

CALIFORNIA COASTAL COMMISSION

SOUTH CENTRAL COAST AREA
89 SOUTH CALIFORNIA ST., SUITE 200
VENTURA, CA 93001
(805) 585-1800

W13a



DATE: October 15, 2015

TO: Commissioners and Interested Parties

FROM: Charles Lester, Executive Director
Steve Hudson, Deputy Director
Jacqueline Phelps, Coastal Program Analyst

SUBJECT: City of Carpinteria LCP Amendment No. LCP-4-CPN-15-0018-1 (Transportation Improvements): Executive Director's determination that action by the City of Carpinteria, acknowledging receipt, acceptance, and agreement with the Commission's certification with suggested modifications, is legally adequate. This determination will be reported to the Commission at the November 4, 2015 meeting in Half Moon Bay.

On October 6, 2015, the Commission approved Local Coastal Program (LCP) Amendment No. LCP-4-CPN-15-0018-1 with suggested modifications. The subject amendment consists of changes to the Land Use Plan (LUP) and Implementation Plan (IP) portions of the certified LCP to allow for the development of a transportation improvement project, which includes the Linden Avenue and Casitas Road interchange, Via Real extension, South Coast HOV lanes, and the Coast Route bike path.

On October 12, 2015, the City Council adopted Resolution No. 5629 (attached) acknowledging receipt of the Commission's certification of LCP Amendment No. LCP-4-CPN-15-0018-1 and accepting and agreeing to all modifications suggested by the Commission. The document was transmitted to Commission staff on October 14, 2015.

Pursuant to Section 13544 of the California Code of Regulations, Title 14, Division 5.5, the Executive Director must determine that the action taken by the City of Carpinteria acknowledging receipt and acceptance of, and agreement with the Commission's certification of the above referenced LCP amendment with suggested modifications is legally adequate and report that determination to the Commission. The certification shall become effective unless a majority of the Commissioners present object to the Executive Director's determination.

I have reviewed the City's acknowledgement and acceptance of, and agreement with the terms and suggested modifications of LCP Amendment LCP-4-CPN-15-0018-1, as certified by the Commission on October 6, 2015, as contained in the adopted Resolution of October 12, 2015 and find that the City's action and notification procedures for appealable development are legally adequate to satisfy the terms and requirements of the Commission's certification. I therefore recommend that the Commission concur in this determination.

RESOLUTION NO. 5629

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CARPINTERIA ACCEPTING COASTAL COMMISSION SUGGESTED MODIFICATIONS TO THE LOCAL COASTAL PROGRAM AMENDMENT LCP-4-CPN-15-0018-1 THUS APPROVING LOCAL COASTAL PROGRAM AMENDMENT PROJECT 09-1522-LCPA TO CREATE THE TRANSPORTATION CORRIDOR WETLAND OVERLAY AND THE WHITNEY SITE AGRICULTURAL OVERLAY DISTRICTS IN THE CITY OF CARPINTERIA ZONING CODE AND ASSOCIATED TEXT AMENDMENTS TO THE COASTAL LAND USE PLAN AND CHANGES TO THE LAND USE AND ZONING MAPS

WHEREAS, the Carpinteria City Council has considered an application for a Local Coastal Program Amendment to change the Zoning Code, Coastal Land Use Plan and Land Use Map to allow for consideration of permits for replacement of the Linden Avenue and Casitas Pass Road US 101 overcrossings, reconfiguration of the northbound on and offramps at Casitas Pass Road, extension of the northbound Linden Avenue onramp and replacement of the southbound Linden Avenue offramp, replacement of the US 101 freeway bridges over Carpinteria Creek and extension of Via Real to Casitas Pass Road and to Linden Avenue (the Project); and

WHEREAS, the City reviewed the environmental impacts of the Project in accord with the California Environmental Quality Act (Public Resources Code §§ 21000, *et seq.*, "CEQA"), the regulations promulgated thereunder (14 Cal. Code of Regulations §§ 15000, *et seq.*, the "CEQA Guidelines"); and

WHEREAS, a Mitigated Negative Declaration (MND) was prepared for the project and was subject to a 30-day review period from August 29, 2013 to September 27, 2013; and

WHEREAS, the Environmental Review Committee reviewed the Draft MND on September 19, 2013 and recommended approval of the document to the Planning Commission with comments; and

WHEREAS, the Planning Commission conducted a special hearing on May 18, 2015 and received evidence in favor of the Local Coastal Program Amendments and recommended approval of the project to the City Council; and

WHEREAS, in accordance with CEQA, the City Council adopted the Final Negative Declaration on June 22, 2015; and

WHEREAS, the City Council has reviewed the policies of the General Plan/Coastal Plan and the Zoning Code standards that are relevant to the project; and

WHEREAS, the City conducted hearings on June 22, 2015 and July 27, 2015, received evidence in favor of the Local Coastal Program Amendments and recommended approval of the LCPA to the California Coastal Commission; and

WHEREAS, the California Coastal Commission held a hearing on October 6, 2015 and voted unanimously to approve the Local Coastal Program Amendments with suggested modifications as detailed in a resolution of certification;

NOW THEREFORE, THE CITY COUNCIL HEREBY RESOLVES AS FOLLOWS:

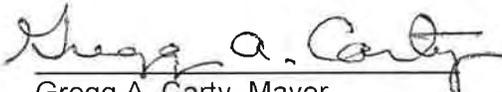
1. The City Council acknowledges receipt of the Coastal Commission's resolution of certification including modifications suggested for final certification for Local Coastal Program Amendment LCP-4-CPN-15-0018-1, accepts and agrees to such modifications, and agrees to issue coastal development permits for the total area included in the certified Local Coastal Program.
2. The City Council does hereby also resolve that this Resolution No. 5629 shall become effective upon the second reading, as read by title only, of Ordinance No. 703.

PASSED, APPROVED AND ADOPTED this 12th day of October 2015, by the following called vote:

AYES: COUNCILMEMBER(S): CLARK, STEIN, SHAW, CARTY.

NOES: COUNCILMEMBER(S): NONE.

ABSENT: COUNCILMEMBER(S): NOMURA.


Gregg A. Carty, Mayor

ATTEST:


Fidela Garcia, City Clerk

I hereby certify that the foregoing Resolution was duly and regularly introduced and adopted at a regular meeting of the City Council of the City of Carpinteria held the 12th day of October 2015.

APPROVED AS TO FORM:

 for _____
Peter N. Brown, on behalf of Brownstein Hyatt Farber Schreck, LLP
acting as City Attorney of the City of Carpinteria

ATTACHMENT 1

**City Council
Resolution No. 5629**

Exhibit A - Findings

October 12, 2015

PROJECT 09-1522-LCPA

Local Coastal Program Amendments Addressing a Transportation Corridor Wetland Overlay District and a Whitney Site Agricultural Overlay District in the Coastal Land Use Plan and the Zoning Code, Redesignating Land from Agriculture to Transportation Corridor and Removing Agriculture Zoning on the Whitney Site

October 12, 2015

FINDINGS PURSUANT TO GOVERNMENT CODE, COASTAL PLAN AND GENERAL PLAN

1.0 Administrative Findings

The City Council hereby incorporates by reference as though set forth in full all Community Development Department staff reports and attachments thereto presented to the City Council and all comments made or received either orally or in writing at the public hearings on this project.

1.1 Procedures

Pursuant to the California Coastal Act, the Administrative Regulations of the California Coastal Commission and the City's Local Coastal Program, it has been found that the process for public review of the subject Local Coastal Program Amendment has been properly conducted as follows:

- a. The City has proposed a Local Coastal Program Amendment, Project 09-1522-LCPA, to create the Transportation Corridor Wetland Overlay and the Whitney Site Agriculture Overlay Districts in the Carpinteria Zoning Code and add text amendments to the Coastal Land Use Plan, modify the Land Use Map for the Whitney Site (APN 001-070-012) and remove the agriculture zoning from the southern portion of the Whitney Site. With the suggested modifications of the LCP Amendments as unanimously approved by the California Coastal Commission on October 6, 2015, the amendments are consistent with the City's Local Coastal Program Land Use Plan, the Interpretive Guidelines of the Coastal Commission and the California Coastal Act.
- b. The project has been reviewed by the City Council at a duly noticed public hearing which included a display ad published in the local newspaper, the Coastal View News.

1.2 California Environmental Quality Act

California Environmental Quality Act (CEQA) findings pursuant to CEQA (California Public Resources Code Sections 21081, 21081.6) and State CEQA Guidelines (California Code of Regulations Sections 15091, 15096, 15097, 15162, 15164):

The City Council certified the Final Mitigated Negative Declaration (MND) dated June 2015 on June 22, 2015. The MND was prepared in accordance with the provisions of the State CEQA Guidelines. The Final MND constitutes adequate CEQA environmental documentation for the project (09-1522-LCPA), and pursuant to CEQA Guidelines section 15162, no additional environmental review is necessary because modifications to the project suggested by the California Coastal Commission and adopted by the City Council are minor and do not raise any of the conditions set forth in CEQA Guidelines section 15162 requiring additional environmental analysis.

The location and custodian of CEQA environmental documentation and file documents which constitute the record of proceedings upon which the project decision is based is the City of Carpinteria Community Development Department, 5775 Carpinteria Avenue, Carpinteria, California.

Mitigation measures applied as conditions of project approval would result in the avoidance or reduction of environmental impacts to insignificant levels. The City Council's approval of the project is conditioned upon the inclusion of these measures. These findings are supported by substantial evidence in the record, including the Certified Final MND dated June 2015 and associated appendix materials. These potential impacts and the relevant mitigation measures are discussed below, along with the appropriate findings under State CEQA Guidelines §15091.

- a. Aesthetics: The MND identified potential visual impacts associated with view blockage for US 101 travelers resulting from the safety mesh for the Coast Route Bike Path, as well as visual effects regarding barrier design compatibility and landscaping for this proposal. The mitigation framework recommended for inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path includes incorporating a barrier design compatible with the US 101 median for the South Coast HOV Lanes, employing a maximum transparency safety screen element, requirement for formal landscape plan preparation, and mandatory review of the design by the City of Carpinteria Architectural Review Board. All visual and aesthetic impacts would be reduced to insignificance with inclusion of these or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.
- b. Air Quality: The MND identified potential short-term air quality impacts associated with construction of the Coast Bike Route, including fugitive dust and construction equipment exhaust. The mitigation framework recommended for inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path includes incorporating standard construction restrictions and conditions from the Santa Barbara County Air Pollution Control District pertaining to PM₁₀ control, grading best management practices for dust minimization, and equipment exhaust minimization. All air quality impacts would be reduced to insignificance with inclusion of these or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.
- c. Biological Resources: The MND identified potential impacts to biological resources associated with the Coast Route Bike Path construction, including disruption of nesting birds, removal of rare plants, and encroachment into wetland. The mitigation framework recommended for inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path includes focused rare plant surveys and avoidance, fencing and protection from run-off of ESHA adjacent to the project alignment, pre-construction nesting bird surveys, restoration plans for encroachment into wetland and buffer, and a detailed construction monitoring plan. All biological resource impacts would be reduced to insignificance with inclusion of these or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.
- d. Cultural Resources: The MND recommended a standard mitigation addressing the potential for discovery of cultural resources during project construction. The potential for cultural resources to exist has been identified as extremely low, this recommended mitigation is a precautionary measure which is applied on a routine basis for construction projects in the region.
- e. Geology/Soils: The MND identified potential short-term erosion impacts associated with construction of the Coast Route Bike Path. The mitigation framework recommended for

inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path requires avoidance of materials stockpiling to the maximum extent feasible, and avoidance of construction during the rainy season. Short-term geology and soils impacts related to project construction would be reduced to insignificance with inclusion of these or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.

- f. Hydrology: The MND identified potential flooding impacts associated with the Coast Route Bike Path. The mitigation framework recommended for inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path includes design and construction of the bike path above the 100-year flood elevation identified for the path alignment. Flooding impacts would be reduced to insignificance with inclusion of this or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.
- g. Noise: The MND identified potential short-term noise impacts associated with construction of the Coast Route Bike Path. The mitigation framework recommended for inclusion in the future CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path establishes construction schedule restrictions for avoidance of evening and nighttime nuisance noise, requires proper equipment mufflers, directs placement of staging areas away from vicinity residential properties, requires noise barriers around construction activities in close proximity to residential property, and dictates advance notification to neighbors of impending construction activities. Short-term noise impacts related to project construction would be reduced to insignificance with inclusion of these or similar mitigation measures in the CEQA document for the Santa Claus Lane portion of the Coast Route Bike Path.

This Page Left Blank Intentionally

ATTACHMENT 1 - EXHIBIT B
Whitney Site Agriculture Overlay District

This Page Left Blank Intentionally

Whitney Site Agriculture Overlay – Zoning Code

October 2015

Chapter 14.41 – WHITNEY SITE (APN 001-070-012) OVERLAY DISTRICT

14.41.010 - Purpose and intent.

The purpose of the Whitney Site overlay district is to provide for specific standards to ensure preservation of the remainder of the Whitney Site for agricultural use through an agricultural easement and to provide for the establishment and maintenance of programs for sustainable agriculture in the Carpinteria Valley. Therefore, this overlay district establishes specific standards related to open field agriculture resource protection and mitigation.

14.41.020 - Applicability.

The provisions of this district shall apply only to the remainder portion of the original 8.95-acre Whitney Site (APN 001-070-012) not dedicated to public roadway purposes, as depicted in Figure 1 and shown on the city's official zoning maps.

14.41.030 - Development standards.

All applicable Local Coastal Program policies and provisions shall apply to the Whitney Site Overlay District unless specifically modified by standards detailed in this section. Development of any portion of the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension on the Whitney Site shall comply with all of the following requirements:

1. New development shall be sited and designed to avoid impacts to agricultural land. If there is no feasible alternative that can eliminate all impacts, then the alternative development location and design that would result in the fewest or least significant impacts shall be selected. Impacts to agricultural land that cannot be avoided through the implementation of siting and design alternatives shall be fully mitigated as outlined in this Section. The portion of the parcel to be converted from agriculture to public roadway facility shall not exceed five acres, encompassing the southern portion of the parcel adjacent to US 101.
2. In order to prevent conversion of the approximately four-acre remainder parcel out of agriculture, ownership of the property shall be transferred to the City of Carpinteria. The remainder parcel shall be restricted to agricultural uses including organic farming, community gardening or agriculture education. Transfer of the property from Caltrans to the City would occur at the conclusion of project construction, and in concert with the transfer of the completed Via Real roadway and right-of-way. An agricultural conservation easement shall be placed on the

property prior to transfer of ownership of the remainder parcel to the City which will prohibit non-agricultural uses on the property in perpetuity. The holder of the agricultural conservation easement shall be a public resources agency or private land trust with agricultural resource preservation experience, acceptable to the City of Carpinteria. Portions of the parcel that lie within the Carpinteria Creek channel, associated environmentally sensitive habitat area (ESHA) or riparian corridor buffer shall be excluded from the agricultural conservation easement to protect environmentally sensitive resources from disturbance associated with agricultural activities.

3. Mitigation shall be provided for the direct loss of agricultural land via implementation of an Agriculture Preservation Program as set forth below. The developer shall contribute funds such that the City of Carpinteria can accomplish the following (at a minimum):

- A. Installation of physical infrastructure for community garden plots at three or more properties in various locations to permit convenient public access. The following properties have been identified to fulfill the above goals; in the event that it is determined to be infeasible to operate a community garden on one or more of the properties detailed in subsection i or iii below, a substitute property(ies) may be enrolled.

- i) Carpinteria Children's Project (formerly Main School campus of the Carpinteria Unified School District). Infrastructure and development may include:

- a. Tilling to prepare planting area
 - b. Grid layout for plots
 - c. Perimeter fencing
 - d. Water source for irrigation
 - e. Small shed for tool/implement storage, not larger than 12' x 12'

- ii) Four-acre Whitney Site remainder parcel. Infrastructure and development may include:

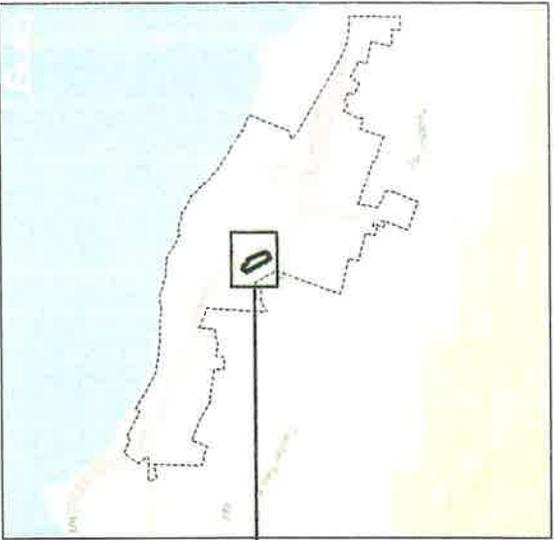
- a. Tilling to prepare planting area within a portion of the site
 - b. Grid layout for plots
 - c. Preservation of a portion of the existing avocado orchard
 - d. Split-rail or similar perimeter fencing on the east, west and south boundaries
 - f. Water source for irrigation
 - g. Small shed for tool/implement storage, not larger than 12' x 12'

- iii) Fifth Street 0.62-acre City-owned parcel between Holly Avenue and Elm Avenue. Infrastructure and development may include:

- a. Tilling to prepare planting area
 - b. Grid layout for plots

- c. Split-rail or similar perimeter fencing
 - d. Water source for irrigation
 - e. Small shed for tool/implement storage, not larger than 12' x 12'
- B. Construction of a solid, grout-filled masonry wall, or an alternative wall of same or better noise attenuating value, approximately six to eight feet in height on the northern boundary of the remainder Whitney Site (abutting APNs 001-252-001 through -010 and APN 001-253-006) shall be completed prior to opening a community garden on the property.
- C. Development of a uniform agreement or lease template for use of individual garden plots by members of the community.
- D. Drafting of an agreement(s) between the City and Carpinteria Unified School District (CUSD) for the Carpinteria Children's Project garden to be maintained and operated by the CUSD for children and families participating in onsite programs.
- E. Funding of one City of Carpinteria half-time staff position, for a period of five years from the date of final approval of the Linden Avenue – Casitas Pass Road and Via Real Extension project, to initiate the community garden system, and to create a fee structure (participation fee or plot lease) to subsidize the continuation of this staff position. In the event funding from the fee structure for community garden participation is not adequate following the five-year subsidy period to sustain a half-time position for administration of the community garden system, the City of Carpinteria will do one or more of the following.
- i. Either restructure the participation fees or commit to providing funds for the difference between participation revenues and the position costs;
 - ii. Investigate administration of the community garden system by another agricultural education entity such as 4H or Future Farmers of America;
 - iii. Pursue grant funding to perpetuate the program; or
 - iv. Assume administration of the community garden system in perpetuity.
- F. Provide funding to CUSD for development of an adult education program to be delivered via offering workshops in plant cultivation at the community gardens and cooking with fresh garden produce (at the Carpinteria High School Culinary Kitchen). Once established, participation fees could fund perpetuation of the program.
- G. Provide funding to CUSD for development of grade-specific curriculum focusing on vegetable cultivation using on-campus gardens at each school facility in Grades 1-8. The lesson plans would increase knowledge and appreciation for fresh produce, supporting interest in both community gardening and the importance of local agriculture resources.
- H. Program administration and access for residents to at least one property enrolled in the community garden system shall be established within one year from approval of a Coastal Development Permit for the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension. The Agriculture Preservation Program shall be substantially implemented within one year following completion of construction of the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension.

- I. To further offset the project-related direct loss of five acres of agricultural land within the City of Carpinteria, the City shall apply to the Local Agency Formation Commission (LAFCO) to remove two agricultural parcels which are currently included in the City's Sphere of Influence on Via Real (APN 001-080-033 and APN 001-180-026). The application to LAFCO to remove these parcels from the Sphere of Influence shall be completed within one year from approval of a Coastal Development Permit for the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension Improvements. Successful removal of the parcels from the Sphere of Influence shall be accomplished prior to completion of construction of the public roadway improvements. If and when the Local Agency Formation Commission approves the removal of APN 001-080-033 and APN 001-080-026 from the City's sphere of influence, the City shall revise the certified Land Use Plan Map to reflect the revised sphere of influence boundary.



Whitney Site Agriculture Overlay District

Proposed Right-of-Way Northern Limit

Whitney Parcel (APN: 001-070-012)



AERIAL SOURCE: CIRGIS 2012
DESIGN SOURCE: CALTRANS 2013

00068



FIGURE 1
Whitney Site Agriculture Overlay District
CITY OF CARPINTERIA LOCAL COASTAL PROGRAM AMENDMENT - MITIGATED NEGATIVE DECLARATION

This Page Left Blank Intentionally

ATTACHMENT 1 – EXHIBIT C

Text Amendments – Coastal Land Use Plan (Agriculture)

This Page Left Blank Intentionally

CARPINTERIA COASTAL LAND USE PLAN
LINDEN AVENUE AND CASITAS PASS ROAD INTERCHANGES AND VIA REAL EXTENSION
Whitney Site Agriculture Overlay District

October 2015

OSC-9m. Protect agriculture and agriculture education opportunities within the Whitney Site Agriculture Overlay District, pursuant to Chapter 14.41 of the Carpinteria Municipal Code.

This Page Left Blank Intentionally

ATTACHMENT 1 - EXHIBIT D

Transportation Corridor Wetland Overlay District

This Page Left Blank Intentionally

Transportation Corridor Wetland Overlay – Zoning Code

October 2015

Chapter 14.45 –TRANSPORTATION CORRIDOR WETLAND OVERLAY DISTRICT

14.45.010 - Purpose and intent.

The purpose of the Transportation Corridor Wetland overlay district is to provide for specific standards of development for the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension, and for the portions located within the City of Carpinteria, the South Coast High Occupancy Vehicle (HOV) Lanes and Santa Claus Lane Bike Path projects. The intent is to ensure a more precise level of planning than ordinarily possible under the local implementation plan. Therefore, this overlay establishes specific standards related to fill or other impacts to wetland or reduction of wetland buffers, mitigation measures, drainage and stormwater management, and coastal access and recreation enhancements.

14.45.020 - Applicability.

The provisions of this district shall apply only to the project area encompassed by the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension, South Coast High Occupancy Vehicle (HOV) Lanes, and Santa Claus Lane Bike Path projects, as depicted in Figure 2 and shown on the city's official zoning maps.

14.45.030 - Development standards.

All applicable LCP policies and provisions shall apply to development that is part of the Transportation Corridor Wetland Overlay (TCWO), unless specifically modified by the standards detailed in this section. Two general classes of wetlands have been identified within the TCWO: natural wetlands and created wetlands. Natural wetlands include components of Carpinteria Marsh, wetlands associated with native creeks and wetlands that are fragmented but are of high value. Created wetlands are relatively low functioning wetlands related to freeway and/or railroad infrastructure that have formed in drainage ditches, basins, water quality Best Management Practices (BMPs) features and/or topographic depressions, and were originally constructed out of dry land. The area abutting a wetland is the wetland buffer and provides a transition between development and the wetland resource. For the purpose of these development standards, wetland buffer only includes undeveloped areas between the wetland and the first line of development; thus, the road median is not a wetland buffer. Each wetland class has been assigned specific mitigation criteria to best facilitate the balance and protection of coastal resources in the TCWO.

All development for the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension, and all development for the portions of the South Coast High Occupancy Vehicle (HOV) Lanes and Santa Claus Lane Bike Path projects located within the City of Carpinteria, shall comply with the following:

1. For the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension, the following project components may result in permanent and temporary direct wetland impacts (e.g., fill) and indirect wetland impacts (e.g., development in wetland buffer):
 - Carpinteria Creek bridge foundation components;
 - Slope retaining elements for the southbound US 101, northbound US 101, northbound Casitas Pass offramp and the Via Real extension;
 - Fill and retaining structures for the Via Real extension travel lanes, sidewalks and bike path;
 - Drainage improvements;
 - Soundwalls;
 - Fill and retaining structures for the Carpinteria Creek bike trail; and
 - Structural foundation elements, travel lane, sidewalk and bikeway, shoulder, fill and retaining structures for the northern end of the Linden Avenue overcrossing.
2. For the South Coast HOV Lanes, the following project components may result in permanent and temporary direct wetland impacts (e.g., fill) and indirect wetland impacts (e.g., development in wetland buffer):
 - Drainage improvements and soundwall along the northern shoulder of northbound 101 immediately east (south) of Santa Ynez Avenue;
 - Drainage improvements, soundwall, roadway, shoulder and fill in the immediate vicinity of Santa Monica Road onramp/offramp;
 - Drainage improvements and soundwall along the northern shoulder of northbound 101 in the vicinity of Taranto Circle; and
 - Drainage improvements and soundwall along the southern shoulder of southbound 101 immediately west (north) of Plum Street to just east of the western terminus of Carpinteria Avenue.
3. For the Santa Claus Lane Bike Path, the following project components may result in permanent and temporary direct wetland impacts (e.g., fill) and indirect wetland impacts (e.g., development in wetland buffer) between the existing US 101 southern shoulder and the UPRR railroad alignment which borders the north side of the Carpinteria Salt Marsh:
 - Fill;
 - Retaining structures;
 - Bike path; and
 - Drainage improvements.

4. Fill or other impacts to wetland or reduction of wetland buffers resulting from new development detailed in items 1 - 3 above, may be approved only in conformance with the following:
 - a. New development shall be sited and designed to avoid fill or other impacts to wetland. If there is no feasible alternative that can eliminate all impacts, then the alternative that would result in the fewest or least significant impacts shall be selected. Impacts to wetland that cannot be avoided through the implementation of siting and design alternatives shall be fully mitigated, with priority given to onsite mitigation. Offsite mitigation measures shall only be approved when it is not feasible to fully mitigate impacts onsite. Mitigation shall not substitute for implementation of the project alternative that would avoid impacts to wetland.
 - b. New development shall be sited and designed to provide a minimum 100-foot setback/buffer strip in a natural condition along the upland limits of wetlands. If there is no feasible alternative that can provide a buffer of 100 feet, then the alternative that would provide the widest buffer shall be selected. Mitigation shall not substitute for implementation of the project alternative that would provide the required wetland buffer.
 - c. Mitigation shall be provided for direct impacts to wetlands (e.g., fill) and indirect impacts to wetlands (e.g., new development in wetland buffers). Mitigation measures shall include, at a minimum, creation or substantial restoration of habitats equal or similar to the impacted habitat type. Adverse direct impacts shall be mitigated at a ratio of 4:1 for natural wetland areas, including salt marsh and wetlands associated with creeks. Adverse direct impacts to riparian habitats shall be mitigated at a ratio of 3:1. Adverse direct impacts to created wetlands (low functioning wetlands associated with freeway infrastructure) shall be mitigated at a ratio of 3:1. Indirect impacts to wetlands, such as development in wetland buffer areas, shall be mitigated by enhancing (e.g., invasive species removal, native screening vegetation, planting appropriate native species, water quality improvement, sound reduction) all portions of the remaining (undeveloped) wetland buffer area within 100 feet of the impacted wetland. Temporary direct and indirect impacts to wetlands and riparian habitat shall be mitigated at a ratio of 1:1.
 - d. Wetland enhancement, restoration or creation plans shall be prepared by a qualified biologist for all areas where mitigation is required by Sections 4.a, b and c, above. Plans shall include details of appropriate wetland enhancement, restoration or creation acreage and location including the following:
 - i. Introduction. Including a purpose statement, existing site resource description and inventory, proposed wetland mitigation site plan and map comparing existing and future site conditions.

- ii. Mitigation Goals. A clear statement of the wetland mitigation goals including the desired wetland habitat type(s), major vegetation components, water quality improvements and hydrologic regime, and wildlife support functions.
 - iii. Planting Plan. Description of the desired relative abundance of particular wetland plant species in desired habitat type(s). Based on these goals, identify the species to be planted (plant "palette"), provide a rationale for and describe the size and number of container plants and/or the rate and method of seed application, and a site plan with planting location and planting guidelines for prescribed species. Seeds and propagules shall come from local native stock.
 - iv. Grading Plan. If wetland enhancement, restoration or creation requires topographic alterations, a formal grading plan shall be included.
 - v. Best Management Practices (BMPs). Erosion control, irrigation and weed eradication plans as necessary.
 - vi. Success Criteria. Selection and rationale of quantifiable success criteria. There must be an empirical basis for the selection of each success criterion (e.g., peer-reviewed literature, reference site data).
 - vii. Monitoring. Monitoring program that includes a detailed description of quantitative sampling design (e.g., sample sizes and sampling techniques such as quadrats, transects, photo plots), statistical procedures proposed for judging if success criteria are achieved, provisions for a five-year monitoring period, annual reporting and contingency measures should the mitigation efforts fail to achieve quantitative success criteria.
 - viii. Final Report. A final monitoring report prepared by a qualified biologist that evaluates whether the required wetland enhancement, restoration or creation has achieved the goals and success criteria set forth in the approved mitigation plan.
5. For each project listed in 1 through 3 above, all of the following post-construction coastal water quality standards shall be met:
- a. Early site design planning shall emphasize run-off management and shall prioritize the minimization of run-off by reducing lane and shoulder pavement widths, using permeable pavements and incorporating sub-grade stormwater retention, and shall use structural and operational Source Control BMPs to control pollutant sources, keep pollutants segregated from stormwater and minimize tainted run-off to the extent feasible. Minimization of impermeable surfaces shall be emphasized in freeway design where it is infeasible to infiltrate or treat freeway run-off that flows directly to high quality wetlands.
 - b. Low impact development strategies shall be preferentially used to treat run-off. Low impact development strategies use small scale control measures to infiltrate, evapo-transpire, filter, detain and retain run-off. These practices shall be maximized where they will pre-treat run-off that flows to wetlands. Earthen-(soil) based and/or bioengineered Caltrans-approved National Pollution Discharge Elimination System (NPDES) BMPs shall be allowed to be located and maintained within wetland buffers where there is no feasible alternative location available to locate the BMPs and where they support wetland protection.

- c. To maximize the treatment and infiltration of run-off from the greatest area of freeway surfaces, post-development BMPs shall include, in addition to Caltrans-certified NPDES BMPs, alternative water quality BMPs. Alternative BMPs shall include, but not be limited to, grading to create or modify topographic depressions to maximize the capture, detention or retention of run-off, amending onsite soils to increase infiltration and detain run-off, adding or replacing vegetation in areas that receive freeway run-off with native plants, and constructing bioswales and biostrips that either do not require mowing or only require infrequent mowing or woody vegetation removal. These alternative BMPs shall be considered for use in areas that receive freeway run-off and wherever Caltrans-certified NPDES BMPs cannot be feasibly installed, including placement within wetland buffers where they support wetland protection.
- d. Created wetlands as defined in this chapter shall be enhanced wherever feasible by increasing the depth and areal extent of the wetland, removing invasive exotic vegetation and planting with native wetland plants. Maintenance of these enhanced created wetlands shall be limited to trimming vegetation that impacts the normal operation of the freeway, clearing drain inlets and trash removal, using non-mechanical methodologies (e.g., manual labor and no heavy equipment).
- e. Stormwater run-off shall be infiltrated, retained or detained onsite, in accordance with the site hydrology and geotechnical considerations. Infiltration BMPs shall be designed, at a minimum, to handle run-off from all storms up to and including the 85th percentile, 24-hour storm event onsite, to avoid negative effects of hydromodification. If it is not practical to retain the 85th percentile, 24-hour storm event onsite, the limitations preventing this practice shall be stated and an equal volume of run-off shall be infiltrated elsewhere within the project limits with preference given to sites within the same watershed.
- f. Where treatment control BMPs (or suites of BMPs) are used, they shall be designed to infiltrate and/or treat the amount of run-off produced by all storms up to and including the 85th percentile, 24-hour storm event for volume-based BMPs, and/or no less than 0.26 inches/hour for flow-based BMPs.
- g. Minimum infiltration or treatment volumes for treatment control BMPs shall be calculated based on the impervious surface area added by the project, in addition to the existing untreated freeway surface. Where it is infeasible to treat the volume of run-off from the existing freeway surface in addition to the new freeway surface, low impact development strategies shall be used to the maximum extent feasible to infiltrate, evapo-transpire, filter, detain and retain run-off from the existing freeway surface.
- h. Where site conditions make it infeasible to infiltrate or treat the stipulated minimum volume of run-off onsite, infiltration or treatment offsite within existing right-of-way can be substituted where it can be demonstrated that offsite infiltration or treatment will result in an equal or greater benefit to coastal water quality.
- i. Treatment BMPs shall prioritize the use of soil-based biofilter techniques, such as bioswales, and shall infiltrate, retain, or detain the maximum possible volume of stormwater run-off in accordance with the site hydrology and geotechnical considerations. Biofilters shall not use invasive plant materials; plants shall be collected locally and/or propagated in a certified nursery with oversight by a

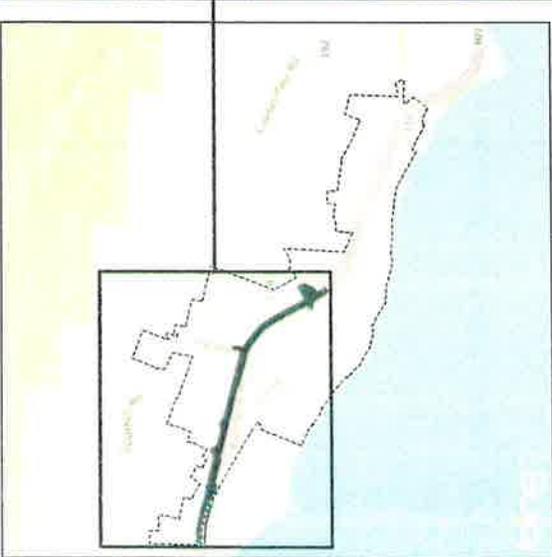
qualified plant ecologist. A biofilter/bioswale design plan, including the native wetland plant palette and the source of plant material, shall be submitted to the City for review and approval.

- j. A post-construction run-off management plan shall be submitted to the City and shall include maps, figures, supporting design calculations, and a narrative explaining the methods and approach proposed to protect or enhance coastal water quality. Maps shall be cross-referenced to grading, drainage and landscaping project plans. The run-off management plan shall include supporting information including but not limited to the infiltration and retention properties of the native or engineered BMP substrate, depth to groundwater, and the hydraulic design and pollutant treatment/removal capability of the proposed BMPs adequate to ensure that water quality will be protected to the maximum extent feasible. The post-construction run-off plan shall be certified by a qualified professional with appropriate credentials, education and training.
 - k. Where site or project conditions constrain any of the minimum requirements or practices in a. through j. above, the qualified professional shall document the nature and extent of the limitations and justify the alternative measures proposed to protect or enhance water quality.
6. To the extent allowed by the "Repair, Maintenance and Utility Hook-up Exclusions from Permit Requirements" (document adopted by the Coastal Commission on September 5, 1978 and incorporated herein by reference), repair and maintenance of the projects listed in 1 through 3 above are excluded from permit requirements. However, such exclusions shall not apply where such repair and maintenance would occur within riparian and wetland habitat or will cause direct impacts to wetland or other resources.
 7. In order to achieve a regionally important improvement to alternative transportation modes for the purpose of increasing access to coastal resources for all members of the public, two bike path/trail segments shall be constructed, including:
 - a. Coast Route Bike Path (Santa Claus Lane to Carpinteria Avenue) - a Class I bike path from the eastern terminus of Santa Claus Lane to the western terminus of Carpinteria Avenue. Construction of this path will close a gap in the coastal bike trail through Santa Barbara County. The Coast Route Bike Path shall be completed and open to the public no later than the completion of the adjacent phase of construction for the South Coast HOV Lanes.
 - b. Rincon Coastal Trail (SR 150/Carpinteria Avenue to Rincon County Park) - a Class I bike path/trail from the eastern terminus of Carpinteria Avenue (at State Route 150) to Rincon County Park. Construction of this path will close a gap between Carpinteria Avenue and the new Class I bike path along US 101 which has a northern terminus at Bates Road. The Rincon Coastal Trail shall be completed and open to the public no later than the completion of the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension construction.
 8. To enhance educational and recreational opportunities, and to increase awareness of sensitive resources within the coastal environment, the following permanent displays are required:
 - a. In conjunction with the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension, an educational or interpretive

display shall be installed within Carpinteria Creek Park and/or at the entrance to the proposed Carpinteria Creek / US 101 bike path undercrossing which explains the importance of Carpinteria Creek, its riparian habitat and associated wetland resources.

- b. In conjunction with the Santa Claus Lane Bike Path, an educational or interpretive display shall be installed which explains the importance of the Carpinteria Salt Marsh.
- c. In conjunction with the Rincon Coastal Trail, an educational or interpretive display shall be installed which describes the nearby harbor seal rookery.

This Page Left Blank Intentionally



-  Transportation Corridor Wetland Overlay District
-  City of Carpinteria



AERIAL SOURCE: CIRGIS 2012
 DESIGN SOURCE: CALTRANS 2013

U+V-D-E-K

FIGURE 2
Transportation Corridor Wetland Overlay District
 CITY OF CARPINTERIA LOCAL COASTAL PROGRAM AMENDMENT - MITIGATED NEGATIVE DECLARATION

This Page Left Blank Intentionally

ATTACHMENT 1 - EXHIBIT E

Text Amendments – Coastal Land Use Plan (Wetland Protection Policy)

This Page Left Blank Intentionally

Wetland Buffer Encroachment Text Amendments – Zoning Code

October 2015

14.42.040 - Development standards.

Definitions of terms used in this chapter are contained in Section 3.9 of the Coastal Land Use Plan and are incorporated herein by reference.

Wetland 1. Wetland shall be defined as land where the water table is at, near or above the land surface long enough to promote the formation of hydric soils or to support the growth of hydrophytes. Where environmental factors may prevent the development of hydric soils and/or hydrophytes, the presence of surface water or saturated substrate at some time during the year or the location within, or adjacent to, vegetated wetland or deepwater habitats will define their limits. If a question exists, the limit will be determined by habitat survey made by a City-approved biologist.

The upland limit of a wetland shall be defined as: a) the boundary between land with predominantly hydrophytic cover and land with predominantly mesophytic or xerophytic cover; b) the boundary between soil that is predominantly hydric and soil that is predominantly non-hydric; c) in the case of wetlands without hydrophytic vegetation or hydric soils, the boundary between land that is flooded or saturated at some time during years of normal precipitation and land that is not.

A buffer strip, one hundred (100) feet in width, shall be maintained in a natural condition along the upland limits of all wetlands. No structures other than those required to support light recreational, scientific and educational use shall be permitted, where such structures are consistent with all other wetland protection policies and where all possible measures have been taken to prevent adverse impacts.

2. New development adjacent to the required buffer around a wetland shall not result in adverse impacts due to additional sediment, run-off, noise and other disturbances.

3. For the following projects: 1) Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension; 2) South Coast High Occupancy Vehicle (HOV) Lanes; and 3) Santa Claus Lane Bike Path, a reduced wetland buffer may be permitted in accordance with the provisions of Policy OSC-3e and the specific standards described in the Transportation Corridor Wetland Overlay District, in Chapter 14.45 of the Zoning Code.

This Page Left Blank Intentionally

ATTACHMENT 1 - EXHIBIT F

Text Amendments – Coastal Land Use Plan (Wetland Protection Policy)

This Page Left Blank Intentionally

CARPINTERIA COASTAL LAND USE PLAN
LINDEN AVENUE AND CASITAS PASS ROAD INTERCHANGES AND VIA REAL EXTENSION
IMPROVEMENTS, SOUTH COAST HOV LANES AND
SANTA CLAUS LANE BIKE PATH

Text Amendments – Wetland Protection Policy

October 2015

OSC-3e. For the following projects: 1) Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension; 2) South Coast High Occupancy Vehicle (HOV) Lanes; and 3) Santa Claus Lane Bike Path, new development in wetland or within the required 100-foot wetland buffer may be permitted in accordance with all of the following requirements and all other development policies and provisions of the LCP.

- a. New development shall be sited and designed to avoid fill or other impacts to wetland. If there is no feasible alternative that can eliminate all impacts, then the alternative location and design for each of the four projects that would result in the fewest or least significant impacts shall be selected. Impacts to wetland that cannot be avoided through the implementation of siting and design alternatives shall be fully mitigated, with priority given to onsite mitigation. Offsite mitigation measures may be approved when it is not feasible to fully mitigate impacts onsite, or where greater habitat value can be achieved or wider wetland buffer can be provided in an offsite wetland restoration or enhancement project. Mitigation shall not substitute for implementation of the project alternative that would avoid impacts to wetland.
- b. New development shall be sited and designed to provide a minimum 100-foot setback/buffer strip in a natural condition along the upland limits of wetland. If there is no feasible alternative that can provide a buffer of 100 feet, then the alternative that would provide the widest buffer shall be selected. Impacts to wetland resulting from a reduced buffer width shall be fully mitigated. Mitigation shall not substitute for implementation of the project alternative that would provide the required wetland buffer.
- c. All new development shall protect post-construction coastal water quality by including the following:
 1. emphasize run-off management in site design;
 2. preferentially use Low Impact Development strategies;
 3. maximize the use of structural and operational Source Control BMPs;
 4. prioritize the use of biologic, soil-based Treatment Control BMPs;
 5. avoid adverse run-off impacts to ESHA and wetland; and
 6. design and manage development to minimize hydromodification, as appropriate.

Implementation Policies

12. Maintain a minimum 100-foot setback/buffer strip in a natural condition along the upland limits of all wetland. No structures other than those required to support light recreational, scientific and educational uses shall be permitted within the setback, where such structures are consistent with all other wetland development policies and where all feasible measures have been taken to prevent adverse impacts. The minimum setback may be adjusted upward to account for site-specific conditions affecting avoidance of adverse impacts.

A minimum wetland buffer of 35 feet from structures, and 25 feet from fencing is allowed for the existing retention basin/drainage pond on the Ellinwood site (APN 004-013-026). The retention basin/drainage pond on this site shall be maintained only in accordance with the specific standards described in the Ellinwood Parcel (APN 004-013- 026) Overlay District, in Chapter 14.43 of the Zoning Code.

For the following projects: 1) Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension; 2) South Coast High Occupancy Vehicle (HOV) Lanes; and 3) Santa Claus Lane Bike Path, a reduced wetland buffer may be permitted in accordance with the provisions of Policy OSC-3e and the specific standards described in the Transportation Corridor Wetland Overlay District, in Chapter 14.41 of the Zoning Code.

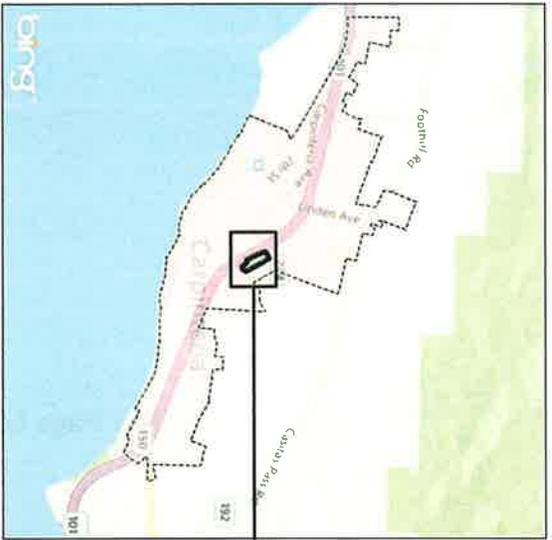
OSC-15e. In order to achieve a regionally important improvement to alternative transportation modes for the purpose of increasing access to coastal resources for all members of the public, the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension and South Coast High Occupancy Vehicle (HOV) Lanes projects shall include the construction of the following bike path / trail segments:

- a. Coast Route Bike Path (Santa Claus Lane to Carpinteria Avenue) - a Class I Bike Path from the eastern terminus of Santa Claus Lane to the western terminus of Carpinteria Avenue. Development of this path will close a gap in the coastal bike trail through Santa Barbara County. The Coast Route Bike Path shall be completed and open to the public no later than the completion of the adjacent phase of construction for the South Coast HOV Lanes.
- b. Rincon Trail (From SR 150/Carpinteria Avenue to Rincon County Park) - a Class I Bike Path/trail from the eastern terminus of Carpinteria Avenue (at State Route 150) to Rincon County Park. Construction of this path will close a coastal trail gap between Carpinteria Avenue and the new Class I trail along US 101 which has a northern terminus at Bates Road. The Rincon Trail shall be completed and open to the public no later than the completion of the Linden Avenue and Casitas Pass Road Interchanges and Via Real Extension construction.

ATTACHMENT 1 - EXHIBIT G

Whitney Parcel Transportation Corridor Land Use Designation

This Page Left Blank Intentionally



 Redesignate Land Use to Transportation Corridor "TC" and Remove Zoning

 Proposed Right-of-Way Northern Limit

 Whitney Parcel (APN: 001-070-012)



AERIAL SOURCE: CIRGIS 2012
 DESIGN SOURCE: CAUTRAMS 2013
 DUDEK



Whitney Parcel Transportation Portion Zoning and Land Use Designations
 CITY OF CARPINTERIA LOCAL COASTAL PROGRAM AMENDMENT

This Page Left Blank Intentionally

ATTACHMENT 1 - EXHIBIT H

Conditions of Approval

This Page Left Blank Intentionally

EXHIBIT H: CONDITIONS OF APPROVAL

CITY COUNCIL PROJECT 09-1522-LCPA

LOCAL COASTAL PROGRAM AMENDMENT, PROJECT 09-1522-LCPA, TO CREATE THE TRANSPORTATION CORRIDOR WETLAND OVERLAY AND THE WHITNEY SITE AGRICULTURE OVERLAY DISTRICTS IN THE CITY OF CARPINTERIA ZONING CODE AND ASSOCIATED TEXT AMENDMENTS TO THE COASTAL LAND USE PLAN AND LAND USE MAP AND REDESIGNATE LAND FROM AGRICULTURE TO TRANSPORTATION CORRIDOR AND REMOVE AGRICULTURE ZONING FROM THE WHITNEY SITE

October 12, 2015

The Conditions set forth in this permit affect the title and possession of the real property which is the subject of this permit and shall run with the real property or any portion thereof. All the terms, covenants, conditions and restrictions herein imposed shall be binding upon and inure to the benefit of the owner, his or her heirs, administrators, executors, successors and assigns. Upon any sale, division or lease of real property, all the conditions of this permit shall apply separately to each portion of the real property and the owner and/or possessor of any such portion shall succeed to and be bound by the obligations imposed on the owner by this permit.

- 1. Project Description.** The LCP Amendments (09-1522-LCPA) approved by the City Council on October 12, 2015 are limited to the written project description, staff report exhibits and conditions of approval on file at the City of Carpinteria, subject to further City review and approval as detailed below.

As a condition of project approval, the following project elements and variations shall be incorporated into proposed plans and the project description and detailed more specifically subject to approval of the Community Development Director in consultation with the Public Works Director as follows:

- 2. Mitigation Measures:** Caltrans / SBCAG shall consider incorporation of the following mitigation measures into the environmental review document(s) for the Santa Claus Lane Bike Path to address potentially significant impacts as identified in the LCPA MND dated June 2015.

- Visual and Aesthetics

AES-1 Barrier Design. The traffic barrier between the US 101 southbound shoulder and the Santa Claus Lane Bike Path shall incorporate color and texture compatible with the center median traffic barrier used for the adjacent section of the South Coast HOV Lanes and similar to the barrier used for the Class I Rincon Bikeway between Bates Road and Mussel Shoals.

AES-2 Safety Screen. The safety screen to be placed atop the traffic barrier to protect path users from blown or thrown objects shall provide the greatest transparency possible for US 101 travelers, consistent with the technical

requirements for the safety screen function and similar to the safety screen used for the Class I Rincon Bikeway between Bates Road and Mussel Shoals.

AES-3 Landscape Plan. A landscape plan shall be prepared to address the replacement of plants removed during construction of the Santa Claus Lane Bike Path, where such plants occur outside the limits of wetland.

AES-4 Design Review. The design of the Santa Claus Lane Bike Path shall be reviewed by the City of Carpinteria Architectural Review Board.

b. Air Quality

AQ-1 PM₁₀ Mitigation Measures. Dust generated by construction activities shall be kept to a minimum with a goal of retaining dust onsite. The following dust control measures shall be implemented:

- a. During clearing, grading, earth moving, excavation, or transportation of cut or fill materials, water trucks or sprinkler systems are to be used to prevent dust from leaving the site and to create a crust after each day's activities cease.
- b. During construction, water trucks or sprinkler systems shall be used to keep all areas of vehicle movement damp enough to prevent dust from leaving the site. At a minimum, this would include wetting down such areas in the later morning and after work is completed for the day and whenever winds exceed 15 miles per hour. If wind speeds increase to the point when such measures cannot prevent dust from leaving the site, construction activities shall be suspended.
- c. Grading operations shall be suspended when wind speeds exceed 20 mph.
- d. Gravel pads shall be installed at all access points to the project site to prevent tracking of mud onto City roadways.
- e. Soil stockpiled for more than two days shall be covered, kept moist, or treated with soil binders to prevent dust generation.

The contractor or builder shall designate a person or persons to monitor the dust control program and to order increased watering as necessary to prevent transport of dust offsite. Their duties shall include holiday and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to City staff and the APCD and shall be posted in three locations along the proposed project site's perimeter for the duration of grading and construction activities.

AQ-2 Grading and Dust Generation. If the construction site is graded and left undeveloped for over three weeks, the applicant shall employ the following methods immediately to inhibit dust generation:

- Seeding and watering to revegetate graded areas; and/or
- Use of a water truck to moisten exposed dirt areas during grading activity.
- Any other methods deemed appropriate by Community Development.

AQ-3 Equipment Exhaust. During project grading and hauling, construction contracts must specify that construction contractors shall adhere to requirements that reduce emissions of ozone precursors and particulate emissions from diesel exhaust. The following shall apply:

- a. All portable diesel-powered construction equipment shall be registered with the state's portable equipment registration program OR shall obtain an APCD permit.
- b. Diesel powered equipment should be replaced by electric equipment whenever feasible.
- c. Diesel construction equipment meeting the CARB Tier 1 emission standards for off-road heavy-duty diesel engines shall be used. Equipment meeting CARB Tier 2 or higher emission standards should be used to the maximum extent feasible.
- d. Other diesel construction equipment, which does not meet CARB standards, shall be equipped with two to four degree engine timing retard or pre-combustion chamber engines. Diesel catalytic converters, diesel oxidation catalysts and diesel particulate filters as certified and/or verified by EPA or California shall be installed.
- e. Catalytic converters shall be installed on gasoline-powered equipment, if feasible.
- f. All construction equipment shall be maintained in tune per the manufacturer's specifications.
- g. The engine size of construction equipment shall be the minimum practical size.
- h. The number of construction equipment operating simultaneously shall be minimized through efficient management practices to ensure that the smallest practical number is operating at any one time.
- i. Construction worker trips should be minimized by requiring carpooling and by providing for lunch onsite.

AQ-4 Diesel Fuel Vehicles. Diesel fuel emissions shall be limited. The following limitations on diesel-fueled vehicles in excess of 10,000 pounds shall apply during all construction activities:

- a. Diesel-fueled vehicles in excess of 10,000 pounds shall not idle in one location for more than five minutes at a time.
- b. Diesel-fueled vehicles in excess of 10,000 pounds shall not use diesel-fueled auxiliary power units for more than five minutes to power heater, air conditioner or other ancillary equipment on any such vehicle.
- c. The applicant shall designate one or more locations as deemed appropriate for the posting of a notice(s) to all drivers of diesel-fueled vehicles in excess of 10,000 pounds of these limitations on vehicle idling in all areas of the property that may be frequented by such vehicles. Such signs shall be maintained in their approved location(s) throughout the construction period.

c. Biological Resources

BIO-1 Fencing and Protection from Run-off. Prior to the initiation of site grading, a qualified biologist shall implement the following measures to protect special status plant species that occur outside of the areas proposed for development:

1. Install orange construction fencing around all construction areas to prevent construction equipment and personnel from entering surrounding areas. Fencing may be installed in a phased manner if construction is also phased.
2. Construction activities shall be confined to the designated construction areas.
3. Fencing shall be monitored on a regular basis throughout the construction period to ensure that the integrity of the fence is maintained.
4. To prevent run-off and erosion from construction areas adjacent to special status vegetation communities or plant species populations from impacting these species, best management practices (BMPs) such as silt fences, straw bales, etc., shall be placed to intercept run-off, or ditches or other diversion structures shall be constructed.

BIO-2 Focused Rare Plant Survey and Avoidance. Focused surveys for special status plant species shall be conducted by a qualified botanist at the appropriate season (blooming period) prior to the commencement of grading/construction activities. The surveys shall be conducted no more than one year prior to commencement of construction activities within suitable habitat, and the surveys shall be conducted at a time of year when the plants can be located and identified. Should special status plant species be documented within the project boundary, avoidance measures shall be implemented to minimize impacts to individual plants wherever feasible. These measures shall include minor adjustments to the boundaries/location of haul routes and other trail features. If, due to design constraints, avoidance of all plants is not possible, then further measures described in BIO-3 shall be implemented to salvage seeds and/or transplant individual plants. All seed collection and/or transplantation methods, as well as the location of the receptor site for seeds/plants (assumed to be within preserved open space areas within the County Park), shall be coordinated with CDFG prior to impacting known occurrences of the special status species.

BIO-3 Mitigation and Monitoring Plan. If as a result of implementing mitigation measure BIO-2 above, it is determined that the proposed project will directly impact a special status plant species, a Mitigation and Monitoring Plan for Special Status Plants (Plan) shall be prepared prior to the initiation of construction.

The Plan shall provide for replacement of individual plants to be removed at a minimum 1:1 ratio, within suitable habitat at a site where no future construction-related disturbance will occur. The plan shall specify the

following: (1) the location of the mitigation site in protected/preserved areas; (2) methods for harvesting seeds or salvaging and transplantation of individual plants to be impacted; (3) measures for propagating plants (from seed or cuttings) or transferring living specimens from the salvage site to the introduction site; (4) site preparation procedures for the mitigation site; (5) a schedule and action plan to maintain and monitor the mitigation area; (6) the list of criteria and performance standards by which to measure the success of the mitigation site (below); (7) measures to exclude unauthorized entry into the mitigation areas; and (8) contingency measures such as erosion control, replanting or weeding to implement in the event that mitigation efforts are not successful.

BIO-4 Pre-construction Nesting Bird Surveys. Within 30 days of any vegetation clearing or ground disturbance associated with construction or grading that would occur during the nesting/breeding season of native bird species potentially nesting on the site (typically mid-February through August in the project region, or as determined by a qualified biologist), the City shall have weekly surveys conducted by a qualified biologist to determine if active nests of special status bird species or of any bird species protected by the Migratory Bird Treaty Act or the California Fish and Game Code are present in the disturbance zone or within 100 feet (300 feet for raptors) of the area to be disturbed. The surveys shall occur on a weekly basis, with the last survey being conducted no more than seven days prior to initiation of disturbance work. If ground disturbance is delayed, then additional pre-disturbance surveys shall be conducted such that no more than seven days will have elapsed between the survey and ground disturbance activities. The City or contractor shall provide the biologist with plans detailing the extent of proposed ground disturbance prior to the survey effort.

If active nests are found, clearing and construction within 100 feet of the nest (300 feet for raptors) shall be postponed or halted, at the discretion of the biologist, until the nest is vacated and juveniles have fledged, as determined by the biologist, and there is no evidence of a second attempt at nesting. Limits of construction to avoid an active nest shall be established in the field with highly visible construction fencing, and construction personnel shall be instructed on the sensitivity of nest areas. The results of the surveys, including graphics showing the locations of any nests detected, and any avoidance measures recommended, shall be submitted to the City within 14 days of completion of the pre-construction surveys to document compliance with applicable state and federal laws pertaining to the protection of native birds.

BIO-5 County of Santa Barbara LCP Amendments. In order to establish precise development standards, biological restoration performance standards and effective mitigation ratios to address the Santa Claus Lane Bike Path encroachment into wetland and wetland buffer areas within Santa Barbara County, the City of Carpinteria shall work with the County of Santa Barbara on a wetland protection overlay district that provides a functional equivalent to the Carpinteria Transportation Corridor Wetland Overlay District.

BIO-6 Wetland Restoration Plan. Prior to issuance of a grading permit for the portion of the bike path within each jurisdiction, wetland restoration plans in accordance with the specifications provided under the Transportation Corridor Wetland Overlay District (or County equivalent) shall be prepared for review and approval by the governing jurisdiction.

D. Cultural Resources

- In the unlikely event that potentially significant cultural materials are encountered during construction, grading should be temporarily redirected and/or suspended until a qualified archaeologist and local Chumash representative are retained to evaluate the find, including mapping and collecting any diagnostic (time-sensitive) artifacts, consistent with City of Carpinteria standards.

E. Geology and Soils

GEO-1 Materials Stockpiling. The stockpiling of earth materials related to the construction of the Santa Claus Lane Bike Path shall be avoided to the maximum extent feasible. Any necessary stockpiles should be designed and maintained such that the material is stored for the shortest time practical.

GEO-2 Construction to Avoid Rainy Season. To the extent feasible, the Santa Claus Lane Bike Path construction shall not occur during the rainy season (November 1 to April 1) in order to avoid short-term erosion of earth materials. If a portion(s) of the construction must occur during the rainy season, erosion control devices and best management practices shall be established and maintained for the duration of the construction which occurs during the rainy season.

F. Hydrology

HYDRO-1 Flood Control. The Santa Claus Lane Bike Path shall be designed and constructed to minimize damage or failure of the path during flooding events. If technically feasible and deemed appropriate, the elevation of the bike path surface should be constructed above the 100-year flood elevation adjacent to the bike path alignment.

3. ***Mitigation Compliance, Monitoring and Reporting Requirement.*** The applicant for any future transportation improvement project that relies on this LCPA shall submit to the Community Development Director a draft monitoring program for mitigation measures identified in the environmental review document(s) prepared for the transportation improvement project. A Project Environmental Coordinator (PEC) and mitigation monitors responsible for permit compliance monitoring shall be retained and paid for by the applicant. The mitigation monitoring program shall include, but not be limited to:
- a. **Mitigation Measures.** A list of the project's mitigation measures.
 - b. **Implementation of Mitigation.** Timing and party responsible for implementation of each mitigation measure.
 - c. **Mitigation Monitoring.** Timing and party responsible for monitoring of each mitigation measure and a list of mitigation monitors to be retained.

- d. Mitigation Reporting. Procedures, timing and responsible party for reporting to the City on project mitigation compliance and monitoring.

Written authorization to proceed and consent to conditions of approval by the applicant shall be provided to the City prior to commencement of any demolition or construction activity.

I HAVE READ AND UNDERSTOOD, AND I WILL COMPLY
WITH ALL ABOVE STATED CONDITIONS OF THIS PERMIT

Applicant Date

