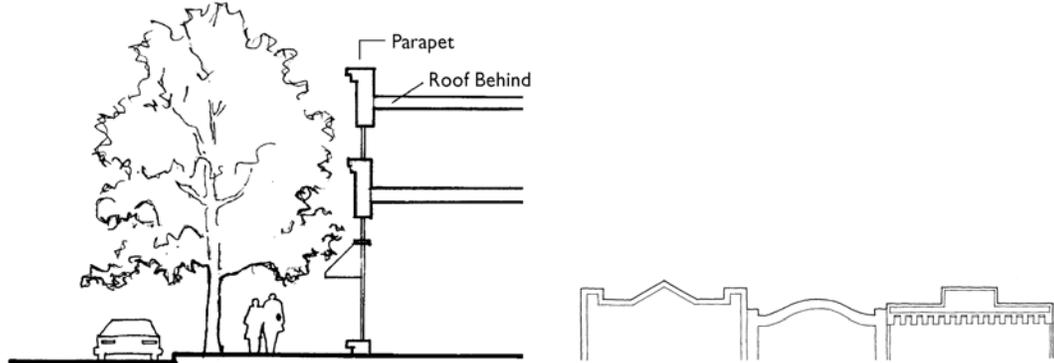


Figure 38b: Parapet Roofs

Roofing Materials. Exposed roofs should be shingles with wood shakes, wood-appearing, or interlocking concrete shingles in tones of grey and brown.

Parking

Parking lots should be designed with respect to the pedestrian, the surrounding built environment and the natural environment as well as the car.

Visible components. Driveways should be the only paved portion of a parking area visible from Main Street.

Main Street Buffer. In some instances parking may occur on Main Street. Where it does occur, a landscape buffer of at least 15 feet should be provided to minimize views of parked cars from the street. On down-slope sites the visual buffer should be high enough to block views of the parking from Main Street. On up-slope sites the parking lot should be setback 15 feet from the top of the bluff. (See Figure 38c)

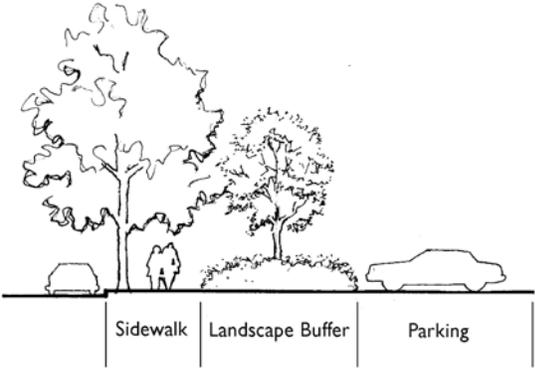


Figure 38c: Main Street Buffer

Paving. The paved area of any parking should be no more than necessary to efficiently accommodate the required number of parking spaces and walking.

Permeable Surfaces. Parking surfaces should be permeable wherever possible. Permeable surfaces and on-site drainage will reduce the need for storm drainage infrastructure and allow storm water purification through the soil, reducing pollution of ground water. (see Figure 38d)

Driveway Width. Driveways to parking lots should be no wider than 20 feet.

Pedestrian Pathways. Clearly delineated pathways should be provided within large parking lots. (see Figure 38d)

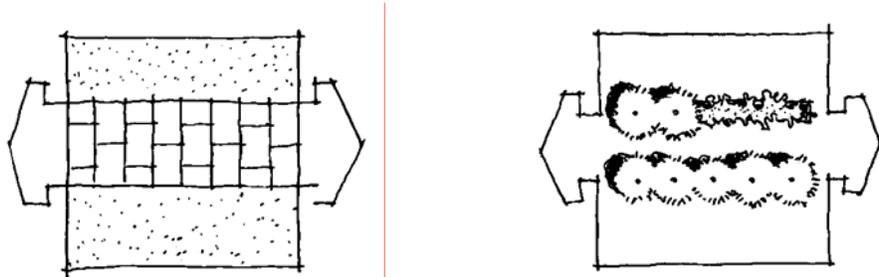


Figure 38d: Parking Lots – Paving and Permeable Surfaces & Parking Lots – Pedestrian Pathways

Grading and Contouring

In order to preserve the character of the hillsides, sensitive and intelligent grading policies should be practiced.

Finish Grading. New graded slopes should be configured to retain the natural character of the site. New contour lines should be rounded to mimic natural contours. [Similar to County Design Guidelines, page 103.] (see Figure 39a)

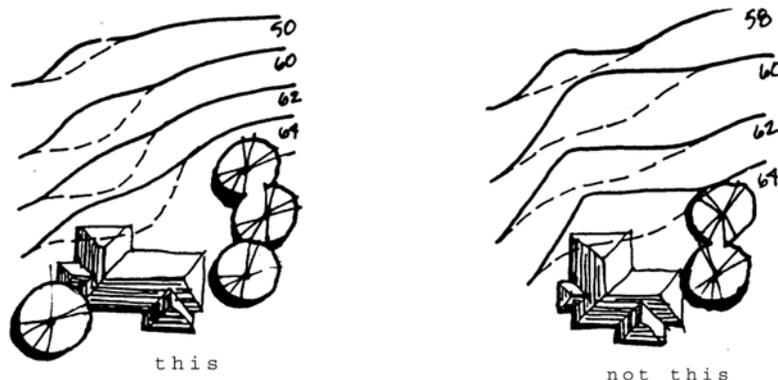


Figure 39a: Finish Grading

Graded Slopes. Artificial slopes that are visible to the public should match the natural slopes in the immediate vicinity. [County Design Guidelines, page 103.]

Feathering. Graded areas should be “feathered” so that there are no abrupt transitions between flat areas and graded slopes, or between graded and ungraded areas. [County Design Guidelines, page 103.] (see Figure 39b)

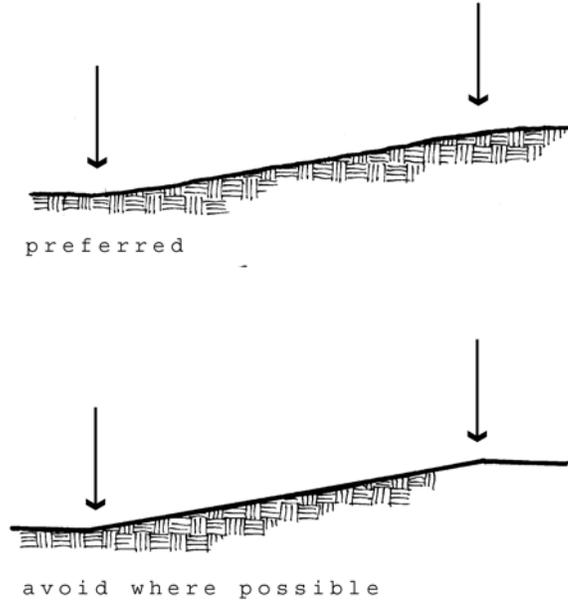


Figure 39b: Feathering

Development on Sloping Sites

The hillsides are one of the dominant physical features that create the setting in which Cambria exists. Preservation of the hillsides and careful, unobtrusive development is essential in maintaining an attractive backdrop for Cambria.

Site Layout. Where the majority of the site is sloping, the flatter areas should be utilized for parking rather than building footprints. Where buildings abut both sides of a parking lot, the difference in ground floor elevations, where feasible, should not be such to cause the parking lot between them to have a slope greater than a 2%. (see Figure 40a)

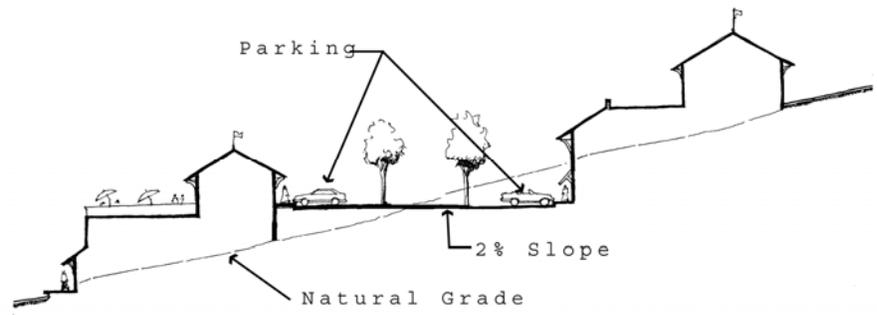


Figure 40a: Development of Sloping Sites

Grading Limitations. Site grading should generally be limited to areas within and adjacent to the building footprint, parking areas and driveways, and where necessary due to unusual site conditions, or where necessary to blend graded areas with adjacent natural contours. [County Design Guidelines, page 102]

Contour Development. Building masses should generally follow the contours. Where possible, large cuts and graded pads should be avoided. On sloping sites, buildings should have multiple levels and be dug into and step down the hill. All buildings in areas with natural slopes above 15% should have stepped foundations or other similar techniques to minimize grading associated with large building pads. [County Design Guidelines, page 102.]

Cut slopes. Cut slopes should be lower than adjacent building heights. A tall grading cut may be appropriate behind a building if out of public view.

Retaining Walls. Where retaining walls need to be greater than four feet in height from natural grade to top of wall, they shall be stepped down a slope, rather than designed as a single vertical wall. If it is necessary to step the walls, the horizontal step must be a minimum of three feet in width to provide adequate space for landscaping. A tall retaining wall may be appropriate behind a building if out of public view. [Similar to County Design Guidelines, page 77.] (see Figure 40b)

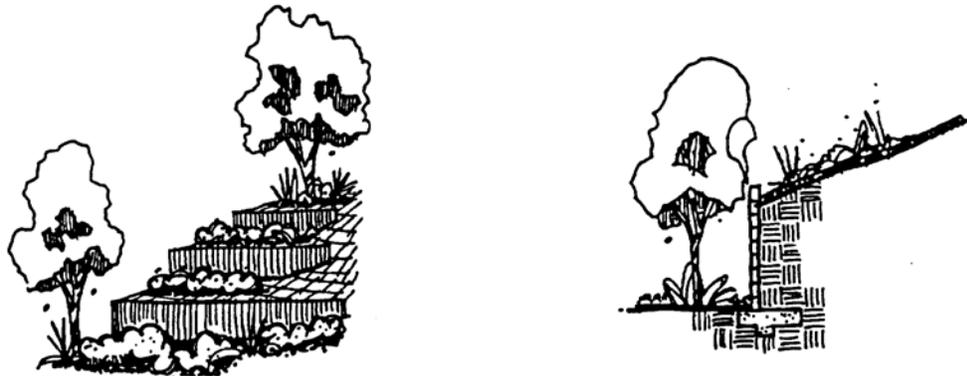


Figure 40b: Retaining Walls

Landscaping

Private landscaping should work with the local climatic opportunities and constraints rather than against them. Private landscaping should accentuate the less “urban” quality of this district in comparison to the two villages.

Preferred Materials. The private landscape should be compatible with the Monterey pine forest habitat and shall be lush, naturalistic, native appearing and highly textured in height and massing.

Up-Slope Parcels. On up-slope parcels landscaping should be utilized to screen and reduce the apparent mass of the building.

Trees. All new development should incorporate trees behind buildings to help mitigate the building's mass and to help integrate the parcel into the surrounding natural landscape.

E. Santa Rosa Creek

This section is strictly guidance for parcels that are adjacent to Santa Rosa Creek in the Mid-Village. The conceptual information provided below would need to be fine-tuned for agency jurisdiction and permitting requirements for each of the individual parcels.

This creek, which is a significant open space resource, runs through the small valley between the Cambria hillsides. Mid-Village, which lies on the north side of the creek, has most of its significant parcels on the creek side of Main Street. In order to preserve the creek and enhance its connection and use as public open space, development between Main Street and the creek should consider the creek as a resource and should consider the constraints of building next to a creek, required by the Coastal Zone Land Use Ordinance [Riparian Setbacks, page 7-26 and PAS].

New development should provide outdoor public open space fronting the creek, and link this space with an access easement to neighboring properties, where possible to provide a sense of vitality in the area and eyes on the creek. On large parcels, retention basins to hold creek waters at high water times should be integrated into the development when possible. Property owners of parcels that are adjacent to Santa Rosa Creek may be subject to other agency reviews and requirements.

F. Key Parcels

Mid-State Bank

The Mid State Bank site, located at the corner of Cambria Drive and Main Street, is by far the largest vacant commercial parcel in Cambria. It is a logical location for new retail and service development.

In order to continue the pedestrian-oriented development character of Cambria's commercial villages, new development on the Mid-State Bank site should be designed to reflect a pedestrian friendly environment by locating the fronts of some buildings near the corner of Main Street and Cambria Drive. The development should include low profile pedestrian scaled buildings and outdoor spaces, such as courtyards and large sidewalks. Further, the new development should provide spaces with seating and pedestrian connections to the adjacent public sidewalks.

In addition to being located at the Cambria Drive Gateway to the community, the Mid-State Bank site is located in the 100 year flood plain of the Santa Rosa Creek. Before any development is approved for this site, a hydrological analysis should be conducted to determine what areas on the site are appropriate for development. This parcel is also designated for a trail in the Cambria Master Plan.

Vacant Parcels in the Proximity of Cambria Village Square

A large parcel located just west of Cookie Crock Market, an undeveloped parcel just east of Cambria Village Square, and the upper portion of the Cambria Village Square parcel are all zoned for commercial retail and are undeveloped. Any development on these lots should respect the steep topography and should be concentrated on the flatter area above Main Street, with access from Knollwood and Tamson

Streets. Development should not be located directly on Main Street if cutting into the slope is necessary to create a building site.

On all of these parcels the slope adjacent to Main Street is generally devoid of trees, and should be reforested to create tree cover over the undeveloped portions of each site and down to Main Street as described in Section B of this chapter.

VII. WEST VILLAGE

This chapter focuses on the West Village. It includes a list of design goals for the Village; a description and illustrative design for streetscape improvements, gateways, open space, circulation and parking; development standards and design guidelines.

A. Goals for the West Village

This Design Plan seeks to fulfill the following goals for the West Village:

- Enhance the identity of the West Village, its districts and its boundaries.
- Encourage commercial development to blend with the existing built context.
- Develop open spaces and a circulation pattern that is connected with the rest of the community's open spaces and enhances the West Village.
- Improve the Main Street streetscape through plantings, street lighting, limited to those necessary for public safety, and other furnishings.
- Retain, improve and add to parking supply in the West Village.
- Take into consideration slope, geology and hydrology.

B. Design Districts

For the purposes of this chapter, the West Village has been divided into four design districts as shown in Figure 42. These districts have different physical characteristics, land use patterns and design features. Different design treatments are proposed in each of the three districts. The four districts are:

- Gateway District. This district, at the far northwestern end of Main Street near Kent Street and Highway One, is the entry to Cambria and the West Village from the north. It is more auto-oriented than the rest of the Village.
- Central District. This district makes up the “heart” of the West Village, running along Main Street between the two legs of the Cornwall Street “U”. This area is characterized by pedestrian-oriented shops facing Main Street, with diagonal parking on the street.
- Civic District. This district makes up the southeastern segment of Main Street, beyond the southeastern leg of Cornwall Street. It includes existing civic uses like the Vet’s Memorial Hall, and the Pinedorado grounds.
- Local Serving District. This district consists of the “back street” area along Cornwall Street, which is typified by local-serving businesses and a few residences.

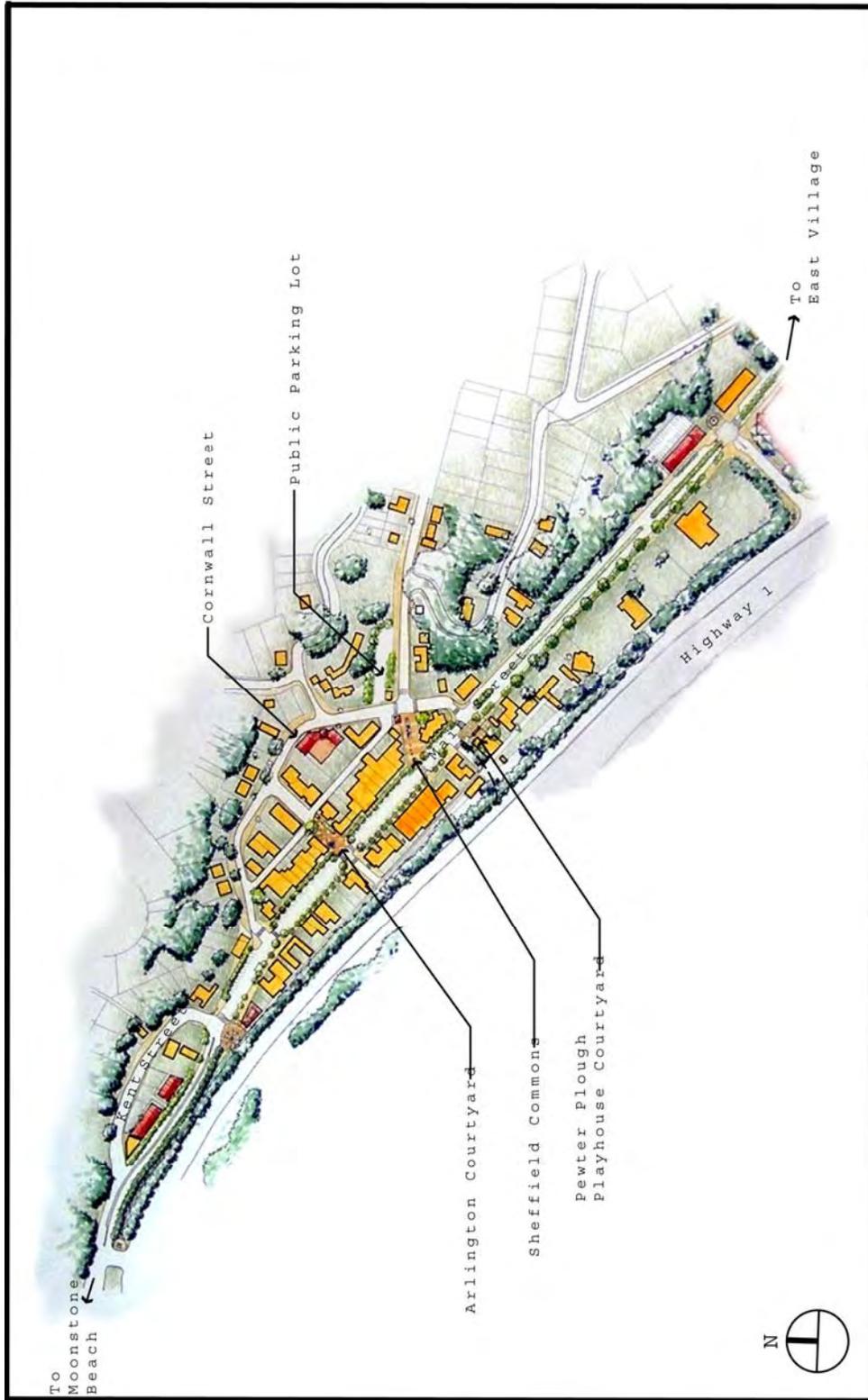


FIGURE 41

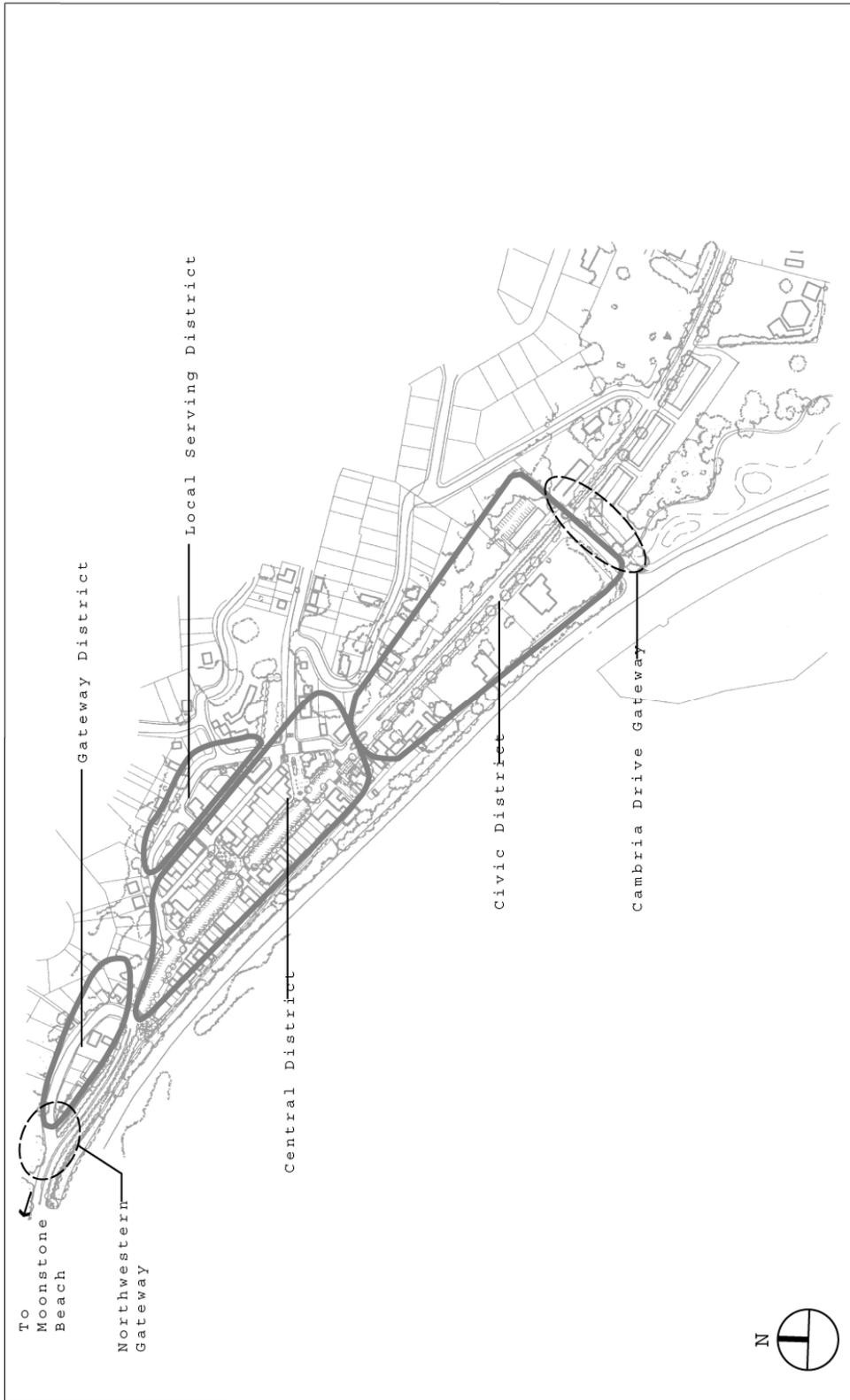


FIGURE 42

DISTRICTS AND GATEWAYS
OF WEST VILLAGE
CAMBRIA DESIGN PLAN

C. Gateways

The West Village has two key gateways: the Northwestern Gateway near the intersection of Windsor Boulevard/Main Street/Highway One and the short segment of Cambria Drive between Main Street and Highway One. The locations of these gateways is shown in Figure 42.

Northwestern Gateway

The Northwestern Gateway would be defined by a series of design interventions, which are described below and illustrated in Figure 43.

- A marker that would be repeated at all entries into Cambria would be located near the intersection. As described in Chapter 5, this marker would be designed to evoke the central California coast and act as a physical logo to the community. At this gateway, the marker would be placed at the end of the finger of land between Main Street and Highway One at Windsor Boulevard.
- New street trees planted on both sides of Main Street would provide a vertical frame to this entry.
- New trees and shrubs would be planted in the Highway One right-of-way to buffer the ambience of the West Village from the traffic on the highway.
- A new building with a small plaza would be constructed on the triangular parcel on the south side of Main Street. The plaza would provide the adjacent commercial space with open space for such uses as café seating.
- Expanded open spaces extending into the street would be installed in front of the Main Street Grill and in front of the Juice Bar. These expanded open spaces and the plaza mentioned above would create a neck down on Main Street, slowing traffic as it leaves Highway One and enters the pedestrian area of the West Village.
- If possible, efforts should be made to work with the owners of the existing Exxon station to lower the existing sign in this gateway area. Although the height of the sign was required to be reduced by a coastal development permit for a car wash in 1999, more subdued signage would be more in keeping with the visual image that this Plan seeks to create for Cambria. Although the height of the sign was required to be reduced by a coastal development permit for a car wash in 1999, the County may be able to work with Exxon as part of the permitting process for any proposed expansion or change at the service station facility.

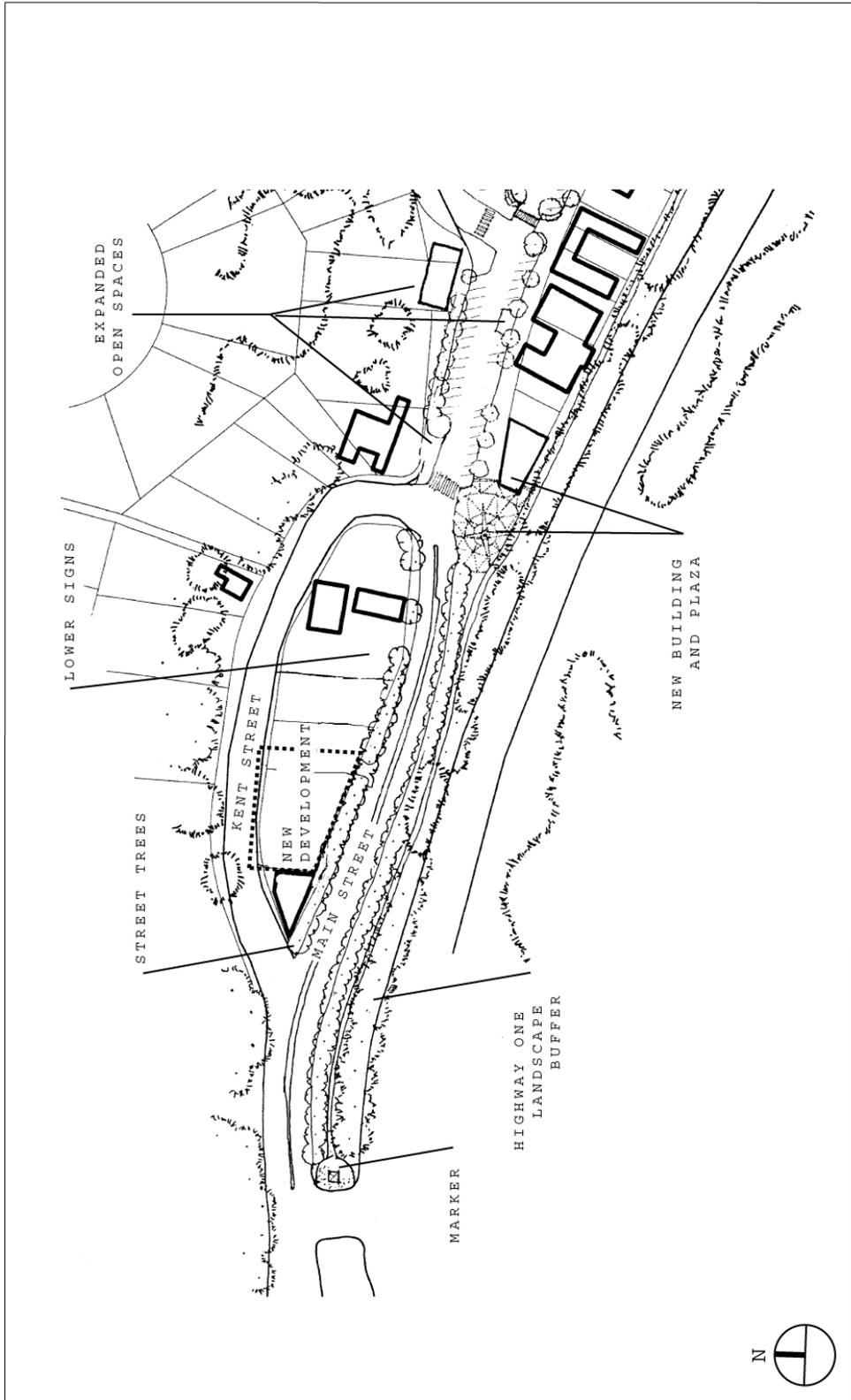


FIGURE 43

NORTHWESTERN GATEWAY

CAMBRIA DESIGN PLAN

Cambria Drive Gateway

Just as with the Northwestern Gateway, the Cambria Drive Gateway would not consist of a single object; instead, it would include a choreographed series of “events” as depicted in Figure 44 and described below.

- The Cambria Drive gateway would be emphasized as the main gateway into the West Village for motorists arriving from the north on Highway One through a community identification sign north of Cambria on Highway One. This sign would be similar to the community identification sign described for the southern end of Cambria in the East Village Chapter.
- Two markers similar to the marker at the Northwest Gateway would be installed at the corner of Cambria Drive and Highway One.
- Behind the markers and along Cambria Drive would be newly planted trees on the Vet’s Hall property and a building and trees on the Mid-State Bank site. These trees and the building would provide a sense of enclosure to the roadway and frame the intersection with Main Street.
- A statue or plaza could be constructed on the northern side of the Cambria Drive/Main Street intersection.
- ~~• As illustrated in the inset graphic, a roundabout instead of a T-intersection with a traffic signal may denote the entry into Cambria.~~
- Additional Monterey pines and Monterey pine understory would be planted on the hill slope beyond the intersection of Cambria Drive and Main Street.
- A crafted direction sign at the intersection of Cambria Drive and Main Street would identify the West and East Villages and other important destinations within the community.

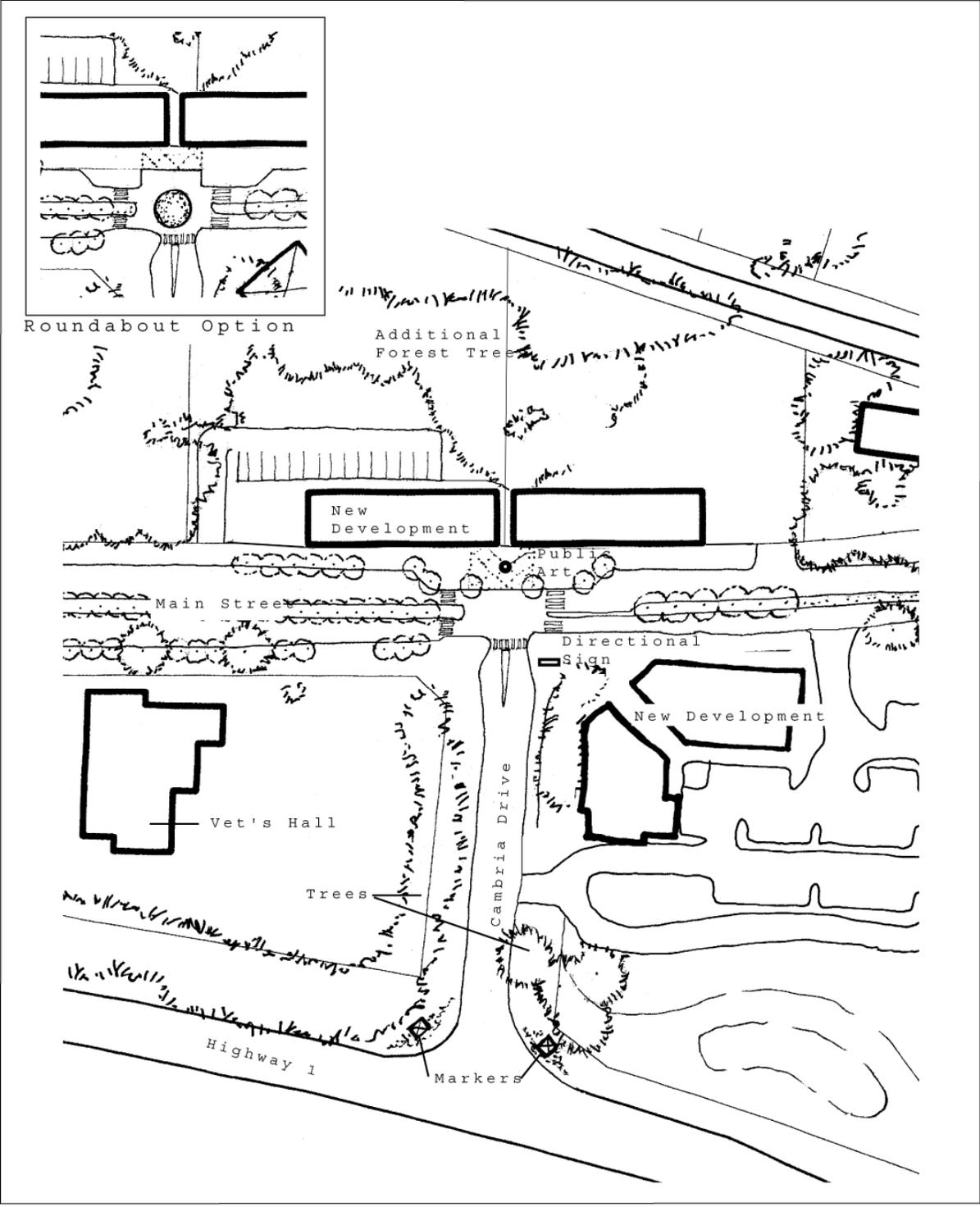


FIGURE 44

CAMBRIA DRIVE GATEWAY

CAMBRIA DESIGN PLAN

D. Main Street Streetscape

Currently, the streetscape along Main Street in the West Village consists of two traffic lanes, diagonal parking and narrow and inconsistent sidewalk widths. Crosswalks, street trees and public pedestrian street furnishings do not exist on Main Street.

The streetscape improvements proposed in this Plan would work within the existing right-of-way of Main Street. The improvements would consist of different elements in each of the three districts, with the most emphasis on the Central District, as described below:

Gateway District

In the Gateway District, street trees would line the two-lane road as it curves into the Central District. Decomposed granite would be placed at the base of the trees creating a pedestrian pathway along Main Street in this district. Bulbouts in front of Main Street Grill and near the juice bar would be installed to narrow the width of the roadway as a means to slow traffic before entering the pedestrian oriented section of the West Village.

Central District

In the Central District, streetscape improvements would include cross walks with bulbouts, wider sidewalks, new street furniture, street trees and a flexible parking zone, as illustrated in Figure 45a.

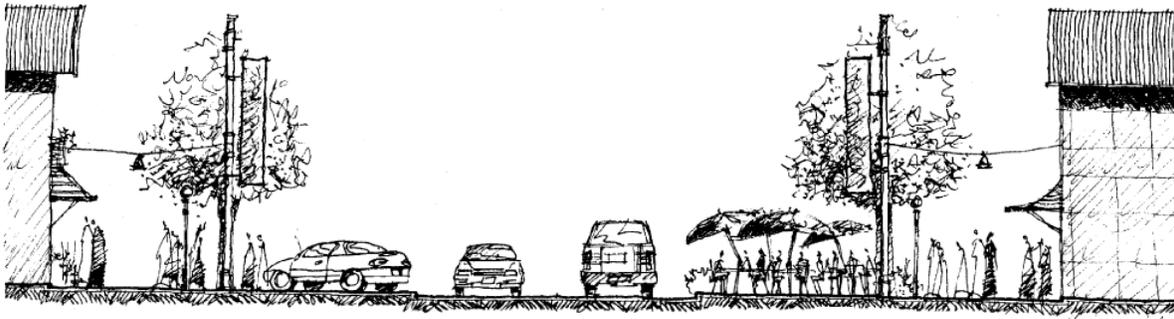


Figure 45a: Section of Main Street at Arlington

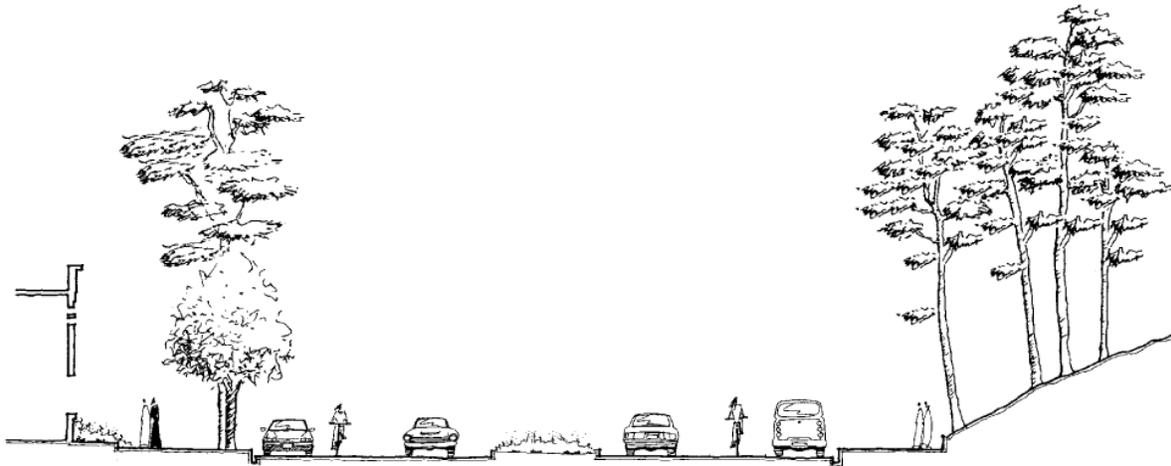


Figure 45b: Section of Main Street at Veteran’s Memorial Hall

Individual elements of this streetscape design are described below:

Street Trees. Street trees should be planted in the Central District of the West Village, with the choice of tree based on regional climate, microclimates created by the trees, water requirements, signage visibility and canopy heights, the openness of the tree, and aesthetic and ecological compatibility with the Monterey Pine that is prevalent in Cambria. Trees for this area would be specified in a streetscape design plan discussed in the Chapter 9.

Sidewalk Widening. The existing sidewalks on Main Street in the Central District are very narrow, particularly on the south side of the street if it can be shown that sidewalk widening will not impede traffic. Sidewalks on both sides of the street should be widened, to create a total width of 11 feet of sidewalk on each side of the street. This can be done by narrowing the width of the road, without any loss of diagonal parking. The travel lanes on Main Street would become narrower, which would slow traffic incrementally, but which would not cause any other problems.

Bulbouts. “Bulbouts” extending from the sidewalk into the diagonal parking area should be installed at intersections along Main Street in the Central District as illustrated in Figure 46a. Such bulbouts would help to narrow the apparent width of the street, allowing for an easier pedestrian crossing, and adding visual relief to the long, straight alignment of Main Street. Bulbouts could accommodate outdoor café seating and trees to add visual interest to the street right-of-way. An illustration is included in Figure 46a.

Flexible Parking Zone. The diagonal parking area in the Central District along Main Street could be re-paved as a “flexible zone” that is appropriate for both parking and other uses as shown in Figure 46a. Such a flexible transition zone would be three inches higher than the roadway, but still lower than the sidewalk. It would allow for flexible use as either diagonal parking or as outdoor space for restaurants or cafes during the warmer seasons.

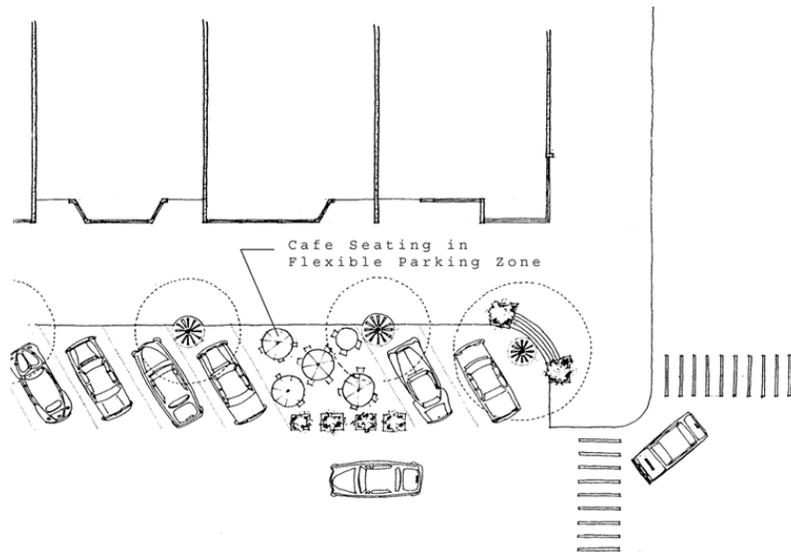


Figure 46a: Typical Bulb Out and Flexible Parking Zone

Light Fixtures. New pedestrian-scaled lighting fixtures should be installed to add to the existing character of the West Village. These fixtures should be limited to those necessary for public safety and be of a design and size that maintains the present character of Cambria.

Trash Receptacles. Trash receptacles would be provided in the Central District. The receptacles would be located within the existing right-of-way of Main Street. The exact specifications for the receptacles would be determined with the other streetscape elements after funding is secured.

Seating. There should be a variety of places for people to sit in the Central District, and two specific types of seating on the sidewalk along Main Street are appropriate:

- The first type of seating is a built-in bench, which would be installed as part of the bulbouts and in portions of the open spaces, as illustrated in Figure 46b.



Figure 46b: Example of Seating at Bulb Out

- The second type of seating would be a bench that can be placed where pedestrians' needs dictate additional locations for seating. Potential bench locations would be in or adjacent to the proposed open spaces and on the sidewalk in the mid-block areas of the four central block fronts on Main Street. These benches could be designed through a competition for local artists and architects.

Plantings. Freestanding planters would be installed on the sidewalk in front of the individual shops along Main Street in the West Village. These planters, which would be maintained by local merchants, would add flowers and color to the streetscape.

The existing planting areas in front of buildings along Main Street should be retained and should be maintained to match the level of maintenance in the public right-of-way.

Utility Under grounding. Existing overhead wires along Main Street in the West Village should be installed in underground chases. This would greatly reduce visual clutter in the West Village and add to the area's character. Implementation of this concept is discussed further in the Chapter 9.

Civic District

In the Civic District, the character and layout of the street would be much different than in the Central District, because of the adjacent steep hillside, the 100-foot right-of-way, and the less intensive uses. As illustrated in Figure 45b, the hillside lines the northern portion of the street, creating a one-sided strip. In this district, Monterey pines and Monterey pine understory would be planted on the hillside, creating vertical definition to the street and bringing this landmark feature of the community closer to the main thoroughfare. The remainder of the streetscape in the Civic District would respond to this gesture, with pine understory trees and plants in a center median and on the southern side of Main Street. A pathway of decomposed granite would be located on the south side of the trees on the south side of Main Street.

E. Open Spaces

Public gathering places are some of the most important features that help to create a thriving downtown. They create a focal point for orientation, a physical place where activities can take place, and a visual identity for a downtown.

In the West Village, potential gathering spaces already exist; they just need to be further defined. These spaces include the stub of Arlington Street between Main Street and the northern alley, the portion of Sheffield Street between Main Street and Cornwall Street, and the area directly in front of and next to the Pewter Plough Playhouse.

Arlington Courtyard

This area of Arlington Street between Main Street and the alleyway can become a defined physical center for the West Village and its tourist-oriented activities. As illustrated in Figure 47, an open space in this area would face southeast, giving it ideal solar exposure and shelter from the wind by the adjacent buildings. The new Arlington Courtyard would provide an area for public festivities, outdoor café seating, and a public transit stop. In addition, this space would connect the currently vacant

Victorian structure on Arlington Street with the rest of the pedestrian-oriented West Village, and thus make the Victorian building more viable. Due to existing vehicular access and driveways within this area, this open space may need to include vehicular access for the properties at the corner of Arlington Street and Main Street.

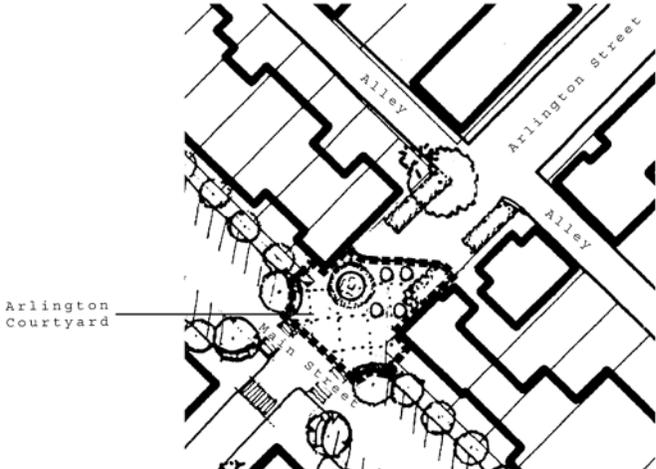


Figure 47

Sheffield Commons

As depicted in Figure 48, the portion of Sheffield Street between Main and Cornwall Streets is a redundant roadway that could provide the West Village with a passive public gathering space at the edge of the community. This space would also act as a connector between Main Street and the existing public parking lot further north, thereby encouraging more people to use the underutilized public parking lot. As with the Arlington Courtyard, the orientation of Sheffield Commons would minimize wind and maximize afternoon sun. The Commons would be a place of respite during a busy day of shopping and an information center for tourists as they come into town from the public parking lot. It would also provide an area for outdoor seating.

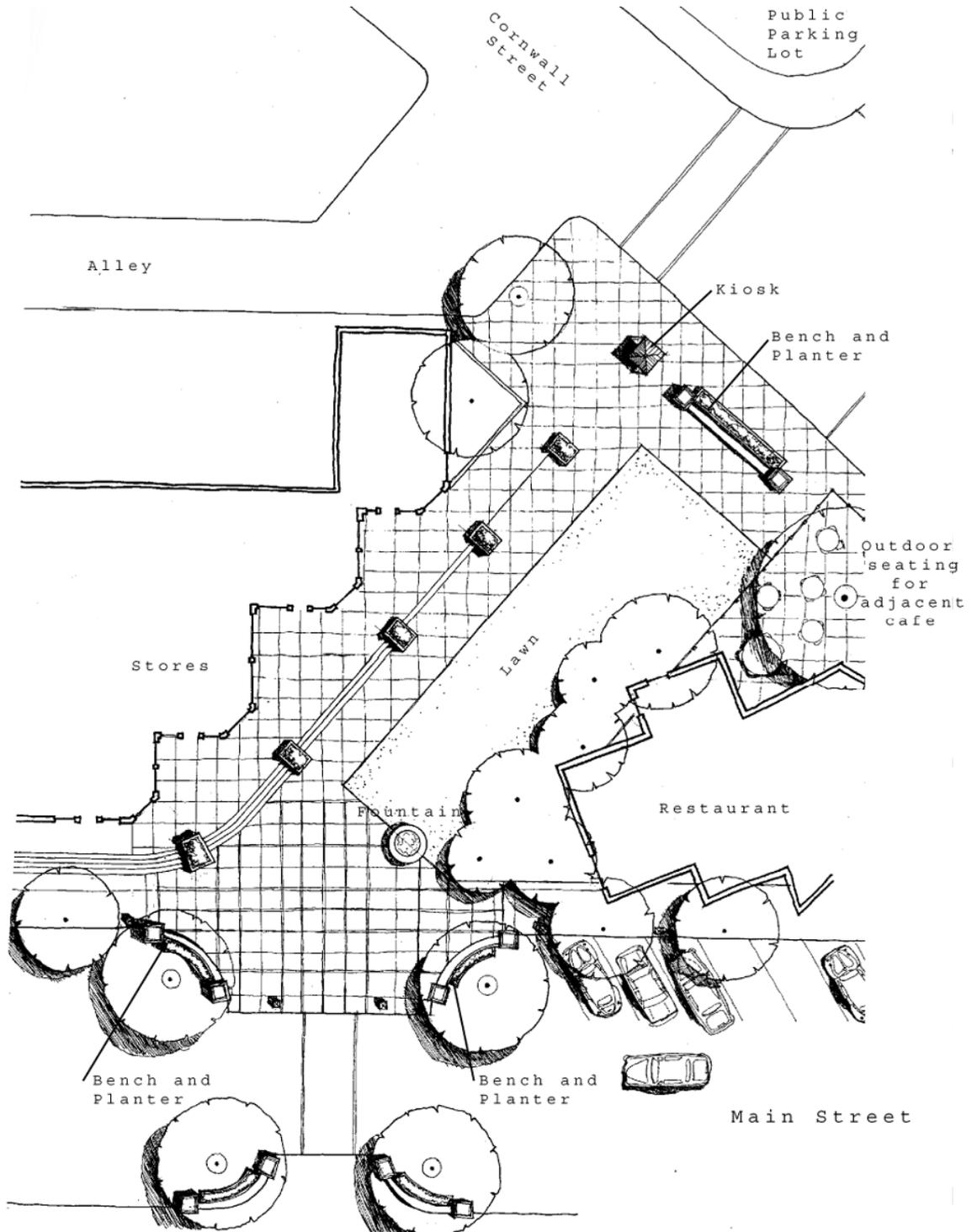


Figure 48: Sheffield Commons

Pewter Plough Playhouse Courtyard

A third public open space would be connected to the Pewter Plough Playhouse, which is already an important cultural facility in the West Village. As illustrated in Figure 49, this open space would expand the Theater's existing courtyard space onto the remainder of the parcel and onto a widened sidewalk on Main Street. The paving in the courtyard would be carried beyond the parcel line and onto the sidewalks to visually connect the spaces, while a large gate would open up the existing courtyard for special events.

At the terminus of Sheffield Street, a landscaped space would be provide a more passive space away from the main courtyard and would provide a visual terminus at the end of the Sheffield stub.

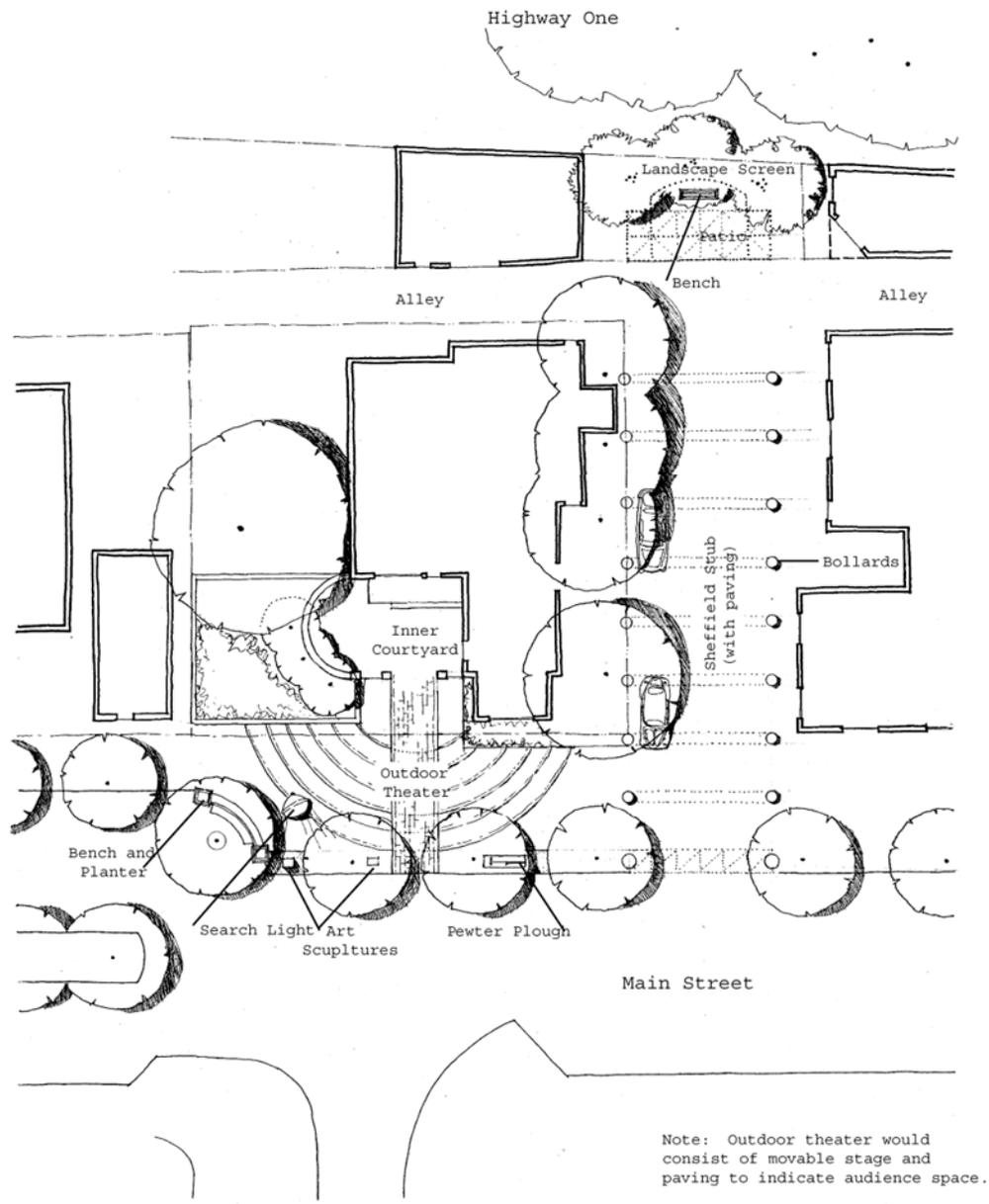


Figure 49: Pewter Plough Playhouse Courtyard

F. Circulation

Vehicular circulation in the West Village would be largely unchanged under this Design Plan, with most traffic continuing to use Main Street. Side streets like Cornwall, Arlington, Kent, and Sheffield Streets would also carry some local traffic, with some changes to Arlington and Sheffield Streets to create additional open space as noted in the preceding section. Arlington Street would be closed to most traffic between the alley and Main Street for the public open space. This would transfer some additional traffic to Cornwall Street and the alleys, so these streets should be improved to accommodate the additional traffic.

Pedestrian and bicycle circulation would be improved through implementation of the *County Bike Plan and County Trails Plan*. As shown in Figure 50, these plans call for bicycle facilities on Main Street and Cornwall Street in the West Village, as well as construction of the multi-use Cross Town Trail along Santa Rosa Creek.

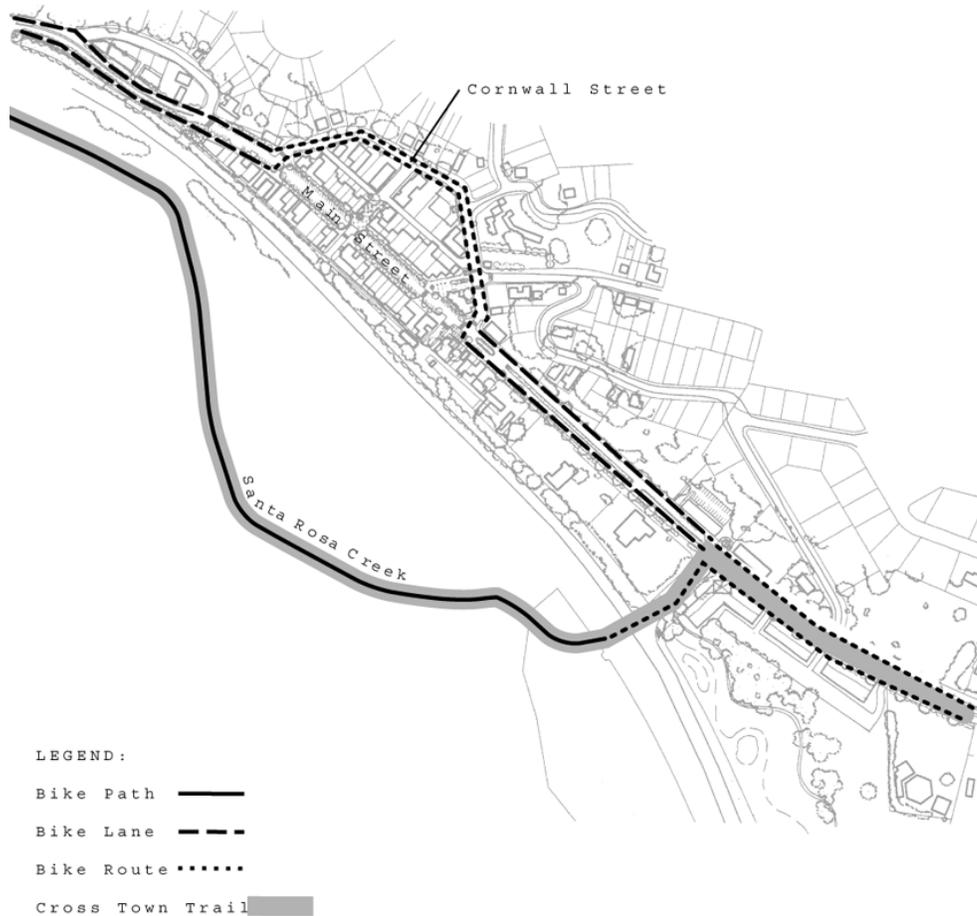


Figure 50: Bicycle and Pedestrian Circulation

G. Parking

According to a count conducted by the consultant, there are currently 174 off-street parking spaces and 170 official on-street parking spaces within the Central, Gateway and Local-Serving Districts of the West Village. Under this Design Plan, the total parking supply would increase from 344 to 396 spaces, for a gain of 52 spaces. Table 4 illustrates the additions and subtractions to on-street parking proposed by this Plan in all of the areas of the West Village except for the Civic District. The table only lists parking facilities where changes would occur, thus the table does not list off-street parking changes because no changes are proposed to off-street parking.

The two major types of parking in the West Village are on-street parking, especially on Main Street, and the public parking lot at Cornwall and Sheffield Streets. Both of these parking resources can be expanded and better exploited. In addition, parking outside of the village could be provided, with trolley stops at the parking locations.

On-Street Parking

Currently, the only significant on-street parking in the West Village is on Main Street. This parking can be expanded, and could also be augmented with parking on Kent Street. A portion of on-street parking on Arlington and Sheffield Streets would be lost due to the proposed public open spaces of Arlington Courtyard and Sheffield Commons which are described in section E. Open Spaces.

Conflicts and problems created by buses and recreational vehicles using on-street parking spaces needs to be resolved. This would be accomplished in this Plan by providing places where these vehicles could park, by providing signs that encourage use of these off-street parking facilities, and by extending the satellite trolley shuttle between these new parking areas and the West Village.

Main Street. There are currently 109 on-street parking spaces on Main Street between Kent Street and the eastern leg of Cornwall Street. The diagonal parking on this portion of Main Street can be retained and augmented, for a total of 145 spaces. Additional parking can be gained in places such as the frontage along abandoned curb cuts, next to unnecessarily large driveway cuts, and by changing some parallel parking to diagonal parking near the northeast gateway. A portion of the 145 spaces could be used as a flexible transition zone, as discussed in the streetscape section.

Parking on Kent Street. Currently, Kent Street is not formally demarcated with parking spaces. Although the right-of-way is narrow and the space between the hillside and parcels on Main Street is tight, the County should look at the possibility of creating a one-way loop and providing diagonal or perpendicular parking on this street.

Public Parking Lots

Currently, there is one public parking lot in the West Village. This 48-space public parking lot at the corner of Cornwall and Sheffield Streets is currently underutilized. Located off of Main Street and without proper signage and pedestrian access, this lot is currently hidden from tourists, who are its most likely users.

This lot should be signed more prominently particularly at Cornwall and Main Streets, and should have a formal pedestrian connection to Main Street, as illustrated in Figure 47b. These changes should help to increase utilization of this lot.

TABLE 4 WEST VILLAGE PARKING CHANGES¹

Location	Existing	Proposed	Gain (+) or Loss (-)
Main Street (Central District)	109	145	+36
Kent Street	19	53	+34
Arlington Street (Main – Cornwall)	18	12	-6
Sheffield Street (Main- Cornwall)	12	0	-12
Total On-Street Changes	158	210	+52

H. Development Standards

~~This section includes development standards that are requirements in the West Village. While the subsequent section includes design guidelines that are intended to be advisory only, the standards in this section are so important to the continued success of the West Village that compliance with them is mandatory for all new development.~~

Front Setbacks

~~All new development and/or remodeling in the Central District shall be built to the sidewalk edge or up to 5 feet behind the sidewalk if the area between the building and sidewalk is landscaped. On the south side of Main Street, this generally means that buildings will be built to the property line. On the north side of the street, buildings will generally have to be setback about three feet from the property line, and this setback will be paved as sidewalk.~~

Side Setbacks

~~All new development in the Central District shall be built to the property line with zero foot side setbacks. Exceptions may be made for a single driveway on an individual parcel in the northwest block face of Main Street (602–750 Main Street) in the Central District. (see Figure 51a)~~

¹ This table only includes locations where parking is proposed to be added, reduced or redesigned. It does not show all parking in the West Village.

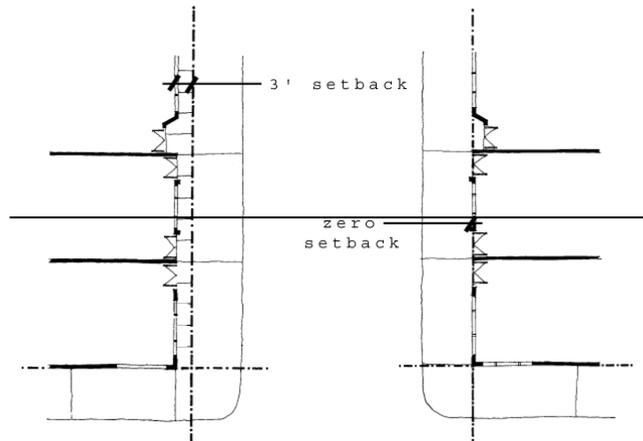


Figure 51a: Side Setbacks

Height Limitation

The height of all commercial buildings shall be a maximum of 35 feet as stated in Coastal Zone Land Use Ordinance.

Fire Walls

All new development and/or remodeling shall be designed in a manner that eliminates the need for firewalls above roofs, as currently exists on Main Street. In order to achieve this, interior firewalls with a “lay-down parapet” should be utilized to avoid the external parapet wall in compliance with Uniform Building Code section 709.4.1 exception number 5.

Building Facades

New development and/or remodeling in the Central District shall be articulated to include bays that match the typical 25-foot bays already found along Main Street. Building bays are recessed sections of the facade that occur between structural and/or ornamental building columns. (see Figure 51b)

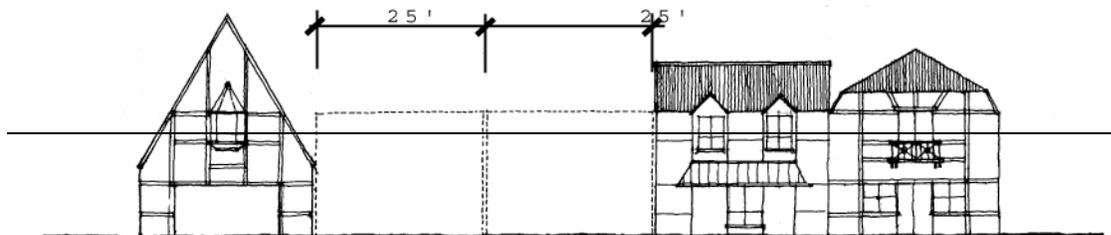


Figure 51b: Building Facades

Uses

In the Central District ground floor frontages shall be retail or visitor-serving uses only. These uses create a sense of activity that is crucial for pedestrian-oriented areas. Upper floor uses may be residences, lodging, offices or shops.

Curb Cuts

No curb cuts shall be allowed on Main Street in the Central District, except on the northwest block face (602–750 Main Street), where no more than one curb cut shall be allowed for each parcel. Where possible, sites should share vehicle access with their neighbors as a means to minimize the number of curb cuts on Main Street. Driveways shall be no more than 10 feet wide. Lots that have auto access via side streets and/or alleys shall have no curb cuts on Main Street. (see Figure 52a)

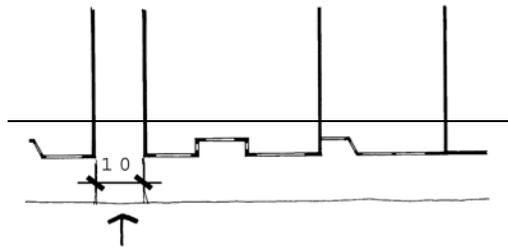


Figure 52a: Curb Cuts

Highway One Frontage

Development on any site in the West Village that has a rear property line along or can be viewed from Highway One shall store trash, recyclables and other materials in a visually concealed area attached to the back of the building, with similar materials and colors as the building, in order to create a neater and cleaner look to the West Village from Highway One. (This area could be an alcove in the building or an area surrounded by a fence or hedge.) All outdoor storage is required by the Coastal Zone Land Use Ordinance to be screened on all sides by a solid wall or fencing. (see Figure 52b & c)

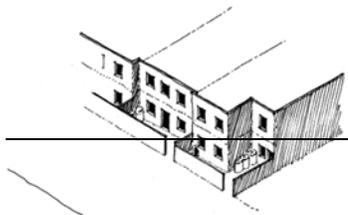


Figure 52b: Highway One Frontage

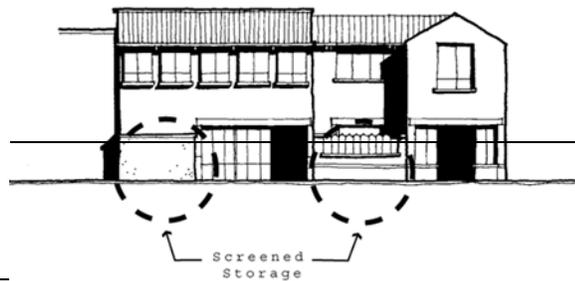


Figure 52c: Highway One Frontage

Signs

Signage shall be limited to reduce the potential for visual clutter and distraction. On-site signs are allowed according to applicable county policies, with the following allowances or limitations. Other policies in the CZLUO, Area Plan and Coastal Plan remain applicable and in effect.

— Wall signs are limited to a maximum of 30 square feet for each business or tenant. Such signs shall be painted directly onto the building or be composed of individually applied letters. (CZLUO limits signage to 15% of the wall area or up to 80 square feet.) (see Figure 53a)

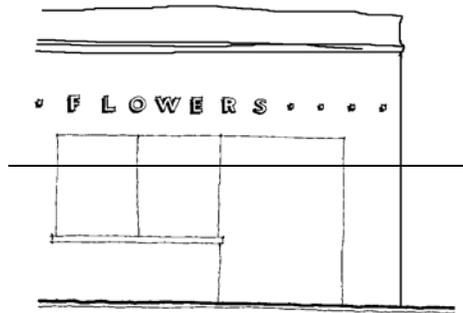


Figure 53a: Wall Signs

— Suspended signs may be utilized if no projecting signs are utilized. (CZLUO allows both a suspended and projecting sign.) (see Figure 53b)

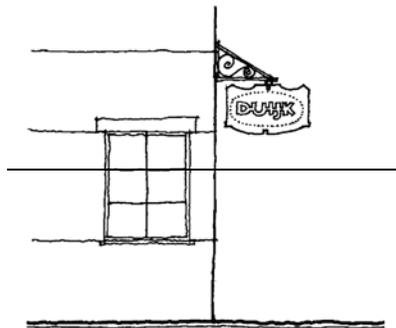


Figure 53b: Suspended Signs

— Monument signs are limited to 32 square feet, and shall not exceed 4 1/2 feet in height, including the base below the sign, and 8 feet in width outside of setback areas. Such signs shall be solid at the base in order to give them a substantial appearance, and the signs shall match the architecture of the building itself. (CZLUO allows up to 60 square feet and a height of 3 feet within the setback areas.) (see Figure 53c)

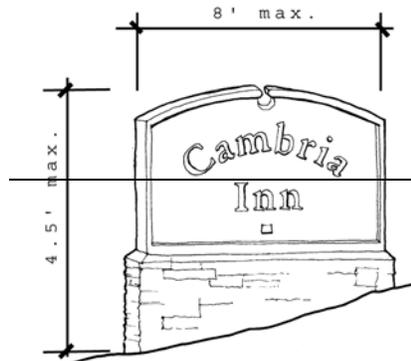


Figure 53c: Monument Signs

~~Projecting signs may be utilized if no suspended signs are utilized. (CZLUO allows both a suspended and projecting sign.) (see Figure 53d)~~

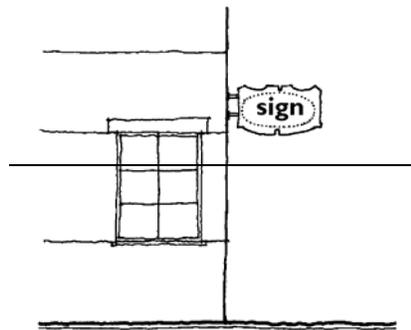


Figure 53d: Project Signs

~~Marquee signs are limited to 30 square feet. (CZLUO allows up to 40 square feet.) (see Figure 53e)~~

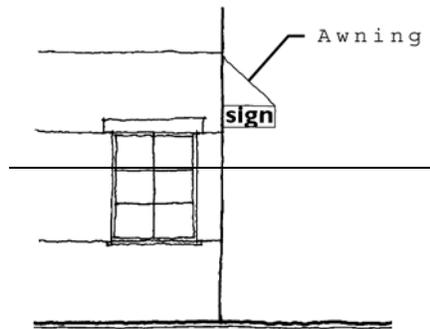


Figure 53e: Marquee Signs

~~Directional Signs, which are signs that identify the direction of generic kinds of uses, such as “restaurants, lodging, bank”, are also subject to land use permit requirements. These signs are limited to a height of five feet and an area of 10 square feet. Such signs may be pole-mounted or monument design.~~

~~Window signs, as defined by the CZLUO, are exempt from the sign requirements of the CZLUO if the total area of such signs does not exceed 15% of the total window area, and no more than one lighted sign per windowpane.~~

~~Signs may only be externally illuminated. No neon signs are allowed.~~

Drive-up Windows

~~No new drive-up windows such as those used for banks and fast food places are allowed. Any existing ones may be reconfigured or relocated but not expanded.~~

Landscaping

~~All landscaping and construction practices shall work to maintain and regenerate the Monterey pine forest and the Santa Rosa Creek riparian corridor. To this end, the following regulation shall be followed:~~

~~Plant materials used to mimic or enhance naturally occurring vegetation should be propagated from native stock taken from an area within a 50-mile radius of Cambria to ensure that the gene pool is not diluted for endemic species. This is particularly true for Monterey pines and riparian plantings.~~

I. Design Guidelines

This section contains design guidelines that provide flexible policy direction for new developments in the West Village. In other words, alternative design features that are determined to be “equal to or better” may be approved as well. Although compliance with these guidelines is not strictly mandatory, this section is intended to give property owners, developers and architects a clear sense of the design that the community hopes to achieve.

Context

In order to harmonize with the surrounding area, new development should reflect the style, features, building lines, massing, roof shapes and other building elements of the existing vernacular architecture in the West Village. (see Figure 54a)



Figure 54a: Context – Harmonious Development

Pedestrian-Oriented Detailing

Ground floor facades should include pedestrian scaled detailing including vertical windows and sill, ornamental decorations, wall lamps, and door overhangs or recessed entries. (see Figure 54b)

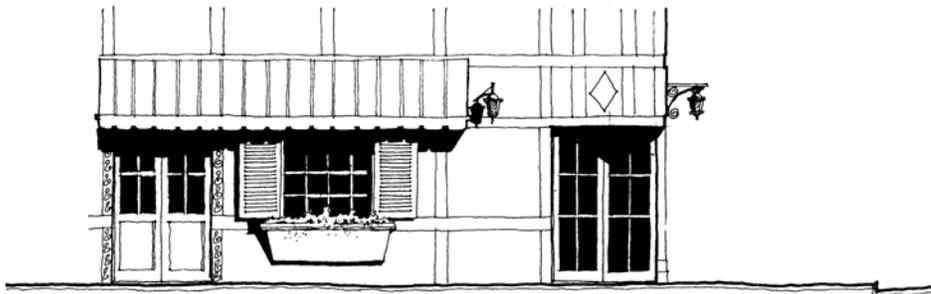


Figure 54b: Pedestrian – Oriented Detailing

Roof Colors

Roofs should be colored in soft tones and be non-reflective.

Roof Style

Roofs should be designed with exposed and sloped roofs rather than with parapets.

Window Patterns

- Ground floors of buildings along Main Street should include large amounts of glass and other openings to promote visual permeability.
- Upper floors should have smaller window openings punched into solid walls.
- Bay windows and dormers are encouraged on the upper floors.

- Individual windowpanes, in mullioned windows, should be no larger than 1 foot horizontally and 2 feet vertically.
- Windows should have clear glass.
- Window frames should have the appearance of painted wood.
- Wooden and louvered shutters are encouraged to accent windows above the ground floor.

Doors

- Building entries should occur within each building bay.
- Openings for doors should not be more than 6 feet horizontally by 8 feet vertically.
- Doors fronting onto Main Street should be made of wood, and may also feature glass panes.
- Building entries should be accentuated by moldings, lighting, overhangs or recessed alcoves.

Parking

In the Gateway, Local Serving and Civic Districts, parking lots should be designed with respect to the pedestrian, the surrounding built environment and the natural environment as well as the car.

Visible components. Driveways should be the only paved portion of a parking area visible from Main Street.

Main Street Buffer. In some instances parking may occur on Main Street. Where it does occur, a landscape buffer of at least 8 feet should be provided to minimize views of parked cars from the street. (see Figure 55a)

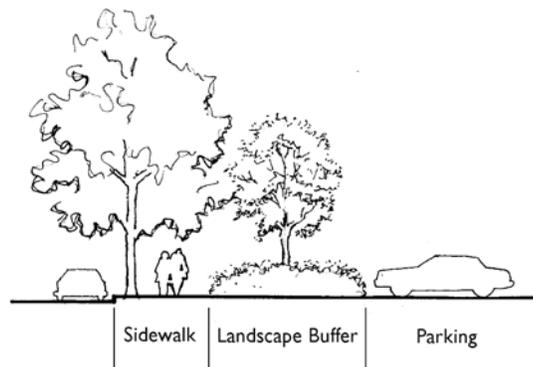


Figure 55a: Main Street Buffer

Paving. The paved area of any parking should be no more than necessary to efficiently accommodate the required number of parking spaces and walking.

Permeable Surfaces. Parking surfaces should be permeable wherever possible. Permeable surfaces and on-site drainage will reduce the need for storm drainage infrastructure and allow storm water purification through the soil, reducing pollution of ground water. (see Figure 55b)

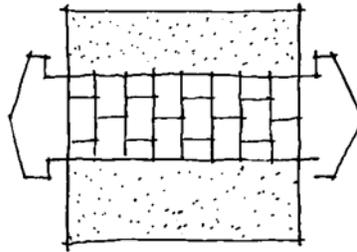


Figure 55b: Parking Lots – Paving and Permeable Surfaces

Pedestrian Pathways. Clearly delineated pedestrian pathways should be provided within large parking lots. (see Figure 55c)

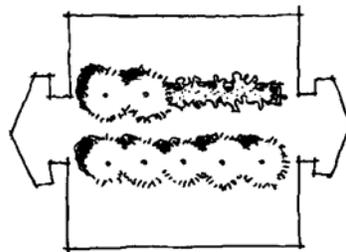


Figure 55c: Parking Lots – Pedestrian Pathways

Existing Fire walls

The visual distraction created by existing fire walls between buildings should be mitigated with elements that help blend the wall in with the architectural detailing of the West Village. Suggestions include vines and murals depicting architectural elements such as timbers and windows. Half-timbers can be used on the exterior, but non-combustible material must be used on the inside wall. Murals depicting landscapes, streetscapes or other scenes are discouraged, since they would draw attention to the firewalls.

Landscaping

Private landscaping in the West Village should compliment Cambria's Monterey Pines, and should be drought tolerant due to the community's limited access to water. Landscaping on private property in the Central District along Main Street should include flower boxes in the upper story windows and gardens, small movable planters or large pots with flowers, small shrubs or vines in front of individual stores.

When in-ground planters and trees are installed on private property, they should not block the views of the surrounding pine forest. Trees should not exceed their adjacent building height and should be columnar in shape. In addition trees should not create excessive shade or create a lot of condensation due to the cool coastal climate of the West Village.

Suspended Signs

In addition to the signs allowed in the Gateway and Civic Districts, suspended signs should be made of a metal or wood material. (see Figure 55d)



Figure 55d: Suspended Signs

J. Key Private Developments

New development on several privately-held parcels will be key to the successful continued development of the West Village. This section suggests how future private development might occur.

Termination of Cambria Drive

The parcel at the intersection of Cambria Drive and Main Street is one of the first sights of the town that people see as they enter the town from the Cambria Drive Gateway. It is a logical location for new civic or retail development.

It is important that this building enhance the gateway by creating a landmark visual image that also respects the view of the Monterey Pines above. As shown in Figure 56a, the building footprint should start approximately forty feet from the eastern parcel line. The building should be of similar massing and setback as the existing development to the east on Main Street. Parking should be placed behind the building and the slope of the hillside behind should remain undeveloped and planted with Monterey pines. As noted in previous sections, this Plan also includes a small plaza or space with public art at this location. Thus the design of the proposed building in this area should enhance and work with this gateway landmark.

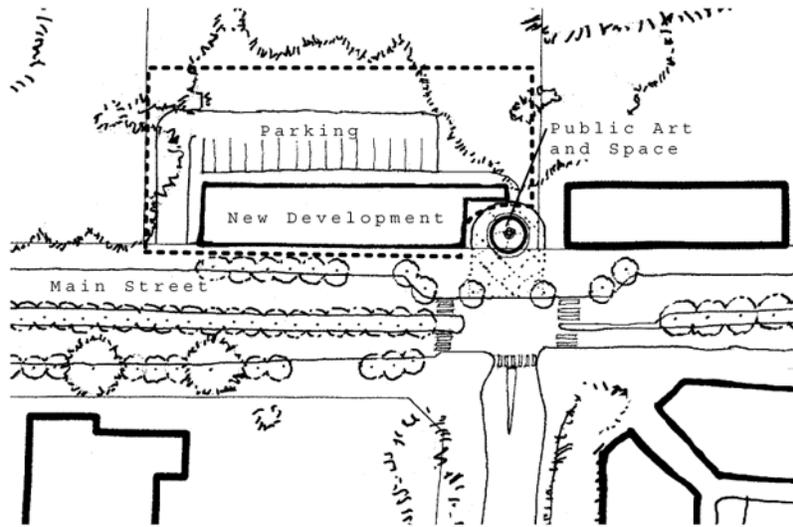


Figure 56a: Termination of Cambria Drive

Vacant Lots in the Gateway District

Two consecutive lots remain undeveloped in the Gateway District as indicated in Figure 56b. Development on these lots could be auto-serving, but should follow the development standards and design guidelines discussed above. Landscaping should buffer the public sidewalks from parking lots as described in the design guidelines.

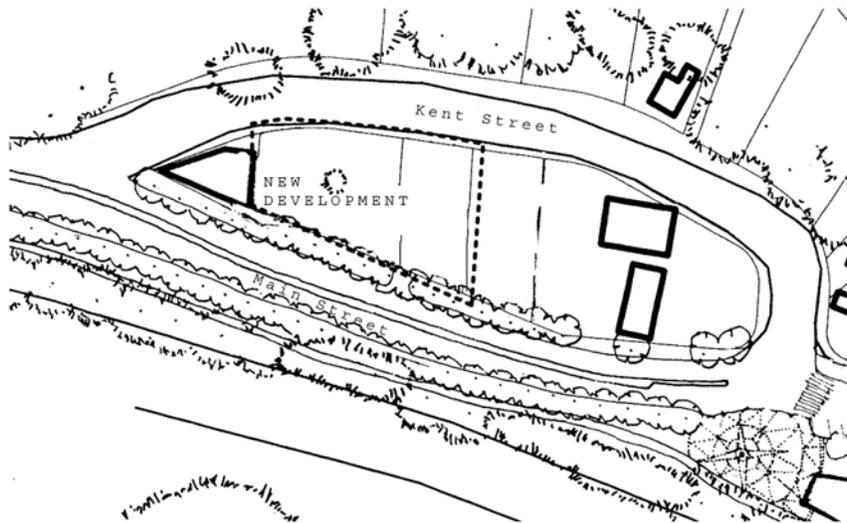


Figure 56b: Gateway District

Vacant Lots in the Local-Serving District

Two consecutive lots remain undeveloped in the Local-Serving District. These lots should be developed with local-serving uses on the ground floor with offices or residences above as illustrated in Figure 57. New development on these sites should be built with a five-foot setback, with local-serving uses facing Cornwall Street and parking located behind the building and accessed via a driveway or the alleyway. The height of the building should be limited to two-stories so as not to block views of the pine studded hills that frame the Village.

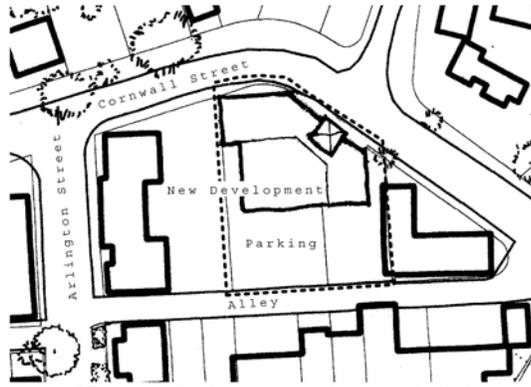


Figure 57: Vacant Lots in Local – Serving District

Properties with Rear Frontages on Highway 1

Properties in the 700 and 800 blocks of Main Street are adjacent to Highway 1, where businesses have traditionally located storage and parking, often resulting in unsightly conditions viewed from the highway. Property and business owners and the County would need to cooperate to achieve a concept of turning the alley into a flat, slightly meandering cobblestone street, primarily for walking with limited access for businesses and deliveries. Storage and waste collection would need to be contained within structures, out of sight of the highway and patrons.”

VIII. MOONSTONE BEACH

This chapter focuses on the Moonstone Beach area of Cambria, which lies between Highway One and the Pacific Ocean, ~~in the Recreation zone~~ northwest of the West Village. Moonstone Beach is typified by a row of motels facing the ocean. Because it is an important visitor-serving area, it deserves special attention in this Design Plan.

Moonstone Beach is designated as a visitor-serving area in the Local Coastal Plan. The viability of the motels and restaurants in the area depends on views of the Pacific Ocean that are unhindered by typical urban features such as street lights, trash containers and street trees. The less vertical intrusion into views from the road and businesses, the greater benefit to the area. In that sense, street parking and any necessary safety devices or signs should be strategically located to maximize visitors' views.

This chapter includes a list of goals for Moonstone Beach, descriptions and illustrations of proposed streetscape improvements, development regulations and design guidelines for private development.

A. Goals For Moonstone Beach

Goals for Moonstone Beach are as follows:

- Enhance the picturesque, wild identity and natural resources of the Moonstone Beach area.
- Ensure high quality in the design of new structures along Moonstone Beach Drive
- Enhance the scenic and visual relationship between Moonstone Beach and Highway One.
- Maximize public access to the coast.

B. Moonstone Beach Drive

Currently Moonstone Beach Drive is a two-lane road designed primarily for auto traffic. Curb, gutter and sidewalk are not allowed in order to preserve the road's rural character. Gravel shoulders provide space for parking and pedestrians. Moonstone Bluff Trail, a pedestrian path between the beach cliff and the road, provides people with an informal place to walk and enjoy the views. To improve pedestrian and bicyclist safety and encourage walking as an alternative form of circulation, this plan proposes that a specific streetscape plan be prepared that considers the following improvements to the Moonstone Beach Drive streetscape and is illustrated in Figure 58:

- Bicycle needs should be accommodated by a modified Class III Bike Route designation, which would feature "bikeway" paving of 3 feet for bicyclists outside the fog-line (one additional foot of pavement to total paved width of 28 feet.
- The 25 miles per hour speed limit should be reinforced with other traffic calming means.

- On the west side of the road, provide a narrow gravel shoulder (class I aggregate base) with a width of approximately five feet to accommodate pedestrians. As part of a comprehensive parking and access plan which includes locating parking at Santa Rosa Creek and Leffingwell Landing, provide an approximately 12 ~~feet~~ foot wide gravel shoulder in existing wide areas where necessary to accommodate parking, trolley stops and pedestrians, ~~primarily in existing wide areas. Where topography or significant vegetation make this width impossible, provide a narrower gravel shoulder with a width of approximately five feet wide to accommodate pedestrians.~~ This approach will formalize parking next to the state park, and assist in reducing the number of “wildcat” trails from the road. It will also visually narrow the streetscape and retain its scenic quality.
- On the east side of the road, vehicle parking should only be located intermittently, where it will not interrupt drivers’ sight distance entering the street from private properties, and where it would not be constrained by landscaping, drainage and other facilities. Where parking is desired, provide an aggregate base shoulder approximately eight feet wide. The shoulder should be graded to provide a nearly level surface next to the edge of pavement for walking. In areas where a narrower width is desirable, reduce the width of the parking lane to approximately four feet to accommodate pedestrians but discourage parking.
- Retain or re-vegetate areas along the road with native plants. Encourage private landscaping to expand to the edge of the shoulder, utilizing river rock in the bottom of the drainage channel.
- Consider replacing the existing “cobra-head” streetlights with lower lights that are in scale with the buildings and the desired rural character. The light fixtures should be designed to light the walkway and not the beach. In addition, light shields should be considered that minimize intrusion into the night sky.
- Existing cypress trees (*Cupressus macrocarpa*) should be maintained, pruned and replanted as necessary to maintain the green, windswept, landscape ambience along Moonstone Beach Drive.
- The drainage along Moonstone Beach Drive should be reviewed by the County to determine if the existing facilities are too level, if improved flow should be constructed, and if erosion and sedimentation are adequately contained. Any culverts that are deemed unsafe should be corrected.

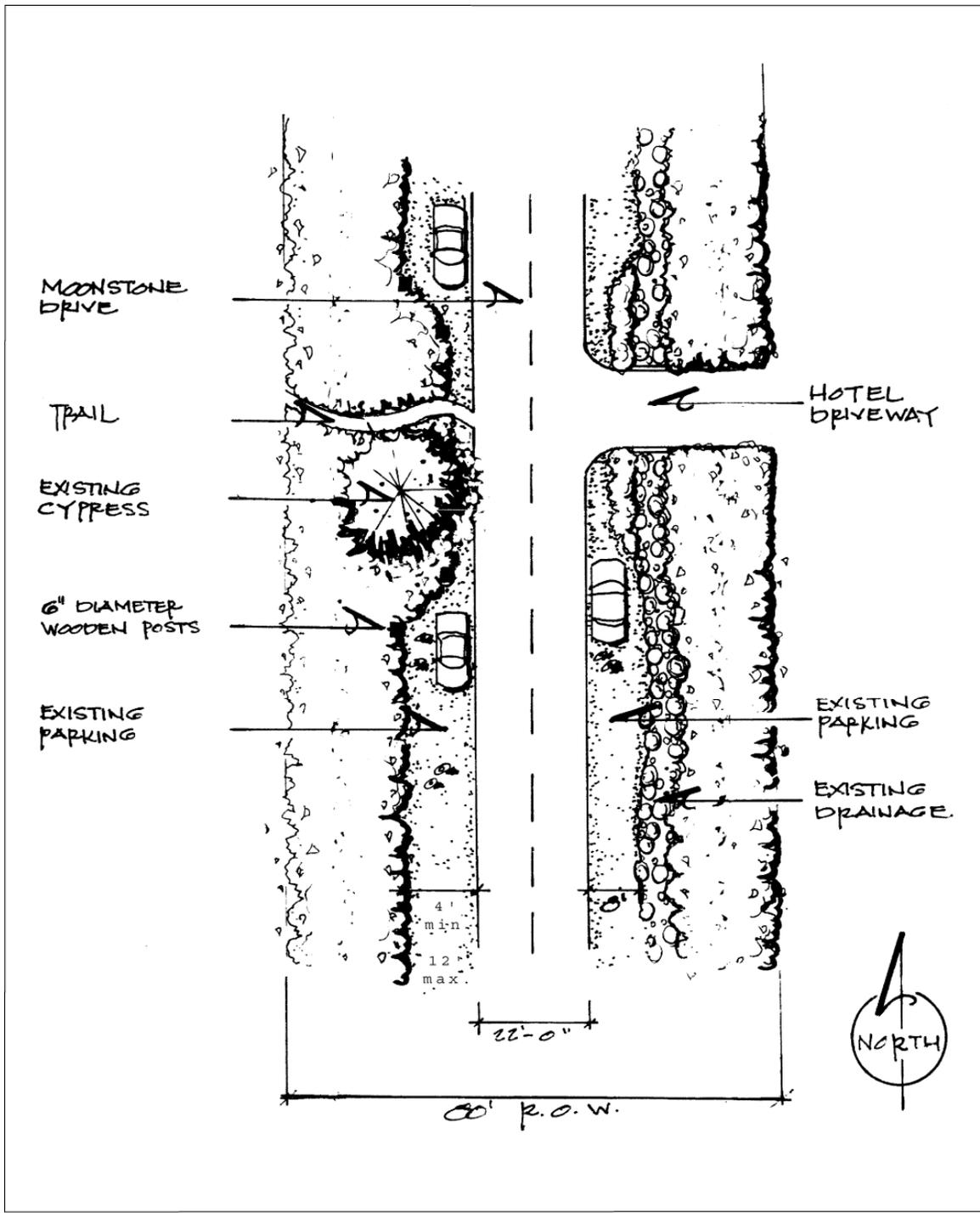


FIGURE 58

TYPICAL PLAN OF
MOONSTONE BEACH DRIVE

CAMBRIA DESIGN PLAN

C. Development Standards

This section includes development regulations that shall be mandatory in the Moonstone Beach area. They are in addition to the existing planning area standards in the North Coast Area Plan for Moonstone Beach. While the subsequent section includes design guidelines that are advisory only, the regulations in this section are so important to the continued success of the Moonstone Beach area and the scenic Highway One corridor that compliance with them is mandatory for all new development.

Signs

Each parcel shall not utilize pole or free-standing signs, as defined in the Coastal Zone Land Use Ordinance, and shall have no more than one monument sign per the existing ordinance. Such signs should match the architecture, materials and colors of the building itself. (see Figure 59a).

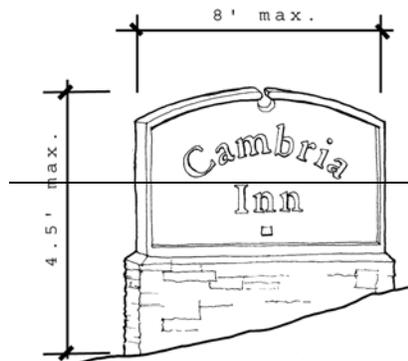


Figure 59a: Monument Sign

D. Design Guidelines

This section contains design guidelines that provide flexible policy direction for new developments in the Moonstone Beach area. The guidelines are to be used in addition to the standards in the North Coast Area Plan. Alternative design features that are determined to be “equal to or better” may also be approved, since this section is intended to give property owners, developers and architects a clear sense of the design that the community hopes to achieve. Compliance with these guidelines is not mandatory, but is strongly encouraged.

Design Style

New buildings should be in harmony with surrounding buildings and not conflict or compete dramatically in style.

Building Orientation

All buildings on Moonstone Beach Drive should have components that face the street. Since most buildings on the street are motels with rooms facing onto a parking lot, this means that special attention should be paid to the creation of other types of spaces on the ends of these buildings where they face the street. For example the ends of motel buildings can be restaurants, cafes, sitting rooms, patios, or

motel rooms with a beach orientation. The design of the building face along Moonstone Beach Drive should provide for special uses such as these. (see Figures 59b & 59c)



Figure 59b: Building Orientation – Restaurant with patio



Figure 59c: Building Orientation

Building Massing/Facades

To avoid long unbroken facades and monotonous use of one material, building facades should be articulated with changes in plane, materials and design in increments of not more than 16 feet. (see Figure 60a)

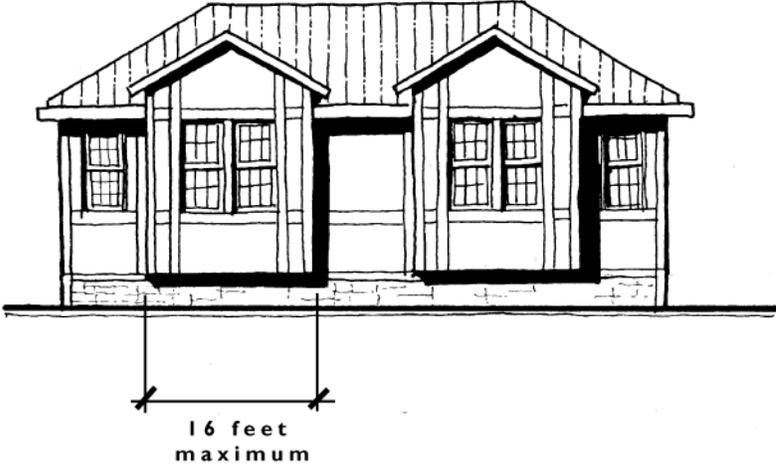


Figure 60a: Building Massing/Facades

Roofs

Exposed sloping roofs are encouraged in the Moonstone Beach area, rather than flat roofs with or without parapet walls.

Exterior Building Materials

- A combination of materials should be used to avoid one material dominating the building's appearance.
- Textured concrete block should be used only as a subordinate material, such as at the base of a building.
- Wood siding should utilize shiplap, channel or board & batten styles. (see Figure 60b)

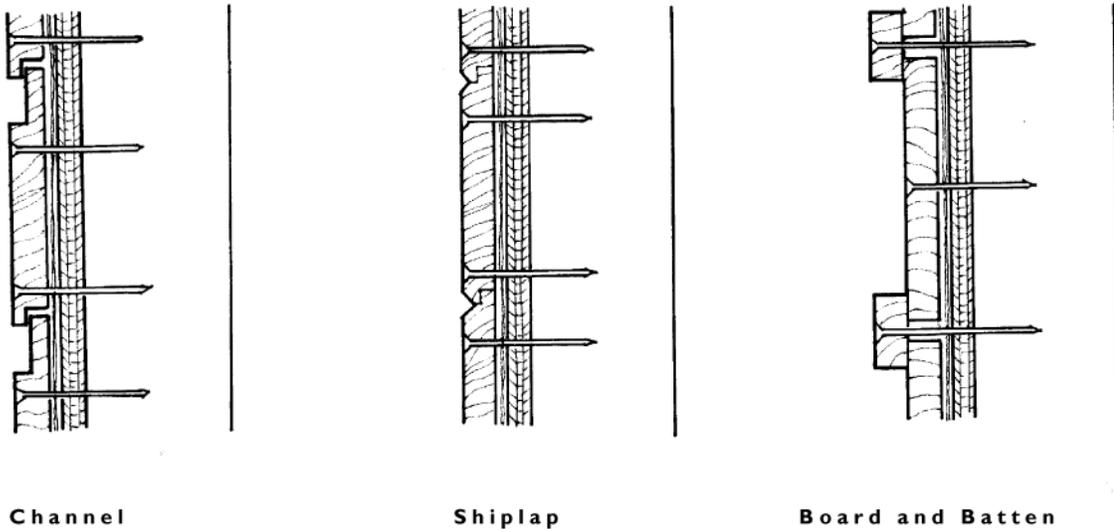


Figure 60b: Wood Siding

Landscaping

- Landscaping, perhaps in combination with a low wall, should be utilized to screen the parking bay from Moonstone Beach Drive, as illustrated.
- To break up the monotonous appearance of a long row of parking spaces, a landscape strip with a minimum width of four feet should be located at a minimum interval of every six parking spaces.
- Landscaping should be used as a scaling device along property lines, at building corners and at the base of buildings, for example, to reduce the bulk of buildings.
- Color plantings should be used as accents and at focal points, such as at signs or outside individual units.

Parking

- Parking lots should be oriented so that the narrowest dimension of the lot is parallel to Moonstone Beach Drive. This dimension should be no wider than one parking bay (single or double loaded). (see Figure 61)
- Driveways to parking lots should be no wider than twenty feet.

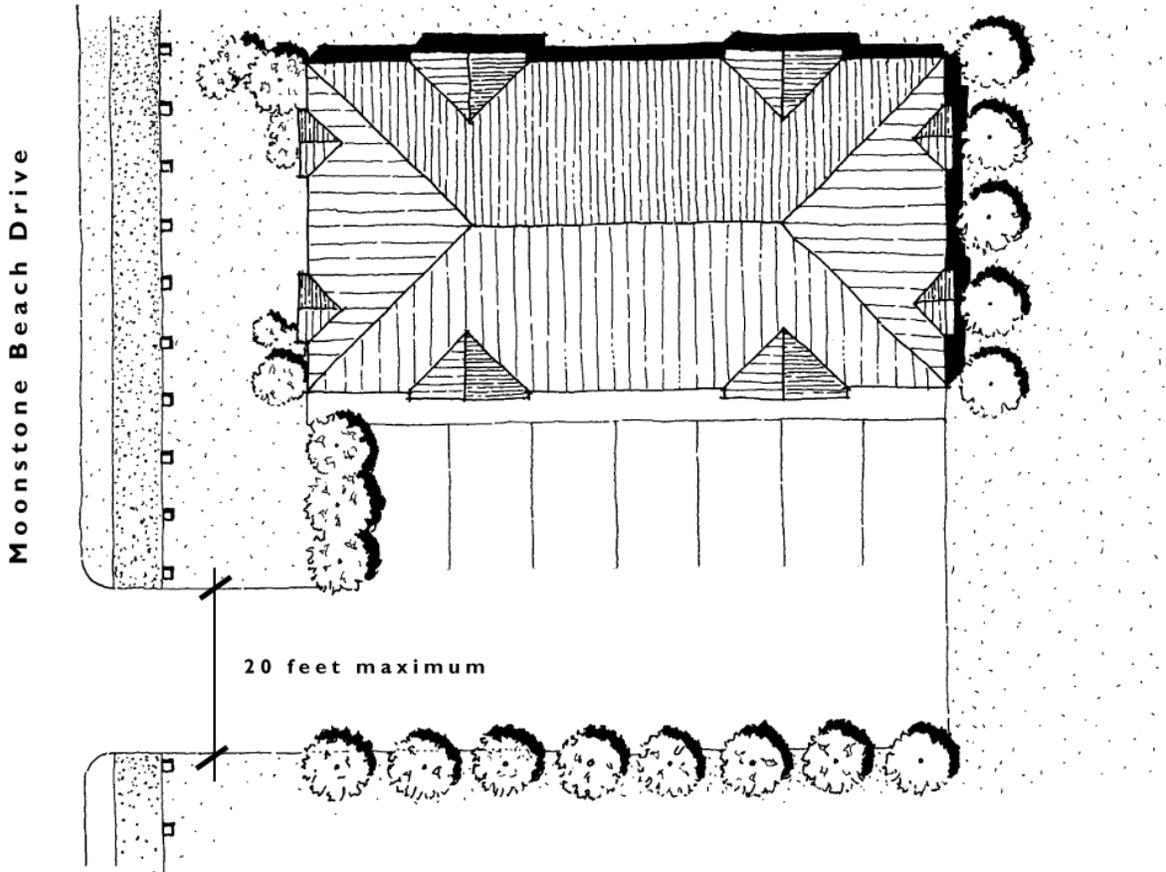


Figure 61: Parking

IX. IMPLEMENTATION

In order to implement this Design Plan, the County and other organizations ~~should~~ will need to accomplish a number of tasks. These tasks, and the parties that should be responsible for them, are described below. Where applicable, standards contained in this design plan have been implemented in the North Coast Area Plan.

A. Further Design Work

Detailed design work will be needed to implement a number of the design ideas in this Plan, as described below.

Main Street Streetscape Plan

The County's draft streetscape plan for Main Street should be revised to reflect the ideas in this Design Plan, and it should then be developed from a conceptual plan into construction drawings. The Streetscape Plan will provide construction-level detail for sidewalks, bulbouts, crosswalks, lighting, street trees and trees on hillsides, furniture such as bench locations, trash containers, landscape medians, sidewalk planters and other related public improvements within the street right-of-way.

East Village Historic Park

Detailed plans should be developed for the East Village Historic Park. As a first step in this process, students at Cal Poly San Luis Obispo could be invited to conduct a studio design project in the area. This should be followed by the hiring of a landscape architect to provide a conceptual plan, followed by construction level documents. Such work could be overseen by the County's Park and Recreation Commission, CCSD or the Cambria Historical Society.

Moonstone Beach Drive

The Parks Division and County ~~Public Works Engineering~~ should design detailed improvements to the pavement, gravel shoulders, landscaping and drainage ways along Moonstone Beach Drive. This work should determine where parking, pedestrian and bicycle circulation is appropriate and provide the means to formalize parking areas with native landscaping and post barriers. An ad hoc advisory committee should be formed to work with the County ~~Public Works Engineering~~ Department and the Cambria Community Services District during the design phase.

Cross Town Trail

The County and CCSD should work together to ~~complete formalize the plans~~ for the Cross Town Trail. ~~Once an acceptable alignment is determined, it should be submitted to the North Coast Advisory Council, the County's Park and Recreation Commission and the Cambria Parks, Recreation and Open Space Commission for approval and inclusion in the Cambria Parks, Recreation and Open Space Master Plan. It should then be submitted to the County Parks Division for inclusion in the County Trails Plan.~~

~~Once the plan for the trail is approved and funding is secured, the trail should be designed in detail by a landscape architect so that it can be constructed.~~

Street Furniture and Community Identification Signs

The County Planning Department should work with community groups to develop specifications for street furniture and community identification signs to be installed in the East and West Villages. One way to do this would be hold one or more design competitions for artists, crafts people and architects to design appropriate street furniture. Otherwise, specifications should be developed from standard fixtures and signs through a committee review process.

Directional Signage

The County should work with a consultant or its own staff to develop a program of directional signage that indicate the location of important but hidden destinations such as the public parking lots in the East and West Villages.

B. Policy Changes and Updates

North Coast Area Plan and Coastal Zone Land Use Ordinance

The County should periodically update ~~is currently in the process of updating~~ the North Coast Area Plan, and this update should include the concepts for Cambria that are included in this and future Design Plans. ~~Particular attention should be paid to changes in land use designations suggested for some of the key parcels addressed in this Design Plan, and to changes to County development standards necessitated by the development standards and design guidelines.~~

Update Engineering Standards

It may also be necessary to update engineering standards overseen by the County’s Public Works Department. For example, ~~the County should ensure that the Public Works Department enforces~~ the sidewalk widths and curb cut standards shown in this Plan should be implemented, ~~in its approvals for private developments in Cambria.~~

County Trails Plan and Parks, Recreation and Open Space Plan Updates

This Design Plan should be considered during ~~a reference for~~ amendments to the County Trails Plan, County Parks and Recreation Master Plan and the CCSD Parks Recreation and Open Space Plan, and vice versa, to make cross references and coordinate between documents.

Forest Management Plan

The California Department of Forestry and Fire Protection (CDF) and Cambria Community Services District (CCSD) have prepared ~~has recently authorized \$100,000 to be used for the preparation of a~~ Forest Management Plan for Cambria. The CCSD is serving ~~will serve~~ as the trustee of the planning funds and the Cambria Forest Committee will guide the preparation and implementation of the Forest Management Plan. This Management Plan, which will deal with issues such as pine pitch canker mitigation, tree removal and replacement, reforestation, fire hazard reduction measures, and funding implementation, should consider ~~and adjust if necessary~~ the design ideas in this Design Plan.

Bulbout, Flexible Parking Zone and Open Space Use Permitting Process

The County Planning Department should ~~consider developing~~ ~~develop~~ a permit process to allow for appropriate on-going and one-time private uses of the bulbouts, flexible parking zone and other public spaces envisioned for the three village areas in this Design Plan.

C. Historic Preservation

As discussed in Chapter 3 of this Design Plan, the Chamber of Commerce, Cambria Historical Society and NCAC have already identified a large number of historic structures in Cambria. These buildings are a major determinant of Cambria's identity. However, more research on exiting historic resources in Cambria is needed, and ~~more measures should also be implemented to protect historic resources.~~

Historical Inventory

Although many historic buildings have been identified in Cambria, a more formal inventory should be conducted. This inventory should use standardized California Department of Parks Forms DP523, and would allow for preliminary determinations of each structure's eligibility for the National Register of Historic Places.

Historic Overlay Zone

As discussed in Chapter 3, an Historic Combining Designation already exists in the Coastal Zone Land Use Ordinance. Where applicable, this designation should be applied to all properties in Cambria that are found to be significant through the Historic Resources Historic Inventory.

~~The Historic Combining Designation in Cambria should also be amended to stipulate that any designated building that are destroyed by fire, neglect or any other cause shall be rebuilt to match their historic conditions.~~

Historic District(s)

In addition to the use of the Historic Combining ~~Designation District~~, which is a land use regulatory tool, the County or a local Cambria body should consider forming one or more historic districts in Cambria. Such a district or districts ~~may would~~ provide various benefits for historic structures, including recognition by locals and tourists, tax benefits, and further design controls. More specific information on Historic Districts is provided in Chapter 8 - Implementation Appendix D.

Historic Plaques

A program should be initiated by the County Planning Department, CCSD, Cambria Historical Society, or the Chamber of Commerce to create informational plaques for historic buildings in the East and West Villages. The program would collect nominal fees from historical renovation projects in order to create individual distinctive plaques that would provide historical information on the individual historic structures. Each plaque would be located on the structure or grounds of the structure and would be visually accessible to the public.

D. Implementation Programs

~~With availability of funding and sufficient public support, In addition to the design projects, policy changes and historic preservation tasks described above,~~ the County and local organizations should also embark on a number of programs that would help to implement this Design Plan.

Tree Replanting Program

Cambria Land Conservancy has initiated a tree replanting service in cooperation with the CCSD. This program should be continued.

In addition, the County and the CCSD should also work with Cambria Land Conservancy to purchase the ranches, which contain parcels of forested land or to obtain conservation easements which would protect the forest within them.

Sign Clean-up

The County, through NCAC, should work with local merchants in a voluntary program to bring existing signs into conformity with the guidelines for new signs in this Plan. This could include education about the sign standards and assistance with the design of new signs.

Additional Parking Lots

The County and business property owners should search for additional locations for parking lots that would serve the needs of businesses to provide off-street parking.

Shared Parking District

The County ~~has studied~~ Planning Department is in the process of studying the financial feasibility of a shared parking district that could develop and manage shared parking in the East and West Villages. The parking lots would be linked to the commercial areas via the existing shuttle service and would provide developers with the option of developing parking on site or paying an in-lieu parking fee to utilize parking space within the shared parking district. County staff ~~has studied~~ is studying 14 possible locations for parking lots. If any of these lots are deemed financially feasible, ~~the NCAC Traffic Committee would need to approve the~~ a new district ~~may need to be formed~~ and funds would need to be collected to develop them. Once developed, the CCSD ~~may would~~ expand the shuttle service to serve these new lots.

RV and Bus Parking

The County planning department should consider the needs of RV and bus parking in the current shared parking district study or do a separate study to determine locations for future bus and RV parking on Cambria. Due to the large size of these vehicles, this type of parking should be located at the perimeter or outside of the Villages in order to minimize bus and RV travel through the Villages and to provide adequate room for parking and screening of the parking area.

Employee Parking

Employees should park at the backs of businesses, on side streets and in remote parking lots, leaving parking on main commercial streets for shoppers and visitors. The County Planning Department should work with the Chamber of Commerce to create an employee-parking program in the East and West Villages.

Parking Information Program

Information should be developed to encourage visitors and employees to use appropriate off-street parking, and specially designed signs should be erected to direct people to satellite parking lots. This program could also include advertisements and other educational tools to encourage people to park in appropriate locations.

Youth Task Force

The County, CCSD or a local group could develop a summer employment program involving youths in the community. Their responsibilities could include maintaining public landscaping and developing a community youth center.

Utility Undergrounding

A utility underground district should be formed in the West Village to install overhead wires in underground conduits.

Business Appearance Improvements Along Highway 1

The County, Chamber of Commerce and other relevant organizations should work with businesses and property owners to conduct a “clean-up” project to improve the appearance of the backs of businesses along Highway 1, to conform with County standards for signs, storage, parking and screening.

Lighting District

A lighting district, which could be administered by CCSD, should be formed to serve the commercial areas of the East, Mid-, West Villages and Moonstone Beach. The formation of the district would allow for the collection of funds to install and maintain light fixtures within these four districts.

Agricultural Land Assistance

~~The County should work with appropriate agencies and the owners of land zoned Agriculture along the Main Street corridor between Highway One and Santa Rosa Creek to help maintain their land in agriculture. Desired outcomes are to control off-site water runoff onto these properties; to minimize erosion and siltation; to educate adjacent land purchasers of the effects of agricultural operations; and to control litter, dumping and trespassing.~~

E. Funding

There are a number of funding sources available to assist in the implementation of the improvements foreseen in this Design Plan.

Existing Local Funds

Several increments of funding have already been set aside for improvements foreseen in this plan. For example, the County has already set aside \$500,000 from its park development fees for development and enhancement of existing open space in Cambria. Similarly, the San Luis Obispo County Council of Governments has set aside \$100,000 for improvements on Moonstone Beach Drive. Some of this money may be used for improvements in this plan.

Private Funding

Private organizations may be able to assist in acquiring some lands and making some improvements foreseen in this Plan. For example, the Cambria Historical Society has stated that it is currently in the process of acquiring the Guthrie-Bianchini House for use as a museum. In some cases property owners improving their properties would be required to fund improvements on adjacent public streets.

Transportation Funds

Many street and trail improvements foreseen in this Plan would require public funding. There are several transportation-related funding sources that can be used for street and trail projects in this Plan.

TEA21 Funds. TEA 21 is the new federal legislation that funds surface transportation projects. TEA 21 funds are split from their federal source into State TEA funds and Regional TEA funds. Funds under TEA 21 are split into two groups. The first group is designated for projects that represent maintenance on existing systems, this group receives 75% of the funds. The second recipient group, system expansions (including the addition of bicycle lanes) receives 25% of total available TEA 21 funds. TEA 21 funds are programmed for the first 3 years of allocation at the County level. The State funding is not currently programmed, and projects will need to have strong Caltrans support. ~~In 2001, there will be another cycle of applications and allocations.~~

There are several potential funding sources within TEA21, which include CMAQ and STP, Article 3, and County Enhancement funds,

State Bicycle Transportation Act. Using an approved Bicycle Plan, the County can also apply for funds to support bicycle projects through the State Bicycle Transportation Act. These funds are available to projects called out in the County’s Bicycle Plan for construction of new facilities.

Historic Preservation Funds

There are relatively few grants available for historic preservation. However, the State Office of Historic Preservation has a “certified local government” program, which provides for state funding of historic resource surveys and preservation plans. The total amount available annually is said to be about \$200,000 with eight or ten grants awarded each year.

Maintenance Costs

In addition to capital and one-time programs, the County and local bodies should work to ensure on-going maintenance of existing and new facilities in Cambria.

As part of this effort, the County and CCSD should identify entities responsible for maintaining each type of improvement, and should also identify funds to offset County maintenance costs. In some cases, County General Funds may be appropriate funding sources, but funding for maintenance could also come from locally paid taxes (such as transient occupancy tax) or assessments on local businesses through a method such as a Business Improvement District. In addition, local businesses and organizations could be asked to contribute in-kind services toward maintenance.

This could take the form of an individual business assuming responsibility for maintenance of improvements in the right-of-way in front of the business, and it could also take the form of businesses or other organizations volunteering to conduct maintenance tasks in specific areas as a community service. Such efforts could be overseen by the Chamber of Commerce or some other local organization.

APPENDIX A

Planning Process

Community participation and input was the keystone to the planning and design process for the Cambria Design Plan.

The process was overseen by a Steering Committee, which was formed to provide a forum for education and consensus building among the citizens of Cambria. The committee was made up of representatives of the community who were appointed by the North Coast Advisory Council. The consultant developed the plan for Cambria based on the committee's direction and public input.

The 18-month community input process included a series of Steering Committee Workshops, which were open to the public. The process included the following steps:

- **Introductory Meeting.** The process was initiated with an introductory meeting in which committee members described their individual interests for the project and their background. Committee members also discussed the scope of work and developed ideas for public outreach.
- **Background Information.** County staff and consultants collected background information regarding Cambria's existing conditions, and presented this information at public meetings of the Steering Committee.
- **Vision and Goals.** The Steering Committee worked with staff and consultants to develop the vision and goals for the Design Plan. This step included work by the consultant to develop three alternatives for the East and West Villages with conceptual open space and circulation plans and typical street sections of major streets within each of the villages. A workshop was held where community members worked in small groups to mark up base maps with their ideas for development in Cambria.
- **Focused Design Input.** Following the discussions regarding vision and goals, the consultant developed separate draft Plan chapters regarding each of the four districts. The Steering Committee held separate meetings to review these chapters, which provided concrete input for the creation of this Design Plan.
- **Public Review Draft.** The Public Review Draft Plan, provided for public input that occurred through written correspondence and a public workshop. Following the community workshop, the Steering Committee will make recommendations to the North Coast Advisory Council (NCAC), which approved the draft with the Steering Committee's recommendations.

- Public Hearing Draft. This is the Public Hearing Draft, which includes the Steering Committee and NCAC recommendations. This draft will be the subject of public hearings with the:
 - Planning Commission.
 - Board of Supervisors.
 - Coastal Commission (as part of the Local Coastal Program).

APPENDIX B

Monterey Pine and Pine Pitch Canker

Background Information

UC Davis plant pathologist, Tom Gordon and local horticultural expert, Burt Wilson, believe that up to 10% of Monterey Pines are resistant to the canker. Burt Wilson stated that the Troy Pine Forest near San Diego fought the canker and lost approximately 80% of its trees. The Troy Pine forest now appears like a forest after a fire and is growing back. Tom Gordon specifically stated that there is 85% infection rate; however that does not equate to a mortality rate. In fact, Gordon recommends planting seedlings from uninfected trees because without regeneration efforts he states the forest will die out from old age and canker.

The Cambria pine forest has been found to be the most biologically diverse of the three pine stands in California and thus is the most likely to survive the pine pitch canker.¹ Raymond Wilson of the Cambria Land Conservancy also believes that local endemic pine stock is more resistant to the canker than the standard stock for sale in most nurseries. The Cambria Land Conservancy has recently received a grant to develop resistant trees from the local stock. Within the next 18 months Tom Gordon of UC Davis who is working with the Conservancy believes that there will be resistant local seedlings available. Thus replacement and new plantings of Monterey pine trees using local Cambria stock is possible and is likely to result in the maintenance of some of the pine forest, even as pine pitch canker takes its toll.

Trees generally succumb to pine pitch canker when they are under stress because they are not living in their ideal habitat conditions. The most common causes of stress to pines in Cambria are a lack of water, intrusion by non-native plants and development near trees. When underground water sources are disrupted, trees downstream are stressed due to a lack of water. Where non-native plants are installed in pine stands, the relationship between the mycorrhiza, a form of beneficial fungi, and the pines is altered. This relationship is essential to a Monterey Pine's health because mycorrhiza help convert nutrients in the soil into a usable form for the trees to absorb and sustain their growth. In addition, where development occurs near trees, the microclimate of the tree is altered and the area of the root zone is often affected creating stress on the tree. This shows that minimization of ground water disruption, avoidance of non-native understory around Monterey pines, and development buffer zones can help to preserve pines in Cambria.

¹ Wilson, Raymond O., A Pre-proposal to Preserve the Urban Cambria Pine Forest.

Newly Fungus Poses Dire Threat To the Majestic Monterey Pines

CAMBRIA, Calif., June 9 (AP) — Scientists fear that 80 percent of the giant Monterey pines that rise in splendor from the rolling hills along the Pacific Coast could be gone in the next 30 years.

A killer fungus, the pine pitch canker, is threatening the coastal pines, along with other species of pines from the Cascades in the north to the Sierra Nevada in the east.

The environmental threat spreads far beyond California. The Monterey species is the most widely planted non-native tree outside the United States, serving as the core of multi-billion-dollar timber industries in Canada, New Zealand and Australia.

"People are panicked about this and probably rightfully so," said Bill Libby, a tree geneticist for a New Zealand timber company who is working in California as a consultant. "We haven't found any way to stop the disease."

Infected trees first go brown at the ends of their branches, then erupt in spots of oozing pitch and debris that give the fungus its name. Water and nutrients are choked off, and the open infections allow bark beetles to bore into tree trunks and lay eggs that hasten the deadly process.

Pine pitch canker is typically spread by insects. The disease been around for about 50 years but had previously been restricted to the southeastern United States and Mexico. Since first being detected in California in 1986, the fungus has turned up all over the state on various kinds of pines. But no species has been as vulnerable as the Monterey pine, which at maturity of 30 years commonly tops out

A disease threatens one of the world's most popular trees.

at 100 feet.

California is home to three of the world's five native Monterey stands. The largest is on the Monterey Peninsula, and the smaller ones are at Cambria, near the famed Hearst mansion at San Simeon, and at Point Año Nuevo, west of San Jose.

The only disease known to have wiped out virtually an entire tree population is chestnut blight, which struck American chestnuts in the Northeast in early 1900's, Mr. Libby said.

Pine pitch canker shares more traits with Dutch elm disease, which killed off many stands of elms on the East Coast and remains a threat. Both are carried by beetles and maintain resistant strains, he said.

The pine fungus has behaved differently in each of California's stands and has already mutated several times.

At the University of California at Davis, the forestry department is testing hundreds of pine species in a search for a tree resistant to the disease.

A state task force, formed in 1985, has spent almost \$1 million studying the problem. A bill pending in the state Legislature would provide an additional \$2 million.



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DAVIS, CALIFORNIA 95616-8680

7 January 2000

Lisa Howard
Community, Design & Environment
1600 Shattuck Avenue
Suite 222
Berkeley, CA 94709



Dear Lisa:

As per your request I am summarizing here my comments on the options for re-establishing a Monterey pine forest in areas where pitch canker has been a problem. My ideas follow from the assumption that it is desirable to maintain the character of the forested areas in and around the community of Cambria, and that retaining Monterey pine as a dominant member of the native forest and the surrounding urbanized forest contributes to that objective. Because our studies have shown that some level of resistance occurs in all the native populations of Monterey pine, regeneration should produce a stand of trees that includes some resistant individuals. For this reason I would offer natural regeneration as the preferred option. If conditions are such that seedlings can become established, pitch canker will eventually challenge them and select for those that are resistant. Of course, the numbers of resistant trees that chance to arise at any given location cannot be predicted and the smaller the area the greater the chance that this number will be unacceptably small. Various measures may be taken to enhance regeneration and urban foresters and/or the California Department of Forestry could be consulted for guidance on this point. It may also be desirable to introduce seed taken from trees in the Cambria area that are known to be resistant. I would consider this to be option number two. Lastly, if the first two options were not successful or deemed to risky to undertake, option three would be to use known resistant seedlings as transplants. Whereas there is presently no source of such resistant material, this may change within the next two years. I hope this information is helpful.

Sincerely,

A handwritten signature in cursive script that reads "Thomas R. Gordon".

Thomas R. Gordon
Professor

Thomas R. Gordon's Letter from the University of California, Davis

**Ken Topping's Notes on the Forest
Design Plan Steering Committee
8-25-98**

In Cambria, the forest is an integral part of community design considerations. It is part of the everyday experience of Cambrians and visitors alike.

- The forest was here long before people. Though it was logged for many years it has managed to survive.
- Early subdividing in the forest gave us our unique neighborhoods. It also has endangered the forest.
- The forest forms a "green belt" around most of Cambria. Various groups including the State Parks Department are interested in preserving portions of this through acquisition.
- The forest provides a "green frame" for most of our activities, especially along the two major ridges above the East and West Villages. It is one of the important features that attract visitors and tourists. So it's important for economic as well as aesthetic and environmental reasons.
- Where lots have been retired through District water position transfers and County Transfer of Development Credit programs, the forest can provide smaller "green areas" within residential neighborhoods.
- The forest tends to reseed itself. That's why the District this year has flagged seedling trees on District-contracted weed abatement lots.
- The County's tree replacement program was designed to offset development impacts on the forest but isn't working because of enforcement and implementation problems.
- In ocean-facing hillside neighborhoods, there is a conflict between resident's feelings about preserving the forest and having ocean views. The trees ultimately tend to be the losers in such cases.
- Pine pitch canker is only one of a series of threats to the forest. To save it, we need to get together on tough issues such as hazardous tree removal, tree replacement, and lot consolidation and come up with a community-based strategy that can be more readily implemented.
- The Design Plan Steering Committee can help refine these ideas in cooperation with other interested groups such as the Forest Management Committee, the Parks Recreation and Open Space Commission, Greenspace, the Cambria and San Luis Obispo County Conservancies.

Ken Topping's Notes on the Forest Design Plan Steering Committee August 25, 1998

A PREPROPOSAL

To Preserve the Urban Cambria Pine Forest

An aerial photograph of the coastal town of Cambria CA. (near Hearst Castle) reveals a Monterey pine forest ecosystem with development dispersed among the trees. A large number of houses are hidden by trees but most buildings are constructed in natural breaks in the forest. Much of the Cambria Monterey Pine Forest is in agricultural areas surrounding the community still relatively undisturbed by development and in a state park preserve. Large sections of forest remain in viable clusters spread throughout the urban area as well.

The Cambria Land Conservancy supports efforts now underway to purchase the ranches which contain parcels of forested land or to obtain conservation easements which would protect the forest within them. Large land conservancies and the California State Parks system have all committed funds to this effort. The Cambria Land Conservancy believes it is also possible to preserve the remaining urban forest areas by purchasing key lots or conservation easements. We have identified the areas where the urban forest is still viable and are seeking funding to preserve them. These clusters would then remain part of the larger forest system on surrounding agricultural land and in the state park preserve.

The Cambria pine forest is historic, one of three along the California coast which are the ancestors of Monterey pines around the world. The Cambria Monterey pine forest has been found to be the most biologically diverse of the three and thus most likely to survive. A significant percentage of Cambria pines have been found to be resistant to pine pitch canker which is currently threatening all three of the original Monterey pine forests.

These pines have withstood the assaults of nature for a hundred thousand years and the assault of man for the past 150 years largely because they are communal in nature, sharing root systems and thus water sources. Ground water is often in short supply but does trickle from the nearby mountains to the sea in underground streams even during periods of pro-

longed drought. When these underground water sources are disrupted, trees downstream are stressed and often succumb to insects and disease.

Many of the surviving urban clusters are in hilly areas on undersized lots difficult to build on. They are beautiful and distributed in a manner that gives Cambria its distinctive ambiance. Inevitably, these lots will be developed if they are not purchased and set aside to preserve the remaining pine forest. Cambrians, individually and through organized groups, have indicated their willingness to help save the forest because it is critical to the ambiance of the community and to its tourist oriented economy.

The Cambria Community Services District is currently in the process of developing a Forest Management Plan which will address the problems of maintaining the urban forest. A consortium of conservation groups is cooperating in efforts to maintain the local pine forest ecosystem which lies outside the city boundaries. The County Firesafe Council is now focused on preventing wildfires in the area. The County Planning department is currently spearheading a cooperative effort to formulate a design plan for the entire village of Cambria. Funding is available for a system of trails linking resources in the community.

The Cambria Land Conservancy has initiated a tree replanting service sanctioned by the county and in cooperation with the Cambria Community Services district. We maintain that such cooperation between private groups and public agencies will result in proper longterm stewardship of the forest.

The local media has been giving good coverage to these developments and, judging by letters to the editor and the content of columns, community interest is at a peak. A recent survey indicates strong support among Cambrians for preserving open space and managing hazards in the forest. In short, the community understands the importance of its urban forest and seems ready to assume responsibility for its maintainance.

Raymond O. Wilson
President, Cambria Land Conservancy (805) 927-8546

APPENDIX C

Policy and Planning Context

The Cambria Design Plan works in conjunction with several County planning documents, including the County General Plan, the North Coast Area Plan, the County-wide Design Guidelines Manual, the Main Street Enhancement Plan and the Cambria Community Services District's Parks, Recreation and Open Space Master Plan, which are all described below.

County General Plan

The San Luis Obispo County General Plan is comprised of a set of documents that meet the requirements of State General Plan law and the California Coastal Act (Division 20 of the Public Resources Code, Section 30000, et seq) and provide a comprehensive framework for the regulation, protection and management of coastal resources. To comply with the requirements of the Coastal Act, the General Plan includes a set of documents that are used to implement the Local Coastal Program.

One of the elements of the General Plan is the Land Use Element and Local Coastal Program. This is not a single document, but a set of documents divided into two major groups, which are the "Land Use Plan" and the "Ordinances." Since the Cambria Design Plan is a planning and design document with conceptual designs and development regulations, this discussion focuses on both types of documents which include the Land Use Plan and the Coastal Zone Land Use Ordinance, Title 23 of the San Luis Obispo County General Plan. The Land Use Plan is comprised of four types of documents, which include Coastal Plan Policies, Coastal Framework for Planning, the Area Plans (of which there are four), and land use category maps for most of the County. The Cambria community is within the jurisdiction of the North Coast Area Plan.

Coastal Plan Policies

The Coastal Plan Policies of the Local Coastal Plan provide overall policy direction for the management of land use within the coastal zone. The Coastal Plan Policies document is divided into chapters that cover shoreline access, recreation and visitor serving facilities, energy and industrial development, commercial fishing and recreational boating, environmental and sensitive habitat, agriculture, public works, coastal watersheds, visual and scenic resources, hazards, archaeology and air quality.

- **Shoreline Access.** The policies within the Coastal Plan encourage the protection of existing coastal access and the provision of new access, and require that new coastal access be required with new development.
- **Recreation and Visitor Serving Facilities.** Coastal Plan policies encourage the preservation of existing recreational opportunities and the expansion of such opportunities where feasible. Visitor-serving recreational facilities are given a priority over non-coastal dependent uses.

- **Energy and Industrial Development.** The Coastal Plan recognizes the need to accommodate coastal-dependent industrial and energy development. However, policies in the Plan encourage decision-makers to weigh the environmental consequences of allowing continued or expanded industrial and energy land uses.
- **Commercial Fishing and Recreational Boating.** Policies in the Coastal Plan encourage the protection of commercial and recreational boating facilities, and give priority, where feasible, to the expansion of such facilities.
- **Environmentally Sensitive Habitats.** Policies within the Plan protect and preserve coastal resources such as wetlands and riparian corridors from development, and where feasible, restore and enhance such resources.
- **Coastal Watersheds.** The Coastal Plan contains policies to help maintain the long-term viability of coastal watersheds and to manage new development so that their long-term viability is maintained.
- **Visual and Scenic Resources.** The Coastal Plan provides guidance for new development relative to the protection of scenic resources, and encourages the preservation of existing resources. Moonstone Beach and Main Street in Cambria are each identified as Special Communities; Moonstone Beach provides visually attractive areas with views of the ocean, while Main Street is identified as possessing an attractive village atmosphere surrounded by hills and pines.

According to the Coastal Commission, the protection of visual and scenic resources concerns “the protection of ocean and coastal views from public areas such as highways, roads, beaches, parks, coastal trails and accessways, vista points, coastal streams and waters used for recreational purposes and other public preserves rather than coastal views from private residences where no public vistas are involved.”

- **Hazards.** These policies provide guidance for the protection of lives and property from natural and human-made hazards within the coastal zone, including floods, unstable geology, erosion, fire, and sea-cliff retreat.
- **Archeology.** The Coastal Plan contains policies relating to the identification and preservation of archaeological resources.
- **Air Quality.** The Coastal Plan encourages the preservation and enhancement of air quality through implementation of the policies and programs of the Air Quality Management Plan.

Coastal Framework for Planning

The Coastal Framework for Planning portion of the Land Use Plan is a broad document that references the Local Coastal Plans, which are each contained in a separate document, and defines the policies and procedures that apply to the unincorporated area of the Coastal Zone, and describes how it is used in relationship to the Coastal Zone Land Use Ordinance. The primary components of the Coastal Framework for Planning are:

- **Land Use Categories.** The document contains descriptions of land use categories, and the population density and building intensity criteria used to evaluate individual development projects. These land use categories are mapped in the Official maps and are further refined in the Local Coastal Plans.
- **Allowable Densities.** The Coastal Framework for Planning specifies parcel sizes, population density and building intensity criteria for consistency of land division proposals with the General Plan and Local Coastal Program.
- **Allowable Uses.** Coastal Table O of the Coastal Framework for Planning defines the land uses that are allowed within a land use category in which a proposed development site is located. Coastal Table O identifies uses that are allowed within a category as either “allowed,” “principally permitted” or “special standards required.” For those uses identified in the latter category, references are cited to the appropriate sections of the Coastal Zone Land Use Ordinance, which define such special standards.
- **Combining Designations.** The document also explains the criteria for application of the land use categories and combining designations to planning areas. Combining designations generally apply to those areas in which there are unique resources or potential hazards.

North Coast Area Plan (NCAP)

The NCAP is a part of the Local Coastal Program that describes County policies specifically applicable to Cambria and the North Coast Planning Area. This document enhances the Coastal Plan Policies and supersedes or supplements the standards in the CZLUO. It includes chapters covering population and housing, public facilities and services, land use, circulation, combining designations, and Planning Area Standards. The Planning Area Standards provide the community with specific development regulations for areas within Cambria. The regulations in this Plan comprise additional standards that will be incorporated in to the NCAP if this Plan is adopted.

Coastal Zone Land Use Ordinance (CZLUO)

The CZLUO provides standards that detail and enforce the policies set forth by the Coastal Plan Policies and the Coastal Framework for Planning. The document is divided into chapters that cover permit applications, permit requirements, site design standards, site development standards, operational standards, combining designation standards, special uses, nonconforming uses and enforcement.

Historic Preservation Policy

Historic preservation policy in Cambria is provided in three county documents: the Local Coastal Program: Framework for Planning (LCP), the Coastal Zone Land Use Ordinance (CZLUO), and the North Coast Area Plan (NCAP). The Coastal Framework for Planning provides the purpose and objectives of an Historic Site Designation. The CZLUO contains policies to apply to the parcels that are under an Historic Site Designation. The NCAP lists and provides brief descriptions for the specific properties that are subject to the Historic Combining Designations in Cambria and other north coast communities.

Currently an Historic Site (H) overlay zone is applied by the CZLUO to some of the East Village historic properties and to 'Nitwit Ridge' in the Happy Hill neighborhood. The purpose of the Historic Combining Designation is to:

- “enhance and perpetuate the use of” historic structures and sites,
- “to promote the development and maintenance of appropriate settings and environments for such structures”, and
- “to promote the enhancement of human life in its educational and cultural dimensions.”

The CZLUO requires a Minor Use Permit (MUP) for construction, alteration or repair of any structure within an Historic Site combining designation. The policy promotes the purposes of the 'H' overlay zone and prohibits demolitions of structures.

Main Street Enhancement Plan

The Main Street Enhancement Plan is a conceptual streetscape plan for Main Street between Burton Drive and Cambria Drive. The plan, which is being prepared by the Engineering Department of the County, proposes to reconfigure the street to include two traffic lanes, a center turn lane or landscaped median, two parallel parking lanes, two bike ways, bulbouts, street trees and a trail on the downslope side of Main Street.

Cambria's Parks, Recreation and Open Space Master Plan

This Master Plan was prepared for the Cambria Community Services District (CCSD) by the County with the understanding that the CCSD would administer the plan, providing local responsibility and authority. The master plan document identifies goals, objectives, policies and locations of trails, public open spaces, recreational uses, management and implementation for all areas of Cambria.

APPENDIX D

Historic District Information

According to ~~Ms. Saunders~~, California Register Coordinator, at the Office of Historic Preservation in Sacramento, the County should look at three basic types of districts; locally regulated, state regulated, or federally regulated.

The locally created historic district would provide the community with more control and more flexibility of the historic resources. The district could create a specific plan, design guidelines, an historic preservation ordinance, and could create any type of incentives the County deems fit. The types of local incentives are limited only by the imagination. Typical incentives that have been utilized by other communities include Transfer of Development Rights, permit fee waivers, and parking requirement waivers. The County should talk to other communities that have created historic districts and are similar in circumstances to Cambria. The community of Mendocino, on the coast in northern California, is an historic district that is overseen by Mendocino County through a Historic Preservation Commission overseen by the County Board of Supervisors. With a locally-regulated historic district the community could determine how loose or how stringent the guidelines and incentives should be.

The state and federally created historic districts are very similar. In either case the historic district would work with the National or California Register of Historical Resources. The criteria for historic resources for each of these agencies is very broad, including buildings and sites that are related to significant events for the community or trends of development. In either of these districts the regulatory control for preservation is maintained through the California Environmental Quality Act (CEQA).

State incentives for historic preservation include the State Historic Building Code and the use of the Mills Act. The State Historic Building Code can be applied to all historic districts, whether local, state or federal. This code would replace the Uniform Building Code (UBC) to allow for building requirements that are more appropriate for older buildings. The use of the Mills Act is also available to districts that are local, state or federally controlled. Under the California law, the Mills Act, the County Assessor can provide property tax relief for owners of historic building as an incentive for rehabilitation. The Mills Act allows owners of residential properties to write off up to one half of their income taxes with proof of preservation or restoration of their historic property. This usually involves a tradeoff for restrictions on building appearance and occupancy and it requires enactment of County legislation.

The Rehabilitation Tax Credit is the big incentive created by the federal government. The tax credit allows owners of income producing properties to apply up to 20 percent of the renovation costs of the historic structure to a tax credit on their federal income taxes. This incentive is only available to historic districts that are federal historic districts.

These new policies and a new district should provide added investment in the community, increased tourism, a reversal of property value decline and additional property tax revenues, as well as preservation and enhancement of these historic resources.

Documents that provide more detailed information on historic resources include:

- Historic Preservation Incentives in California, compiled by the California Office of Historic Preservation
- 1998 California Historical Building Code, by California Building Standards Commission
- Historical Resource Registration Programs in California, by the California Office of Historic Preservation
- California Register of Historical Resources: The Listing Process, by the California Office of Historic Preservation
- California Register and National Register: A Comparison, by the California Office of Historic Preservation
- The National Register of Historic Places, by the United States Department of the Interior National Park Service
- National Register Bulletin: How to Apply the National Register Criteria for Evaluation, by the United States Department of the Interior National Parks Service