

CALIFORNIA COASTAL COMMISSION

NORTH CENTRAL COAST DISTRICT OFFICE
45 FREMONT ST, SUITE 2000
SAN FRANCISCO, CA 94105-2219
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Th12b



Prepared May 10, 2011 (for May 12, 2011 hearing)

[Click here to go to the original staff report.](#)

To: Coastal Commissioners and Interested Persons

From: Charles Lester, Deputy Director
Ruby Pap, District Supervisor
Roxanna Farshchi, Environmental Services Intern

**Subject: STAFF REPORT ADDENDUM for Item Th12b
Appeal Number A-2-SMC-11-010 (San Mateo County Parks Dept., Moss Beach,
San Mateo Co.)**

The purpose of this addendum to the staff report is to correct minor errors in the findings and respond to concerns raised by the Appellant regarding drainage issues. Staff continues to recommend that the Commission approve the permit application with special conditions. Additional text is shown in underline and deleted text is shown in ~~striketrough~~.

- 1) Add to page 8, paragraph 3, starting at “this trail connection will be the only means of accessing the beach and tide pools at the north and south.” Although there is a parallel trail which runs along the bluff top of the Reserve, this section very narrowly and steeply can be connected back to the bridge over San Vincente Creek. Not only is this area not a feasible alternative connection to the north end of the reserve for anyone with physical difficulties, but it is also not a feasible alternative for school groups, which generally consist of 30 or more students. Additionally, as a result of the tsunami events on March 11, 2011, the extreme erosion along the bluff of the Reserve has resulted in closing the bluff top path for school groups, and has made the area completely inaccessible for anyone with physical difficulties.
- 2) Delete the following on page 14 of the staff report: ~~The County, acting as CEQA lead agency, found the project to be categorically exempt from environmental review under CEQA.~~ Add the following: San Mateo County Parks completed a mitigated negative declaration for the project.
- 3) Add to page 12, at the end of paragraph 4: As seen in Exhibit 3, page 8, the cross-section of the trail south of the bridge will be comprised of a fabric filter and subsequent clean fill in order to allow drainage to flow properly through the trail at this location.



California Coastal Commission
May 2011 Meeting in Sonoma
Staff: Roxanna Farshchi

Th12b

Due to a severe medical problem I am unable to attend today's session. I am very sorry about this but have no choice in the matter. So maybe someone will read this statement which I would have very much like to have presented to you for I care very deeply about the Reserve and visit it often.

My wife and I have lived within a 1/4 mile of the FMR for 35 years and usually visit it 1 or 2 times week since we moved here. We enjoy the place very much. Based on this experience, my experience with the County when Sony did the filming and my experience with the way the County's has gone about implementing its Master plan for FMR I would characterize the SM County Parks Dept. as being very poor stewards of the Reserve.

Considering the specific issue before you, the width of the Dardanelle Trail, at a meeting a few month ago I asked Dave Holland if the width of the trail had been determined using formal calculation and science based on the number of people using the trail and the way they would use it or if it had been determined more on the basis of his experience and an educated guess. His answer, which was recorded on video, was that it was the latter, based on his experience.

In follow up questions I asked if this new width of 8' was chosen to allow Park Service trucks easy access to the property. Mr. Holland denied that the path would be used by any Park vehicles at that meeting.

But it turns out that Mr. Holland was not correct about this. At the Planning Commission Meeting where this part of the FMR improvements was approved, Parks Dept. Superintendent Gary Lockman admitted that Park Service vehicles would use the trail from time to time both to collect garbage and to provide emergency access. This is in fact one of the reasons the County doesn't want a trail width being less than 8' because then vehicles couldn't drive on it.

Actually 8' is not quite correct. In some places that have to be sloped to comply with ADA, Mr. Holland stated shoulders will be needed in order to stabilize the path. These shoulders will be about 12" - 18" wide depending on location so in effect the overall width of this "trail" is going to be more like 10'-11'.

Subsequent to that meeting I visited Yosemite as we have done often in the past 40 years. I noted the path widths around Lower Yosemite Falls from the bus stop to the main entrance to the falls and to some of the lesser paths in the area. This is one of the most popular spots in the park.

I have photographed in this area extensively over the years with my old style view camera like Ansel Adams used. I am typically at a particular spot for 30-90 minutes at a time. Based on my personal experience and visitor data I researched, I can say that the 10' wide paved path btw the bus stop and the main entrance comfortably accommodates 1500-2000 people a day and that figure includes some bikes, people pushing baby strollers and people in wheelchairs. The secondary paths in the area are about 6' wide. Typically in an hour I'd see 40-50 people, a few bikes and a few people pushing strollers in the busier parts of the secondary areas. In the more remote paths in the secondary areas, the width is also 6' and the number of people varies from 20-30 people and hour to 10-20 people an hour mostly dependent on time of day.

Based on my personal experience in FMR, and the last published FMR annual visitor count that I could find, about 200,000 people/year, the number of FMR visitors are less than 1/10th the number that visits Lower Yosemite Falls. So the number of people that walk the Dardanelle Trail on a busy weekend at the busiest time is at most is 20-30 people an hour plus maybe a few bikes. The width of the Dardanelle Trail is now is about 8' wide for the first 200' from the SE Cypress Entrance and then narrows to 3'-4' wide, as it makes its way to the ocean. It is uneven dirt and is not conducive to strollers. It is not wheelchair accessible.

The initial width of 8' wide is a by-product of Sony's filming *Memoirs Of A Ghesha* at the Resevere btw Dec. 2004 and Jan. 2005. Sony drove 20' box trucks, pickup trucks and a fleet of 6-10 ATVs on this section of the trail to the point a 1/4 mile to the NW and the path width doubled to 10'. In the past 7 years the edges have filled

in resulting in its current 8' width.

A path that is 5' - 6' wide is more than enough to carry the traffic that uses the Dardanelle Trail based on my experience on this exact trail, based on my experience on the other parts of the Reserve, based on walking on the POST property by the tracking station which has 5' -6' wide paths and double the traffic of the Dardanelle Trail, and walking extensively in Yosemite and in many other National Parks and wild places in the US, Canada, Jamaica, England, France, Nepal, China and Japan.

If it the proposed trail got a hard packed service such as the GravelPave proposed and if it were properly graded, it could certainly accommodate strollers and wheelchairs. And I would strongly support such a path provided it was a light greyish color that fit in well with the surroundings.

I agree that sometimes people would have to work out passing each other. It happens once and awhile on the bluffs. But I've never seen someone on a bike collide with someone walking or even heard about it. But my wife who rides her bike to work from our home in Moss Beach to Cunha Middle School in HMB where she teaches using the Coastal trail has had to dump her bike more than once and has seen accidents between bikes and people several times. This has happened on the WIDEST part of the trail which is some places is 10' wide. It seems like people just don't pay attention and mosey down the middle of the trail.

When I asked Mr. Holland if he had any information about the frequency of collisions on the Coastal Trail in HMB or in the Reserve he said he had no info.

So it comes down to only 3 reasons Parks wants the path to be 8' wide.

1. To provide more clearance between people walking, pushing a stroller or in a wheelchair and other traffic like people on bikes or skateboards on the trail in the interest of safety. My wife's experience and the lack of data do not support this concept though it is anecdotal. On the other hand the Parks Dept. has not provided any evidence that a wider trail is any safer than a narrow one especially when the traffic on it is very light.
2. To provide a link btw Lake St. and Vermont Ave and Cypress Avenue. Currently the only way to do this is to walk on the Dardanelle Trail or go out to Hwy 1.

I personally believe that if there was a road linking Lake and Vermont to Cypress, that the County would be far less aggressive in maintaining that an 8' wide paved path was absolutely necessary. But I don't think the Reserve should suffer because of this. Let the County find funds to build such a road. That's the correct solution.

3. Provide a way for Parks to drive their vehicles on it. They steadfastly denied this at many meetings but finally in the last moments of the Planning Commission meeting they admitted this in public for the very first time.

I think you should reject the current proposal until they come up with a trail that is, well, a trail and more in keeping with the character of the FMR. Putting a road there which is what the County is requesting, is completely out of scale with FMR.

People come to the Reserve to go to the tide pools not to walk on the Dardanelle Trail. So a smaller, hard surfaced path of 5'-6' keeps the character of the Reserve intact. It will be wheelchair accessible, will be friendly to strollers and will be more than adequate to carry the expected load.

Alan Harris

Th12b

Senior Coastsiders

•Meals •Transportation •Case Work •Information & Referral •Volunteer Coordination •Recreation & Education

RECEIVED

April 4, 2011

MAY 09 2011

David Holland, Director
San Mateo County Department of Parks
455 County Center, 4th Floor
Redwood City, CA 94063

COASTAL COMMISSION
NORTH CENTRAL COAST

Board of Directors

- Suzanne Black
- Michele Borovac
- Howard Hayes
- Marian Herreid
- Susan Kealey
- James Kellenberger
- Tom Kirkpatrick
- Janet Schade
- Nancy Stern
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**Board Members
Emeritus**

Dr. G. Desalermos

**Executive
Director**

Cara Schmaljohn

Dear Mr. Holland:

Half Moon Bay Senior Coastsiders is pleased to support the Department of Park's efforts to bring public access and trails to the Fitzgerald Marine Reserve. We welcome the opportunity that will offer our senior and disabled citizens access and the enjoyment of this natural wonder in our community by meeting compliance with the Americans with Disabilities Act (ADA)

Title II of the ADA requires that State and local governments give people with disabilities an equal opportunity to benefit from all of their programs, services, and activities. This includes parks and trails, and does not exempt nature reserves operated by governmental entities.

Currently, the Fitzgerald Marine Reserve is not accessible to people with disabilities (including children) with mobility challenges and/or use wheelchairs and other mobility adaptive devices (walkers, canes etc.).

It is unimaginable to us, that any member of our community would be opposed to allowing the Fitzgerald Marine Reserve and its rich biodiversity to be accessible to all residents and visitors in San Mateo County.

Sincerely,

Signature on File

Janet Schade, Senior Coastsiders Board of Directors

Cc: Honorable Carole Groom, San Mateo County Board of Supervisors
David Boesch, County Manager
Lisa Mancini, Director, Aging and Adult Services

66

Funded primarily
by private donations
and:

San Mateo County
Area Agency
On Aging

Main St. Beautification
Committee
Half Moon Bay

Th126

Roxanna Farshchi

From: nina feero [ninaf@sbcglobal.net]
Sent: Friday, April 08, 2011 3:54 PM
To: Roxanna Farshchi
Subject: Fitzgerald Marine Reserve

As a volunteer naturalist at the Fitzgerald Marine Reserve, I write to support emergency passage of the Coastal Development Plan for the Reserve. The imminent loss of the bridge over San Vicente Creek will effectively shut off a wonderful hiking trail, and severely limit access to the tidepools during the winter. Docent-led field trips to the tide pools are an important part of school curricula throughout the greater Bay Area. We lead field trips for children as young as third grade through college-level marine biology classes. These trips are so important to many teachers that they schedule them a year in advance to ensure access. The low tides during the winter months occur often during daytime hours, while summer low tides are often during night and very early morning hours. So it is important to have access during the winter when the creek is high. Please help us get our bridge replaced and our trail improved.

Thank you for your consideration
Nina Feero



Developmental Disabilities Board Area 5

Protecting and advocating for the rights of persons with developmental disabilities in Alameda, Contra Costa, Marin, San Francisco and San Mateo counties

Th 126

April 21, 2011

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California Coastal Commission
Northern Central Coast District Office
45 Fremont Street, Suite 2000
San Francisco, CA. 94105-2219

APR 25 2011

CALIFORNIA
COASTAL COMMISSION

Dear Coastal Commission,

On behalf of Area Board 5, a State agency that advocates on behalf of people with disabilities, I am writing to urge the Coastal Commission to approve the Accessibility Improvement Project slated for the Fitzgerald Marine Reserve. This rocky seashore hosts a living community of marine life that is of great interest to visitors and naturalists alike. The reserve is set aside to protect this complex but fragile community for us and future generations. 5 years ago, a plan to build a multiuse trail through the east side of the park went before the Planning Commission, where it was approved. This trail would connect with the Coastal Trail.

With this new trail in place, all people, including those with disabilities, will be able to enjoy the beauty and wonders of nature. It is critical this project is approved immediately. Any continued delays in the process will cost the County \$750,000 in a grant that had been obtained for the renovation of the trail. The grant is due to expire in June if the project is not started by them.

Please don't let this happen. Please support access to Fitzgerald Reserve for all people.

Sincerely,

Signature on file

Rocio Smith
Executive Director



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*Elizabeth S. Furber
35 W. 28th Ave., #101
San Mateo, CA 94403
April 25, 2011*

Th/26

RECEIVED

APR 26 2011

CALIFORNIA
COASTAL COMMISSION

California Coastal Commission
Northern Central Coast District Office
45 Fremont Street, Suite 2000
San Francisco, CA 94105-2219

Greetings:

Concerning the recommendation for a multi-use trail at the Fitzgerald Marine Reserve, please support the proposal for community access to the Reserve, including the disability community. The park is a "Reserve," and it should be reserved for all of us.

Thank you for all your efforts to make the coast available to everyone.

Elizabeth Furber

Signature on file

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Roxanna Farshchi

Th12b

From: sabrina brennan [SABRINA@DFM.COM]
Sent: Monday, April 25, 2011 8:56 AM
To: Ruby Pap
Cc: Roxanna Farshchi; Charles Lester
Subject: FMR Trail: Staff Report for Application: A-2-SMC-11-010

Hello Ruby,

I appreciate all the work Coastal Commission staff has put into studying the proposed FMR trail project.

I have a couple questions about the staff report.

First Question

The FMR has two strands of the Coastal Trail, not one. The Dardanelle 1/4 mile strand is located on the east side of the Reserve near the spring fed ponds and the trail will be closed during construction. While construction is in progress visitors will still have trail and beach access on the Coastal Trail strand that is located on the west side of the Reserve. The staff report states the following, "this trail connection will be the only means of accessing the beach and tide pools at the north and south." This statement is not accurate. Can the staff report be corrected?

"Additionally, during times of high tide and seal pupping, this trail connection will be the only means of accessing the beach and tide pools at the north and south. School children in groups of 30 or more often participate in guided tours of the area. " -page 8 of the staff report

Second Question

Does the County have a plan for drainage mitigation for the seasonal creek that flows just west of San Vicente creek and across the trail site? This was the seasonal creek you mistook for San Vicente Creek on the day of the site visit. What will happen to the water that flows downhill from the spring fed ponds, across the proposed trail site, and into San Vicente Creek?

Thank you,
Sabrina Brennan

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COMMISSION ON DISABILITIES

April 25, 2011

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TH/26

APR 27 2011

Sara Wan, Chair
California Coastal Commission
Northern Central Coast District Office
45 Fremont Street, Suite 2000
San Francisco, CA 94105-2219

COASTAL COMMISSION
NORTH CENTRAL COAST

Dear Ms. Wan:

The San Mateo County Commission on Disabilities (CoD) respectfully urges the California Coastal Commission to support and approve the San Mateo County Parks' Coastal Trail Accessibility Improvement Project that would bring the public access and trails at the Fitzgerald Marine Reserve into compliance with the Americans with Disabilities Act (ADA).

Title II of the ADA requires that State and local governments give people with disabilities an equal opportunity to benefit from all of their programs, services, and activities. This includes parks and trails, and does not exempt nature reserves operated by governmental entities.

Currently, the Fitzgerald Marine Reserve is not accessible to people with disabilities (including children) who have mobility challenges and/or use wheelchairs and other mobility adaptive devices (walkers, canes etc.).

It is unimaginable to us that certain members of our community would be opposed to allowing the Fitzgerald Marine Reserve and its rich biodiversity to be accessible to all residents and visitors in San Mateo County.

If you have any questions regarding our position, please contact our CoD Staff Liaison, Craig McCulloh (573-2480 or via email cmcculloh@co.sanamateo.ca.us).

Sincerely,

Signature on file

Susy Castoria, President
San Mateo County Commission on Disabilities

Cc: Honorable Carole Groom, San Mateo County Board of Supervisors
David Boesch, County Manager
David Holland, Assistant County Manager
Lisa Mancini, Director, Aging and Adult Services

Aging and Adult Services Division

Lisa Mancini, Director

Board of Supervisors: Rose Jacobs Gibson • Carole Groom • Don Horsley • Adrienne Tissier
225 37th Avenue • San Mateo, CA 94403 • PHONE 650.573.2580 • TDD 650.573.2220 • FAX 650.372.0729
<http://www.smco-cod.org>

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Th12b

Fel Anthony Amistad
846 Sextant Ct * San Mateo, CA 94404
(650) 544-5221 email: felamistad@gmail.com

April 27, 2011

Roxanna Farshchi
CA Coastal Commission
45 Fremont St, Suite 2000
San Francisco, CA 94105

Dear Commissioner Roxanna Farschi:

Preserving our coast, such as the Fitzgerald Marine Reserve is a valuable and noble act. It is our treasure here in San Mateo County which owns the property and has attempted to be a good steward. When the County residents and leaders wanted to plan for the future of the Reserve, they invited the community to help in the decisions that would affect the way our park is used. Please keep this Reserve accessible especially to our challenged citizens who deserve every right to visit and use it. Please support community access to Fitzgerald Reserve, including the disability community. The park is a "Reserve," and it is reserved for all of us and not exclusively for just a few.

Background

The Parks Department spent 5 years and held 13 public meetings to develop a Master Plan, and two more years developing an EIR, which involved an additional 4 public meetings. When all the meetings were finished and reports from hydrologic, archeological, geotechnical, and biological experts were collected and paid for by taxpayers, a plan to build a multiuse trail through the east side of the park went before the Planning Commission, where it was approved. This trail would connect with the Coastal Trail.

The trail would be eight feet wide which is four feet narrower than is recommended for multiuse trails. Eight feet is probably what we need for a twin stroller being passed by a horse or a couple of cyclists or a person who uses a wheel chair. Any less than that will force people off the path into environmentally sensitive areas, trashing the edges of the trail.

There were objections to the plan, all but one of which was resolved. One person, Sabrina Brennan, still did not approve of this plan, and filed an appeal to the Coastal Commission, as was her right to do. She said if the trail was reduced to six feet wide she would drop her appeal. The hearing was the same day as the tsunami evacuation, and it was held in Santa Cruz. The Coastal Commission went ahead with the meeting, but did not approve or deny the trail, they asked for more studies, and postponed a ruling to the next meeting, which is in May in Mendocino.

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This continued process may cost the County \$750,000. Also, a grant that had been obtained for the trail will expire if the project is not started by June. Even if the plan is approved in May, we will lose the grant if there are any further appeals or conditions, or if we simply can't find a contractor who can commence work in time. We will also have to pay for all the newly requested studies. ***Therefore "time is of the essence." We have to act before it is too late.***

We also must understand where our tax dollars are being spent. One person can make a difference to "protect" a park in her backyard from safe access by the disabled. Further, if the improvements aren't made, the temporary bridge over the creek where the trail meets the parking lot will be removed because the flooding this winter has weakened its banks. Then no one will have access to the trail. The community would be deprived of use of a public reserve area. Let us be fair and equitable.

Again, it is clear and sensible not to let this happen. Please support community access to Fitzgerald Reserve, including the disability community. It is only reasonable and prudent to do what is right for the majority of the community! Please consider that time is running out and the community is not getting what it deserves.

Sincerely,

Signature on file

Fel Anthony Amistad 
County resident,
Member, Commission on Disabilities, San Mateo County



HEATH FAMILY CHIROPRACTIC, INC.

MARK T. HEATH, D.C.

339 MAIN STREET
HALF MOON BAY, CA 94019

(650) 726-5265

TH126

April 27, 2011

Attn: Roxanne Farshchi

I am writing this note in response to an article I read in the Half Moon Bay Review regarding the Fitzgerald Reserve trail. I find it difficult to believe that one woman can hold up the progress of the trail that can make access to the Reserve available to people who

"Chiropractic Care for the Entire Family"

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otherwise could not get to the
beautiful Reserve and it's surround
ing area.

Please make this trail available
to everyone. Thank you

Cathy Heath

Let commission know: Reserve trail is for everyone

Posted: Wednesday, April 6, 2011 12:43 pm

Susy Castoria

Let commission know: Reserve trail is for everyone

I want to be sure that every reader of your newspaper is aware of the power of one.

I don't know how many people on the coast visit Fitzgerald Marine Reserve regularly, but if you do, you know what a treasure it is. You may also know that there is a battle being fought there.

The county owns the property and has attempted to be a good steward. When it wanted to plan for the future of the Reserve the county invited the community to help in the decisions that would affect the way our park is used.

The Parks Department spent five years and held 13 public meetings to develop a Master Plan, and two more years developing an Environmental Impact Report, which involved an additional four public meetings. When all the meetings were finished and reports from hydrologic, archeological, geotechnical and biological experts were collected and paid for by taxpayers, a plan to build a multiuse trail through the east side of the park went before the Planning Commission, where it was approved. This trail would connect with the Coastal Trail.

The trail would be eight feet wide, which is four feet narrower than is recommended for multiuse trails. Eight feet is probably what we need for a twin stroller being passed by a horse or a couple of cyclists or a person who uses a wheelchair. Any less than that will force people off the path into environmentally sensitive areas, trashing the edges of the trail.

There were objections to the plan, all but one of which were resolved. One person, Sabrina Brennan, still did not approve of this plan, and filed an appeal to the Coastal Commission, as was her right. She said if the trail was reduced to six feet wide she would drop her appeal. The hearing was the same day as the tsunami evacuation, and it was held in Santa Cruz. The Coastal Commission went ahead with the meeting, but did not approve or deny the trail. Instead, commissioners asked for more studies, and postponed a ruling to the next meeting, which is in May in Mendocino.

This continued process may cost the County \$750,000 -- a grant that had been obtained for the trail will expire if the project is not started by June. Even if the plan is approved in May, we will lose the grant if there are any further appeals or conditions, or if we simply can't find a contractor who can commence work in time. We will also have to pay for all the newly requested studies.

We all need to be aware of where our tax dollars are being used. One person can make a difference to "protect" a park in her backyard from safe access by the disabled. Further, if the improvements aren't made, the temporary bridge over the creek where the trail meets the parking lot will be removed because the flooding this winter has weakened its banks. Then no one will have access to the trail. That would be a shame.

Please don't let this happen. Please support community access to Fitzgerald Reserve, including the disability community. The park is a "Reserve," and it is reserved for all of us. Make your voice heard by the Coastal Commission. You, too, can make a difference. The address is:

California Coastal Commission

Northern Central Coast District Office

45 Fremont St., Suite 2000

San Francisco, CA 94105-2219

Susy Castoria is a volunteer and president of the San Mateo County Commission on Disabilities. She lives south of Half Moon Bay.

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From: Lucy and Buz Williams <rolfegolf@earthlink.net>
Date: Thu, 28 Apr 2011 11:42:27 -0700
To: Roxanna Farshchi <rfarshchi@coastal.ca.gov>
Conversation: Accessible Trails
Subject: Accessible Trails

Th 126

Roxanna Farshchi
CA Coastal Commission
45 Fremont St, Suite 2000
San Francisco, CA 94105

Dear Roxanna,

I realize this letter was signed and sent by someone else. However, we discussed it as a group in San Mateo County Commission on Disabilities meeting. We all thought it was a well written letter and covered our concerns. I am a commissioner who is on crutches and used to walk trails without crutches. I would like to continue to be able to walk the trails.

Thank you for considering our concerns about accessibility.

Sincerely,

Lucy Hupp Williams
Commissioner, San Mateo County Commission on Disabilities and Board President, San Mateo Center for the Independence of Individuals with Disability

Dear Editor,

I want to be sure that every reader of your newspaper is aware of the power of one. I don't know how many people on the coast visit Fitzgerald Marine Reserve regularly, but if you do, you know what a treasure it is. You may also know that there is a battle being fought there.

The County owns the property and has attempted to be a good steward. When they wanted to plan for the future of the Reserve they invited the community to help in the decisions that would affect the way our park is used.

The Parks Department spent 5 years and held 13 public meetings to develop a Master Plan, and two more years developing an EIR, which involved an additional 4 public meetings. When all the meetings were finished and reports from hydrologic, archeological, geotechnical, and biological experts were collected and paid for by taxpayers, a plan to build a multiuse trail through the east side of the park went before the Planning Commission, where it was approved. This trail would connect with the Coastal Trail.

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- There were objections to the plan, all but one of which were resolved. One person, Sabrina Brennan, still did not approve of this plan, and filed an appeal to the Coastal Commission, as was her right to do. She said if the trail was reduced to six feet wide she would drop her appeal. The hearing was the same day as the tsunami evacuation, and it was held in Santa Cruz. The Coastal Commission went ahead with the meeting, but did not approve or deny the trail, they asked for more studies, and postponed a ruling to the next meeting, which is in May in Mendocino.

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We all need to be aware of where our tax dollars are being used. One person can make a difference to "protect" a park in her backyard from safe access by the disabled. Further, if the improvements aren't made the temporary bridge over the creek where the trail meets the parking lot will be removed because the flooding this winter has weakened its banks. Then no one will have access to the trail. That would be a shame.

Please don't let this happen. Please support community access to Fitzgerald Reserve, including the disability community. The park is a "Reserve," and it is reserved for all of us. Make your voice heard by the Coastal Commission. You, too, can make a difference. The address is:

California Coastal Commission
Northern Central Coast District Office
45 Fremont Street, Suite 2000
San Francisco, CA 94105-2219

Sincerely,
Susy Castoria

Th12b

4/28/11

Dear California Coastal Commission;
I am in support of the SMC Parks
Dept Plan for an accessible trail in
Fitzgerald Marine Reserve.

Thank you.

Signature on file

Gerda Pohn

1910 Garden Dr #204

Burlingame CA 94010

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Th126

April 28, 2011

California Coastal Commission
45 Fremont Street, Suite 2000
San Francisco, CA 94015-2219

Attention: Roxanna Farshchi

RE: Fitzgerald Marine Reserve Accessible Multi-Use Trail

As a native of San Mateo County I deeply appreciate the beauty of our environment. I am writing to express my extreme dismay that one person can negate five years of careful planning and extensive public meetings, engineering studies, geotechnical and biological planning to develop a Master Plan and two more years developing an EIR all paid for by taxpayers like myself.

All this effort was expended to protect and preserve the Marine Reserve for now and the future and enable those with disabilities to visit and enjoy the beautiful area.

If this work does not commence by June of 2011 the grant that had been obtained for the accessible trail in the amount of \$750,000 will expire. It is unconscionable that one person because of a personal opinion could negate all this planning and effort.

I am also deeply concerned that the Coastal Commission actually went ahead with a meeting in Santa Cruz the same day of the Tsunami when all Highways leading there were blocked off and members of the public were not allowed to travel in that area.

Please support community access to Fitzgerald Marine Reserve including the disability Community. Thank you very much for your consideration.

Signature on file

Alexa J. Johanson
P.O. Box 5072
Redwood City, CA 94063

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RECEIVED
Th/26
MAY 04 2011
CALIFORNIA
COASTAL COMMISSION

MEMORANDUM

DATE: May 3, 2011
TO: All Interested Parties
FROM: Janet Diehl 
Project Manager, Coastal Conservancy
RE: Fitzgerald Marine Reserve Coastal Trail –
Notice of Proposed Action

The Coastal Conservancy will consider awarding a grant for a project in Moss Beach at its regular meeting to be held on May 19, 2011 in Oakland. You are being notified because you own property near this site or because you've expressed interest in this project in the past. As you can see from the agenda excerpt on the back of this notice, the Conservancy will consider this local item:

- A grant of up to \$250,000 to the County of San Mateo to build a segment of the Coastal Trail, including a bridge over San Vicente Creek, at the Fitzgerald Marine Reserve in Moss Beach, San Mateo County.

You may recall receiving a similar notice two months ago, when I expected that the proposed project would be considered at the Conservancy's March meeting. The project was removed from the March meeting's agenda, however, in order to provide more time for the permitting process. It will now be considered by the Conservancy on May 19.

A map depicting this project site is enclosed. A staff report describing the proposed project will be available at the Coastal Conservancy website at www.scc.ca.gov on May 11. If you'd like more information, please call me at (510) 286-4164.

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1330 Broadway, 13th Floor
Oakland, California 94612-2530
510-286-1015 Fax: 510-286-0470





Meeting Notice

Douglas Bosco (Public Member), Chairman
Marisa Moret (Public Member)
Ann Notthoff (Public Member)
John Laird, Secretary for Natural Resources; Bryan Cash (Designated)
Sara Wan, Coastal Commission Chair; Susan Hansch (Designated)
Ana Matosantos, Director, Department of Finance; Karen Finn (Designated)

Senate Representatives
Anthony Cannella (District 12)
Noreen Evans (District 2)
Joseph Simitian (District 11)

Assembly Representatives
Luis Alejo (District 28)
William Monning (District 27)
Das Williams (District 35)

Sam Schuchat, Executive Officer
Glenn Alex, Legal Counsel

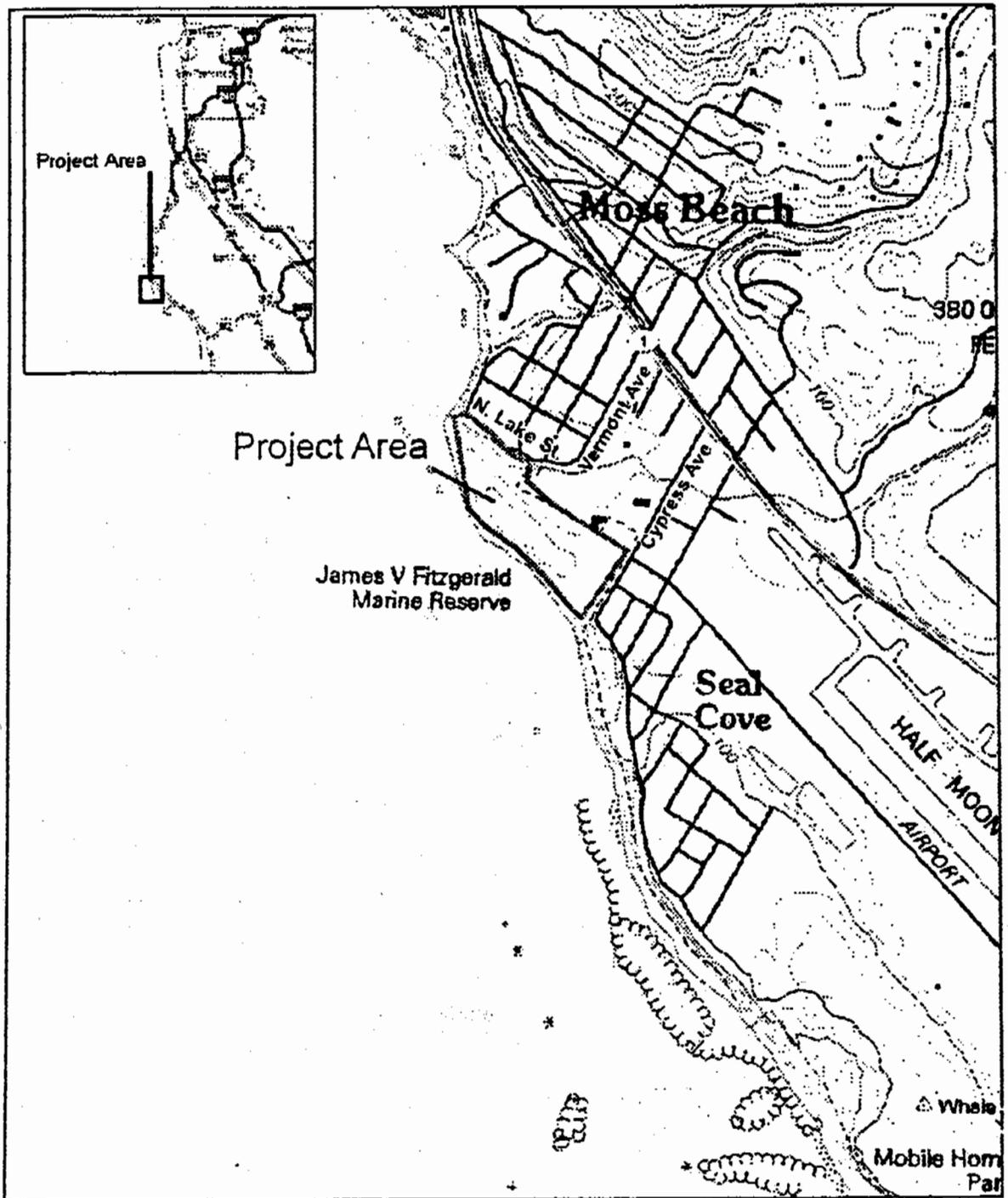
TENTATIVE AGENDA

DATE: Thursday, May 19, 2011
TIME: 9:00 a.m.
LOCATION: State Coastal Conservancy – 11th Floor Conf. Room
1330 Broadway
Oakland, CA

[NOTE: Item below is excerpted from a 5-page agenda. For the complete agenda, see the Coastal Conservancy website at www.scc.ca.gov after May 6. Staff reports for all proposed projects will be posted on the Conservancy's website on May 11.]

11. Consideration and possible Conservancy authorization to disburse up to \$250,000 to the County of San Mateo to build a segment of the Coastal Trail, including a bridge over San Vicente Creek, at the Fitzgerald Marine Reserve in Moss Beach, San Mateo County.

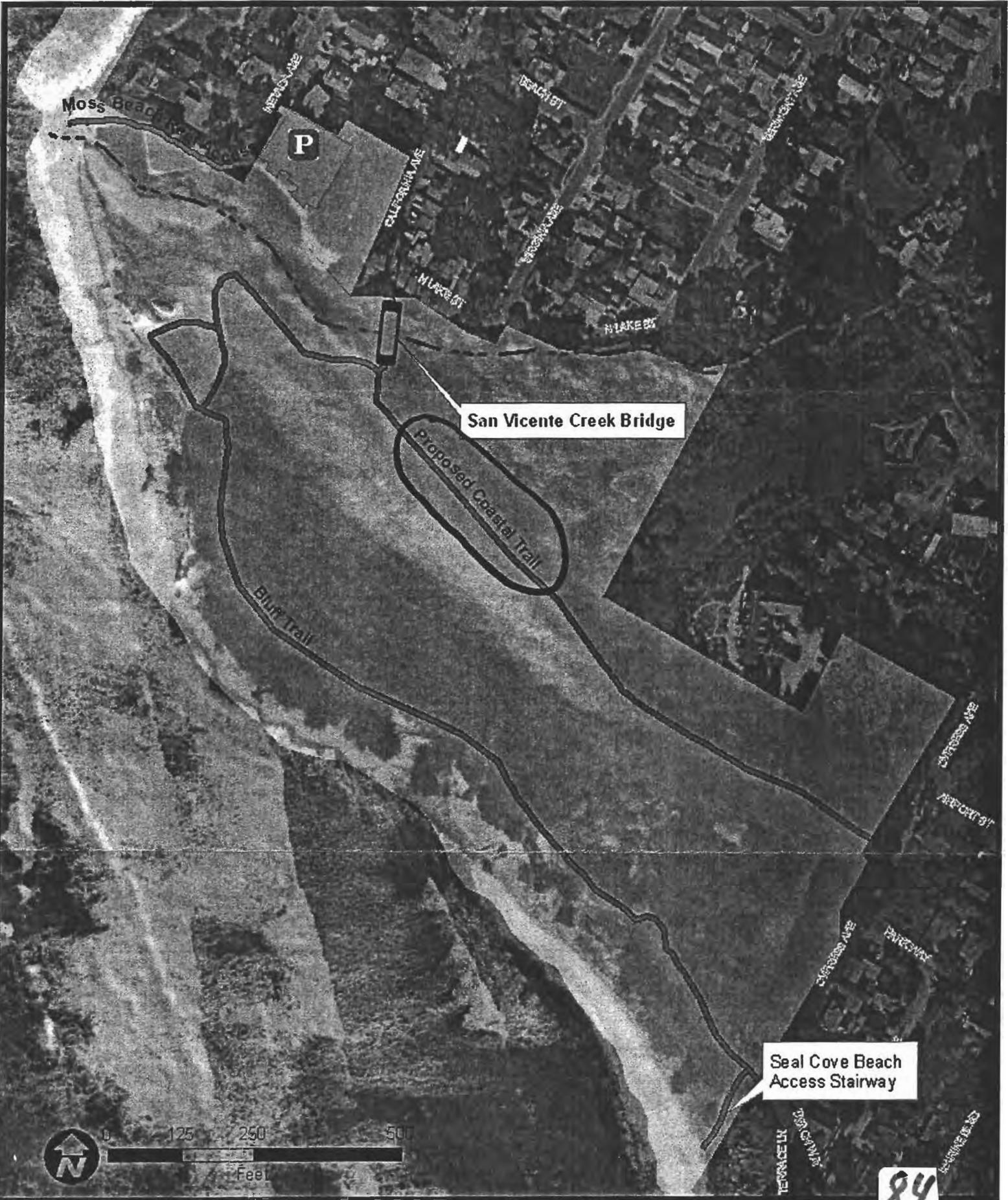




LOCATION MAP

NOT TO SCALE 

(over)



**Fitzgerald Marine Reserve
California Coastal Trail and
San Vicente Bridge Site Map**

- Legend**
- Proposed California Coastal Trail
 - San Mateo County Park Trails
 - Streams
 - San Mateo County Parks

1 inch = 212 feet

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RECEIVED
MAY 03 2011
CALIFORNIA
COASTAL COMMISSION
Th12b

DOROTHY PHILBRICK
Founder

ANDREW FRISCH
Executive Director

DIANE SMITH
Associate Director

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Executive Member

Ginny Hughes
Executive Member

Bernadette Mellott
Executive Member

Melanie Rogers
Executive Member

Debbi Jones-Thomas
Executive Member

Paula Uccelli
Executive Member

Dear Coastal Commission,

I am writing in support of the ADA accessibility at Fitzgerald Marine Reserve.

It is sad the one person can stop the forward progress that would benefit so many. I understand the County stands to loose \$ 750,000. in grant monies. This would be a sad waste of lost money. Thank you,
Diane Smith

85

Th 12b

650 728 3813

FROM THE DESK OF
NEIL MERRILEES

MERMADE4@YAHOO.COM

May 5, 2011

Coastal Commission
45 Fremont Street, Suite 2000
San Francisco, CA 94105-2219

Re: A-2-SMC-11-010

As a 25 year midcoast resident, I am writing to support the plan for Coastal Trail through the Fitzgerald Marine Reserve, and to state that I feel the appeal (A-2-SMC-11-010) has no merit. This trail plan is the result of over 15 years of public input, involving realignment, discussion of natural and archeological resources, and the value of public access to the Coast.

The width of the existing coastal trail coming north from Half Moon Bay is 8-10 feet wide. While not every trail on the coast is this wide or needs to be this wide, it is of great benefit to have one trail that is wide enough for bicycles, pedestrians, and wheelchairs to safely travel in opposing directions. This protects natural resources by keeping users on the path (The current dirt path is a source of muddy silt running directly into the Fitzgerald Marine Reserve). It also has the benefit of allowing visitors and residents to enjoy the coast without traveling by car.

The bridges along the Coastal Trail on the midcoast are all 8' or wider, ranging from 8-12 feet wide. I have attached a photo of this narrow 8' bridge with bicycles passing each other, to suggest what a reduction to 6' would be like. Any pathway that is narrower than 6' will encourage bicycle riders to go off path, which would defeat the whole purpose of the trail; to protect natural resources while improving public access.

This trail, when constructed as according to the proposed plan, will offer an opportunity to share this resource with visitors on foot, bicycles, wheelchairs, walkers, strollers and even tricycles. An added benefit will be the opportunity for people to enjoy the Coast, without adding more vehicle traffic. We need this trail.

Please vote to deny the appeal, and move forward with another incredible section of the California Coastal Trail.

Sincerely

Neil Merrilees

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FROM THE DESK OF
NEIL MERRILEES



THE NARROWEST BRIDGE ON THE MIDCOAST COASTAL TRAIL, (PILARCITOS CREEK) THE 8' WIDTH PROVIDES JUST ENOUGH ROOM FOR BICYCLES TO PASS WITHOUT HITTING HANDLEBARS. BRIDGE AND TRAIL WIDTH ON THE COASTAL TRAIL THROUGH THE MIDCOAST RANGE FROM 8-12 FEET.

Th 126

470 Furtado Lane
Half Moon Bay, California 94019

May 8, 2011

California Coastal Commission
45 Fremont St.
Suite 2000
San Francisco, California 94105

RECEIVED

MAY 09 2011

COASTAL COMMISSION
NORTH CENTRAL COAST

Dear Members of the California Coastal Commission,

I am writing this letter in support of approving the permits to allow the construction of the Dardanelle Trail in Moss Beach, California.

I am the Mother of a 27 year old son with cerebral palsy and he is totally non-ambulatory. He uses an electric wheelchair for mobility, and particularly loves being able to ride on the Coastside Trail. I was thrilled when I heard that there were plans to extend the trail to the Fitzgerald Marine Reserve. I was equally dismayed when I heard that someone had appealed the project, based on the width of the trail.

Electric wheelchairs are usually about 26 inches wide-just over 2 feet. If you have bicyclists, strollers, walkers and skateboards passing each other in opposite directions, the absolute minimum width should take into consideration the various modes of transportation used on the trail.

I look forward to the approval of this project.

Thank you for your attention to this matter.

Sincerely,

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Signature on File

Mary Lou Williams, Mother of Mark Williams

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Th12b

Dear Editor,

I want to be sure that every reader of your newspaper is aware of the power of one. I don't know how many people on the coast visit Fitzgerald Marine Reserve regularly, but if you do, you know what a treasure it is. You may also know that there is a battle being fought there.

The County owns the property and has attempted to be a good steward. When they wanted to plan for the future of the Reserve they invited the community to help in the decisions that would affect the way our park is used.

The Parks Department spent 5 years and held 13 public meetings to develop a Master Plan, and two more years developing an EIR, which involved an additional 4 public meetings. When all the meetings were finished and reports from hydrologic, archeological, geotechnical, and biological experts were collected and paid for by taxpayers, a plan to build a multiuse trail through the east side of the park went before the Planning Commission, where it was approved. This trail would connect with the Coastal Trail.

The trail would be eight feet wide which is four feet narrower than is recommended for multiuse trails. Eight feet is probably what we need for a twin stroller being passed by a horse or a couple of cyclists or a person who uses a wheelchair. Any less than that will force people off the path into environmentally sensitive areas, trashing the edges of the trail.

There were objections to the plan, all but one of which were resolved. One person, Sabrina Brennan, still did not approve of this plan, and filed an appeal to the Coastal Commission, as was her right to do. She said if the trail was reduced to six feet wide she would drop her appeal. The hearing was the same day as the tsunami evacuation, and it was held in Santa Cruz. The Coastal Commission went ahead with the meeting, but did not approve or deny the trail, they asked for more studies, and postponed a ruling to the next meeting, which is in May in Mendocino.

This continued process may cost the County \$750,000--a grant that had been obtained for the trail will expire if the project is not started by June. Even if the plan is approved in May, we will lose the grant if there are any further appeals or conditions, or if we simply can't find a contractor who can commence work in time. We will also have to pay for all the newly requested studies.

We all need to be aware of where our tax dollars are being used. One person can make a difference to "protect" a park in her backyard from safe access by the disabled. Further, if the improvements aren't made the temporary bridge over the creek where the trail meets the parking lot will be removed because the flooding

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this winter has weakened its banks. Then no one will have access to the trail. That would be a shame.

Please don't let this happen. Please support community access to Fitzgerald Reserve, including the disability community. The park is a "Reserve," and it is reserved for all of us. Make your voice heard by the Coastal Commission. You, too, can make a difference. The address is:

California Coastal Commission
Northern Central Coast District Office
45 Fremont Street, Suite 2000
San Francisco, CA 94105-2219

Sincerely,
Susy Castoria

CALIFORNIA COASTAL COMMISSION

45 FREMONT, SUITE 2000
SAN FRANCISCO, CA 94105-2219
VOICE (415) 904-5200
FAX (415) 904-5400
TDD (415) 597-5885

Th12b

Filed: February 1, 2011
SI Hearing: March 11, 2011
Staff: Roxanna Farshchi
Staff Report: April 22, 2011
Hearing Date: May 12, 2011
Commission Action:

STAFF REPORT: REGULAR CALENDAR

Application: A-2-SMC-11-010

Applicant: San Mateo County Parks

Project Location: Fitzgerald Marine Reserve, Moss Beach (San Mateo County).
APNs 037-103-110, 037-111-060, 037-200-130, 037-101-250

Project Description: Appeal of a San Mateo County decision granting a coastal development permit with conditions to San Mateo County Department of Parks to improve an existing Dardanelle Trail / California Coastal Trail at Fitzgerald Marine Reserve in Moss Beach, CA.

Substantive Project Files: (1) San Mateo County File Number PLN 2010-00093; (2) *Biological Resources Assessment* prepared by WRA Consultants in February 2010; (3) *Biological Assessment* prepared by WRA Consultants in February 2010; (4) *Wetland Delineation* prepared by WRA Consultants in February 2010; (5) *Fitzgerald Mitigation and Monitoring Report* prepared by WRA Consultants in February 2010; (6) *Notice to Adopt Negative Declaration*; (6) Memo from John Dixon, Ph.D. to Roxanna Farshchi, dated March 25, 2011

SUMMARY OF STAFF RECOMMENDATION *DE NOVO*:
APPROVAL WITH CONDITIONS

Applicant San Mateo County Parks proposes to improve the existing, undeveloped Dardanelle Trail at Fitzgerald Marine Reserve in Moss Beach. Improvements include using gravel/pave to make the trail ADA accessible, as well as construction of a new bridge connecting the trail over San Vicente Creek to the north end of the Reserve. Staff recommends that the Commission approve with conditions the coastal development permit for the proposed trail project. To support the Commission's *de novo* review of the originally appealed project, staff biologist John Dixon conducted a site visit on March 23, 2011 to determine the presence of wetlands on site (see Exhibit 4). Based on this visit, Staff determined that there are no wetland habitats present directly adjacent to the trail or that would otherwise be directly impacted by the proposed development. There are also no other sensitive habitats or other resource constraints that would require an alternative trail alignment or design. The trail is appropriately designed for the anticipated uses and demand, including ADA accessibility. Staff therefore recommends that the Commission approve the project with special conditions to assure an adequate interpretive signage plan, native landscaping, and construction measures to protect water quality, and to incorporate local conditions of approval. As conditioned, staff believes that the development is consistent with the certified San Mateo County LCP and the public access and recreation policies of the Coastal Act.

The Motion to adopt the Staff Recommendation of Approval is found at the top of page 3.

Staff Notes:

1. Procedure

On March 11, 2011, the Coastal Commission found that the appeal of San Mateo County's conditional approval of a coastal development permit (PLN 2010-00093) for the subject development raised a substantial issue with respect to the grounds on which the appeal had been filed, pursuant to Section 30625 of the Coastal Act and Section 13115 of Title 14 of the California Code of Regulations. As a result, the County's approval is no longer effective, and the Commission must consider the project *de novo*. The Commission may approve, approve with conditions (including conditions different than those imposed by the County), or deny the application. Since the proposed project is within an area for which the Commission has certified a Local Coastal Program and includes area between the first through public road and the sea, the applicable standard of review for the Commission to consider is whether the development is consistent with San Mateo County's certified Local Coastal Program (LCP) and the public access and recreation policies of the Coastal Act. Testimony may be taken from all interested persons at the *de novo* hearing.

I. STAFF RECOMMENDATION, MOTION, AND RESOLUTION ON DE NOVO:

Motion:

I move that the Commission approve Coastal Development Permit No. A-2-SMC-11-010 subject to conditions.

Recommendation of Approval:

Staff recommends a **YES** vote. Passage of this motion will result in approval of the permit as conditioned and adoption of the following resolution and findings. The motion passes only by affirmative vote of a majority of the Commissioners present.

Resolution to Approve Permit:

The Commission hereby approves a coastal development permit for the proposed development and adopts the findings set forth below on the grounds that the development as conditioned will be in conformity with the certified San Mateo County LCP and the public access policies of the Coastal Act.

II. STANDARD CONDITIONS:

1. Notice of Receipt and Acknowledgment. The permit is not valid and development shall not commence until a copy of the permit, signed by the permittee or authorized agent, acknowledging receipt of the permit and acceptance of the terms and conditions, is returned to the Commission office.
2. Expiration. If development has not commenced, the permit will expire two years from the date on which the Commission voted on the application. Development shall be pursued in a diligent manner and completed in a reasonable period of time. Application for extension of the permit must be made prior to the expiration date.
3. Interpretation. Any questions of intent or interpretation of any condition will be resolved by the Executive Director or the Commission.
4. Assignment. The permit may be assigned to any qualified person, provided assignee files with the Commission an affidavit accepting all terms and conditions of the permit.
5. Terms and Conditions Run with the Land. These terms and conditions shall be perpetual, and it is the intention of the Commission and the permittee to bind all future owners and possessors of the subject property to the terms and conditions.

III. SPECIAL CONDITIONS

1. **Final Project Plans.** Prior to Construction, Permittee shall submit two copies of Final Project Plans to the Executive Director for review and approval. The plans shall be

substantially in conformance with the plans received by the Coastal Commission dated February 1, 2011 except that they shall be revised to address the following requirements:

- i. Trail width shall be a maximum of eight (8) feet.
- ii. No developed shoulders shall be provided and areas immediately adjacent to the trail shall be replanted with native vegetation following construction.
- iii. Plans shall indicate locations of interpretive signage consistent with the sign plan required pursuant to special condition 2 below.

Permittee shall undertake development in accordance with the approved Final Plans.

2. Public Access Signs/Materials. Prior to Construction, applicant shall submit a public access signage plan to the Executive Director for review and approval. The plan shall identify all signs, handouts, brochures, and any other project elements that will be used to facilitate, manage, and provide public access to the approved project, including identification of all public education/interpretation features that will be provided on the site (educational displays, interpretive signage, etc.). Sign details showing the location, materials, design, and text of all public access signs shall be provided. The signs shall be designed so as to provide clear information without impacting public views and site character. At a minimum, public access directional signs shall be placed at both ends of the trail segment and at least one interpretive display shall be provided along the trail. Public access signage shall include the California Coastal Trail marker, and acknowledge the participants in the design and provision of the public access components, including the San Mateo County Parks and Recreation, California Coastal Conservancy, and the California Coastal Commission.

3. Construction Best Management Practices

- a. Erosion control measures shall be placed between all work areas and the creek and along the trails to prevent the introduction of material into the waterways, to minimize sedimentation and turbidity associated with bridge installation and trail construction.
- b. Contractor education shall be conducted to inform construction crews of the resources of concern and potentially present at the site, measures required to protect them (construction boundaries, flagging, and non-disturbance buffers), reporting requirements and instructions, project biologist contact information, information sheets, and discussion of other permit conditions that must be followed during construction.
- c. No heavy equipment shall operate in the creek.

- d. Prior to commencement of construction activities, permittee shall provide evidence the Executive Director of review and approval, or that no approval is needed, by the Regional Water Quality Control Board and California Department of Fish and Game.

4. Local Government Conditions and Project Changes

All local conditions of approval are incorporated into this permit except as they may directly conflict with special conditions 1-3 of this authorization. This action has no effect on conditions imposed by a local government pursuant to an authority other than the Coastal Act. Any proposed changes in the project shall be reported to the Executive Director. No changes shall occur without a Commission amendment to this coastal development permit unless the Executive Director determines that no amendment is legally required.

IV. FINDINGS AND DECLARATIONS

The Commission finds and declares the following:

A. Project Location, Setting, and Description

1. Location and Purpose

On January 12, 2011, San Mateo County (County) approved a coastal development permit (CDP) authorizing San Mateo County Parks (applicant) to construct coastal trail public access improvements on an existing .26 mile trail segment located within the Fitzgerald Marine Reserve in Moss Beach. The County has proposed the project as a segment of the California Coastal Trail (CCT) that would provide ADA compliant multi-use public access, including for bicyclists. The improvements include removal of an existing temporary pedestrian bridge across San Vicente Creek (to be replaced by a prefabricated 8' wide, 60' long fiberglass clear span bridge located approximately 30 feet upstream of the current bridge location), and construction of an ADA compliant 8' wide trail in the location of the existing approximately 6' wide unimproved trail. Pursuant to Coastal Act Section 30603 this approval is appealable to the Commission because the approved development is located between the first public road and the sea, and within 100 feet of a wetland and stream areas.

2. Project Description

San Mateo County Parks proposes the improvement of a ¼ mile trail known as Dardanelle Trail which connects the northern and southern ends of Fitzgerald Marine Reserve, located in Moss Beach. The project would convert the existing unpaved pedestrian trail into a gravel-paved multiuse trail, open to pedestrian, bicycle, equestrian, and ADA capable use. Improvements include expanding the currently 3-6 foot wide trail to 8 feet, comprised of gravelpave (a pervious gravel structure). In addition to improvements to the trail, the current, temporary bridge over San Vicente Creek, connecting California Street with the Dardanelle Trail, will be replaced with a

60 foot, clear span, pre-fabricated fiberglass bridge, which will be located approximately 30 feet upstream from the current bridge location. With installation of the bridge, and upgrade of the trails, the public will be able to more easily travel the length of the Reserve at this location.

The Dardanelle Trail runs along the inner boundary of the Reserve, over San Vincente Creek and it connects the parking area of the reserve through to the southern boundary. Currently, the Reserve is not ADA compliant. Additionally, it is currently estimated that there are approximately 150,000 visitors annually at the Reserve, including large school groups as well as tourists. Part of the rationale for improving the trail, in addition to providing disabled access through the Reserve, is to provide better access for such visits.

The improvements of the trail include both widening and improving the path. The trail, which currently varies in width to be between 3 and 6 feet, will be made a uniform 8 feet wide the entire length of the trail. In addition, gravelpave will be used to develop the trail. Gravelpave is a structure which provides heavy load bearing support and true containment of gravel to create a porous surface with unlimited traffic volume and/or duration time for parking¹. As originally approved, the trail would be surfaced with a pervious gravel structure and have unimproved shoulders of 2 and 1 foot for a total width of 11 feet (see Exhibit 2 for more detail). Since that time, County Parks has clarified that there will be no shoulders and that the trail would have native vegetation on either side of its maximum 8' width. Special Condition 1 requires submittal of Final Plans showing and requiring compliance with this change to the project. In accordance with County-approved conditions, Parks will mitigate adjacent removed vegetation, including native strawberry and ferns, by transplanting the indicated species to another viable location.

The Bridge over San Vincente Creek will be a pre-fabricated clear-span fiberglass bridge, 8 feet wide and 60 feet long. The bridge will also be relocated to be 30 feet upstream from the current location of the temporary bridge. This relocation is necessary to ensure minimal impact to both the bridge and the riparian corridor, which has changed greatly in the last few years. The bridge location will allow for more natural ebb and flow of the creek channel, and minimization of erosion potential to the bridge supports in the future.

Lastly, the bridge and subsequent end of the Dardanelle Trail will be located in an archaeological site. Measures have been approved by both the Native American Heritage Commission and San Mateo County to address protection of resources in addition to special conditions during construction.

B. Public Access and Trail Design

The proposed project will redevelop and improve an existing trail connection from the north to the south of the reserve, and connect the bluff top section of the Reserve to the parking lot area. This will also be the only through connection between California Street and Cypress Avenue, with the exception of re-routing to Highway 1. The following policies apply to this project.

LCP Public Access Policies

¹ <http://www.invisiblestructures.com/gravelpave2.html>.

LUP Policy 10.21:

In all areas where topography permits, provide shoreline access for the disabled by building paths and ramps for wheelchairs without altering major landforms.

LUP Policy 10.26 (a):

Provide improvements and management practices in sensitive habitats and their buffer zones adequate to protect the resources. Include, but do not limit, improvements and management practices to the following: (1) in areas not subject to tidal action, interpretive trails posted with educational signs which minimize public intrusions and impacts, (2) brochures and educational displays at trailheads leading to areas subject to tidal action, (3) organized tours, (4) limited number of persons per visitor tour, (5) restricted number of access points which are improved and managed and (6) limit the seasons of the year when public access is permitted.

LUP Policy 10.37:

Continue to provide for the improvement, expansion, and maintenance of access to the Fitzgerald Marine Reserve according to table 10.6, Site Specific Recommendations. Accept dedications of access easements or fee interests which provide access to or expand the size of the reserve.

LUP Table 10.6:

Develop access along the bluffs and to the beaches of the Fitzgerald.

Coastal Act Public Access Policies

Section 30210 of the Coastal Act:

In carrying out the requirement of Section 4 of Article X of the California Constitution, maximum access, which shall be conspicuously posted, and recreational opportunities shall be provided for all the people consistent with public safety needs and the need to protect public rights, rights of private property owners, and natural resource areas from overuse.

Section 30211 of the Coastal Act:

Development shall not interfere with the public's right of access to the sea where acquired through use or legislative authorization, including, but not limited to, the use of dry sand and rocky coastal beaches to the first line of terrestrial vegetation.

Section 30214 (a) of the Coastal Act:

(a) The public access policies of this article shall be implemented in a manner that takes into account the need to regulate the time, place, and manner of public access depending on the facts and circumstances in each case including, but not limited to, the following:

(1) Topographic and geologic site characteristics.

(2) The capacity of the site to sustain use and at what level of intensity.

(3) The appropriateness of limiting public access to the right to pass and repass depending on such factors as the fragility of the natural resources in the area and the proximity of the access area to adjacent residential uses.

LCP Visual Resource Policies:

LUP Policy 10.23(c):

Design and site trail improvements to blend with the natural environment. Prohibit the disturbance or alteration of land forms which would cause or contribute to erosion or geologic hazards.

Analysis

Development of the Dardanelle Trail will facilitate ADA compliant public access, along the reserve, connecting the northern and southern ends. Additionally, during times of high tide and seal pupping, this trail connection will be the only means of accessing the beach and tide pools at the north and south. School children in groups of 30 or more often participate in guided tours of the area. By having an 8 foot wide improved trail with interpretive signage, students will be able to more easily remain on the trail at all times, and learn about the Reserve along the way. Also, review of colors and materials for both the Bridge and gravelpave are required as conditions 24 and 25 of the County approval.

Replacing the temporary bridge across San Vincente Creek will increase public safety and access by providing a long-term solution to the series of unsuccessful bridges in the past few years. Moving the site of the bridge upstream by 30 feet will provide for a safe and secure location for a new bridge that will withstand the natural meandering of the riparian corridor.

Lastly, providing for ADA compliant public access is a goal of the San Mateo County LCP, and can be appropriately fulfilled at this location.

Therefore, as proposed, the project is generally consistent with the Coastal Act policy to provide maximum access, as well as requirements of the LCP. Overall, the proposed trail improvements will increase public access and recreational opportunities for the public to enjoy Fitzgerald Marine Reserve in a manner consistent with Sections 30210, 30211 and 30214 of the Coastal Act. However, to assure that maximum access is provided consistent with the Coastal Act and LCP, Special Condition 2 requires a signage plan indicating all interpretive/educational signs and materials that will be provided with the project. Only as conditioned can the Commission find

that the project is consistent with Coastal Act policies 30210, 30211, 30241 and LCP Policy 10.26(a).

With respect to the size and design of the trail, the project will result in the conversion of an existing informal unpaved trail to a more engineered looking, larger gravel-paved multi-use trail. However, the width of the trail, 8 feet, is generally appropriate for the anticipated uses and intensities, is only two feet wider than the width of most of the current trail, and will provide for optimum access for many different types of users. The trail will also be colored to blend in with the surrounding landscape. As required by Condition 1, the trail will not have developed shoulders and will have native vegetation immediately adjacent, which should soften and minimize the more formal design of the new trail. Therefore, as conditioned, the Commission finds that with respect to the size and width of the trail, the project is consistent with LCP Policies 10.21, 10.23(c), 10.26, and 10.37 and Coastal Act Policies 30210, 11, and 30214(a).

C. Wetlands and Native Strawberry

LCP Policies

LUP policy 7.18:

Buffer zones shall extend a minimum of 100 feet landward from the outermost line of wetland vegetation. This setback may be reduced to no less than 50 feet only where (1) no alternative development site or design is possible; and (2) adequacy of the alternative setback to protect wetland resources is conclusively demonstrated by a professional biologist to the satisfaction of the County and the State Department of Fish and Game. A larger setback shall be required as necessary to maintain the functional capacity of the wetland ecosystem.

LUP policy 7.19:

Within buffer zones, permit the following uses only: (1) uses allowed within wetlands (Policy 7.16) and (2) public trails, scenic overlooks, and agricultural uses that produce no impact on the adjacent wetlands

LUP policy 7.49:

Require any development, within one-half mile of the coast, to mitigate against the destruction of any California wild strawberry in one of the following ways:

- a. Prevent any development, trampling, or other destructive activity which would destroy the plant, or*
- b. After determining specifically if the plants involved are of particular value, successfully transplant them or have them successfully transplanted to some other suitable site. Determination of the importance of the plants can only be made by a professional doing work in strawberry breeding.*

Analysis

The County's administrative record of wetland issues and potential impacts was not sufficient to determine the consistency of the project with the LCP. On March 23, 2011, staff biologist John Dixon conducted a site visit to the Fitzgerald Marine Reserve, and concluded that the areas adjacent to the trail were not wetlands, but rather, the soils constituted a clay compound (see Exhibit 4). Thus, there will be no direct impacts to wetlands from the trail project.

However, at one point the trail does come within 60 feet of the Dardanelle Pond. According to policy 7.18 of the LCP, the trail is thus located within the potentially required wetland buffer zone.

Section 7.19 designates permitted uses in buffer zones, including "(1) uses allowed within wetlands (Policy 7.16) and (2) public trails, scenic overlooks, and agricultural uses that produce no impact on the adjacent wetlands."

Being a public trail, the improvements constitute an allowable use in a wetland buffer zone. Additionally, the improvements will be located 60 feet or more away from the pond and will not have significant impacts on it. Therefore, the proposed project is consistent with LCP Policies 7.18 and 7.19.

Lastly, improvement of the trail will require the removal of Native Strawberry, which is categorized as a unique species under the LUP (policy 7.49). However, LUP policy 7.49 (b) allows relocation of the strawberry in circumstances such as this; and special condition 20 under the County's approval, which is incorporated into this approval, requires removal and transplantation of the Strawberry. Therefore the project is consistent with the LUP Policy 7.49(b).

D. Riparian Areas

LCP policies:

LUP policy 7.5:

- a. *As part of the development review process, require the applicant to demonstrate that there will be no significant impact on sensitive habitats. When it is determined that significant impacts may occur, require the applicant to provide a report prepared by a qualified professional which provides: (1) measures which protect resources and comply with the policies of the Shoreline Access, Recreation / Visitor-Serving Facilities and Sensitive Habitats Components, and (2) a program for monitoring and evaluating the effectiveness of mitigation measures.*
- b. *When applicable, require as a condition of permit approval the restoration of damaged habitat(s) when in the judgment of the Planning Director restoration is partially or wholly feasible.*

LUP Policy 7.9:

- a. *Within corridors, permit only the following uses: (1) education and research, (2) consumptive uses as provided for in the Fish and Game Code and Title 14 of the California Administrative Code, (3) fish and wildlife management activities, (4) trails and scenic overlooks on public land(s), and (5) necessary water supply projects.*
- b. *When no feasible or practicable alternative exists, permit the following uses: (1) stream dependent aquaculture, provide that non-stream dependent facilities locate outside of corridor, (2) flood control projects, including selective removal of riparian vegetation, where no other method for protecting existing structures in the floodplain is feasible and where such protection is necessary for public safety or to protect existing development, (3) bridges when supports are not in significant conflict with corridor resources, (4) pipelines, (5) repair or maintenance of roadways or road crossings, (6) logging operations which are limited to temporary skid trails, stream crossings, roads and landings in accordance with State and County timber harvesting regulations, and (7) agricultural uses, provided that no existing riparian vegetation is removed, and no soil is allowed to enter stream channels. (*emphasis added*)*

LUP Policy 7.10:

Require development permitted in corridors to: (1) minimize removal of vegetation, (2) minimize land exposure during construction and use temporary vegetation or mulching to protect critical areas, (3) minimize erosion, sedimentation, and runoff by appropriately grading and replanting modified areas, (4) use only adaptive native or non-invasive exotic plant species when replanting, (5) provide sufficient passage for native and anadromous fish as specified by the State Department of Fish and Game, (6) minimize adverse effects of waste water discharges and entrainment, (7) prevent depletion of groundwater supplies and substantial interference with surface and subsurface waterflows, (8) encourage waste water reclamation, (9) maintain natural vegetation buffer areas that protect riparian habitats, and (10) minimize alteration of natural streams.

LUP Policy 7.13:

Require uses permitted in buffer zones to: (1) minimize removal of vegetation, (2) conform to natural topography to minimize erosion potential, (3) make provisions (i.e., catch basins) to keep runoff and sedimentation from exceeding pre-development levels, (4) replant where appropriate with native and non-invasive exotics, (5) prevent discharge of toxic substances, such as fertilizers and pesticides, into the riparian corridor, (6) remove vegetation in or adjacent to manmade agricultural ponds if the life of the pond is endangered, (7) allow dredging in or adjacent to manmade ponds of the San Mateo County Resource Conservation District certified that siltation imperils continued use of the pond for agricultural water storage and supply, and (8) require motorized machinery to be kept to less than 45dBA at any wetland boundary except for farm machinery and motorboats.

LUP Policy 7.9(a) (4) allows trails in riparian corridors on public lands. Further LUP Policy 7.9 (b) (3) allows bridges in riparian corridors if no other alternative exists, and where supports are not in significant conflict with corridor resources. Pursuant to Policy 7.5, permit requirements include determining whether there is a significant impact on the resources, and if there is, a mitigation plan must be submitted.

The bridge over San Vincente Creek is a necessary component of the project because there is no other means of connecting the Dardanelle Trail with the northern area of the Reserve, which includes the parking area. In addition, this project, composed of both the trail and bridge components, is the only method of connecting North Lake Street with Cypress Avenue, with the exception of re-routing to Highway 1 before returning to the northern end of the reserve.

In regards to the proposed location and size of the bridge, the County staff report notes in section (A) (2) (c):

Were a new bridge proposed in the same location as the existing temporary bridge, the long-term erosion pattern on the north side of the creek will likely threaten future bridge abutments at that location, necessitating the placement of rip-rap within the creek channel. It would also likely require the removal of three cypress trees to accommodate the bridge abutments as well as additional grading to allow for ADA compliant approach to the north end of the bridge.

Commission Staff field observations of the creek channel and potential future erosion and natural meandering confirms the location and length of the proposed bridge are appropriate. Additionally, a recent temporary bridge replacement underlines the fast erosion of this area, necessitating constant replacement of longer temporary bridges.

The project would not significantly impact any resources after mitigation. The applicant has submitted a mitigation and monitoring report as a result of the determination of significant impacts on the riparian corridor from the proposed bridge. Further, County conditions of approval require mitigation measures, which are incorporated into the project pursuant to Special Conditions 19, 26, 27, and 28. Mitigation measures include:

County Condition 19:

The applicant shall compensate for impacts to California wild strawberry by replacing ice plant with native strawberry at a 1:1 ratio of planted strawberry habitat to impacted area. California wild strawberry will be planted in areas currently dominated by ice plant that are adjacent to those locations where impacts to existing strawberry are proposed. If no ice plant is found within the areas of the trail improvements, replanting of wild strawberry in a 1:1 ratio shall occur in a suitable location as close as possible to the area of the removed wild strawberry. Temporary impacted areas of California strawberry as a result of project activities will be replanted and returned to pre-construction conditions. California wild strawberry planting activities will take place concurrently with trail improvements. To ensure optimal survival and establishment, plantings will be installed in the fall before the onset of the rainy season. This

compensation shall be documented through the submittal of mitigation planting plan, which shall include final success criteria, implementation measures, maintenance and monitoring plan.

County Condition 26:

No additional trees beyond those notated in this staff report shall be removed. Removal of any tree with a diameter equal to or greater than 12 inches as measured 4.5 feet above the ground shall require a separate tree removal permit. For each tree removed, the applicant shall replant in a 2:1 ratio with native trees²

County Condition 27:

The disturbance or removal of native vegetation shall not exceed the minimum necessary to construct the trail. In particular, the applicant shall minimize disturbance to the area of the Western sword fern and rushes to the maximum extent possible. Any Western sword fern or rushes that must be removed by trail construction shall be carefully removed, save, and replanted as part of the revegetation of this area

County Condition 28:

The applicant shall remove cape ivy in advance of any disturbance or removal of soil and spot treat resprouts with an herbicide. Cape ivy roots or root segments shall be removed from any soil being moved during construction.

Special condition 4 incorporates these mitigation measures into the project. As conditioned, the Commission finds that the proposed bridge is consistent with LCP Policies 7.5, 7.9, 7.10, and 7.13.

E. Cultural Resources

LCP Policies

LUP policy 1.24:

Based on County Archaeology / Paleontology Sensitivity Maps, determine whether or not sites proposed for new development are located within areas containing potential archaeological / paleontological resources. Prior to approval of development proposed in sensitive areas, require that a mitigation plan, adequate to protect the resource and prepared by a qualified archaeologist / paleontologist be submitted for review and approval and implemented as part of the project.

² The County staff report notes that the project will require the removal of:
...one 40-inch d.b.h. cypress, three dead cypress (44", 46" and 58" d.b.h.) and possible removal of four trees (40" d.b.h. cypress, 48", 40" and 40" d.b.h. eucalyptus) based on an in field evaluation following grading activities.

LUP policy 10.24:

Define fragile resources as: (1) exposed rocky cliff faces, steep slopes as defined in the Hazards Component, and hilly coastal terraces (e.g., San Pedro Bluff and Devil's Slide), (2) all sensitive habitats defined in the Sensitive Habitats Component, and archaeological / paleontological resources.

LUP policy 11.18(a):

Conduct studies by a qualified person agreed by the County and the applicant during the planning and design phases of facilities located within or near sensitive habitats and archaeological / paleontological resources to determine the least disruptive locations for improvements and the methods of construction.

These studies should consider the appropriate intensity of use, improvements and management to protect the resources and reduce or mitigate impacts.

The County concluded that the project would not significantly affect the archaeological site, stating:

The archaeology site will be preserved with a protective fill cap, minimally sized 3-inch "x-strong" galvanized pipe pilings will be used for the installation of the bridge abutment within the archaeology site boundary, and the trail will be crown-sloped instead of cross-sloped to minimize ground disturbance.

Further, Condition 23 of the County's Findings and Conditions of Approval requires certain measures to be taken to protect archaeological resources including monitoring the site during construction (Exhibit 3). The Commission concurs with the County's findings with respect to the protection of cultural resources. With the incorporation of local conditions of approval, the project is consistent with LUP Policies 1.24, 10.24, and 11.18(a).

F. California Environmental Quality Act (CEQA)

Section 13096 of the California Code of Regulations requires that a specific finding be made in conjunction with coastal development permit applications showing the application to be consistent with any applicable requirements of CEQA. Section 21080.5(d)(2)(A) of CEQA prohibits a proposed development from being approved if there are feasible alternatives or feasible mitigation measures available which would substantially lessen any significant adverse effect which the activity may have on the environment.

The County, acting as CEQA lead agency, found the proposed project to be categorically exempt from environmental review under CEQA. The Coastal Commission's review and analysis of land use proposals has been certified by the Secretary of Resources as being the functional equivalent of environmental review under CEQA. This staff report has discussed the relevant coastal resource issues associated with the proposal, including the significant adverse environmental

effects expected due to the project, and has recommended appropriate conditions to avoid and/or lessen any potential for adverse impacts to said resources. All public comments received to date have been addressed in the findings above. All above Coastal Act/LCP findings are incorporated herein in their entirety by reference. As such, there are neither additional feasible alternatives nor feasible mitigation measures available which would substantially lessen any significant adverse environmental effects which approval of the proposed project, as modified, would have on the environment within the meaning of CEQA. Thus, if so modified, the proposed project will not result in any significant environmental effects for which feasible mitigation measures have not been employed consistent with CEQA Section 21080.5(d)(2)(A).

EXHIBITS

- Exhibit 1 – Regional Location Map
- Exhibit 2 – Project Plans
- Exhibit 3 – County Findings and Conditions of Approval
- Exhibit 4 – Memo by CCC Staff Biologist John Dixon
- Exhibit 5 – Correspondence

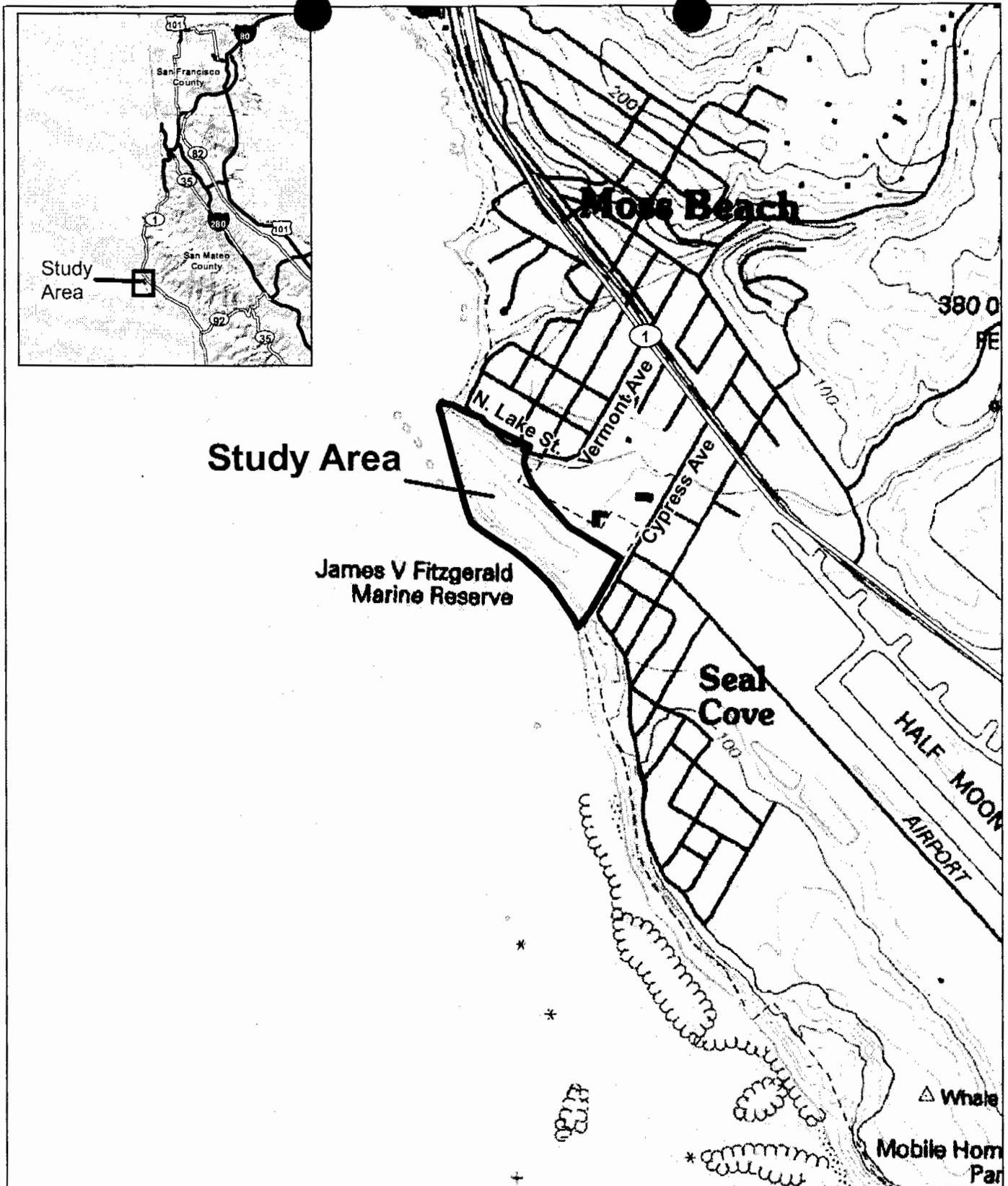
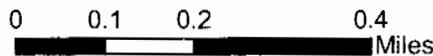


Figure 1. Location Map

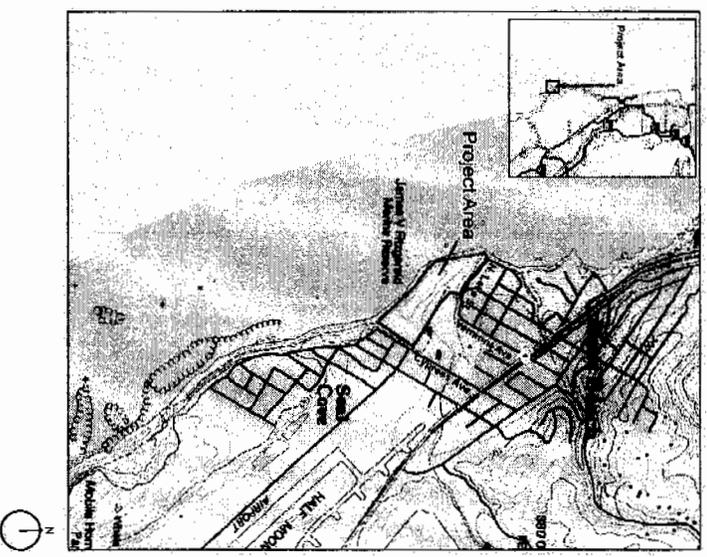
Fitzgerald Marine Reserve
Half Moon Bay, CA



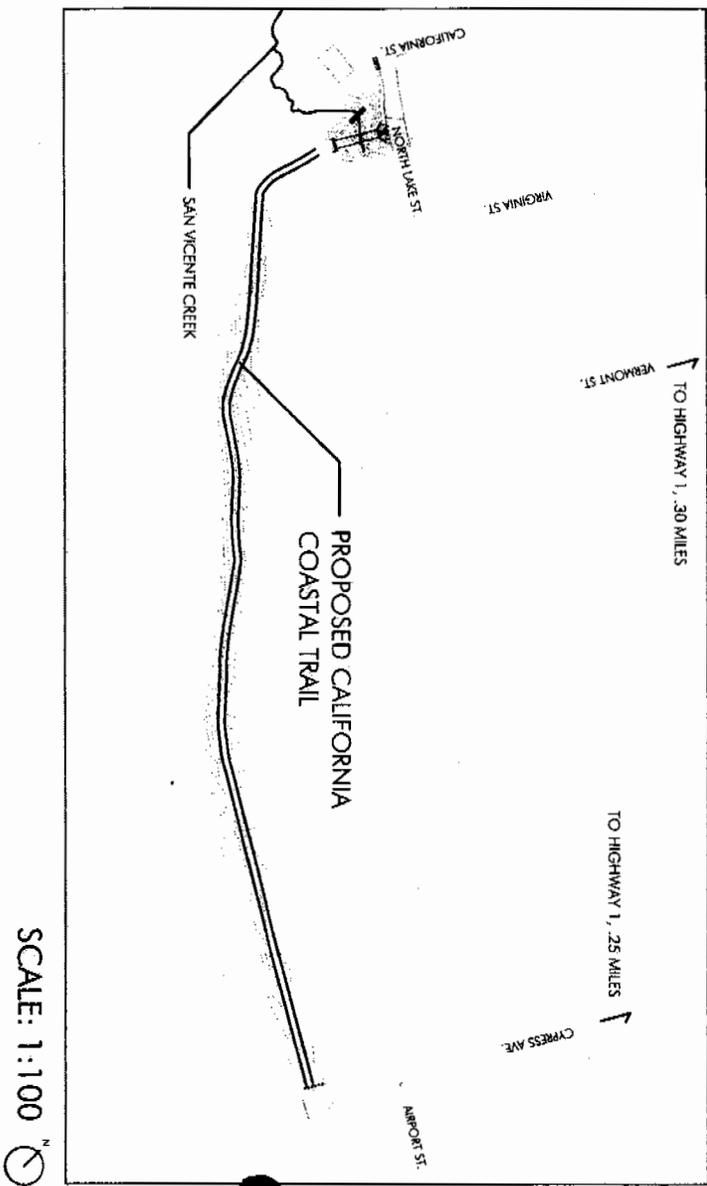
Date: October 2009
Map By: Sundaran Gillespie
Filepath: I:\Acad2000\18000\18054\gis\Arcmap\Location Map.mxd

FITZGERALD MARINE RESERVE COASTAL TRAIL MOSS BEACH, CALIFORNIA

LOCATION MAP



SITE MAP

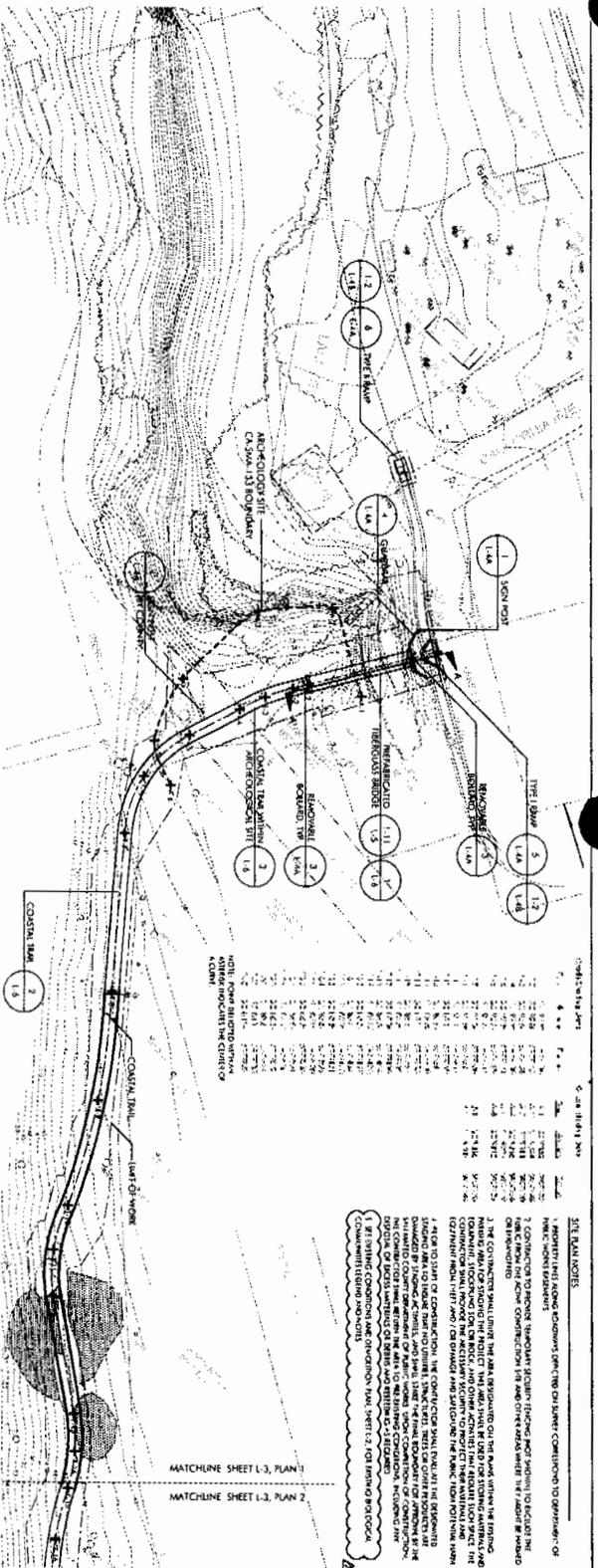


San Mateo County Planning Commission Meeting

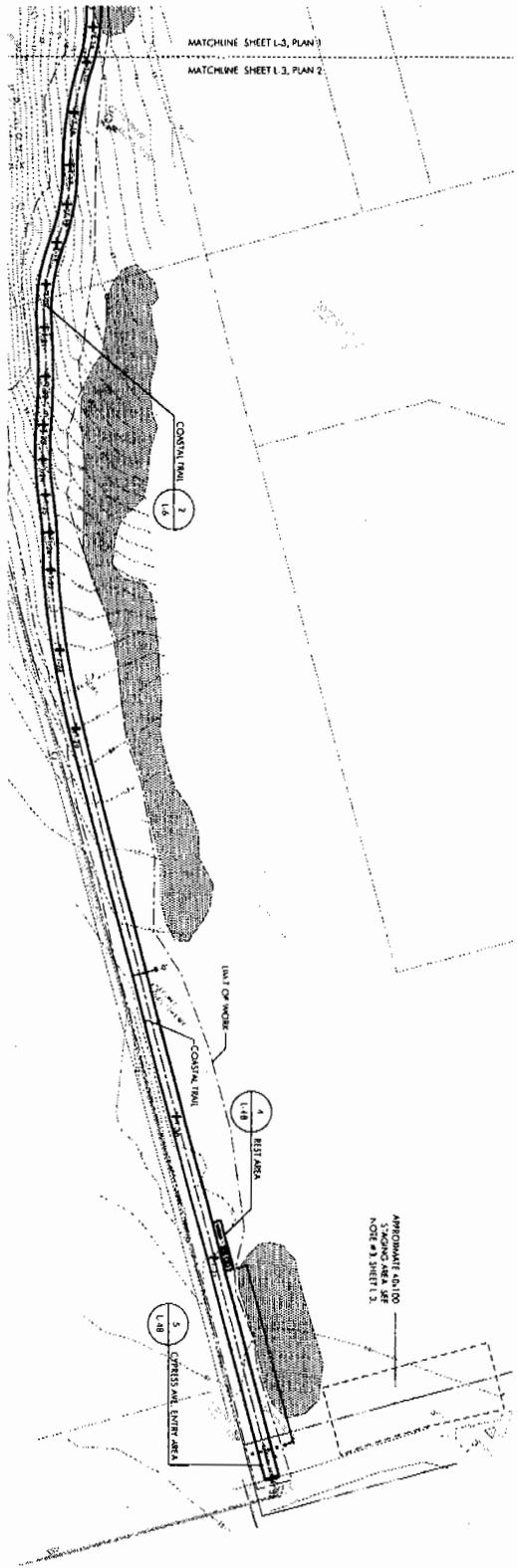
Owner/Applicant: **SAN MATEO COUNTY PARKS**

File Numbers: **PLN 2010-00093**

Attachment: **C**



1 SITE PLAN
SCALE 1" = 30'



2 SITE PLAN
SCALE 1" = 30'

Coastal-Trail-Site-(Rev. 2)

San Mateo County Planning Commission's Meeting

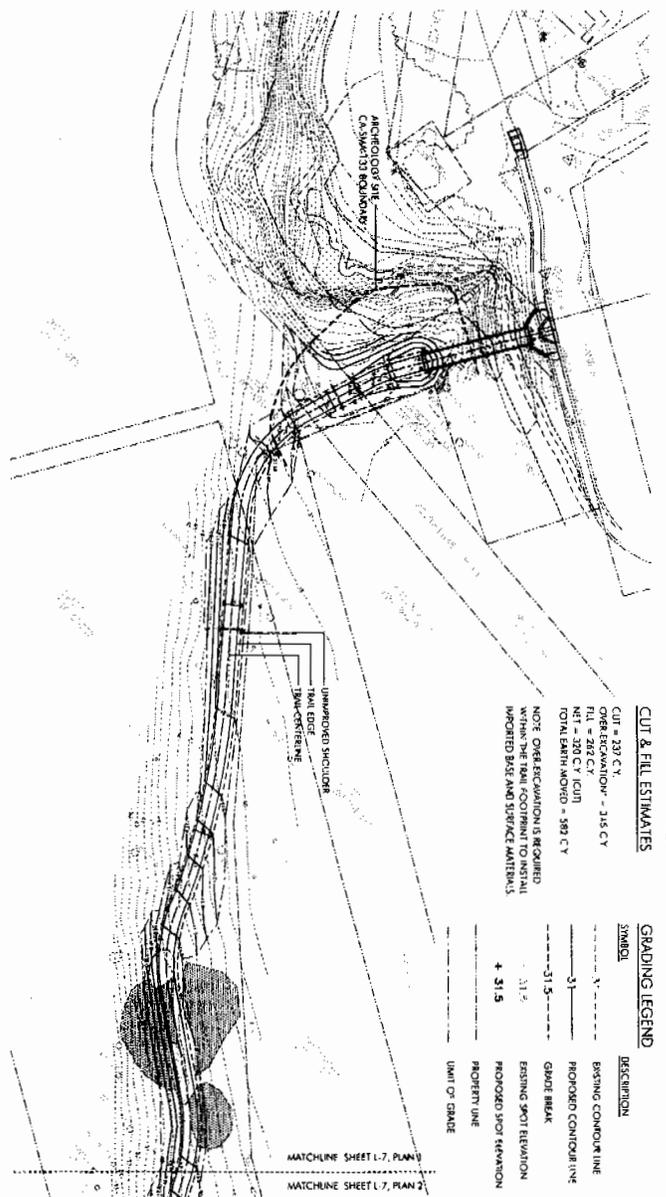
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File Numbers: **PLN 2010-00093**

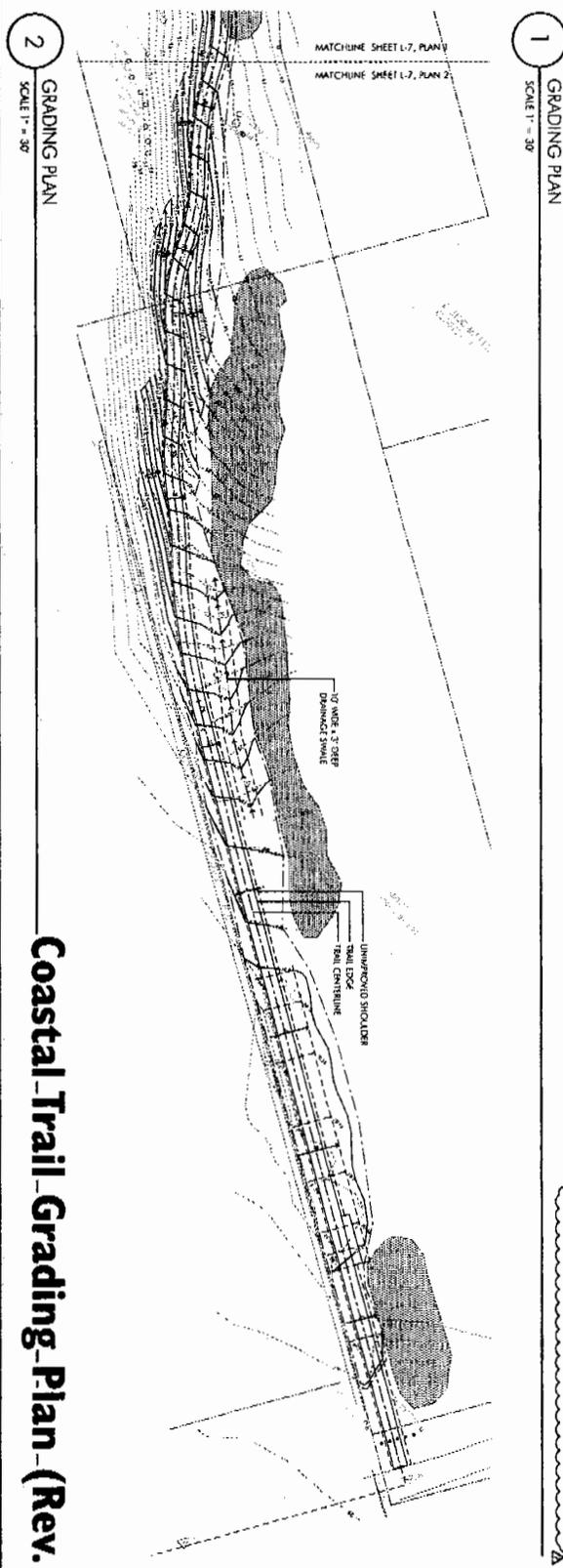
Attachment: *e*

GENERAL NOTES:

1. THE CONTRACTOR SHALL LIMIT THE AREA OF DISTURBANCE OF THE TRAIL TO THE MINIMUM NECESSARY TO ACCOMMODATE THE TRAIL AND SHALL REPAIR AND RESTORE THE TRAIL TO ORIGINAL CONDITION OR BETTER.
2. THE CONTRACTOR SHALL LIMIT THE AREA OF DISTURBANCE OF THE TRAIL TO THE MINIMUM NECESSARY TO ACCOMMODATE THE TRAIL AND SHALL REPAIR AND RESTORE THE TRAIL TO ORIGINAL CONDITION OR BETTER.
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5. THE CONTRACTOR SHALL LIMIT THE AREA OF DISTURBANCE OF THE TRAIL TO THE MINIMUM NECESSARY TO ACCOMMODATE THE TRAIL AND SHALL REPAIR AND RESTORE THE TRAIL TO ORIGINAL CONDITION OR BETTER.



- GRADING LEGEND**
- | SYMBOL | DESCRIPTION |
|--------|-------------------------|
| --- | EXISTING CONTOUR LINE |
| --- | PROPOSED CONTOUR LINE |
| --- | GRADE BREAK |
| --- | EXISTING SPOT ELEVATION |
| --- | PROPOSED SPOT ELEVATION |
| --- | PROPERTY LINE |
| --- | LIMIT OF GRADE |
- GRADING NOTES**
- THE GRADING PLANS INDICATE FINISHED GRADE ELEVATIONS.
 - SEE SHEET 1.1 FOR LOCATIONS OF STAKING AND STOCKING AREAS.
 - NO GRADING SHALL OCCUR WITHIN THE BOUNDARY OF EXISTING TREES TO REMAIN. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING TREES.
 - GRADE CONSTRUCTION FENCE SHALL BE INSTALLED AND MAINTAINED AT THE LOCATIONS SHOWN ON THE DRAWINGS IN ORDER TO PROTECT THE EXISTING TREES AND TO PREVENT THE CONSTRUCTION FROM ENTERING THE TRAIL RIGHT-OF-WAY. FENCING SHALL BE 4' HIGH AND SECURED WITH HEAVY TPOUS AT 6' ON CENTER. MATERIAL AND SHALL BE OF SET 7' MINIMUM FROM THE LIMIT OF GRADE.
 - OUTSIDE OF THE ANCHORAGE SITE BOUNDARY THE TRAIL SHALL BE GRADED WITH A COMPACTED 7% CROSS SLOPE ACROSS TRAIL AND UNIMPROVED SHOULDERS. THE TRAIL SHALL BE GRADED WITH A COMPACTED 2% CROWN BOUNDARY THE TRAIL SHALL BE GRADED WITH A COMPACTED 2% CROWN ACROSS TRAIL AND UNIMPROVED SHOULDERS, AS INDICATED ON PLANS.
 - TORUS SHALL BE EXCAVATED TO A DEPTH OF EIGHT (8) INCHES FROM THE AREAS WITHIN THE LIMIT OF GRADING AND TRANSPORTED TO THE STOCKING AREAS DESIGNATED ON THE PLANS. ANY EXCESS TORUS GENERATED WITHIN THE LIMIT OF GRADING SHALL BE TRANSPORTED TO THE STOCKING AREAS. THE DEPTH OF TORUS SHALL BE INDICATED ON PLANS AND INCLUDE THE VOLUME OF MATERIAL THAT CAN BE HAULED BY A SINGLE TRUCK SHALL BE LIMITED TO THE VOLUME THAT CAN BE FULLY COVERED AND PROTECTED DURING TRANSPORT. IF HAULING OPERATIONS RESULT IN ANY RELEASE OF MATERIAL ONTO THE ROAD SURFACE, THE CONTRACTOR SHALL CLEAN THE ROAD SURFACE IMMEDIATELY. THE CONTRACTOR SHALL OBTAIN THE NECESSARY PERMITS AND APPROVALS FROM THE COUNTY, INCLUDING THE CONSTRUCTION OF A PERMITS FENCE, CONSTRUCTION ENTRANCE, AND ADEQUATE EROSION CONTROL MEASURES TO MINIMIZE SOIL EROSION AFTER OCTOBER 30TH.
 - SEE EXISTING CONDITIONS AND DEMONSTRATION PLAN, SHEET 1.2, FOR EXISTING PROPOSED CONTOUR LINES AND NOTES.



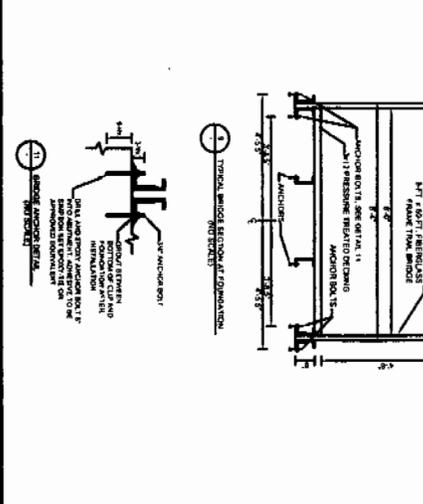
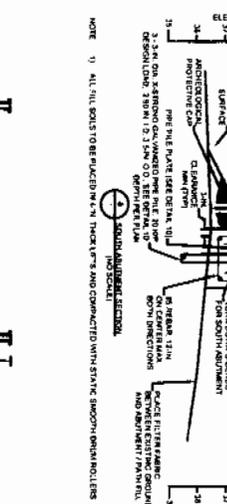
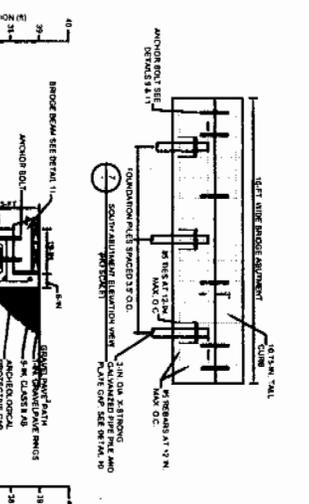
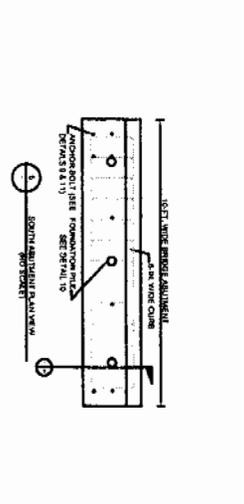
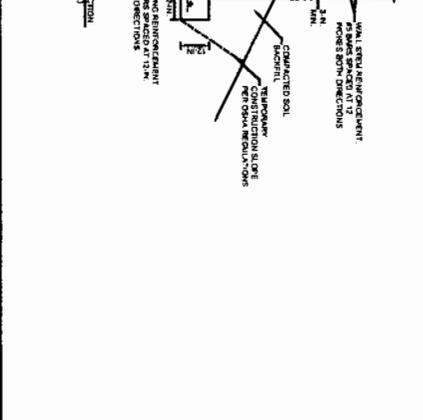
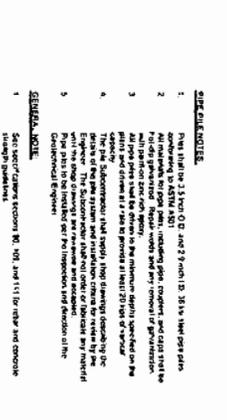
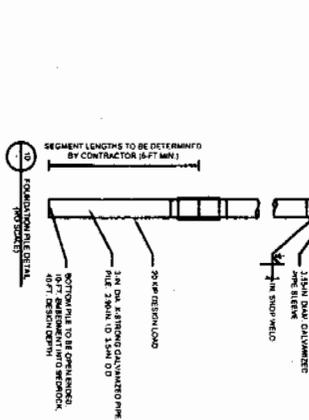
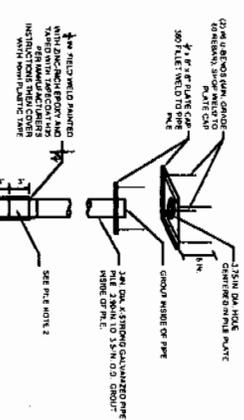
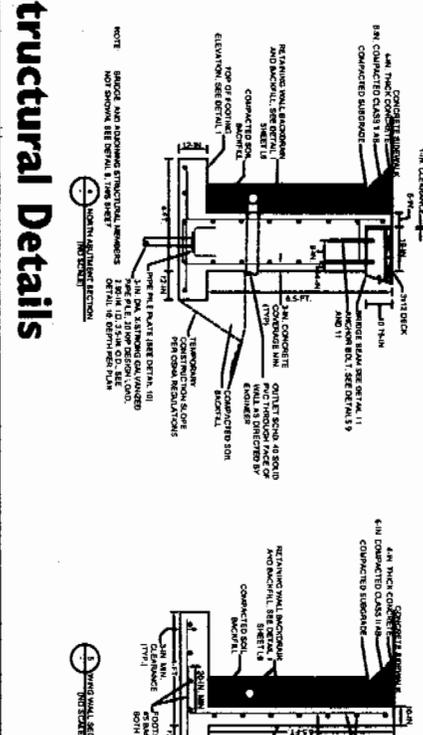
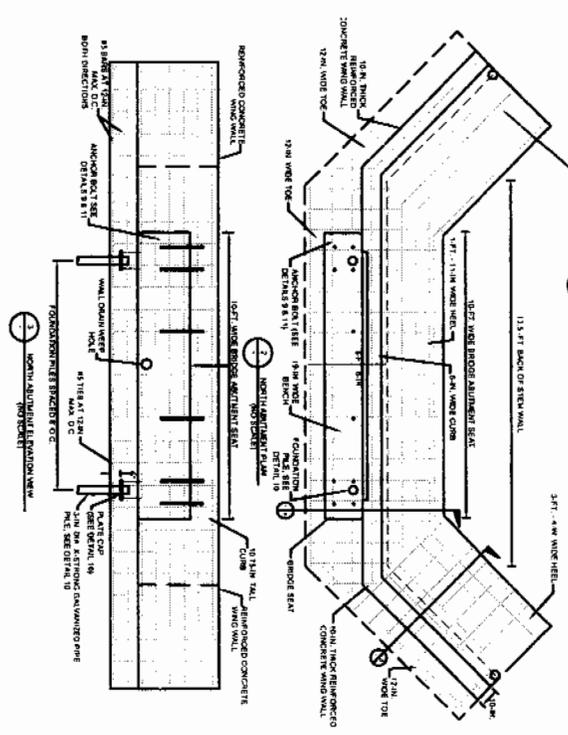
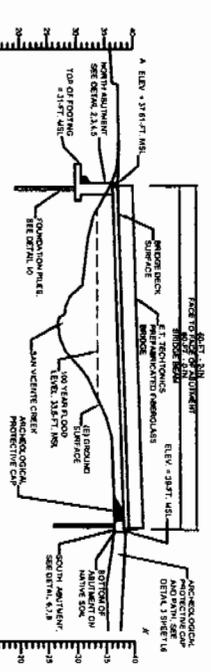
1 GRADING PLAN
 SCALE 1" = 30'

2 GRADING PLAN
 SCALE 1" = 30'

Coastal Trail Grading Plan (Rev. 2)

San Mateo County Planning Commissions Meeting
 Owner/Applicant: **SAN MATEO COUNTY PARKS**
 File Numbers: **PLN 2010-00093**

Attachment: *e*



NOTE: BRIDGE AND ADJOINING STRUCTURAL MEMBERS SHALL BE CONSTRUCTED WITH 3000 PSI STRENGTH CONCRETE AND 40,000 PSI YIELD STRENGTH REINFORCING STEEL UNLESS OTHERWISE SPECIFIED.

1. ALL TILT-SLABS TO BE PLACED IN A THICK 1/2" AND COMPACTED WITH STYRE-FOAM BEACH ROLLERS.

2. ALL TILT-SLABS TO BE PLACED IN A THICK 1/2" AND COMPACTED WITH STYRE-FOAM BEACH ROLLERS.

3. ALL TILT-SLABS TO BE PLACED IN A THICK 1/2" AND COMPACTED WITH STYRE-FOAM BEACH ROLLERS.

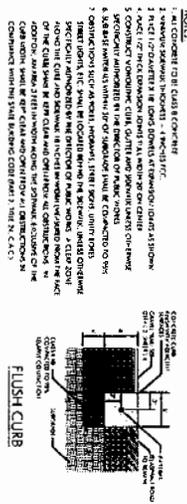
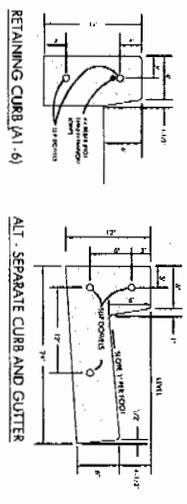
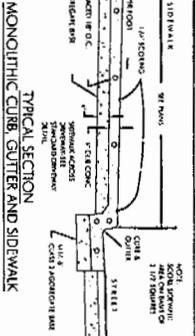
4. ALL TILT-SLABS TO BE PLACED IN A THICK 1/2" AND COMPACTED WITH STYRE-FOAM BEACH ROLLERS.

5. ALL TILT-SLABS TO BE PLACED IN A THICK 1/2" AND COMPACTED WITH STYRE-FOAM BEACH ROLLERS.

Structural Details

San Mateo County Planning Commission's Meeting

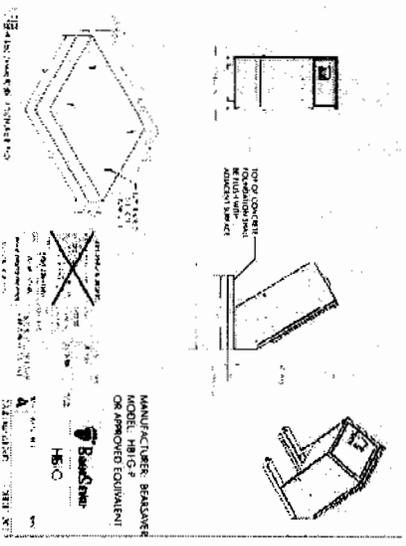
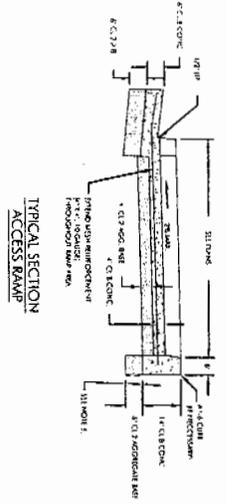
Owner/Applicant: **SAN MATEO COUNTY PARKS**
 File Numbers: **PLN 2010-00093**



1 SIDEWALK
NOT TO SCALE

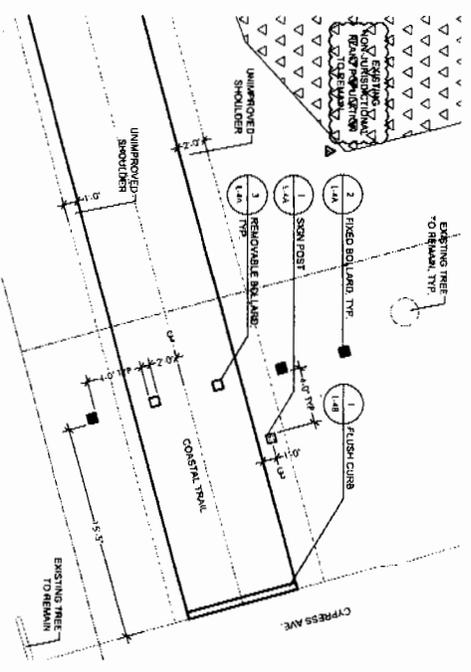
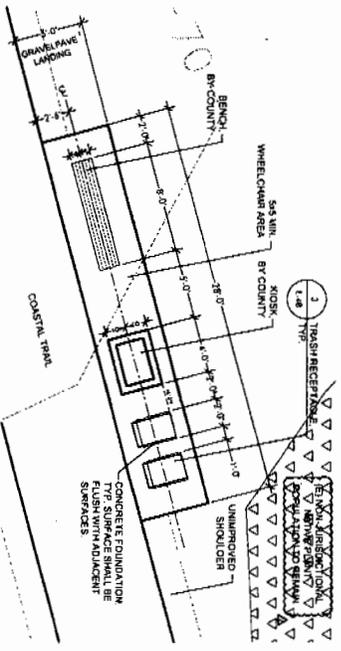
2 TYPICAL ACCESS RAMP SECTION
NOT TO SCALE

3 TRASH RECEPTACLE
NOT TO SCALE



4 REST AREA
SCALE: 1/4" = 1'-0"

5 CYPRESS AVE. ENTRANCE
SCALE: 1/4" = 1'-0"



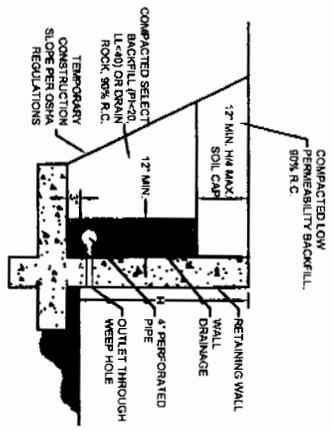
**Coastal Trail
Site Details
(Rev. 2)**

San Mateo County Planning Commission's Meeting

Owner/Applicant: **SAN MATEO COUNTY PARKS**

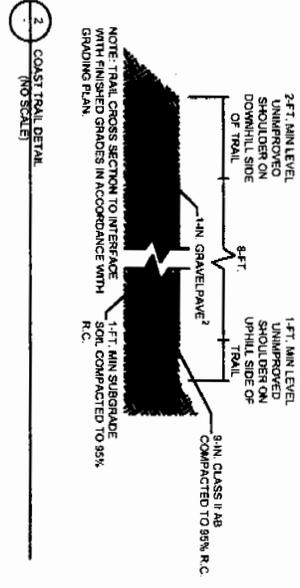
File Numbers: **PLN 2010-00093**

Attachment: **C**

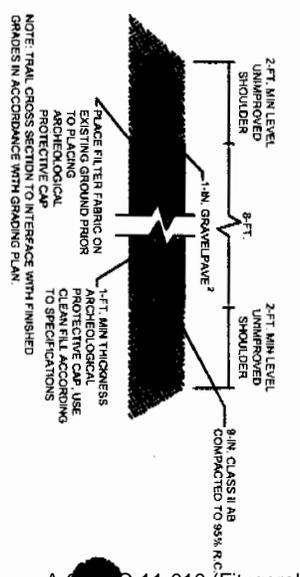


- NOTES:
1. Wall drainage should consist of clean, free draining 3/4 inch diameter rock (Class 18 Permeable Material) wrapped in filter fabric (Must 140# or equivalent) or Class II Permeable Material. Alternatively, a pre-fabricated drainage panel (Maxdrain G100# or equivalent) installed per the manufacturer's recommendations, may be used in lieu of drain rock and fabric.
 2. Perforated pipe shall be SCH 40 or STD 35. Open pipe perforations down and spaced at 1 1/2" to a gravity outlet. Alternatively, drainage can be made through 3" diameter weep holes spaced approximately 20' apart.
 3. Clean outs should be installed at the upslope end and at significant direction changes of the perforated pipe. Additionally, all angled connectors shall be long bend sweep connectors.
 4. During construction, the contractor should use appropriate methods (such as temporary bracing and/or light compaction equipment) to avoid over-stressing the walls. Visits shall be completed installed prior to construction in front of or above the retaining wall.
 5. All work and materials shall conform with Section 69 of the latest edition of the State of California Standard Specifications (Caltrans).

1. STANDARD RETAINING WALL BACKDRAIN (NO SCALE)



- GRAVELPAVE NOTES:
1. GRAVELPAVE RINGS AND INFILL TO BE TERRACOTTA BROWN COLOR.
 2. INFILL SHALL CONSIST OF POORLY-GRADED ANGULAR GRAVEL, 3/8" DIAMETER MAX.; COLOR AND TYPE TO BE SELECTED BY COUNTY OF SAN MATEO.
 3. GRAVELPAVE SHALL BE PLACED ON A 1/2" THICKNESS CONCRETE WALKS ADJACENT TO GRAVELPAVE AREAS MUST BE COMPLETED PRIOR TO INSTALLATION OF GRAVELPAVE. REFER TO MANUFACTURER'S TECHNICAL SPECIFICATIONS FOR ADDITIONAL INSTALLATION NOTES.
 - 4.



3. COAST TRAIL WITHIN ARCHEOLOGICAL SITE DETAIL (NO SCALE)

Structural Details

San Mateo County Planning Commission's Meeting

Owner/Applicant: **SAN MATEO COUNTY PARKS**

File Numbers: **PLN 2010-00093**

Attachment:

Golden Gate National Recreation Area
Sam Herzberg
Gary Lockman
January 14, 2011
Page 3

Attachment A

County of San Mateo
Planning and Building Department

FINDINGS AND CONDITIONS OF APPROVAL

Permit or Project File Number: PLN 2010-00093

Hearing Date: January 12, 2011

Prepared By: Melissa Ross, Project Planner

Adopted By: Planning Commission

FINDINGS

Regarding the Environmental Review, Found:

1. That the Mitigated Negative Declaration is complete, correct and adequate for this project, and prepared in accordance with the California Environmental Quality Act and applicable State and County Guidelines.
2. That, on the basis of the Initial Study and comments hereto, there is no evidence that the project, subject to the mitigation measures contained in the Mitigated Negative Declaration, will have a significant effect on the environment.
3. That the Mitigated Negative Declaration reflects the independent judgment of San Mateo County.
4. That the mitigation measures identified in the Mitigated Negative Declaration, agreed to by the applicant, placed as conditions on the project, and identified as part of this public hearing, have been incorporated in to the Mitigation and Reporting Plan in conformance with California Public Resources Code Section 21081.6.

Regarding the Coastal Development Permit, Found:

5. That the project, as described in the application and accompanying materials required by Section 6328.7 of the San Mateo County Zoning Regulations and as conditioned in accordance with Section 6328.14 of the San Mateo County Zoning Regulations, conforms

with the plans, policies, requirements and standards of the San Mateo County Local Coastal Program as discussed in the staff report.

6. Where the project is located between the nearest public road and the sea, that the project is in conformity with the public access and public recreation policies of Chapter 3 of the Coastal Act of 1976 (commencing with Section 30200 of the Public Resources Code).
7. That the project conforms to the specific findings required by policies of the San Mateo County LCP. As discussed in the staff report under Section A.2, the applicant has agreed to implement the mitigation measures identified in the biological report in order to minimize any potential impact to biological resources to a less than significant impact.

CONDITIONS OF APPROVAL

Current Planning Section

1. This approval applies only to the proposal, documents and plans described in this report and submitted to and approved by the Planning Commission at its January 12, 2011 meeting. Minor revisions or modifications to this project may be made subject to the review and approval of the Community Development Director.
2. Prior to the beginning of grading and construction activities, the applicant shall submit to the Planning Department for review and approval, an erosion and drainage control plan which shows how the transport and discharge of soil and pollutants from the project site will be minimized. The goal is to prevent sediment and other pollutants from leaving the project site and to protect all exposed earth surfaces from erosive forces. Said plan shall adhere to the San Mateo Countywide Stormwater Pollution Prevention Program "General Construction and Site Supervision Guidelines," including:
 - a. Stabilizing all denuded areas and maintaining erosion control measures continuously between October 15 and April 15.
 - b. Removing spoils promptly, and avoiding stockpiling of fill materials when rain is forecast. If rain threatens, stockpiled soils and other materials shall be covered with a tarp or other waterproof material.
 - c. Storing, handling, and disposing of construction materials and wastes so as to avoid their entry to a local storm drain system or water body.

- d. Avoiding cleaning, fueling or maintaining vehicles on-site, except in an area designated to contain and treat runoff.

The approved erosion and drainage control plan shall be implemented prior to the beginning of grading activities.

3. This permit shall be valid for one year in which time, the applicant shall commence construction. Any extension of this permit shall require submittal of an application for permit extension and payment of applicable permit extension fees.
4. Bats. If construction will occur during the bats maternity roosting season (defined as: April 1 through August 31), then the applicant shall perform pre-construction surveys for bats. Surveys shall be conducted by a qualified biologist no less than 14 days prior to removal of trees, snags or buildings within the Study Area. Ultrasonic acoustic surveys and/or other site appropriate survey methods shall be performed to determine the presence or absence of bats utilizing the Study Area as roosting or foraging habitat. If special status bat species are detected during surveys, appropriate species and roost specific mitigation measures shall be developed. Such measures may include postponing removal of trees, snags or structures until the end of the maternity roosting season or construction of species appropriate roosting habitat within, or adjacent to the Study Area.

Trees, snags and buildings may be removed outside of the maternity roosting season without performing pre-construction bat surveys. However, if buildings are to be demolished, internal entrance surveys shall be performed by a qualified bat biologist no less than 14 days prior to demolition to determine if buildings currently or previously support roosting bats. If bats are determined to be present, appropriate methods shall be used to exclude bats from the building. Such methods may include installation of one way "valves" to allow bats to exit, but not allow them to re-enter the building. Species and roost appropriate mitigation measures shall be developed based on the results of the survey in consultation with the California Department of Fish and Game (CDFG).

- *5. Dusky-footed woodrat. If stick houses are observed, they shall be avoided if possible. If any stick house must be destroyed, observance and live trapping should be done by a qualified biologist to determine if it is occupied, and if so whether by a male or female. If by a lactating female, the nest should be left undisturbed for one month until the young are mature enough to be relocated.

6. Pacific harbor seal. The Pacific harbor seal is known to haul out offshore of the Study Area. Construction activities in the Study Area may have the potential to impact this species through acoustic or visual disturbance.

Visual and acoustic disturbance during construction may affect the behavior of the Pacific harbor seal. Construction noise levels shall be kept lower than 160 decibels - the limit recommended by the National Marine Fisheries Service (NMFS) for the protection of marine mammals. The applicant shall coordinate with NMFS for the issuance of a marine mammal Incidental Harassment Authorization Permit or Letter of Concurrence for potential disturbances to the seals, if NMFS determines that one is necessary.

7. Birds. Nesting birds, including a number of special status birds, may be impacted if construction activities occur in or near potential breeding habitat (aquatic and upland vegetation) during the breeding season from February through August.

If possible, the clearing of vegetation and the initiation of construction shall be done in the non-breeding season between September and January. If these activities cannot be done in the non-breeding season, a qualified biologist shall perform pre-construction breeding bird surveys within 14 days of the onset of construction or clearing of vegetation. If nesting birds are discovered in the vicinity of planned construction, a buffer area around the nest will be established until the nest is vacated. The size of the buffer would be dependent on the habitat, level of disturbance and the particular species of nesting bird. Tree removal shall be prohibited during the nesting season and trees shall not be felled into sensitive areas in the archaeological area, the creek and its riparian area.

8. California red-legged frog and San Francisco garter snake. Upland dispersal, foraging and estivation habitat for California red-legged frog (CRLF) and San Francisco garter snake (SFGS) may be impacted by the proposed project.
 - a. Prior to the start of groundbreaking activities, all construction personnel will receive training on listed species and their habitats by a USFWS-approved biologist. The importance of these species and their habitat will be described to all employees as well as the minimization and avoidance measures that are to be implemented as part of the project. An educational brochure containing color photographs of all listed species in the work area(s) will be distributed to all employees working within the project site(s). The original list of employees who attend the training sessions will be maintained by the applicant and be made available for review by the USFWS upon request.

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- b. Wildlife exclusion fencing will be erected and maintained around the perimeter of the project and project staging areas to prevent SFGS and CRLF from entering the site. Installation of the fence will be performed under the supervision of a USFWS-approved biologist. Once the fencing is installed, workers will clear all vegetation within this area with belt driven weed whackers or other hand tools to a height of 4-6 inches. Following the removal of vegetation, pre-construction surveys will be performed prior to the start of any groundbreaking activities by a USFWS-approved biologist. Fencing will be equipped with one-way escape funnels. Fencing will extend a minimum of 36 inches above ground level and will be buried 4-6 inches into the ground. Exclusion fencing will be checked a minimum of one time per week by biological monitors for the duration of the project to identify problems or weaknesses in fence integrity and function. All compromised portions will be repaired and/or replaced immediately. Upon completion of the project, all fencing material will be removed from the site and disposed of properly.
- c. A USFWS-approved biologist shall be present at the work site until such time as all removal of CRLF and/or SFGS, instruction of workers, and habitat disturbance have been completed. After this time, the contractor or permittee shall designate a person to monitor on-site compliance with all minimization measures. The USFWS-approved biologist shall ensure that this individual (on-site biological monitor) receives training outlined (above) under Section (a) and in the identification of CRLF and SFGS. The on-site biological monitor and the USFWS-approved biologist shall have the authority to halt any action that might result in impacts that exceed the levels anticipated by the Corps and USFWS during review of the proposed action. If work is stopped, the Corps and USFWS shall be notified immediately by the USFWS-approved biologist or on-site biological monitor.
- d. The on-site biological monitor(s) will remain on-site for the duration of the proposed project, including vegetation removal, grading and cleanup activities. If a CRLF or SFGS is observed at any time, the on-site biological monitor will have the authority to halt work on the project site until these animals are no longer within the work area. If construction activities are occurring at more than one location at a time, each area must have a minimum of one on-site biological monitor present to increase the likelihood that listed species are detected.
- e. Tightly woven fiber netting or similar material shall be used for erosion control or other purposes at the project to ensure that the CRLF and SFGS do not get trapped.

This limitation will be communicated to the contractor. Plastic mono-filament netting (erosion control matting), rolled erosion control products or similar material shall not be used at the project site because red-legged frog, garter snake, and other species may become entangled or trapped in it.

- f. Because CRLF and SFGS may take refuge in cavity-like and den-like structures such as pipes and may enter stored pipes and become trapped, all construction pipes, culverts, or similar structures that are stored at a construction site for one or more overnight periods will be either securely capped prior to storage or thoroughly inspected by the on-site biologist and/or the construction foreman/manager for these animals before the pipe is subsequently buried, capped, or otherwise used or moved in any way. If a red-legged frog is discovered inside a pipe by the on-site biologist or anyone else, the on-site biologist shall move the animal to a safe nearby location and monitor it until it is determined that it is not imperiled by predators or other dangers. If a SFGS is found, it should be allowed to passively leave the work area on its own, as determined by the on-site biologist, or moved by a SFGS permitted biologist with permission from USFWS and CDFG.

- g. To prevent inadvertent entrapment of CRLF and SFGS during construction, the on-site biologist and/or construction foreman/manager shall ensure that all excavated, steep-walled holes or trenches, more than 1-foot deep, are completely covered at the close of each working day by plywood or similar materials, or provided with one or more escape ramps constructed of earth fill or wooden planks and inspected by the on-site biologist. Before such holes or trenches are filled, they will be thoroughly inspected for trapped animals by the on-site biologist and/or construction foreman/manager. If at any time a trapped CRLF or SFGS is discovered by the on-site biologist or anyone else, it should be allowed to passively leave the work area on its own, as determined by the on-site biologist, or moved by a SFGS permitted biologist with permission from USFWS and CDFG.

9. California Red-Legged Frog Specific Conditions

- a. Pre-construction surveys for the CRLF will be performed by USFWS-approved biologists throughout the action area immediately prior to groundbreaking activities. If a CRLF is observed, a USFWS-approved biologist will remove these individuals to locations outside of the work area but within the greater property boundary. Red-legged frogs will not be removed from the vicinity or remain in captivity over night unless in the care of a certified wildlife veterinarian.

- b. Under no circumstances will mosquito fish (*Gambusia affinis*) be introduced at any location within the Study Area. If pesticide application for mosquito abatement becomes necessary, the applicant will contact the USFWS and the San Mateo Mosquito Abatement District for further guidance.
- c. Upon completion of the construction and rehabilitation phases of the proposed project, the applicant will monitor the property regularly and according to a USFWS-approved monitoring plan to ensure the successful establishment of native plants in all restored areas. Any plant species observed in these areas determined to be invasive will be removed.
- d. Because dusk and dawn are often the times when CRLF are most actively foraging and dispersing, all construction activities should cease one-half hour before sunset and should not begin prior to one-half hour before sunrise.

10. San Francisco Garter Snake Specific Conditions

- a. Pre-construction surveys for the SFGS will be performed by USFWS-approved biologists throughout the action area immediately prior to groundbreaking activities. If at any time during the survey, vegetation removal, or construction phases of the proposed project a SFGS is observed within the action area, the animal will be allowed to passively leave the work area unless in circumstances where the animal is determined to be trapped as discussed above.
 - b. All vegetation removed during trail construction activities will remain on-site upon completion of trail development. A portion of this vegetation will be utilized to facilitate the rehabilitation of the existing unofficial trails. The remainder of the removed vegetation will be bundled into small piles and placed near on-site aquatic areas to provide cover for local SFGS.
- *11. The applicant shall restore the area of the removed bridge and old trail alignment through decompaction or scarifying of the compacted trail area, placement of soil over decompacted areas where appropriate, and revegetation which incorporates species native to the area, including willows adjacent to the stream. The restored area shall be maintained and monitored to ensure success over a 5-year period.
12. All best management practices prescribed by the San Mateo County Planning Department

for work within sensitive habitat areas will be implemented to the full extent:

13. Designated construction staging areas will be utilized as the staging areas for the trail and bridge construction activities. All vehicles associated with project activities will be clustered within these areas at the end of each workday or when not in use to minimize habitat disturbance and water quality degradation. Before vehicles move from the staging areas at the start of each workday or before they return to this location at the end of each workday, the on-site biological monitor will check under the vehicles and their tires to ensure no listed species are utilizing the equipment as temporary shelter.
14. No lighting will be incorporated at any location into the project design.
15. To discourage recreational users from leaving designated trails, interpretive signs describing the sensitivity of the habitat and how to utilize the property in an ecologically sensitive manner will be placed at trailheads and wetlands adjacent to enhanced trails. If rehabilitated trails show continued signs of usage, the applicant will implement additional preventative measures, such as the installation of additional signage or fencing. Trailhead signs will also describe the importance of prohibitions on unrestrained domestic pets and the associated fines for violating these laws.
16. No trash shall be deposited on the site during construction activities. All trash shall be placed in trash receptacles with secure lids or stored in vehicles.
17. Fueling and maintenance of equipment will be conducted off-site and at least 50 feet from any wetland.
18. Permanent and temporary disturbances and other types of project-related disturbance to the habitats of the garter snake shall be minimized to the maximum extent practicable. To minimize temporary disturbances, all project-related vehicle traffic shall be restricted to established roads, construction areas, and other designated areas. These areas should be established in locations disturbed by previous activities to prevent further adverse effects.
19. The applicant shall compensate for impacts to California wild strawberry by replacing ice plant with native strawberry at a 1:1 ratio of planted strawberry habitat to impacted area. California wild strawberry will be planted in areas currently dominated by ice plant that are adjacent to those locations where impacts to existing strawberry are proposed. If no ice plant is found within the areas of the trail improvements, replanting of wild strawberry in a 1:1 ratio shall occur in a suitable location as close as possible to the area of the removed

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wild strawberry. Temporary impacted areas of California strawberry as a result of project activities will be replanted and returned to pre-construction conditions. California wild strawberry planting activities will take place concurrently with trail improvements. To ensure optimal survival and establishment, plantings will be installed in the fall before the onset of the rainy season. This compensation shall be documented through the submittal of a mitigation planting plan, which shall include final success criteria, implementation measures, maintenance and monitoring plan.

20. The applicant shall implement the following dust control measures during grading and construction activities:
 - a. Water all active construction and grading areas at least twice daily.
 - b. Cover all trucks hauling soil, sand, and other loose materials or require all trucks to maintain at least two feet of freeboard.
 - c. Pave, apply water two times daily, or apply (non-toxic) soil stabilizers on all unpaved access roads, parking areas and staging areas at the project site.
 - d. Sweep streets daily (with water sweepers) if visible soil material is carried onto adjacent public streets.
 - e. Enclose, cover, water twice daily or apply (non-toxic) soil binders to exposed stockpiles (dirt, sand, etc.).
- *21. Noise levels produced by proposed construction activities shall not exceed the 80-dBA level at any one moment. Construction activities shall be limited to the hours from 7:00 a.m. to 6:00 p.m., Monday through Friday. Construction operations shall be prohibited on Saturday and Sunday and any national holiday.
22. The proposed pedestrian bridge shall be supported by a concrete abutment that will be constructed at or above existing grade on the south bank of San Vicente Creek. The abutment shall be supported by three 3-inch diameter metal pilings, which shall be installed to a depth (> 37 feet) that will contact bedrock.
 - a. Pilings shall be installed using a small vibratory hammer mounted on small rubber tired or tracked Bobcat or equivalent equipment.

- b. Heavy equipment shall be rubber tired or rubber tracked and of the smallest size necessary to complete any earthwork.
 - c. Pilings shall be placed within the smallest area possible subject to ground disturbance near the creek edge.
 - d. Any area, within the archaeological site or immediately adjacent to the recorded site boundary, subject to disturbance by workers or equipment, shall be protected by installing straw layer/wood mats on the surface prior to work. The straw layer/wood mats shall be placed by hand and removed by hand to avoid surface disturbance.
23. A segment of the California Coastal Trail is present within the archaeological resource. The segment shall be installed above the existing grade in order to minimize disturbances to the ground surface.
- a. Engineered fill shall be placed over the ground surface in the locations of the trail in order to protect the archaeological site. The fill shall have a minimum depth of 12 inches and may increase in thickness to approximately 24 inches at the south bridge abutment.
 - b. A permeable geo-textile fabric will be installed on top of the undisturbed ground surface and underneath the engineered fill to mark the location of the undisturbed ground surface. Standard installation methods for the trail within the archaeological site shall:
 - c. Omit compaction of sub-grade;
 - d. Place fill soils for the protective cap in 4-inch lifts; and,
 - e. Compact with static smooth drum roller instead of vibratory compaction.
 - f. Any area, within the archaeological site or immediately adjacent to the recorded site boundary, subject to disturbance by workers or equipment, shall be protected by installing straw layer/wood mats on the surface prior to work. The straw layer/wood mats shall be placed by hand and removed by hand to avoid surface disturbance.
 - g. Heavy equipment shall be rubber tired or rubber tracked and of the smallest size necessary to complete any earthwork.

- h. Hand construction of the trail within the archaeological site boundary shall be utilized wherever practical to minimize mechanical damage.
 - i. Fill material shall be brought to the project area via the existing unimproved trail that is accessed from Cypress Avenue. No improvements shall be made to the trail.
 - j. Heavy equipment and temporary fill stockpiles shall be prohibited within or adjacent to the recorded archaeological site boundary. The “no-go” zones along trail alignment within site boundary shall be marked during construction. Install barrier fencing to confine equipment and construction traffic to trail within site boundary.
 - k. Temporary fill stockpiles may be placed along the existing trail if they are more than 25 feet from the recorded site boundary.
 - l. Fill shall be transferred from stockpile areas to areas within the archaeological site by hand whenever possible or using equipment if the pressure applied by the combined weight of the equipment and fill shall not cause compaction of the ground surface within the site (Note: applied pressure shall not exceed 15 pounds per square inch). Only rubber tire or rubber track equipment shall be used. All ground disturbing construction or construction with the potential for native soil ground disturbance within or within 50 feet of the recorded site boundary of CA-SMA-133 shall be monitored by a professional archaeologist meeting the Secretary of the Interior’s standards.
 - m. Archaeological monitor shall have the authority to temporarily halt any ground disturbing construction to identify and evaluate any archaeological materials inadvertently exposed during construction. The exposure of significant resources could result in the development of a treatment program including scientific removal, analysis and reporting. The exposure of any Native American burials shall be handled in accordance with state law.
24. The applicant shall submit colors for the E.T. Techtonics fiberglass bridge for review and approval by the Current Planning Section prior to installation. Color(s) shall blend with the surrounding vegetation (e.g. grey-green). Any color or materials, additions, or modifications of the approved color are subject to review and approval by the Community Development Director prior to installation.

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25. The applicant shall install a GravelPave² system for the trail improvements. Colors shall blend with the surrounding environment (e.g. tan or grey). Colors shall be reviewed for approval by the Current Planning Section. Any color or materials, additions, or modifications are subject to review and approval by the Community Development Director prior to installation.
26. No additional trees beyond those notated in this staff report shall be removed. Removal of any tree with a diameter equal to or greater than 12 inches as measures 4.5 feet above the ground shall require a separate tree removal permit. For each tree removed, the applicant shall replant in a 2:1 ratio with native trees.
- *27. The disturbance or removal of native vegetation shall not exceed the minimum necessary to construct the trail. In particular, the applicant shall minimize disturbance to the area of the Western sword fern and rushes to the maximum extent possible. Any Western sword fern or rushes that must be removed by trail construction shall be carefully removed, save, and replanted as part of the revegetation of this area.
- *28. The applicant shall remove cape ivy in advance of any disturbance or removal of soil and spot treat resprouts with an herbicide. Cape ivy roots or root segments shall be removed from any soil being moved during construction.
29. The applicant shall minimize ground disturbance and only install unimproved shoulders where the current trail width is less than 8 feet and only where necessary for structural support of the new trail system. Unimproved shoulders shall not be installed where the topography immediately adjacent to the new trail is flat (e.g. Cypress Avenue Trailhead).

CALIFORNIA COASTAL COMMISSION

45 FREMONT, SUITE 2000
SAN FRANCISCO, CA 94105-2219
VOICE (415) 904-5200
FAX (415) 904-5400
TDD (415) 597-5885

**M E M O R A N D U M**

FROM: John Dixon, Ph.D.
Ecologist

TO: Roxanna Farshchi

SUBJECT: Coastal Trail at Fitzgerald Marine Reserve

DATE: March 25, 2011

Wetland Research Associates (WRA) conducted a wetland delineation at this site in August 2008. There were several areas parallel to the trail that were dominated by wetland indicator species, particularly blackberry and rushes. The area with the strongest wetland indicators would potentially be impacted by the proposed trail improvements. WRA concluded that, "[t]his area is determined not to be a CCC/LCP wetland due to the principle dominance of facultative (sic) species in the identified community, positive evidence of upland conditions as shown by hydrological observations in the winter, and the lack of hydrology indicators, or hydric soils." In fact both FAC and FACW species were present and a predominance of such wetland indicator species is presumptive evidence of a wetland under the Coastal Act and LCP. Furthermore, the "evidence of upland conditions" was an anecdotal report that there was no inundation during the winter rainy season; the soil was not examined to determine whether it was saturated. In any event, the action of the local government was appealed, partly because of the evidence that presumptive wetlands would be impacted by the project, and the Commission found that a substantial issue was raised.

When wetland delineations are conducted during the summer dry season, it is difficult to assess hydrology. Therefore, we reexamined the area next to the trail during the winter rainy season. On the morning of March 23, we visited the site and examined the area with a predominance of FAC and FACW species that was designated Sample Point 8 by WRA. At that location, the ground slopes gently down from the trail to a fence line. The vegetation is primarily comprised of rushes and blackberry, as documented in the wetland delineation. I examined the soil column to a depth of 15 inches near the trail and near the fence line. The dark soil is a gravelly loam to clay loam. There were no redoximorphic features evident. Judging from rainfall records at nearby stations, this has been a normal to somewhat wetter than normal rain year. There probably was around 2.5 to 3 inches of rain in the 10 days prior to our visit, most of it falling on March 18 and 19, and it was raining at the time of our visit. In the soil pits, earth worms were present a few inches below the soil surface and the soil was merely damp in the upper 15 inches. There was just sufficient moisture to knead the soil into a plastic ball for assessing soil texture. Given the recent significant rainfall, I believe that the striking lack of moisture in the shallow soil is evidence that the area is an upland and that the wetland indicator species that are present are not growing as hydrophytes at this location.

From: Chris & Hollie Dunham
142 Orval Ave
Moss Beach, CA 94038

11 April 2011

Subj: **Dardanelle Trail (Moss Beach, CA)**

To: California Coastal Commission
Northern Central Coast District Office
45 Fremont St, Suite 2000
San Francisco, CA 94105-2219

RECEIVED

APR 12 2011

**CALIFORNIA
COASTAL COMMISSION**

Dear Commissioners,

We're writing to support the upgrade as proposed by the San Mateo Parks Department. Previously we've written the San Mateo Parks Department to support their planning and have offered to write others involved in the approval process. Now is the time to write to you.

Our family and friends have been enjoying the trails near the Fitzgerald Marine Reserve for over 10 years now. We really appreciate the new, safe steps leading to the beach near Seal Cove. It is an example of great planning and execution. What we saw related to the Dardanelle Trail improvement also seemed well thought out and planned.

We respectfully disagree with Sabrina Brennan's appeal of the plan. She does not speak for us and she is one of the individuals responsible for a narrower (we're agnostic about 8' versus 10' but the Parks Department made a convincing case the latter made sense), more expensive and less durable path in the proposal before the Commission. As taxpayers we didn't appreciate that change.

We expect you are obligated to weigh public comments as well as rules and regulations. We have provided input on the former, but have no expertise related to the latter. However, one "thought experiment" we'd leave you with is this. Imagine if the 8' trail and safe bridge had been part of the way residents and visitors had accessed this part of the coast for some time (say 10 years or so). Now imagine that someone would like to reduce the path width. I suspect you can easily anticipate how the community would respond and trust in this instance you'll do the best to provide safe and accessible access to this part of the coast.

Thanks for your time.



Chris & Hollie Dunham
142 Orval Ave
Moss Beach, CA 94038

cc: San Mateo Parks Department

Roxanna Farshchi

From: Scott Snow [ssakolsnow@gmail.com]
Sent: Thursday, April 07, 2011 8:28 PM
To: Roxanna Farshchi
Subject: Fitzgerald Marine Reserve Coastal Trail and Bridge

Hello Roxanna,

Please approve the County's Coastal Development Plan for Fitzgerald Marine Reserve Coastal Trail and Bridge. Without a bridge in place across San Vicente Creek and with limited access onto the beach from the main access ramp when the creek is running high and fast, accessing Seal Cove on foot is impossible. This means there is no safe way to take our tours of children to the area of the reef that provides the most learning opportunities.

Thank you for your consideration.

Scott
Volunteer Naturalist and Docent, FMR

Scott A. Snow
415-218-2095 Mobile
ssakolsnow@gmail.com

Scott A. Snow
415-218-2095 Mobile
ssakolsnow@gmail.com

Roxanna Farshchi

From: L8428@aol.com
Sent: Wednesday, April 06, 2011 3:28 PM
To: Roxanna Farshchi
Cc: acrumpgaray@yahoo.com
Subject: Fwd: Fitzgerald Marine Reserve Coastal trail improvement

Please see the below email.

From: acrumpgaray@yahoo.com
To: rfarschchi@coastal.ca.gov
CC: L8428@aol.com
Sent: 4/6/2011 2:01:03 P.M. Pacific Daylight Time
Subj: Fitzgerald Marine Reserve Coastal trail improvement

Hello Roxanne

I am Anne-Ly, and a volunteer naturalist at FMR.

I am very hopeful that the coastal trail at the Reserve will be improved soon, and a new bridge added over the creek.

I have volunteered for several years, enjoy FMR immensely, and think it is one of our coastal gems. The new stairs at the south end have helped me with access to the Reserve. With the eroding cliff over the northern section, access to the Reserve needs serious improvement.

Now is the time for this to be approved and continued. Please add my voice on this matter,

sincerely, Dr. Anne-Ly Crump-Garay

Roxanna Farshchi

From: L8428@aol.com
Sent: Wednesday, April 06, 2011 4:16 PM
To: Roxanna Farshchi
Subject: Coastal Development Plan for Fitzgerald Marine Reserve Coastal Trail and Bridge

I have been a volunteer naturalist at Fitzgerald Marine Reserve for over 8 years, and would like to urge the Coastal Commission to approve San Mateo County's CDP for Fitzgerald Marine Reserve which includes improving the section of the California Coastal Trail which runs through part of Fitzgerald Marine Reserve (the Dardanelle Trail) and more importantly which will include a new bridge for access across San Vicente Creek.

The Friends of Fitzgerald Marine Reserve provides the staffing for all school groups that schedule tours through San Mateo County Dept. of Parks. Annually, approximately 5,000 students are provided with guided educational tours of the marine reserve by our trained volunteer naturalists. Volunteers are also available on weekends to provide visitors with information about the many species of animals found living in the tidepools. Because of the harbor seal population (approximately 230), many days the beach is blocked off in order to protect the harbor seals who have hauled out and are resting on the beach. On those days, visitors to FMR are instructed to cross San Vicente Creek using the bridge which is located opposite California Avenue at Lake Street, the approximate location of the proposed new bridge. From there, visitors hike up above the reserve on the bluff top to stairs leading down to Seal Cove beach on the southern end of the intertidal. Currently, the existing bridge is compromised due to excessive erosion as a result of the heavy rains received this winter. If there is no bridge across San Vicente Creek, access to the intertidal via Seal Cove at the southern end of the main intertidal area of FMR is impossible by foot. This inability to access the intertidal will prevent us from providing tours and prevent anyone wishing to visit Fitzgerald Marine Reserve the opportunity to "experience the wonder of FMR."

I understand this matter will be before the Commission at the May meeting for a decision. Please approve the Coastal Development Plan for Fitzgerald Marine Reserve Coastal Trail and Bridge.

Thank you.

Linda Ciotti
FFMR Volunteer Coordinator
(650) 728-0129

Roxanna Farshchi

From: Jill Morris [jilljmorris@gmail.com]
Sent: Wednesday, April 06, 2011 4:28 PM
To: Roxanna Farshchi
Subject: Fitzgerald Marine Reserve bridge

Dear Ms. Farshchi,

It was recently brought to my attention that the bridge that crossed the San Vicente Creek at FMR will be removed. I understand that there are no immediate plans to replace it.

You may be aware that the bridge is crucial to the park. Because of recent weathering of the beach near the entrance to the reef and heavy activity of the creek, visitors can not reach the beach and park. The only way to reach the beach and view the whales and harbor seals is to cross the bridge and hike up the ridge to the seal cove stairs. We don't want visitors taking risky hikes down the unstable cliffs or over the large rocks in order to enjoy the park.

If the bridge is not replaced locals, tourists and visiting school kids will lose their ability to interact with our local wildlife. It would be a shame to lose access to one of our more precious natural habitats.

I hope that the coastal commission will make immediate plans to work with the park service and local residents to install a new bridge. I urge you to please **approve the County's Coastal Development Plan for Fitzgerald Marine Reserve Coastal Trail and Bridge as quickly as possible.**

Thank you,
Jill Morris, resident of Half Moon Bay

Roxanna Farshchi

From: Betty Cosgrove [cosabl@comcast.net]
Sent: Wednesday, April 06, 2011 5:54 PM
To: Roxanna Farshchi
Subject: access to Fitzgerald Marine Reserve.

Dear Ms. Farshchi,

I am one of the eight-two docent naturalists at Fitzgerald Marine Reserve where I have been guiding School groups of children for nine years.

As you know the creek is at full flood and cannot be crossed without a bridge either from the current bridge location or from the frontal aspect of the reef where it is impossible to cross at present.

Also the access from that area has been stopped due to the fear of landslides.

If the bridge is removed from San Vincente Creek there will be no access to Fitzgerald Marine Reserve.

We see and teach thousands of children yearly. This would be a total catastrophe for all the schools (many whom are even out of area) for their academic requirements as well as for the teaching the care and stewardship of the Reserve.

There has to be an alternative!

We beg of you to urgently approve the County Coastal Development Plan for Fitzgerald Marine Reserve.

Yours sincerely,
Betty Cosgrove