

CALIFORNIA COASTAL COMMISSION

NORTH CENTRAL COAST DISTRICT
455 MARKET STREET, SUITE 300
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Th13a

A-2-MAR-21-0045 (BOLINAS AFFORDABLE HOUSING)

SEPTEMBER 9, 2021

EXHIBITS

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SUBJECT PROPERTY AT 31 WHARF ROAD (LOOKING NORTHWEST)



SUBJECT PROPERTY AT 31 WHARF ROAD (LOOKING NORTHEAST)



VIEW OF 41 WHARF ROAD LOOKING EAST FROM SUBJECT PROPERTY



VIEW OF 23 WHARF ROAD LOOKING WEST FROM SUBJECT PROPERTY

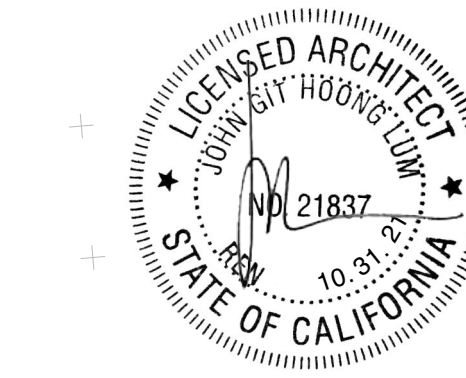


VIEW OF WHARF ROAD LOOKING SOUTH FROM SUBJECT PROPERTY

DOWNTOWN BOLINAS AFFORDABLE HOUSING

31 WHARF ROAD - BOLINAS, CA 94924

APN: 193-061-03



JOHN LUM ARCHITECTURE
3246 SEVENTEEN STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.9554

BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-061-03

bolinas community land trust
6 Wharf Road
bolinas, ca 94924

client:

ABBREVIATIONS:

& #	AND AT NUMBER
CL	CENTER LINE
EL	PROPERTY LINE
(E)	EXISTING
(N)	NEW
(R)	REPLACE
ABV.	ABOVE
ADJ.	ADJACENT
A.F.F.	ABOVE FINISH FLOOR
ALUM.	ALUMINUM
ARCH.	ARCHITECTURE
ASPH.	ASPHALT
BD.	BOARD
BASE BD.	BASE BOARD
BLDG.	BUILDING
BLK.	BLOCK
BLKG.	BLOCKING
BOT.	BOTTOM
BM.	BEAM
B.U.	BUILT-UP
CLG.	CEILING
CNTL.	CONTROL
CONT.	CONTINUOUS
CTR.	CENTER
CLR.	CLEAR
C.L.	CENTER LINE
DBL.	DOUBLE
D.F.	DOUGLAS FIR
DIM.	DIMENSION
DN.	DOWN
D.S.	DOUBLE POLE
DWG.	DOWN SPOUT DRAWING
E	EAST
EA	EACH
EL OR ELEV.	ELEVATION
ELEC.	ELECTRICAL
EQ.	EQUAL
EXP.	EXPOSED
EXT.	EXTERIOR
F.A.U.	FORCED-AIR UNIT
FDN.	FOUNDATION
F.F.	FINISHED FLOOR
F.F.E.	FINISHED FLOOR ELEVATION
FIN.	FINISH
F.O.C.	FACE OF CONCRETE
F.O.S.	FACE OF STUD
F.O.P.	FACE OF PLYWOOD
FP.	FIRE PLACE
FURN.	FURNACE
GALV.	GAUGE
GND.	GALVANIZED
GYP.BD.	GROUND
H.C.	GYPSPUM BOARD
HDR.	HOLLOW CORE
HDWD.	HEADER
H.V.A.C.	HARD WOOD
ID.	HEATING, VENTILATION, AIR CONDITIONING
INSUL.	INSIDE DIMENSION
INT.	INSULATION
JST.	INTERIOR
MAX.	JOIST
M.C.	MAXIMUM
MECH.	MEDICINE CABINET
MEMB.	MECHANICAL
MANUF.	MEMBRANE
MIN.	MANUFACTURER
N	MINIMUM
N.I.C.	METAL
NO.	NORTH
O	NOT IN CONTRACT
O.C.	NUMBER
OPCI	OVER
OPNG.	ON CENTER
O.D.	OWNER FURNISHED, CONTRACTOR INSTALLED
P.C.	OPENING
PL	OUTSIDE DIMENSION
PLYWD.	PLUMBING CHASE
PT.	PLATE
PTD.	PLYWOOD
R	PRESSURE TREATED
RET. AIR	POINT
RM	PAINTED
R.W.L.	RADIUS
S	RETURN AIR
S.S.D.	ROOM
SQ. FT.	REDWOOD
SHT.	RAIN WATER LEADER
SHTG.	SOUTH
SIM.	SEE STRUCTURAL DRAWINGS
S.P.	SQUARE FOOT
S.S.T.	SHEET
ST	SHEATHING
SUP. AIR	SIMILAR
T&G	SINGLE POLE
TBD	SQUARE
THK.	STAINLESS STEEL
T.O.P.	STEEL
T.O.F.	STANDARD
T.O.W.	SUPPLY AIR
T.R.	TONGUE AND GROOVE
TYP.	TO BE DETERMINED
U.O.N.	THICK
VARS	TOP OF PLATE
V.D.C.	TOP OF SLAB
VERT.	TOP OF FINISHED FLOOR
VIE.	TOP OF WALL
W	TOILET PAPER HOLDER
W2	TOWEL RACK
W3	TYPICAL
W.H.	UNLESS OTHERWISE NOTED
	VARIABLE
	VERIFY DURING CONSTRUCTION
	VERTICAL
	VIEW IN FIELD
	WEST
	WITH
	WOOD
	WOOD
	WATER PROOF
	WATER HEATER

SYMBOLS:

WALL TYPES

NEW WALL (NON-RATED)

NEW 1-HOUR FIRE-RATED WALL

NEW 2-HOUR FIRE-RATED WALL

NEW LOW WALL

EXISTING WALL

DEMO WALL

LINE TYPES

OVERHEAD LINE

HIDDEN LINE

PROPERTY LINE

SETBACK LINE

CENTER LINE

BREAK LINE

ELEVATION LINE

DIMENSIONS

FACE OF FINISH

CL OF STUD

MARKERS

DETAIL MARKER

PLAN DETAIL MARKER

ELEVATION MARKER

SECTION MARKER

INTERIOR ELEVATION MARKER

REVISION MARKER

DOOR TAG

WINDOW TAG

STEP

GENERAL NOTES:

AIA DOCUMENT 201, "GENERAL CONDITIONS FOR THE PERFORMANCE OF A CONTRACT," ARE HEREBY INCORPORATED INTO THESE DRAWINGS AND SHALL BE CONSIDERED AS PART OF THE REQUIREMENTS FOR THE COMPLETION OF WORK. SUPPLEMENTARY CONDITIONS TO THE CONTRACT ALSO APPLY.

1. ALL CONSTRUCTION SHALL CONFORM TO CURRENT MARIN COUNTY CODES AND ANY OTHER GOVERNING CODES, AMENDMENTS, RULES, REGULATIONS, ORDINANCES, LAWS, ORDERS, APPROVALS, ETC. THAT ARE REQUIRED BY APPLICABLE PUBLIC AUTHORITIES. IN THE EVENT OF CONFLICT THE MOST STRINGENT REQUIREMENTS SHALL APPLY.

2. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR CHECKING CONTRACT DOCUMENTS, FIELD CONDITIONS, AND DIMENSIONS FOR ACCURACY AND CONFIRMING THE WORK CAN BE BUILT OR DEMOLISHED AS SHOWN BEFORE PROCEEDING WITH THE WORK. IF THERE ARE ANY QUESTIONS REGARDING THESE OR OTHER COORDINATION QUESTIONS, THE GENERAL CONTRACTOR IS RESPONSIBLE FOR OBTAINING A CLARIFICATION FROM THE ARCHITECT BEFORE PROCEEDING WITH THE WORK IN QUESTION OR RELATED WORK.

3. ANY ERRORS, OMISSIONS OR CONFLICTS FOUND IN THE VARIOUS PARTS OF THE CONSTRUCTION DOCUMENTS SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT, BEFORE PROCEEDING WITH THE WORK.

4. CONTRACTOR SHALL THOROUGHLY EXAMINE THE PREMISES AND SHALL BASE HIS BID ON THE EXISTING CONDITIONS, NOTWITHSTANDING ANY INFORMATION SHOWN OR NOT SHOWN ON THE DRAWINGS.

5. CONTRACTOR TO MAINTAIN ALL PROPER WORKMAN'S COMPENSATION AND LIABILITY INSURANCE THROUGHOUT THE DURATION OF PROJECT.

6. SUBSTITUTIONS, REVISIONS, OR CHANGES MUST HAVE PRIOR APPROVAL OF THE ARCHITECT.

7. DURING THE BIDDING AND NEGOTIATION PERIOD THE GENERAL CONTRACTOR AND SUBCONTRACTOR(S) SHALL CONFIRM IN WRITING APPROX. ON-SITE DELIVERY DATES FOR ALL CONSTRUCTION MATERIALS AS REQUIRED BY THE CONSTRUCTION DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IN WRITING OF ANY POSSIBLE CONSTRUCTION DELAYS AFFECTING OCCUPANCY THAT MAY ARISE DUE TO THE AVAILABILITY OF THE SPECIFIED PRODUCT.

8. ALL WORK SHALL BE PERFORMED SUCH THAT DAMAGE TO EXISTING LANDSCAPE AND/OR PERSONAL PROPERTY IS PREVENTED OR MINIMIZED.

9. CONTRACTOR SHALL TAKE MEASURES TO PROTECT ADJACENT PROPERTIES. USE VISQUEEN, PLYWOOD, ETC. TO MINIMIZE NOISE, DUST, ETC.

10. IN THE EVENT THAT FOUNDATION EXCAVATION MIGHT AFFECT ADJACENT PROPERTIES, CONTRACTOR SHALL TAKE ALL APPROPRIATE STEPS TO NOTIFY THE PROPERTY OWNER OF THE CONDITION, AND TO ADEQUATELY PROTECT THE ADJACENT STRUCTURE.

11. WRITTEN DIMENSIONS REFER TO FACE OF FINISH OR CENTER-LINE UNLESS OTHERWISE NOTED. EXTERIOR WALLS ARE DIMENSIONED TO FACE OF SHEATHING, U.O.N.

12. DIMENSIONS ARE TO TOP OF FIN. FLOOR, SLAB OR DECK IN SECTION OR ELEVATION, UNLESS OTHERWISE NOTED.

13. "SIM" OR "SIMILAR" MEANS COMPARABLE CHARACTERISTICS FOR THE ITEM NOTED. VERIFY DIMENSIONS AND ORIENTATION ON PLAN.

14. "TYP" OR "TYPICAL" MEANS IDENTICAL FOR ALL SIMILAR CONDITIONS UNLESS NOTED OTHERWISE.

15. DIMENSIONS NOTED "CLR" OR "CLEAR" ARE MINIMUM REQUIRED DIMENSIONS AND CLEARANCES MUST BE ACCURATELY MAINTAINED.

16. CONTRACTOR TO VERIFY DIMENSIONS AND CONDITIONS IN FIELD. IF CONDITIONS ARE SIGNIFICANTLY DIFFERENT THAN REPRESENTED IN DRAWINGS, VERIFY CONDITIONS WITH ARCHITECT.

17. ALL MATERIALS AND EQUIPMENT TO BE NEW UNLESS OTHERWISE NOTED.

18. ALL MATERIALS AND EQUIPMENT TO BE INSTALLED PER MANUFACTURER'S INSTRUCTIONS.

19. WINDOW AND DOOR SIZES ARE NOMINAL DIMENSIONS. REFER TO MANUFACTURER'S SPECIFICATIONS FOR ACTUAL ROUGH OPENINGS.

20. WHERE LOCATIONS OF WINDOWS AND DOORS ARE NOT DIMENSIONED THEY SHALL BE CENTERED IN THE WALL OR PLACED TWO STUD WIDTHS FROM ADJACENT WALL AS INDICATED ON DRAWINGS, UNLESS OTHERWISE NOTED.

21. ALL CHANGES IN FLOOR MATERIAL SHALL OCCUR AT CENTERLINE OF DOOR OR FRAMED OPENING, UNLESS OTHERWISE INDICATED ON THE DRAWINGS.

22. SEALANT, CAULKING, FLASHING, ETC. LOCATIONS SHOWN ON DRAWINGS ARE INTENDED TO BE INCLUSIVE. FOLLOW MANUFACTURER'S INSTALLATION RECOMMENDATIONS AND STANDARD INDUSTRY AND BUILDING PRACTICES.

23. ALL ATTICS, RAFTER SPACES, SOFFITS, CRAWL SPACES, ETC. TO BE FULLY VENTILATED PER APPLICABLE CODE.

24. PROVIDE WOOD BLOCKING FOR ALL TOWEL BARS, ACCESSORIES, ETC.

25. MEET ALL CALIFORNIA ENERGY CONSERVATION REQUIREMENTS INCLUDING BUT NOT LIMITED TO:
A. MINIMUM ROOF/CEILING INSULATION R-19
B. MINIMUM WALL INSULATION IN FRAMED EXTERIOR WALLS R-13
C. MINIMUM FLOOR INSULATION OVER CRAWL OR UNOCCUPIED SPACES R-13
D. ALL INSULATION TO MEET CEC QUALITY STANDARDS.
E. INFILTRATION CONTROL:
1. DOORS AND WINDOWS WEATHER-STRIPPED.
2. EXHAUST SYSTEMS DAMPENED.
3. DOORS AND WINDOWS CEC CERTIFIED AND LABELED.
4. ALL JOINTS AND PENETRATIONS CAULKED AND SEALED.
F. DUCTS CONSTRUCTED AND INSTALLED PER UMC.
G. ELECTRICAL OUTLET PLATE GASKETS SHALL BE INSTALLED ON ALL RECEPTACLES, SWITCHES AND ELECTRICAL BASES ON EXTERIOR WALLS.

26. SMOKE ALARMS ARE TO BE INSTALLED IN ALL SLEEPING ROOMS. SMOKE ALARMS SHALL BE HARDWIRED TO 110V HOUSE WIRING AND WIRED TOGETHER IN SERIES. MINIMUM ONE ALARM PER STORY. REF. PLANS FOR LOCATIONS.

27. GENERAL CONTRACTOR IS TO COORDINATE INSTALLATION OF N.I.C. ITEMS WITH OTHER TRADES

28. LOCATION/SPECIFICATION OF SAFETY GLAZING (TEMPERED GLASS) ARE SOLE RESPONSIBILITY OF CONTRACTOR. ALL DOORS W/ GLAZING AND ALL GLAZING OF WINDOWS WITHIN 24" OF EDGE OF ANY DOOR SHALL BE WITH TEMPERED GLASS (UBC SECTION 2406)

29. CONTRACTOR SHALL CHECK AND VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE PROCEEDING WITH CONSTRUCTION. DO NOT SCALE DRAWINGS. NOTED DIMENSIONS TAKE PRECEDENCE.

PROJECT DATA:

CODES

2019 CALIFORNIA BUILDING CODE
2019 CALIFORNIA MECHANICAL CODE
2019 CALIFORNIA ELECTRICAL CODE
2019 CALIFORNIA PLUMBING CODE
2019 CALIFORNIA FIRE CODE
2019 CALIFORNIA RESIDENTIAL CODE
2019 CALIFORNIA ENERGY CODE
2019 CALIFORNIA GREEN BUILDING STANDARDS CODE
APPLICABLE MARIN COUNTY MUNICIPAL CODES

PROJECT ADDRESS

31 WHARF ROAD
BOLINAS, CA 94924

PROJECT DESCRIPTION

8 NEW 'AFFORDABLE HOUSING' RESIDENTIAL UNITS 0/ RETAIL & UNDERGROUND PARKING.

- 4 FRONT UNITS EACH INCLUDE 3 BEDROOMS, 2 BEDROOMS, KITCHEN & LIVING ROOM, 1 UNIT TO BE 'ACCESSIBLE' AND OTHER FRONT UNITS TO BE 'ADAPTABLE' PER CBC CH.11A

- 4 REAR UNITS EACH INCLUDE 2 BEDROOMS, 1 BATH, KITCHEN & LIVING ROOM.

PARKING SPACES: 16 OFF-STREET (3 RETAIL + 7 RESIDENTIAL (INCLUDING 1 ADA VAN SPACE)); 4 ON-STREET (INCLUDING 1 ADA VAN SPACE)

LOCATED IN HIGH FIRE SEVERITY ZONE, SUBJECT TO WUI REQUIREMENTS

PLANNING INFORMATION

ASSESSORS PARCEL NUMBER	193-061-03
ZONING DISTRICT	C-VCR, C-RA-B2
BUILDING HEIGHT	PROPOSED - 25'-0" (25'-0" MAX)
NUMBER OF STORIES	PROPOSED - 4

BUILDING INFORMATION

CONSTRUCTION TYPE	TYPE - III
OCCUPANCY	GROUP R-2 / M / S-2
MINIMUM ROOF CLASS	CLASS A

GROSS FLOOR AREA:

COMMERCIAL	1182 SQ.FT.	CONDITIONED
UNDERGROUND PARKING	5499 SQ.FT.	UNCONDITIONED
2ND FLOOR PARKING	1673 SQ.FT.	UNCONDITIONED
BUILDING A	4787 SQ.FT.	CONDITIONED
BUILDING B	3596 SQ.FT.	CONDITIONED

TOTAL PROPOSED	8383 SQ.FT.	RESIDENTIAL
	1182 SQ.FT.	COMMERCIAL
	7172 SQ.FT.	PARKING

16737 SQ.FT. TOTAL

PROJECT DATA PER MARIN COUNTY DEVELOPMENT CODE

LOT AREA:	101,150 SQ.FT.	EXISTING
	101,150 SQ.FT.	PROPOSED
BUILDING AREA:	0 SQ.FT.	EXISTING
	16,754 SQ.FT.	PROPOSED
FLOOR AREA:	0 SQ.FT.	EXISTING
	8,383 SQ.FT.	PROPOSED
FLOOR AREA RATIO:	0.00	EXISTING
	0.08	PROPOSED
AREA OF ADTTL. DISTURBANCE:	.27 ACRES	PROPOSED
LOT COVERAGE:	93.037 SQ.FT.	PERVIOUS
	8,113 SQ.FT.	IMPERVIOUS
GRADING CALCULATIONS:	2,287 CU.YD.	CUT
	144.7 CU.YD.	FILL
	2142.3 CU.YD.	OFF-HAUL

MINIMUM SETBACKS	0'-0"
MAXIMUM HEIGHT	25'-0"

VEHICLE PARKING CALCULATIONS:

RESIDENTIAL:	5 SPACES PER UNIT INCLUSIVE OF ACCESSIBLE & GUEST SPACES PER MCC24.04.340(e)(1)(i)
RESIDENTIAL PROVIDED:	(5 SPACES / UNIT) X (8 UNITS) = 4 SPACES = 4 SPACES + 2 COMPACT SPACES = 7 ACCESSIBLE SPACES = 7 SPACES
NON-RESIDENTIAL:	1 SPACE PER 200 SQ.FT. PER MCC24.04.340-B 1/200 X 1,200 SQ.FT. = 6 SPACES
NON-RESIDENTIAL PROVIDED:	= 4 SPACES + 5 COMPACT SPACES = 9 SPACES
TOTAL REQUIRED:	10 SPACES
TOTAL PROVIDED:	16 SPACES
BICYCLE PARKING REQUIRED:	
SHORT TERM:	1 SPACE PER (3) 2-BEDROOM UNITS & 1 SPACE PER 3-BEDROOM UNIT PER MCC24.04.340-A = 6 SPACES
LONG TERM:	1 SPACE PER 2-BEDROOM UNITS & 1.5 SPACES PER 3-BEDROOM UNIT PER MCC24.04.340-A = 10 SPACES
EV CHARGING SPACES REQUIRED:	
RESIDENTIAL:	1 PER UNIT PER MCC19.04.140 = 8 SPACES
NON-RESIDENTIAL:	10% OF SPACES + REMAINING CAPABLE = 1 + 5 CAPABLE

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PROJECT PARTICIPANTS:

ARCHITECT:

JOHN LUM ARCHITECTURE
3246 17TH STREET
SAN FRANCISCO, CA 94110

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1. 415.558.9550 x0010
1. 415.558.9554

OWNER:

BOLINAS COMMUNITY LAND TRUST
3246 17TH STREET
BOLINAS, CA 94924
ARIANNE DAR
1.415.868.9890

GEOTECHNICAL ENGINEER:

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1390 EL CAMINO REAL
SAN CARLOS, CA 94070

LUCAS OTTOBONI
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CIVIL ENGINEER:

AYS ENGINEERING INC
PO BOX 5693
PETALUMA, CA 94955

TROY PEARCE
1.707.763.6620

SURVEYOR:

PACIFIC LAND SURVEYS
PO BOX 315
BODEGA, CA 94922
SUSAN RUSCHMEYER
1.707.875.3208

GENERAL CONTRACTOR:

T.B.D.

STRUCTURAL ENGINEER:

T.B.D.

TITLESHEET

A0.00

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Exhibit 3

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SUBJECT PROPERTY



SUBJECT PROPERTY



VIEW OF 23 WHARF RD. FROM SUBJECT PROPERTY



VIEW OF 41 WHARF RD. FROM SUBJECT PROPERTY



VIEW ACROSS STREET

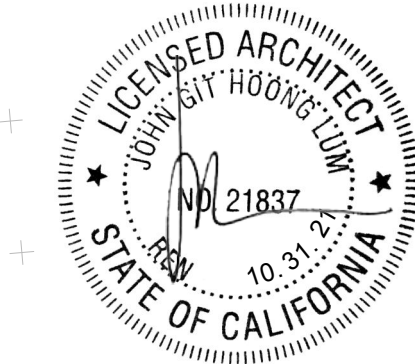


VICINITY MAP



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John Lum Architecture



JOHN LUM ARCHITECTURE
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BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 182-061-03

bolinas community land trust
6 wharf road
bolinas, ca 94924

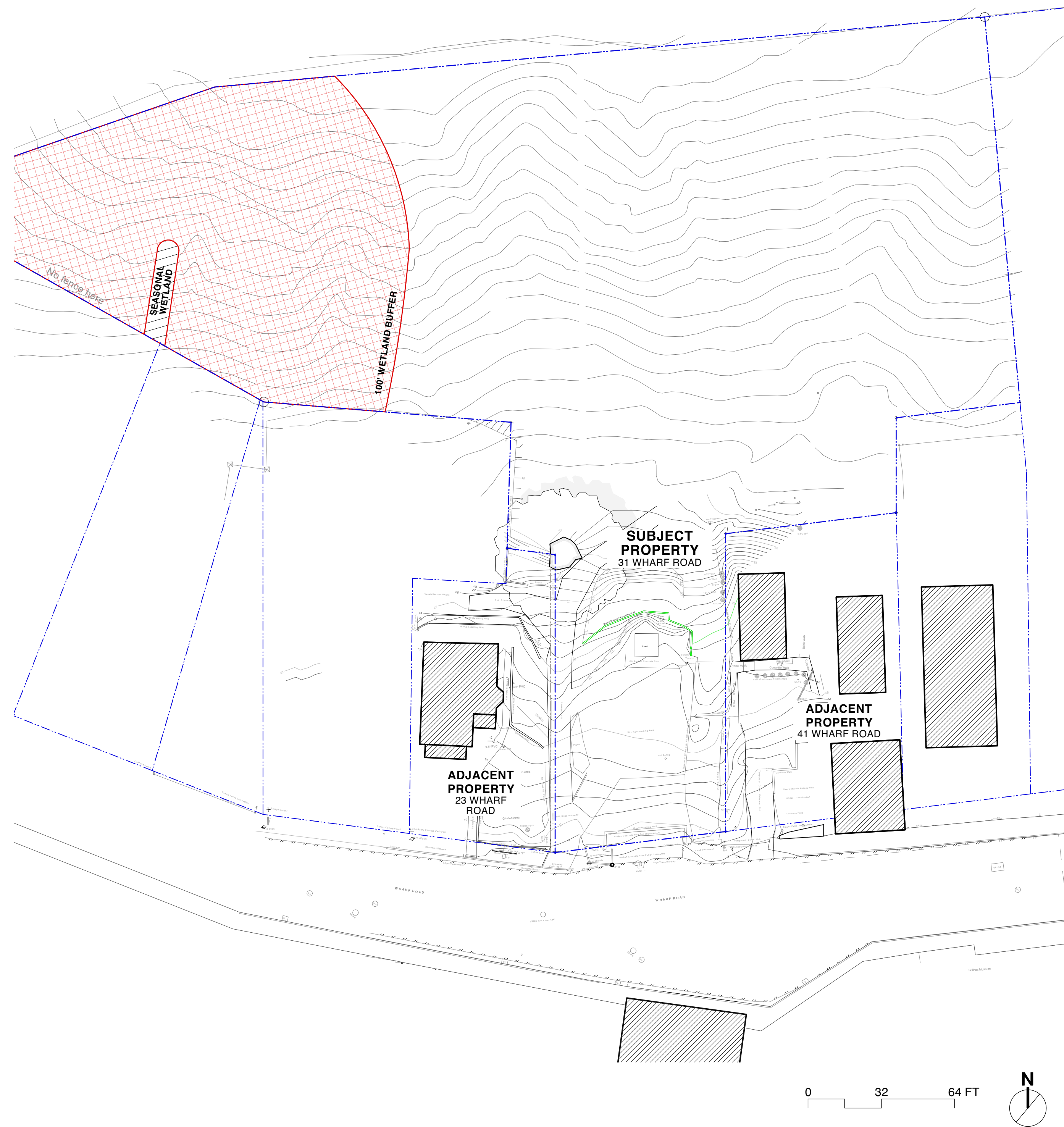
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date	issues / revisions	by
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06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

SITE PHOTOS

A0.01

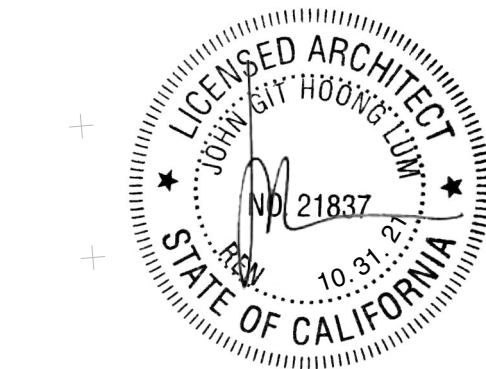
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1
A0.02
EXISTING SITE PLAN
Scale: 1/32" = 1'-0"



2
A0.02
PROPOSED SITE / ROOF PLAN
Scale: 1/32" = 1'-0"



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CALIFORNIA 94924
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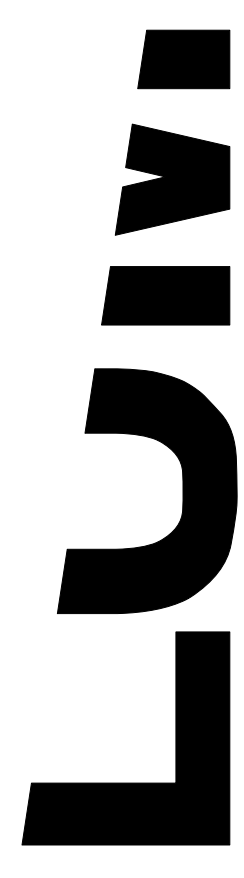
bolinas community/land trust
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bolinas, ca 94924

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12.04.20	Planning Submittal	hm
12.18.20	Planning Submittal update	di
02.01.21	Planning Submittal update	di

(E&P) SITE PLAN - FULL

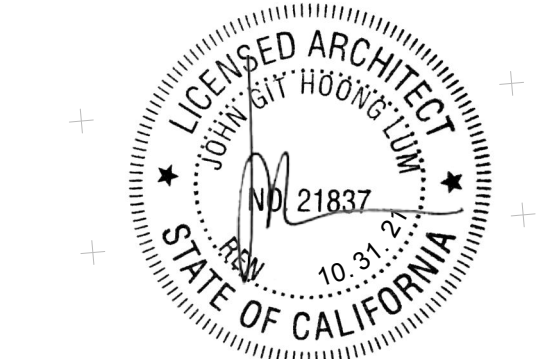
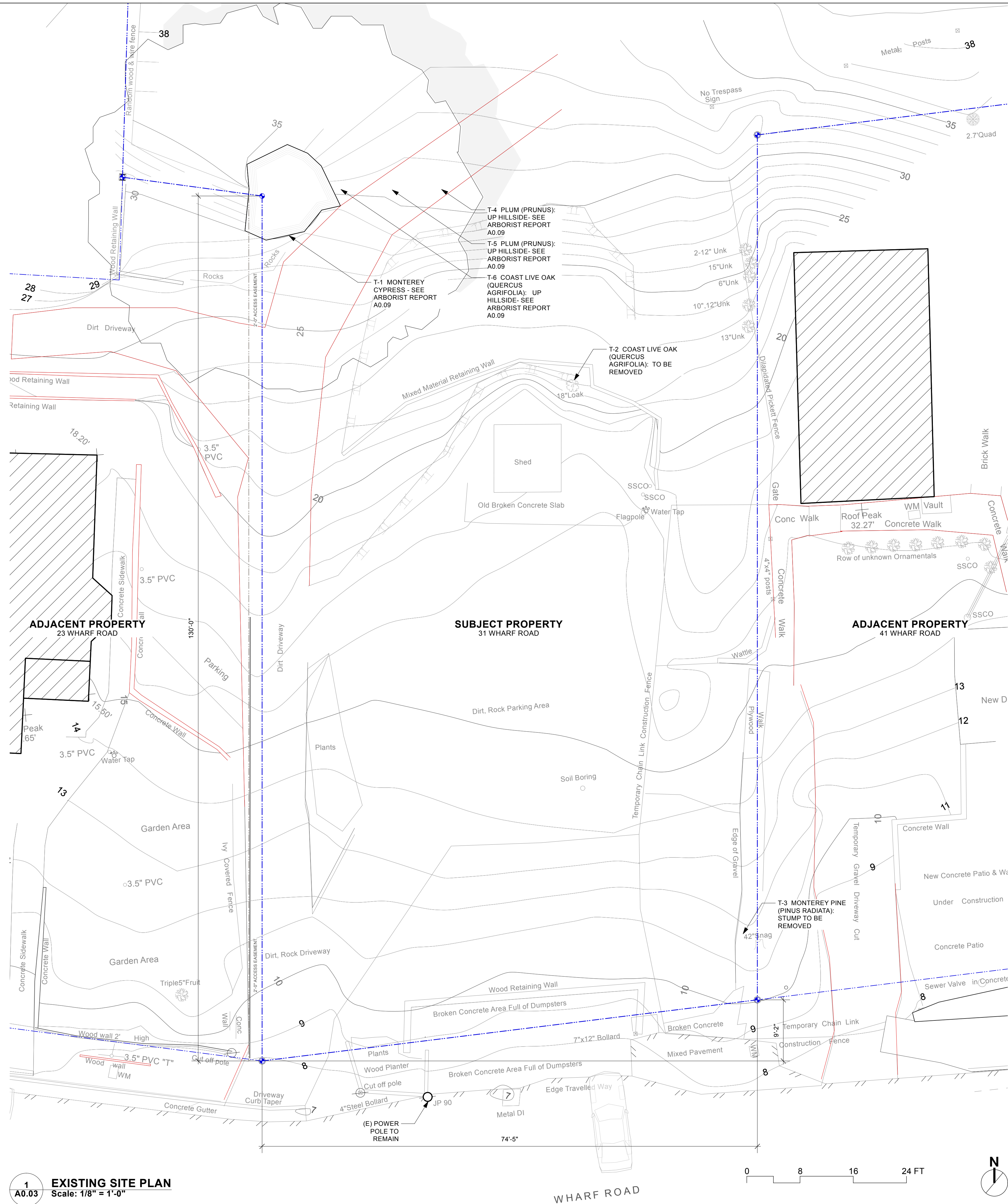
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Exhibit 3
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BCLT - DOWNTOWN BOLINAS

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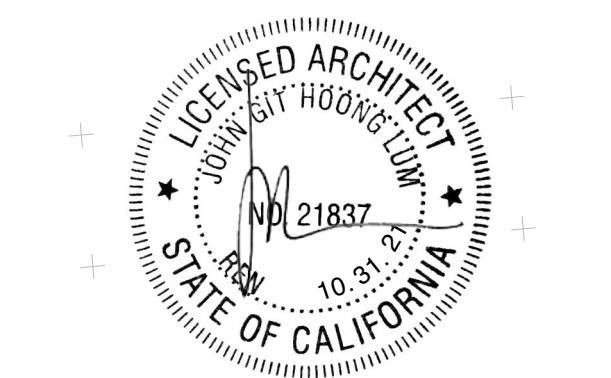
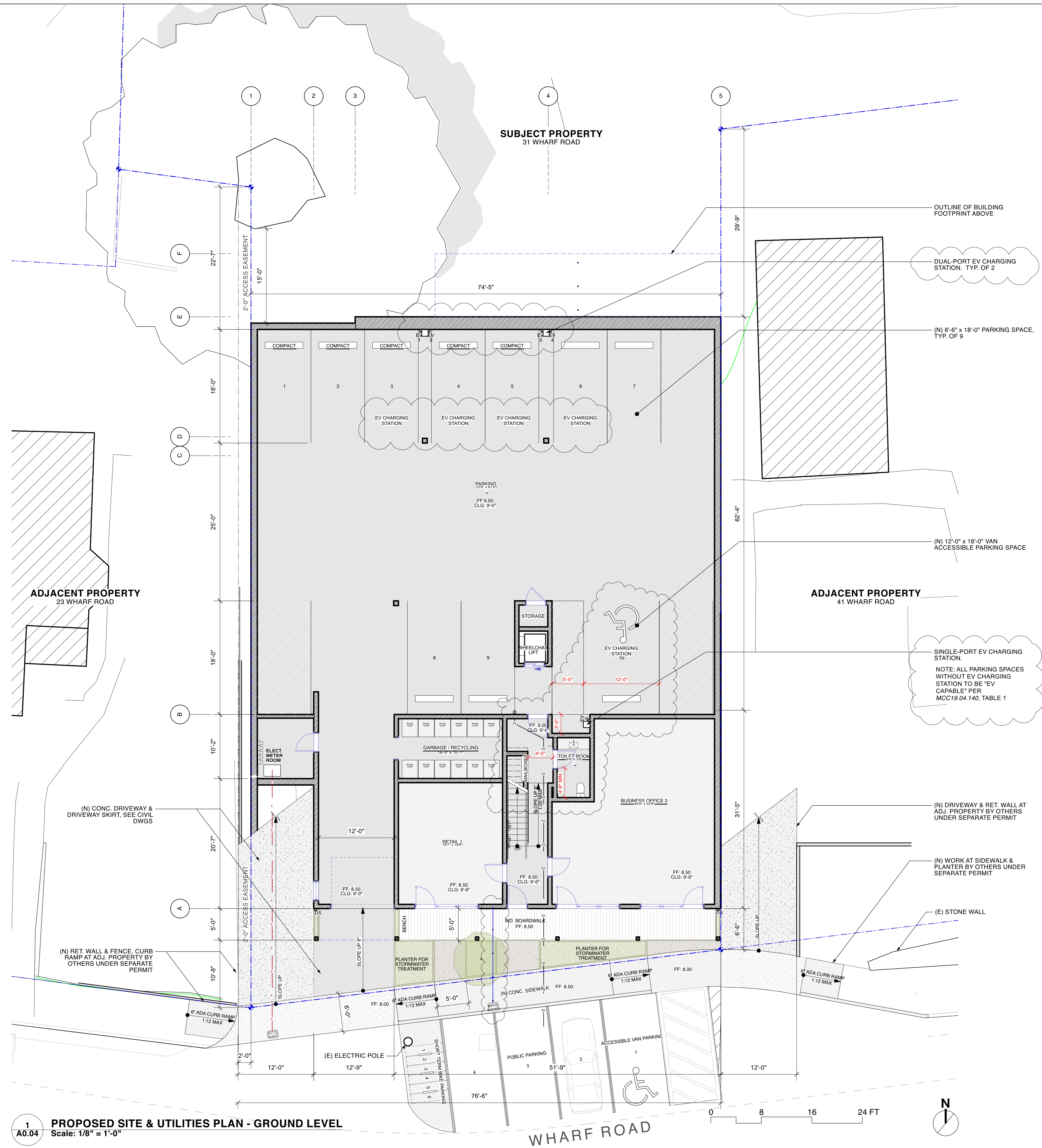
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(E) SITE PLAN

A0.03

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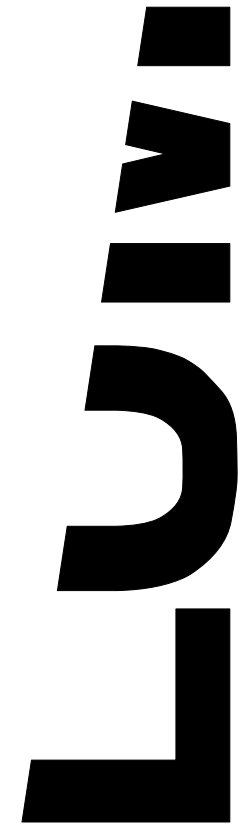
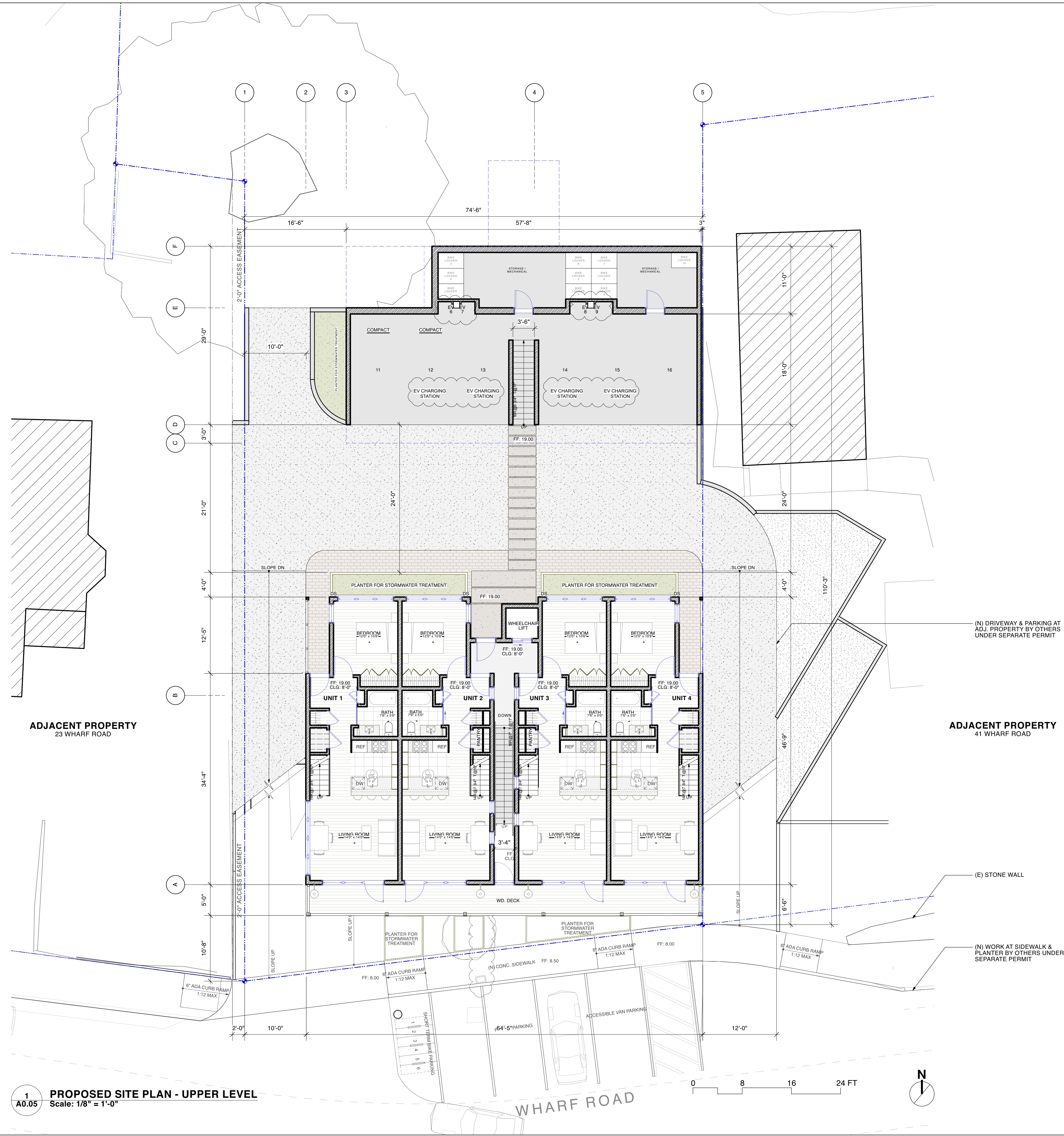
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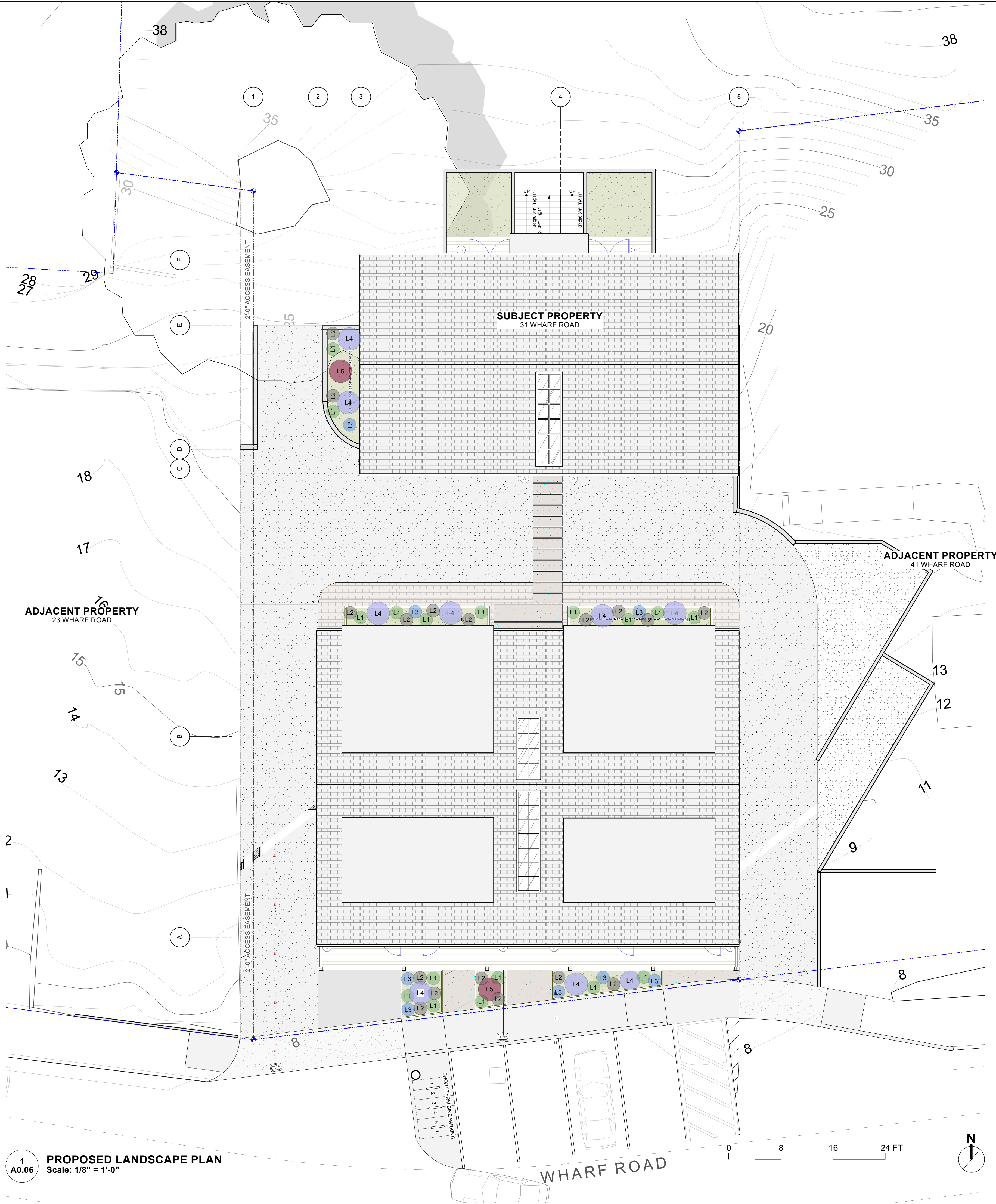
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12.04.20	Planning Submittal	hm
12.18.20	Planning Submittal update	di
02.01.21	Planning Submittal update	di

(P) SITE PLAN - UPPER LEVEL

A0.05

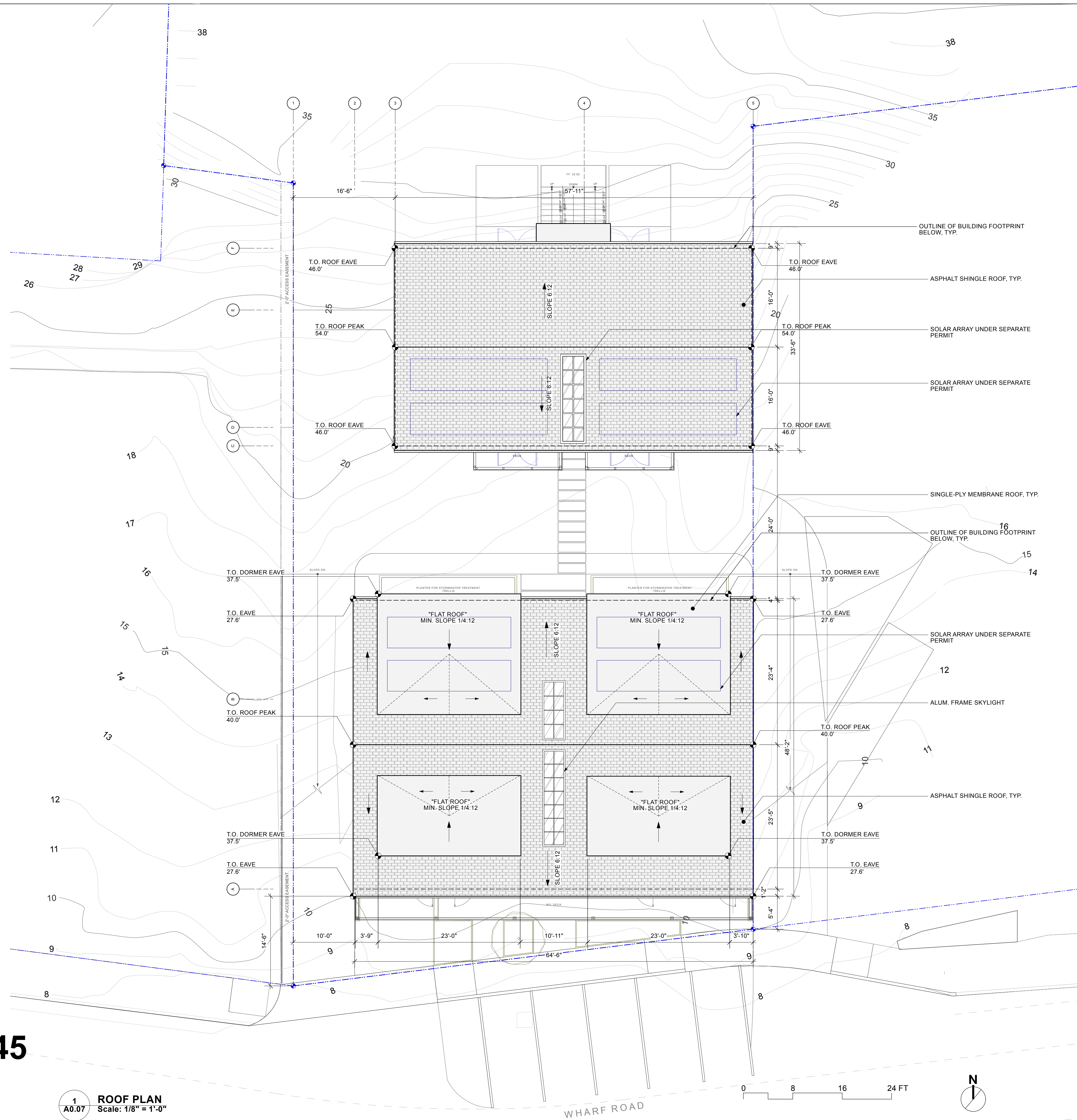
PLANT LEGEND:		
COMMON NAME (SCIENTIFIC NAME)	QNTY.	
L1 ELIJA BLUE FESCUE (FESTUCA GLAUCA 'ELIJA BLUE')	23	
L2 CANYON PRINCE WILD RYE (LEYMUS CONDENSATUS)	23	
L3 LAVENDER (LAVANDULA INTERMEDIA 'GRASSO')	15	
L4 CALIFORNIA MOUNTAIN LILAC (CEANOTHUS CONCHA)	8	
L5 MOUNTAIN LILAC TREE (CEANOTHUS RAY HARTMAN)	1	
L1		
L2		
L3		
L4		
L5		



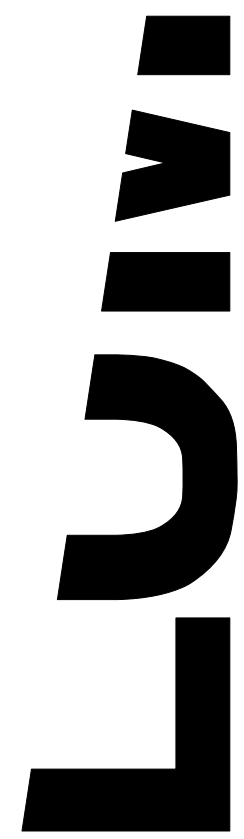
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A0.06
PROPOSED LANDSCAPE PLAN
Scale: 1/8" = 1'-0"

date	issues / revisions	by
05.20.20	Design Development	hm
06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

filename: BCLT-DOWNTOWN_DO_201123.vwx



1
A0.07
ROOF PLAN
Scale: 1/8" = 1'-0"



JOHN LUM ARCHITECTURE
3246 SEVENTEENTH STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.9554



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 183-051-03

bolinas community land trust
6 Wharf Road
bolinas, ca 94924
client:

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
08.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

ROOF PLAN

A0.07

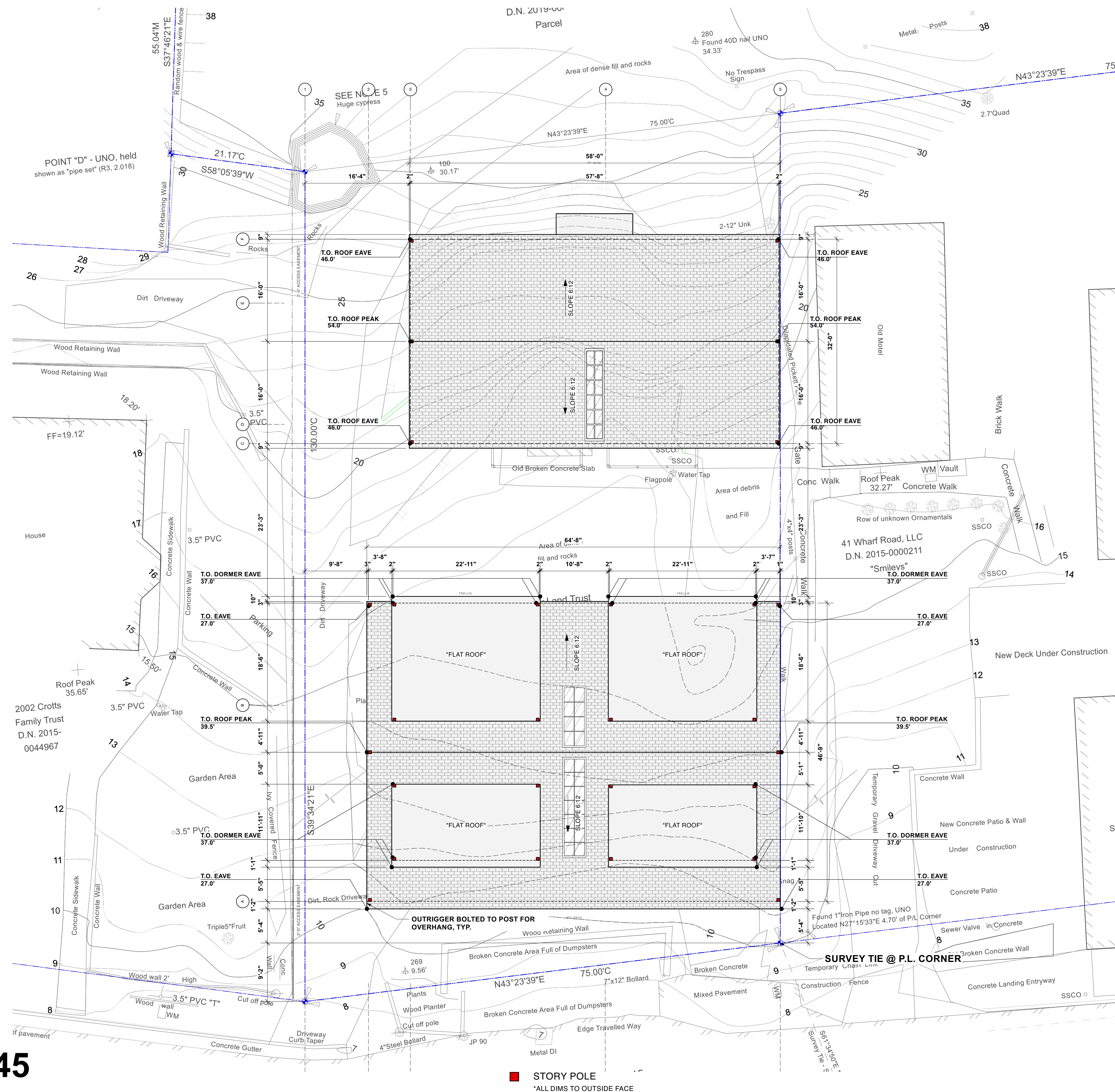


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06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm
12.18.20	Planning Submittal update	di

STORY POLE PLAN

A0.08

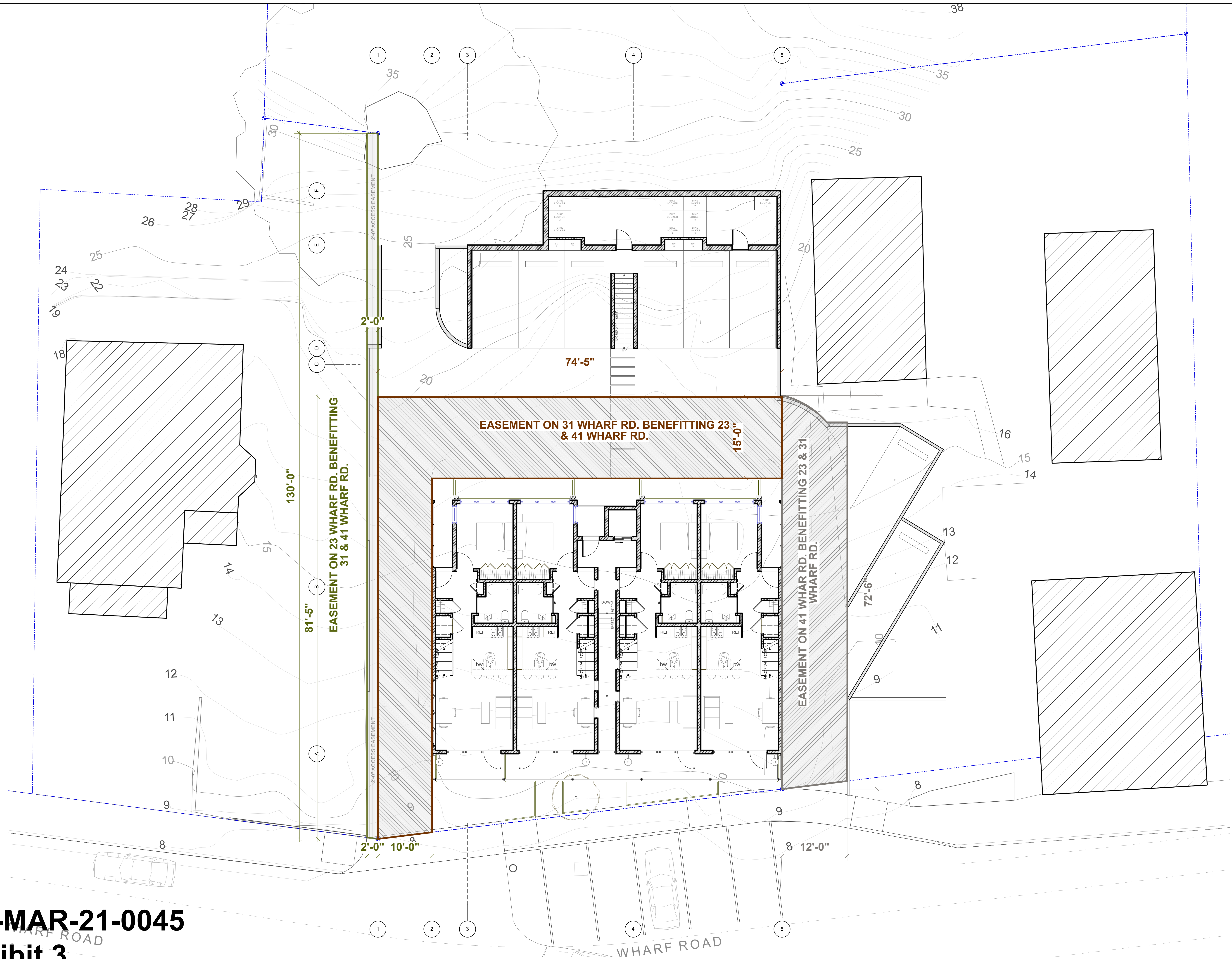


A-2-MAR-21-0045
Exhibit 3
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Scale: 1/8" = 1'-0"

Filename: BCIT-DOWNTOWN DD 201216.vwx

filename: BCLT-DOWNTOWN_DO_201123.vwx



EASEMENT PLAN
Scale: 1/8" = 1'-0"



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 183-051-03

client: bolinas community land trust
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bolinas, ca 94924

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06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

EASEMENT PLAN

A0.09




filename: BCLT-DOWNTOWN_DO_201123.wkx

Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

Client: Arianne Dar, Bolinas Community Land Trust
Project Location: Henry Malmberg, John Lum Architecture
Inspection Date: Wharf Road, Bolinas
Parcel Number: 193-061-03
Arborist: Ben Anderson


URBAN FORESTRY ASSOCIATES, INC.

Summary

The subject property on Wharf Road is large and has many trees. The development is in an area of the property with relatively few trees, close to the street. Two coast live oak trees (*Quercus agrifolia*) and two plum trees (*Prunus* sp.) will need to be removed for the project. One large Monterey cypress (*Hesperocyparis macrocarpa*) is near the development but should suffer no long-term negative effects.

Assignment

Henry Malmberg of John Lum Architecture contacted Urban Forestry Associates to request an assessment of the impact to existing trees resulting from the proposed development of the subject parcel. I inspected the site on December 10, 2019. Arianne Dar and Evan Wilhelm met me on site.

Observations

Site

The subject property is in downtown Bolinas in the Coastal Zone. There is a developed residential property to the southwest and a commercial property under current development to the northeast. The site description in the project data on the provided plan set states the lot is 101,150 square feet. Most of this area is northwest of the proposed area of development and well-stocked with native and exotic tree species, mostly oak and pine (*Quercus* spp. and *Pinus* spp., respectively). The front portion of the property nearest Wharf Road is largely undeveloped, with only a few retaining walls and sheds. It appears to mostly be used as a parking and storage area.

Tree 1

The largest and most prominent tree on the site is a mature Monterey cypress 20 feet northwest of the edge of the proposed development activity (see Figure 1). The trunk of the tree is approximately eight feet in diameter (visual estimate). Large failures are visible in the canopy of the tree but only partially visible due to obstruction from other areas of the canopy. I reviewed the historic photography of the tree on Google Maps and Google Earth and see that a major failure occurred between June 2017 and February 2018.

The tree is in good health and has good form¹ but the structure is poor to fair². There are many old wounds from pruning and branch failures spread all over the trunk and canopy of the tree. These wounds allow boring insects and decay fungi to enter the structural wood of the tree, reducing its ability to carry load and support the tree. The base of the tree is in relatively good condition with no evidence of Signant decay or root disease that would elevate the likelihood of whole-tree failure. The main concern is large branch failure from high in the tree.

Tree 2 (removal)

Near the center of the development area is a coast live oak growing out of a retaining wall. The trunk diameter measured 19.7 inches. The tree is generally in good health with good form and structure.

Tree 3 (removal but previously dead)

A mature Monterey pine (*Pinus radiata*) on the front east corner of the property died in 2017 and was cut back

¹ Words in **bold** are defined in the glossary.
² See explanation of condition ratings in Table 1.

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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

to a stump, approximately 15 feet high.

Tree 4 (removal)

A small plum tree is growing northwest of the development area and is reportedly in the path the well-drilling rig will need to reach the only available drilling site. The tree has four stems that share a common attachment just above grade. The largest stem diameter is nine inches. Decay is present in the base of the tree that is significant enough to compromise the stability of the tree. Health, form, and structure are all poor to fair.

Tree 5 (removal)

This is another plum tree near the path of the drilling rig. The trunk diameter is 9.7 inches. The health, form and structure are all fair.

Tree 6 (removal)

A mature coast live oak sits at the only suitable location to drill the well for the site. This trunk diameter is 28.3 inches. The tree is in good health with good form and structure.

Discussion

According to the "Tree Removal Permit Fact Sheet" from the County of Marin Planning Division, the Native Tree Protection and Preservation ordinance does not apply to properties located in the coastal zone. Significant vegetation is protected by the Local Coastal Program. Of the subject trees, the only tree I consider significant vegetation for habitat and aesthetics is Tree 1, though it may have serious structural problems obscured from the ground.

Conclusions

The project will require the removal of all the subject trees but Tree 1. This is a total of four living trees. As they are in the Coastal Zone, none of these trees requires a tree removal permit.

Tree 1 is too large and the visual access to the canopy was too limited for a proper risk assessment. It is clear however that the tree needs a **crown reduction** and crown cleaning to remove deadwood, crossing and broken branches, and to shrink the overall size of the tree if it is to remain near homes and active use areas.

The industry standard for root management³ states:

"Cutting roots at a distance greater than six times the trunk diameter (dbh) minimizes the likelihood of affecting both health and stability. At this distance, approximately 25% of the root system would be lost. Cutting roots any closer to the tree is more likely to compromise stability.

Linear cuts on one side of a tree can reduce stability when the cut is made at a distance from a distance from the trunk that is less than three times the trunk diameter. Severe loss of stability is common when cuts are made at a distance that is less than 1 to 1.5 times the trunk diameter."

A 6x limit is almost never utilized. Typically, it is very difficult getting developers to limit linear cuts (trenches and foundations) within 3x the diameter. In practice, I limit the 3x distance to approximately 10', as I have never seen roots pull out of the ground that far from a tree of any size during a failure.

The proposed development appears to be at least 20 feet away from Tree 1 and so I do not believe it will have any significant negative effect on the health or stability of the tree so long as the protection zone recommended on the map in Figure 1 and all the recommendations found in the Arborist's Checklist (below) are observed.

³ Costello, Larry; Gary Watson; and Thomas Smiley. *Best Management Practices – Root Management*. 2017. Champaign, Illinois: International Society of Arboriculture


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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

SCOPE OF WORK AND LIMITATIONS

Urban Forestry Associates has no personal or monetary interest in the outcome of this investigation. All observations regarding trees in this report were made by UFA, independently, based on our education and experience. All determinations of health condition, structural condition, or hazard potential of a tree or trees at issue are based on our best professional judgment. The health and hazard assessments in this report are limited by the visual nature of the assessment. Defects may be obscured by soil, brush, vines, aerial foliage, branches, multiple trunks, other trees, etc. Even structurally sound, healthy trees can fail during severe storms. Consequently, even a low risk rating is not a guarantee of no risk, hazard, or sound health.


Benjamin Anderson, Urban Forester
ISA Board Certified Master Arborist & TRAQ
RCA #686, WE #10160B
(415) 454-4212

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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

Table 1. Condition ratings table. Taken from *Guide for Plant Appraisal, 10th edition*

Rating category	Health	Condition components	Form
Excellent	High vigor and nearly perfect health with little or no twig dieback, discoloration, or defoliation	Nearly ideal and free of defects.	Nearly ideal for the species. Generally symmetric. Consistent with the intended use.
Good	Vigor is normal for the species. No significant damage due to diseases or pests. Any twig dieback, defoliation, or discoloration is minor.	Well-developed structure. Defects are minor and can be corrected.	Minor asymmetries/deviations from species norm. Mostly consistent with the intended use. Function and aesthetics are not compromised.
Fair	Reduced vigor. Damage due to insects or diseases may be significant and associated with defoliation but is not likely to be fatal. Twig dieback, defoliation, discoloration, and/or dead branches may comprise up to 50% of the crown.	A single defect of a significant nature or multiple moderate defects. Defects are not practical to correct or would require multiple treatments over several years.	Major asymmetries/deviations from species norm and/or intended use. Function and/or aesthetics are compromised.
Poor	Unhealthy and declining in appearance. Poor vigor. Low foliage density and poor foliage color are present. Potentially fatal pest infestation. Extensive twig and/or branch dieback.	A single serious defect or multiple significant defects. Recent change in tree orientation. Observed structural problems cannot be corrected. Failure may occur at any time.	Largely asymmetric/abnormal. Detracts from intended use and/or aesthetics to a significant degree.
Very poor	Poor vigor. Appears to be dying and in the last stages of life. Little live foliage.	Single or multiple severe defects. Failure is probable or imminent.	Visually unappealing. Provides little or no function in the landscape.
Dead			

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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

TREE WORK STANDARDS AND QUALIFICATION

All tree work, removal, pruning, planting, shall be performed using industry standards as established by the International Society of Arboriculture. Contractor must have a State of California Contractors License for Tree Service (C61-D49) or Landscaping (C-27) with general liability, worker's compensation, and commercial auto/equipment insurance. Contractor standards of workmanship shall adhere to current Best Management Practices of the International Society of Arboriculture (ISA) and the American National Standards Institute (ANSI) for tree pruning, fertilization and safety (ANSI A300 and Z133.1).

INSPECTION SCHEDULE

Inspection of site: Prior to Equipment and Materials Move In, Site Work, Demolition and Tree Removal:

The Project Arborist will meet with the General Contractor, Architect / Engineer, and Owner or their representative to review tree preservation measures, designate tree removals, delineate the location of tree protection / non-intrusion zone fencing, specify equipment access routes and materials storage areas, review the existing condition of trees and provide any necessary recommendations.

Inspection of site: After Installation of Tree Protection Zone (TPZ) fencing:

Inspect site for the adequate installation of tree preservation measures. Review any requests by contractor for access, soil disturbance or excavation areas within root zones of protected trees. Assess any changes in the health of trees since last inspection.

Inspection of site: During excavation or any activities that could affect trees:

Inspect site during any activity within the TPZ of preserved trees and any recommendations implemented. Assess any changes in the health of trees since last inspection.

Final Inspection of Site: Inspection of site following completion of construction:

Inspect for tree health and make any necessary recommendations.

ARBORIST'S CHECKLIST

An urban forester, certified or consulting arborist shall establish the Tree Protection Zone (TPZ) prior to starting the demolition work. Four-foot-high metal wire deer fencing will be erected by the contractor and inspected by the arborist to limit access to the TPZ. This will protect the trunk and root zone throughout construction.

The Arborist shall have a pre-demolition meeting with contractor or responsible party and all other foremen or crew managers on site prior to any work to review all work procedures, access and haul routes, and tree protection. The contractor must notify the Arborist if roots are exposed or if trunk or branches are wounded.

Storage of equipment shall be as far away from protected trees as possible and optimally on asphalt or ground protected by mulch / plywood.

Heavy equipment use should be limited around trees and the roots. No equipment may be transported or used on bare ground within the root zone. A 6" layer of mulch and plywood must be placed under the path for access and egress. The protective "bridge" shall be maintained by the contractor and inspected by the arborist when on site.

Any damage to trees due to demolition or construction activities shall be reported to the arborist within 6 hours, so that remedial action can be taken. Any damage done to the trees in violation of the contract agreement shall be appraised as a casualty loss by the arborist and provided to the tree owner.

All trenching within the TPZ shall be done pneumatically or by hand, being careful not to damage any of

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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

the bark of any root encountered.

An arborist shall inspect all grading, trenching, tunneling or other excavation within the root zones of trees prior to backfill.

No chemicals or other waste materials shall be dumped within 20' of the base of any tree. There shall be no material storage in the TPZ.

Any tree pruning will be done in accordance with the latest version of ISA or ANSI best management practices/ standards. All pruning will be inspected by the arborist.

The arborist must perform a final inspection to ensure that no unmitigated damage has occurred and to specify any pest, disease or other health care. The arborist shall specify and oversee any necessary restorative actions.

Any suspected omissions or conflict between various elements of the plan shall be brought to the attention of the arborist and resolved before proceeding with the work.

Page 6 of 8

Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

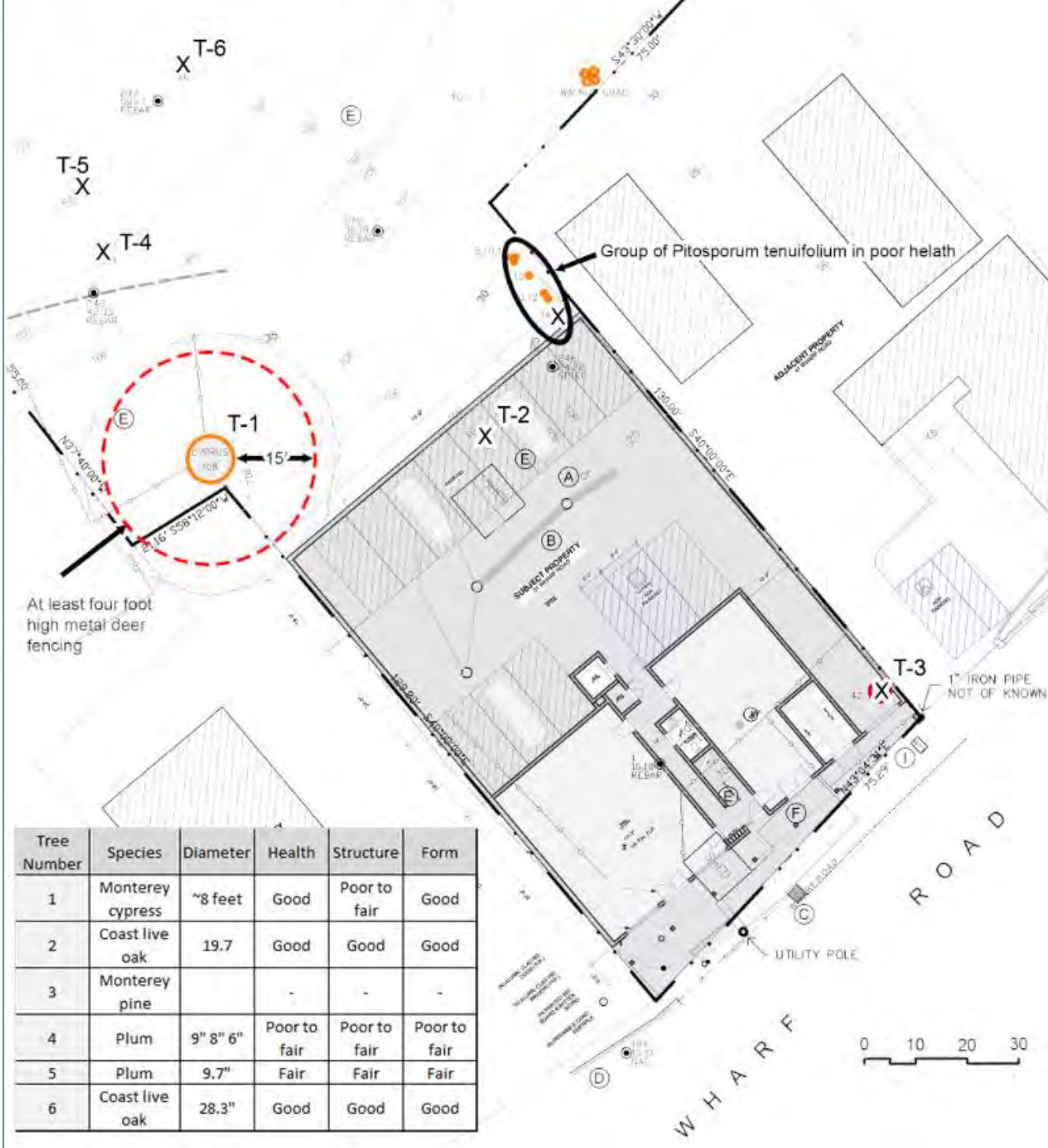


Figure 1. Map of tree location and location of fencing for Tree 1

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Urban Forestry Associates, Inc.
Wharf Road Parcel Development Tree Impact Assessment

December 11, 2019

GLOSSARY

Crown reduction - A crown reduction is a type of pruning where the overall size of the canopy is reduced by removing mostly small diameter wood (typically less than 3") from the ends of branches as well as deadwood. Interior foliage is left mostly untouched. It is essentially the opposite of "lion's tailing" a tree, which is a common practice of uneducated tree companies where branches are stripped of interior foliage and left with a tuft of leaves at the end. Lion's tailing has a big visual impact and is relatively quick and easy to perform but is counter to ISA Best Management Practices and actually increases the risk of branch failure. Crown reductions are more difficult, take longer and have less of a dramatic visual impact, but are much better for the tree and at reducing risk for anything under the canopy.

Form - The plant's overall appearance as it relates to its shape or silhouette. Can be negatively affected by crown asymmetries.

Structure - Overall stability of the tree or its branches. This can be negatively affected by things such as acute angle crotches, decay cavities, strong leans, stem girdling roots, ambrosia beetles, history of failures, etc.

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BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-061-03

bolinas community land trust
6 Wharf Road
bolinas, ca 94924

client:

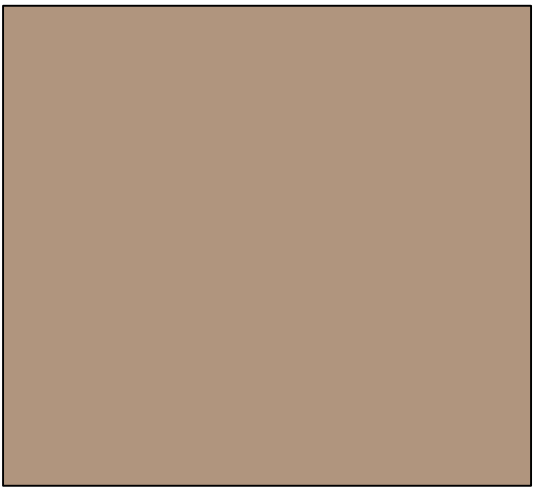
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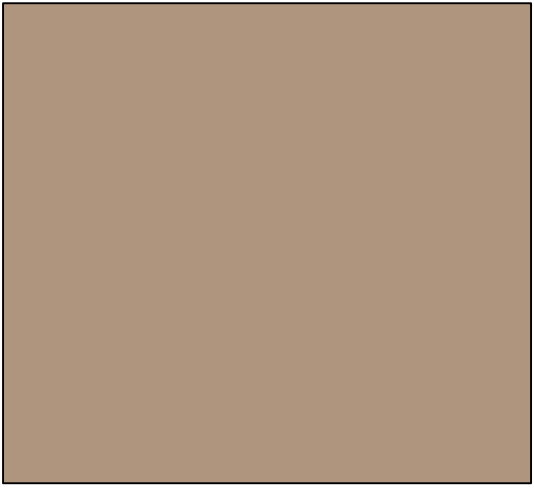
JOHN LUM ARCHITECTURE
3246 SEVENTEEN STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.9554

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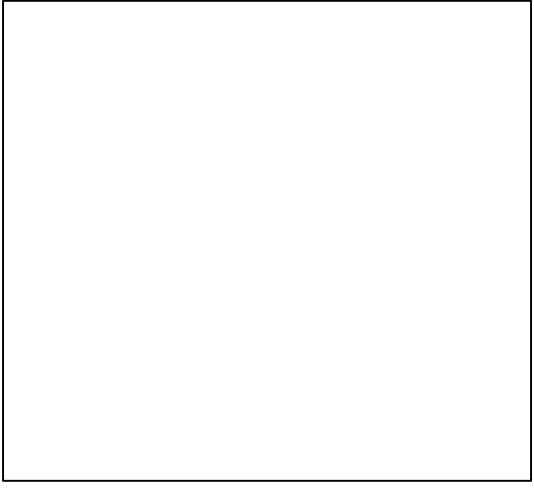
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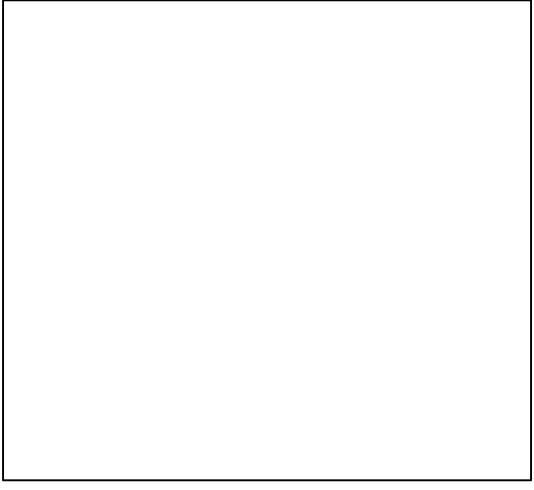
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MATERIAL: VERTICAL GRAIN WESTERN RED CEDAR - 5 1/2"
COLOR: NATURAL



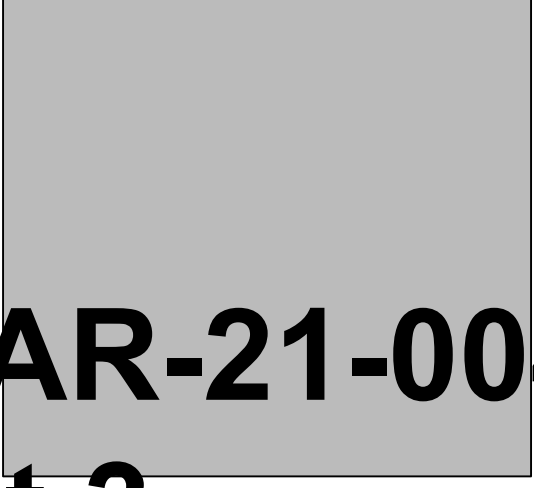
M2 - TRIM / RAILINGS / POSTS
MATERIAL: WESTERN RED CEDAR
COLOR: NATURAL



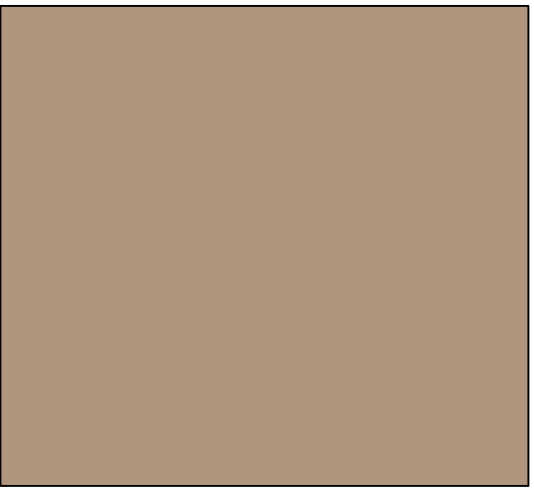
M3 - WINDOW & DOOR TRIM
MATERIAL: PTD. 1X3 CEDAR
COLOR: WHITE



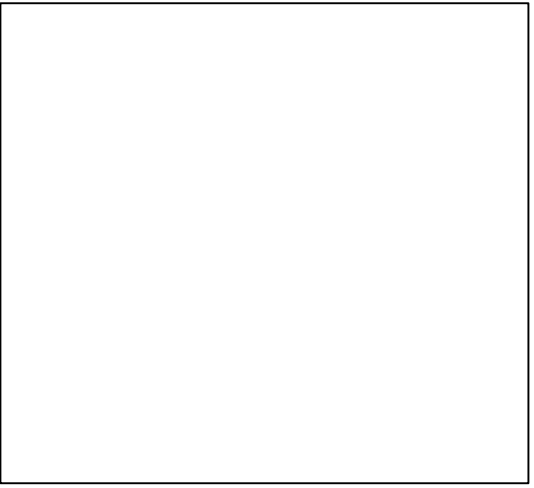
M4 - HORIZONTAL SIDING
MATERIAL: PAINTED FIBER-CEMENT - 5 1/2"
COLOR: PTD. WHITE
MFR: ARTISAN ASPYRE COLLECTION BY JAMES HARDIE



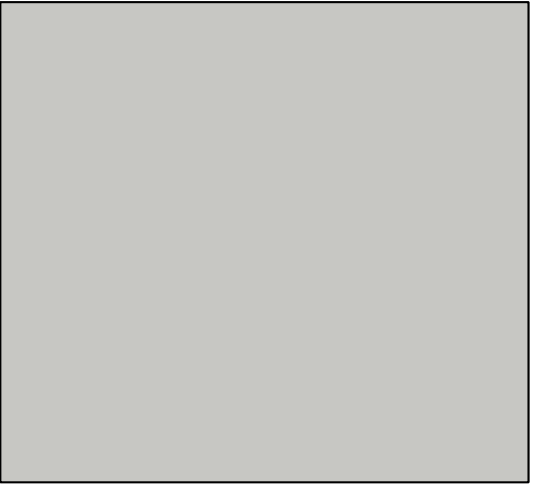
M5 - RETAINING WALLS
MATERIAL: BOARD-FORMED CONCRETE
COLOR: GRAY



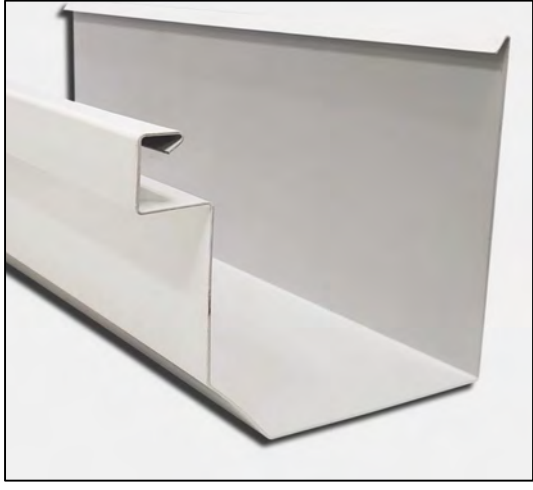
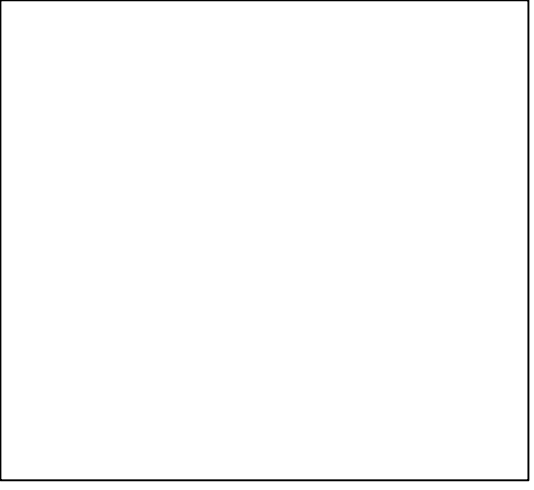
M6 - WINDOWS & DOORS
MATERIAL: ALUMINUM CLAD WOOD
COLOR: LIGHT BROWN (EXT.), NATURAL (INT.)
MFR: MARVIN ULTIMATE



M7 - WINDOWS & DOORS
MATERIAL: ALUMINUM CLAD WOOD
COLOR: WHITE (EXT.), NATURAL (INT.)
MFR: MARVIN ULTIMATE



M8 - ROOFING
MATERIAL: STANDING SEAM METAL 18" O.C.
COLOR: GALVALUME / GRAY
MFR: CUSTOM-BILT METALS

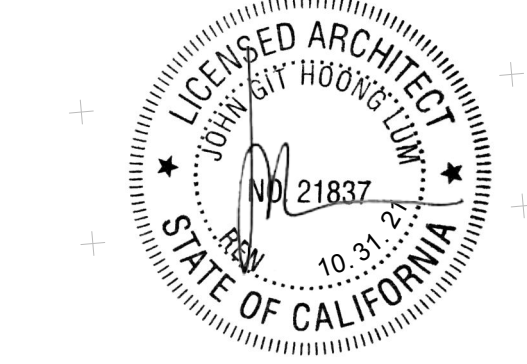


M9 - GUTTERS
MATERIAL: GALVANIZED SHEET METAL, SQUARE PROFILE
COLOR: GRAY

LIGHTING FIXTURES



L1 - WALL-MTD. GOOSENECK WALL FIXTURE
MAKE/MODEL: REJUVINATION - CARSON
MATERIAL: PTD. ALUMINUM



BCLT - DOWNTOWN BOLINAS

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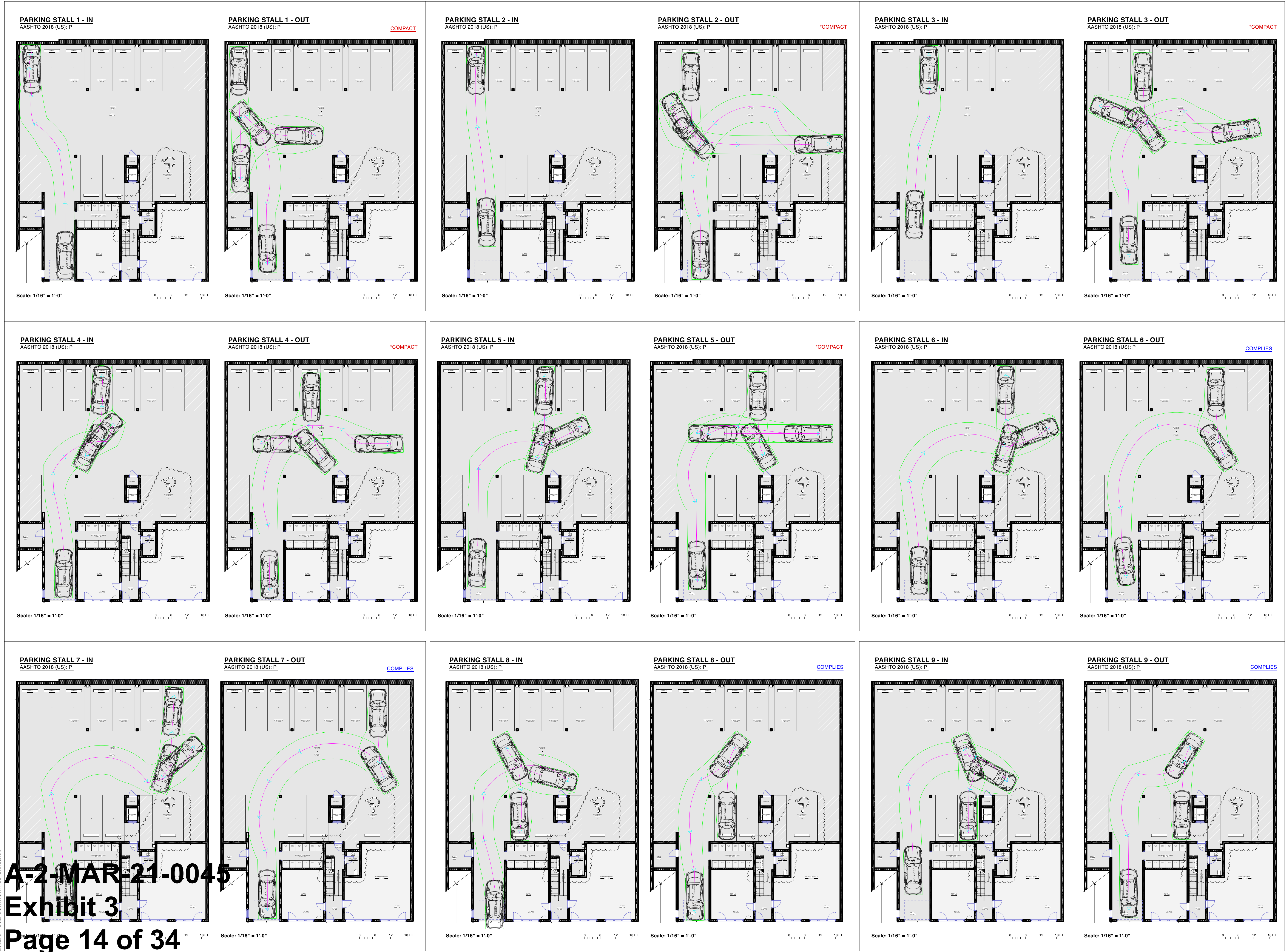
bolinas community/land trust
6 Wharf Road
bolinas, ca 94924

client:

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06.10.20	Planning Pre-App Review	hm
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MATERIAL BOARD

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A-2-MAR-21-0045
Exhibit 3
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BCLT - DOWNTOWN BOLINAS

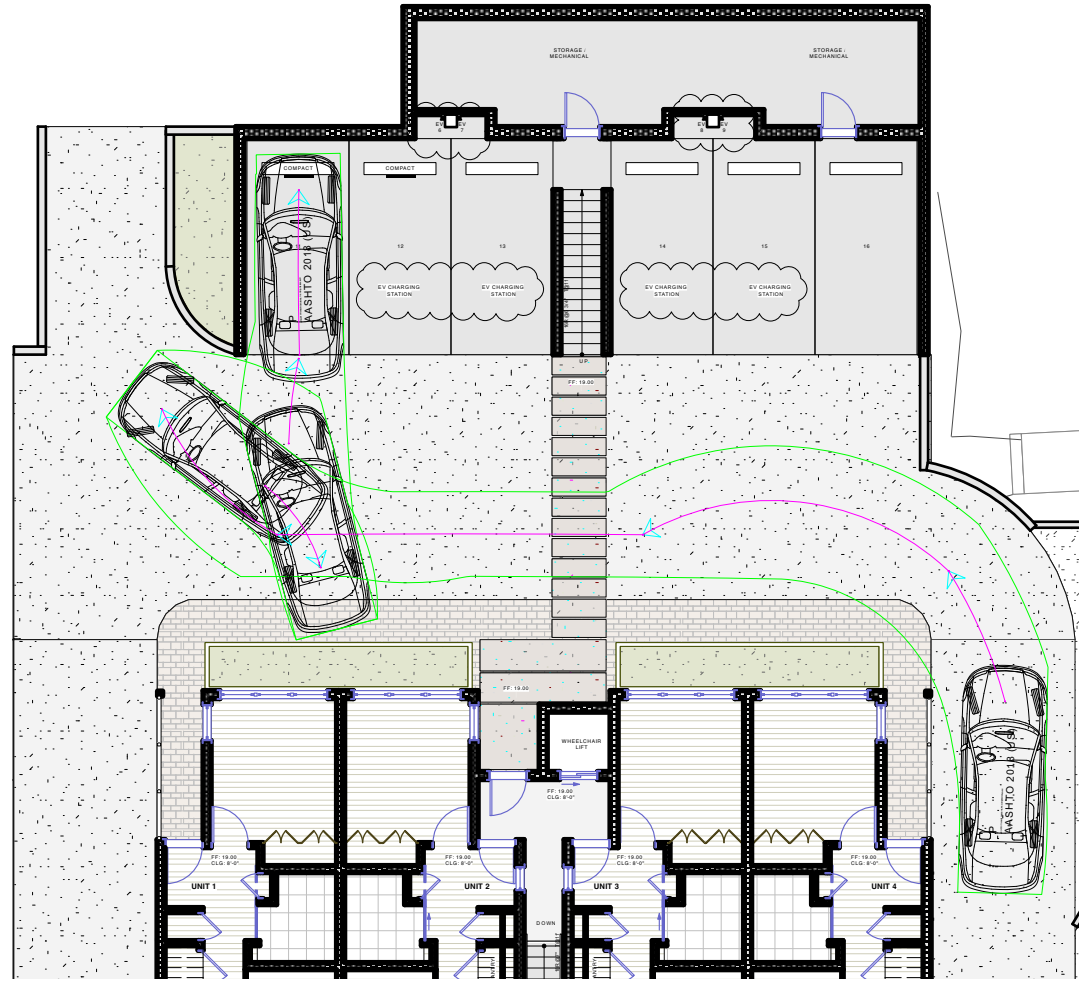
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02.01.21	Planning Submittal update	di

PARKING DIAGRAM
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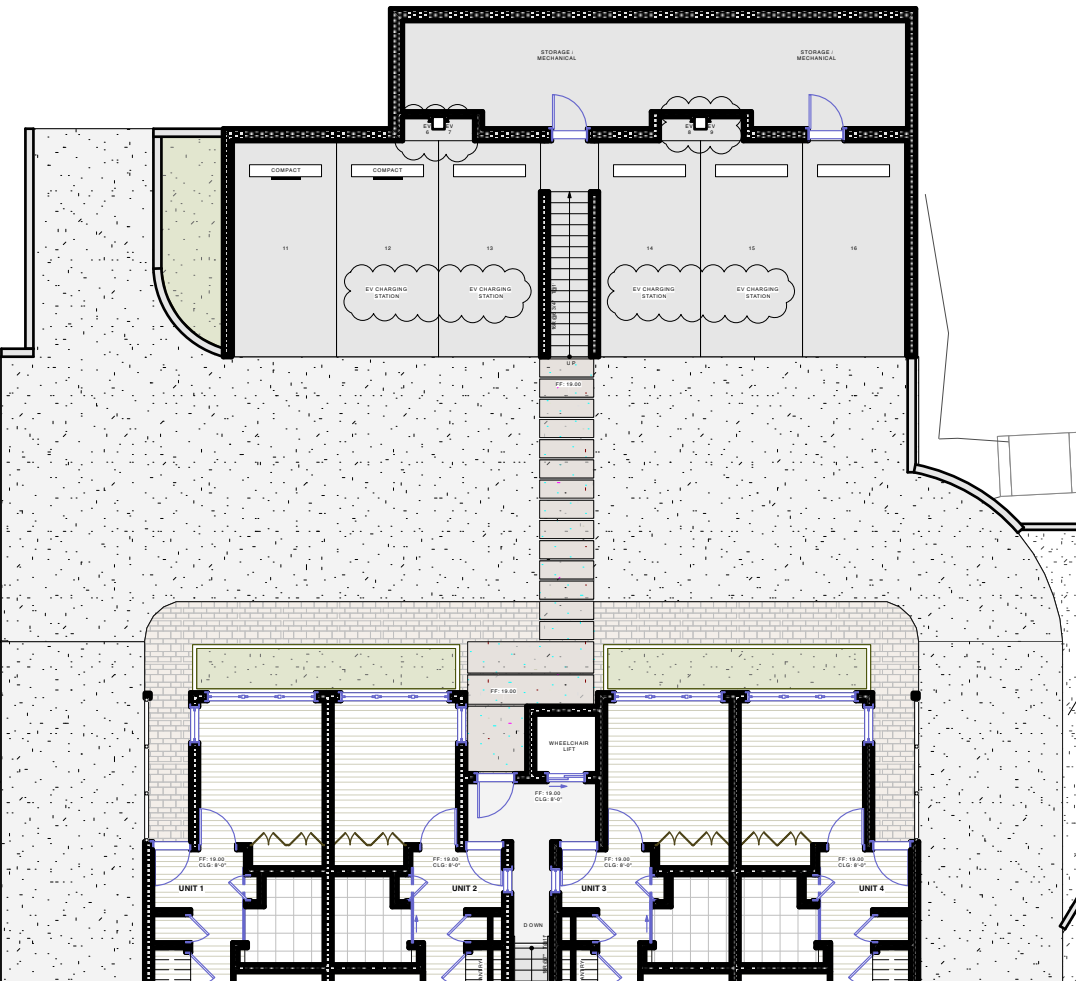
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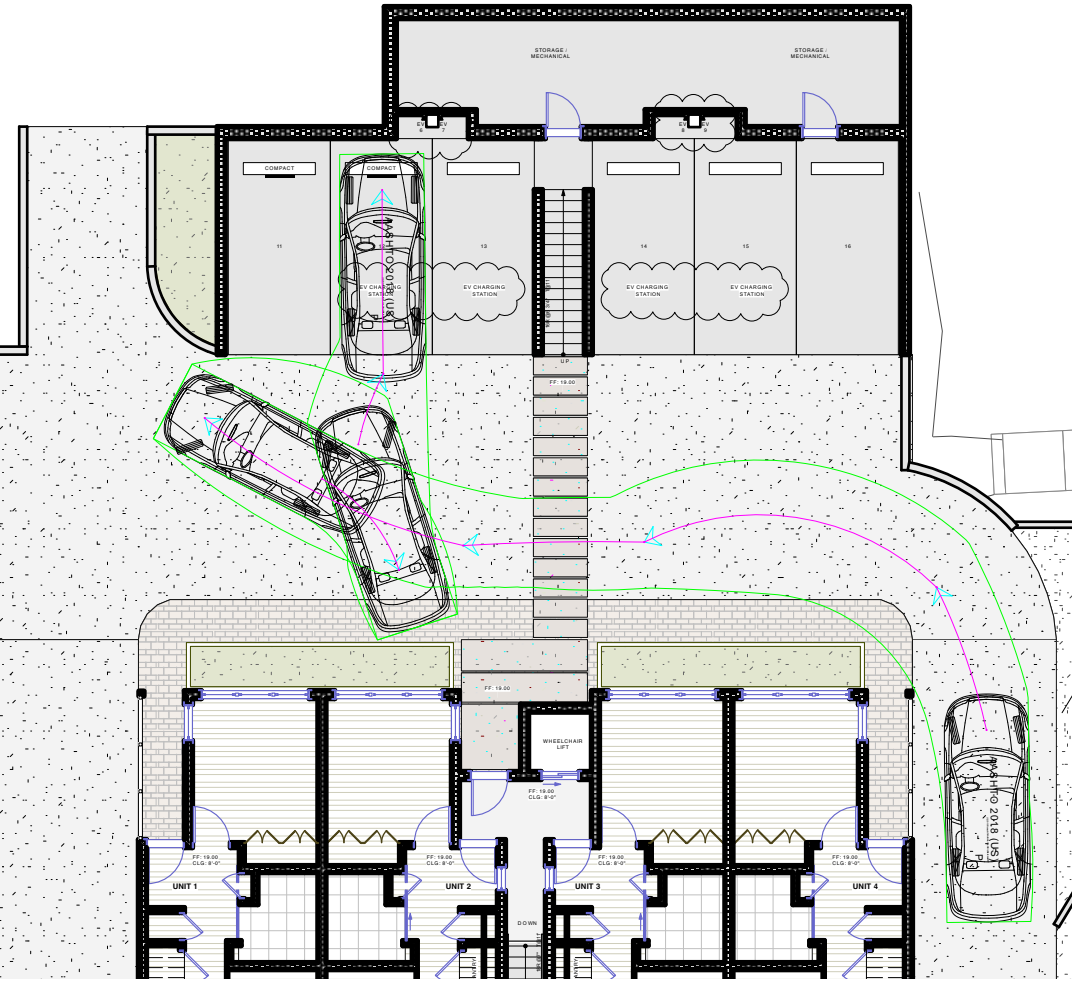
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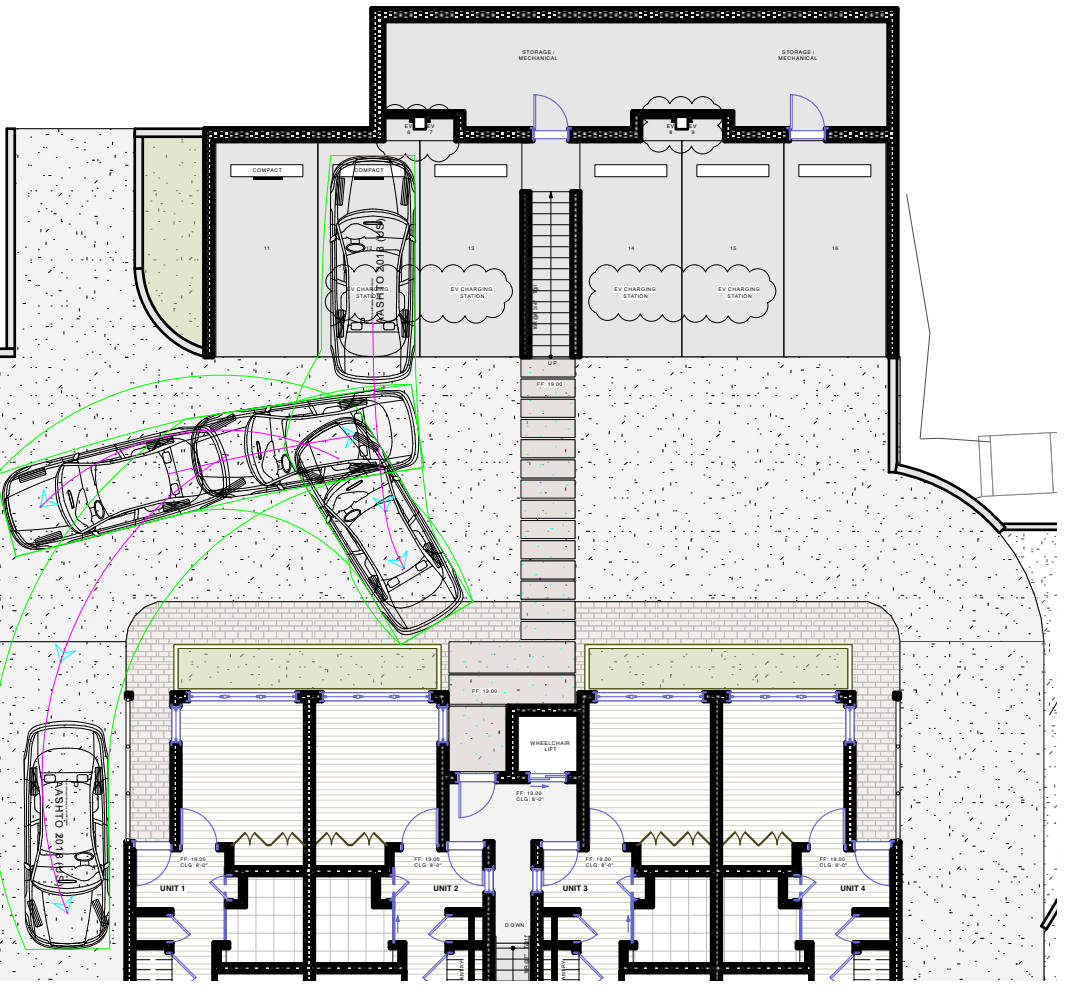
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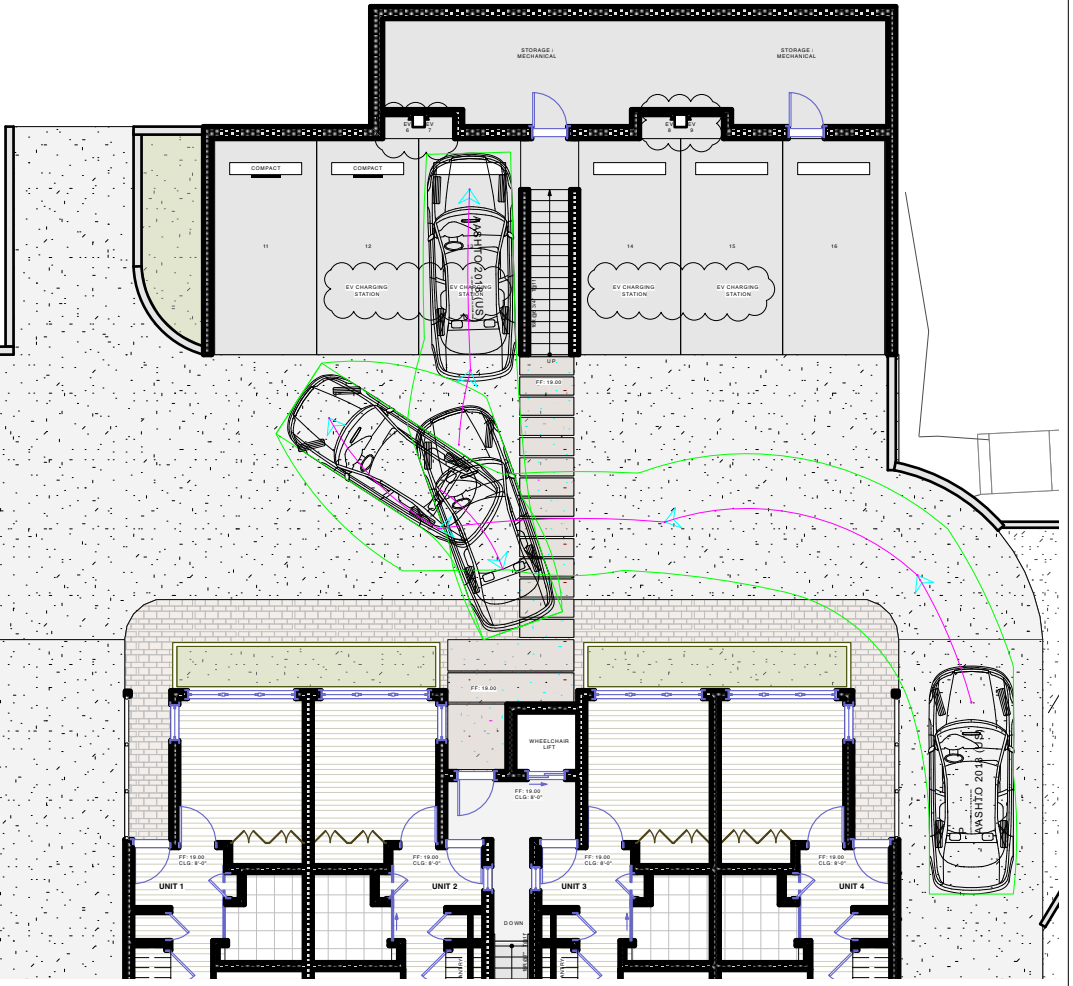
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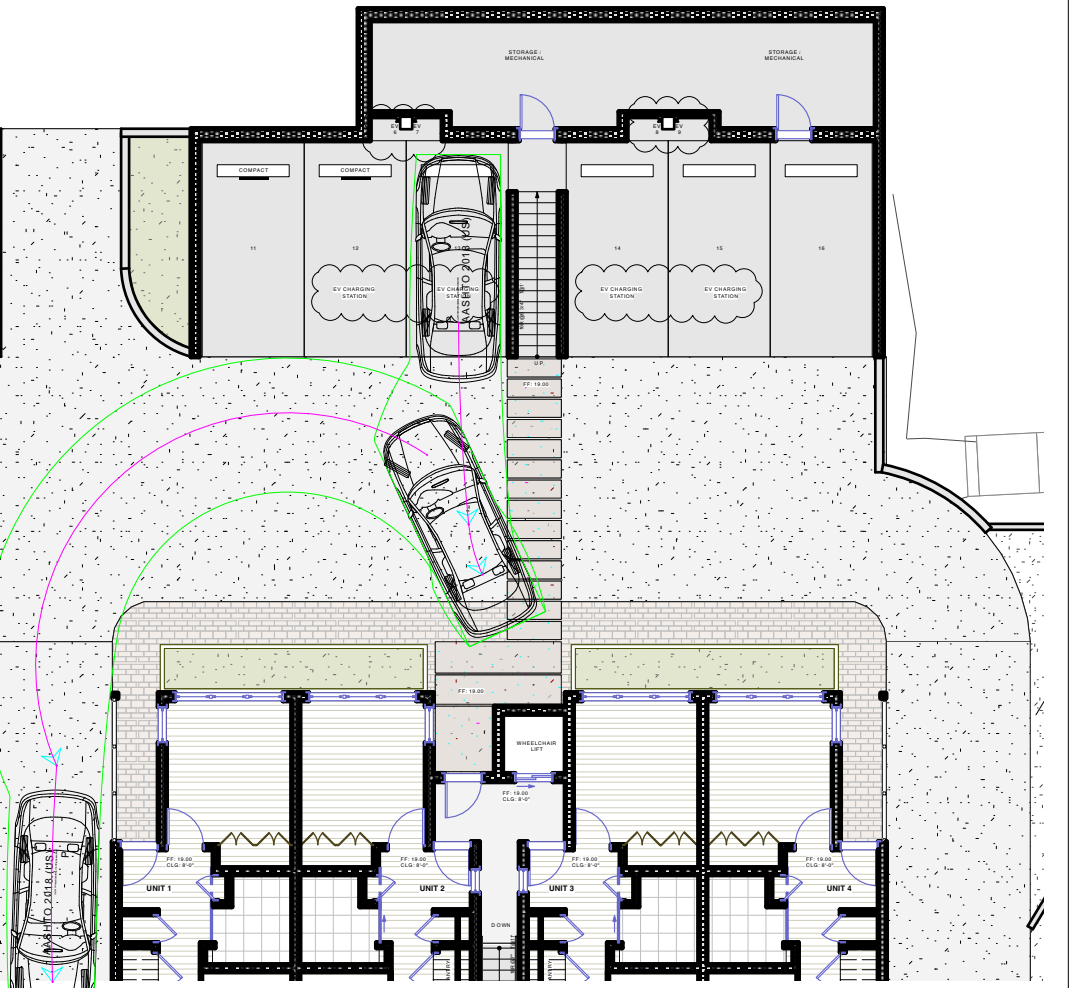
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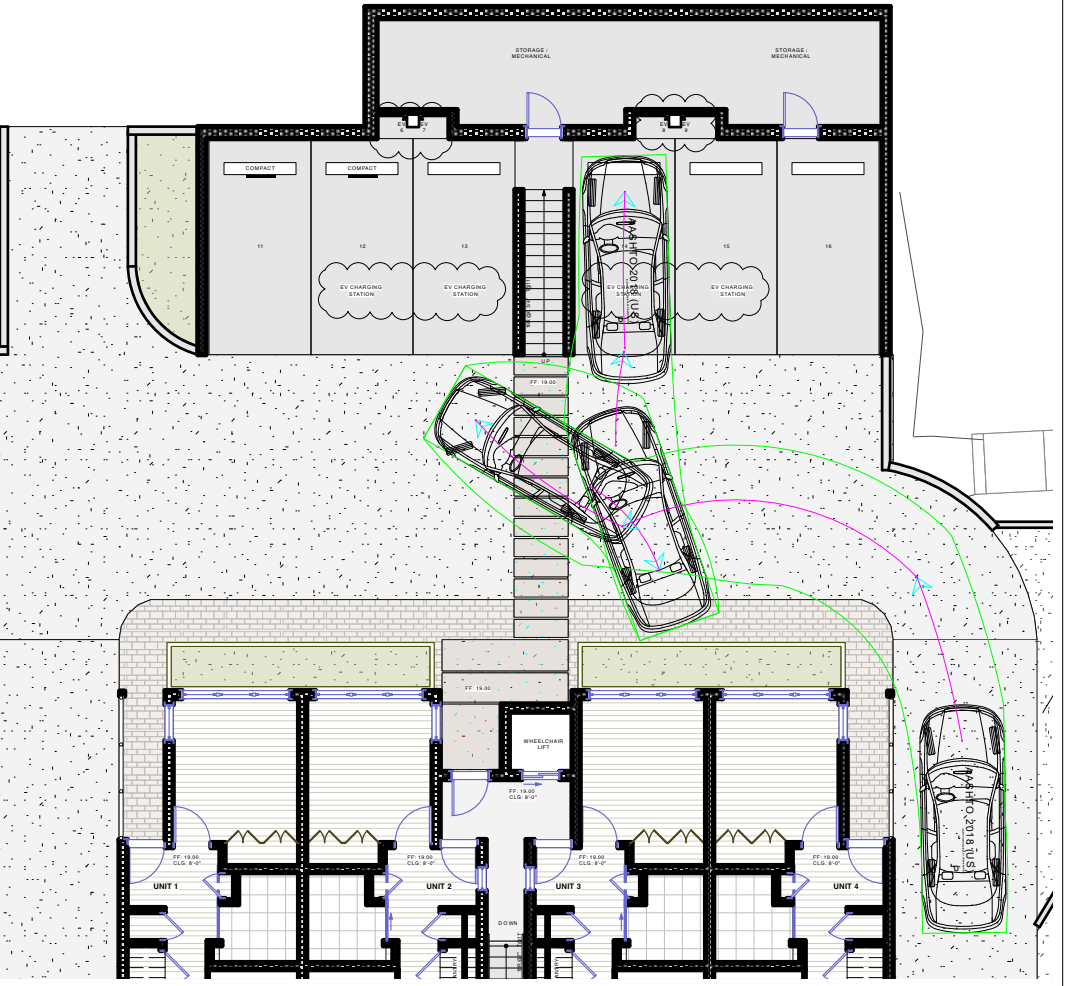
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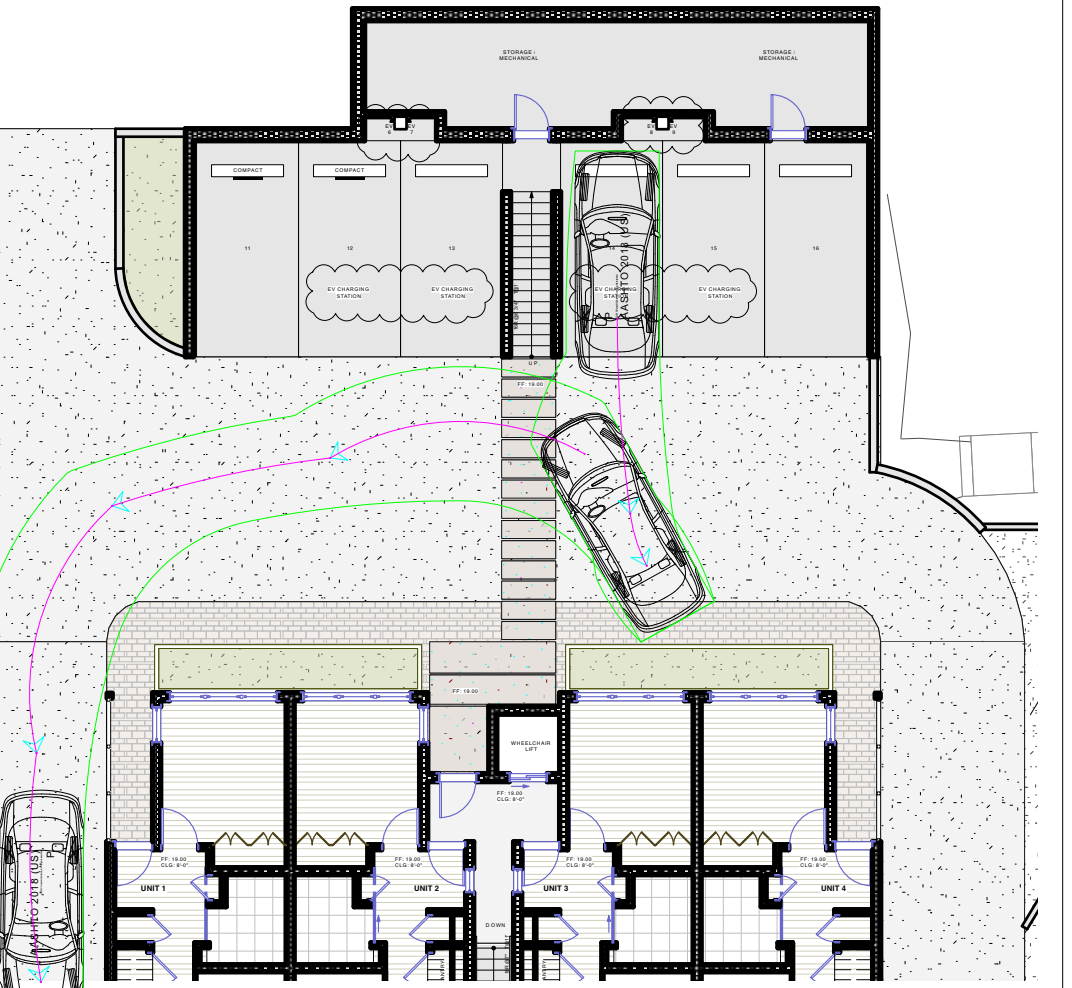
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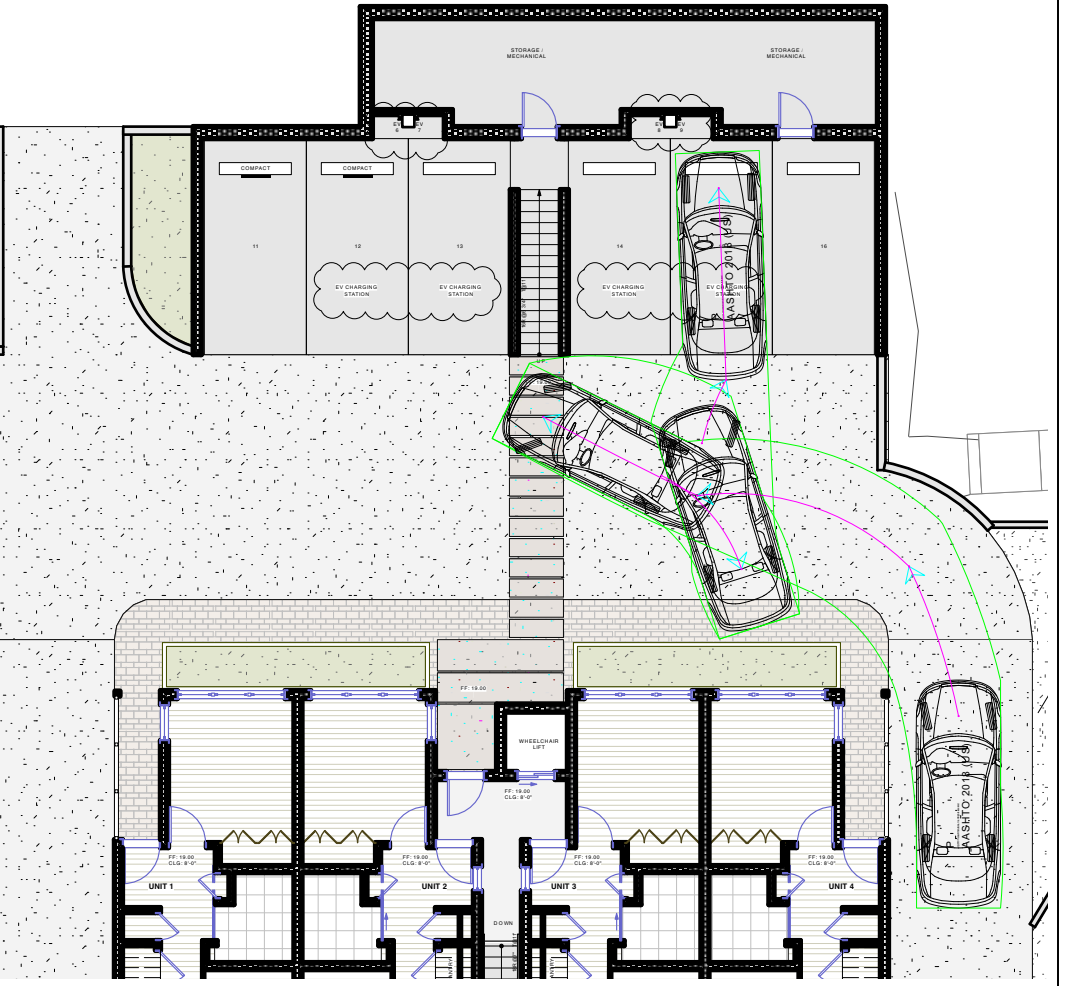
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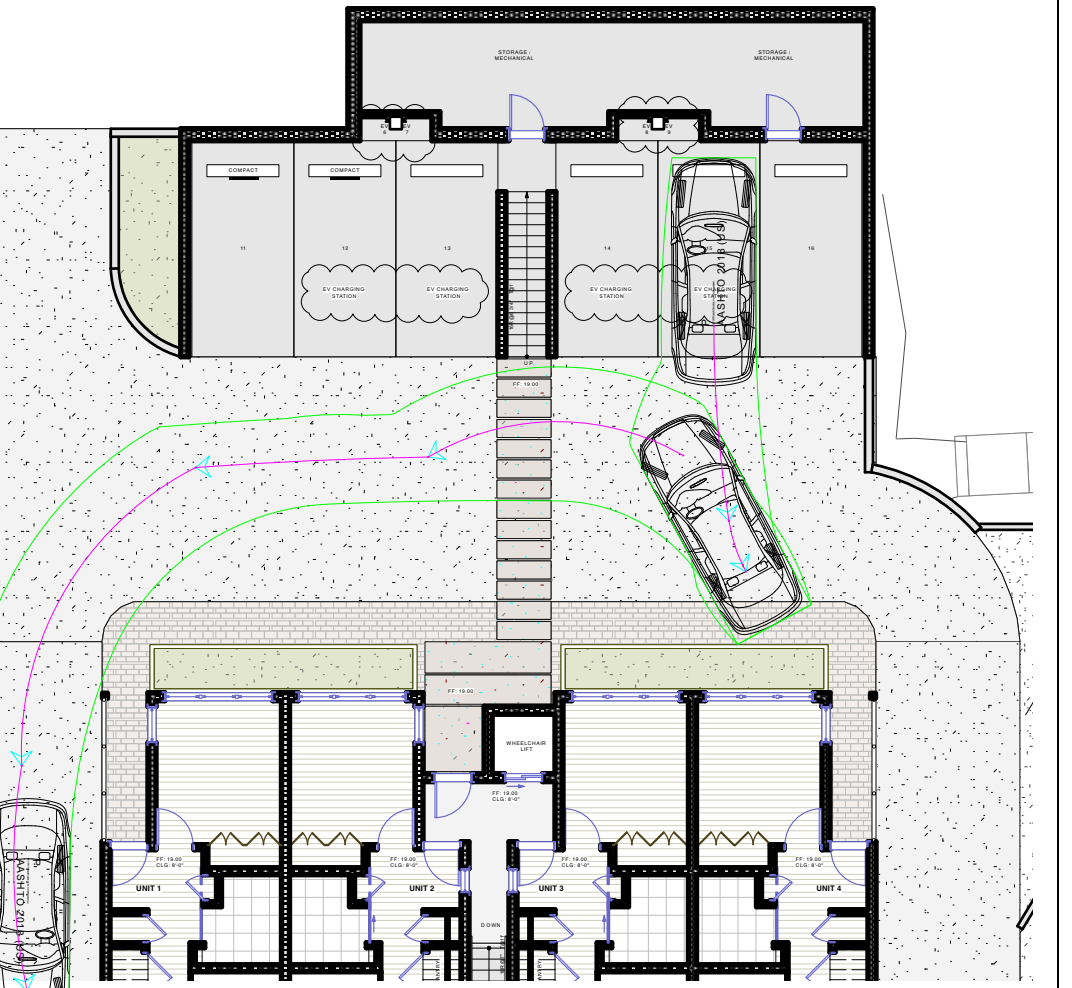
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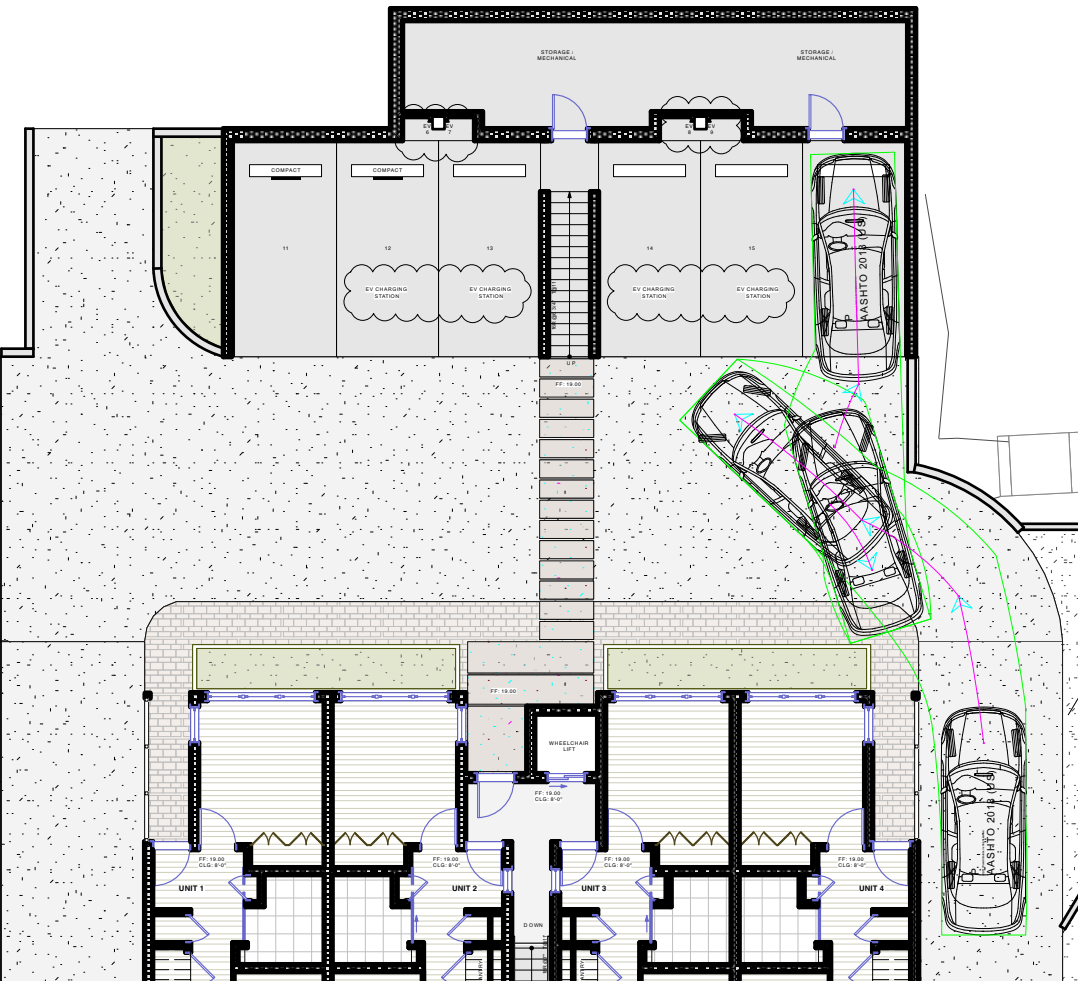
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COMPLIES



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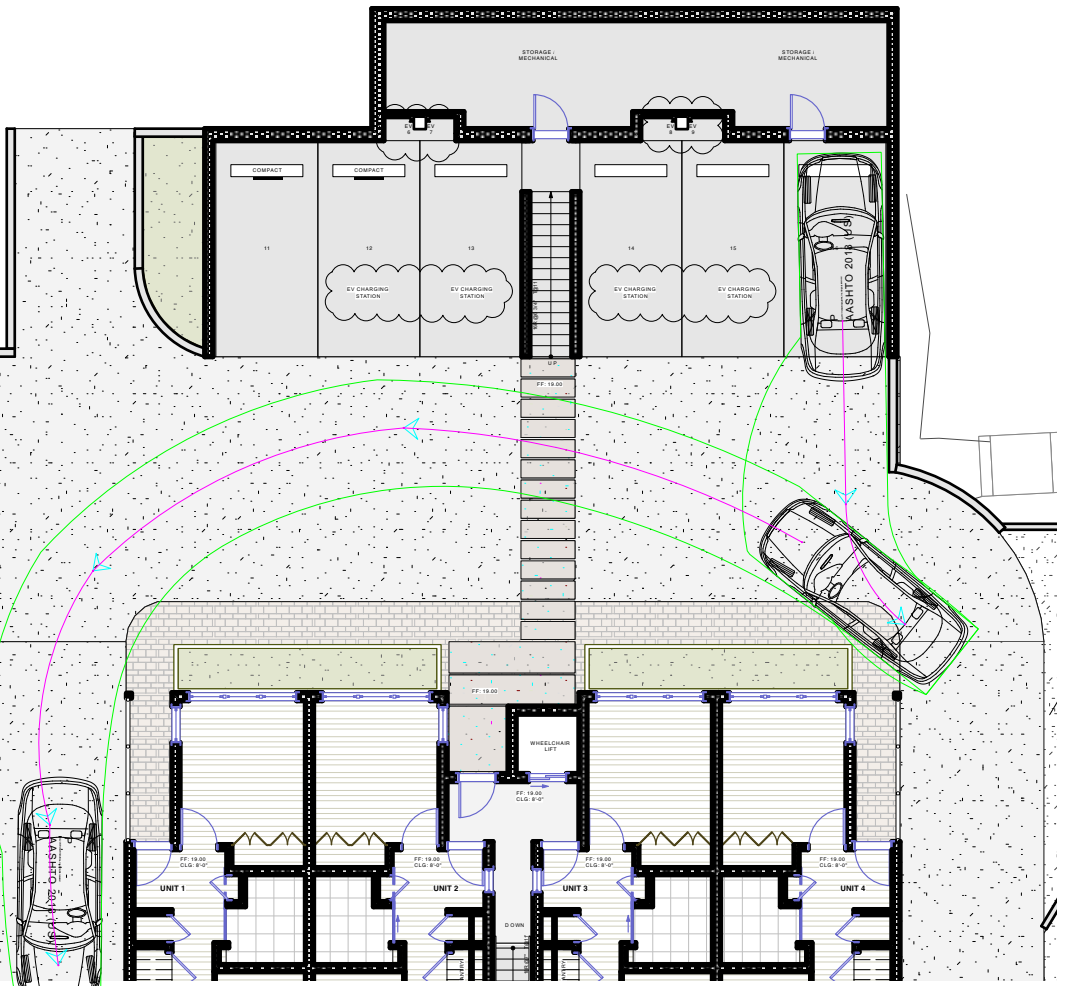
PARKING STALL 16 - IN
AASHTO 2018 (US): P



Scale: 1/16" = 1'-0"

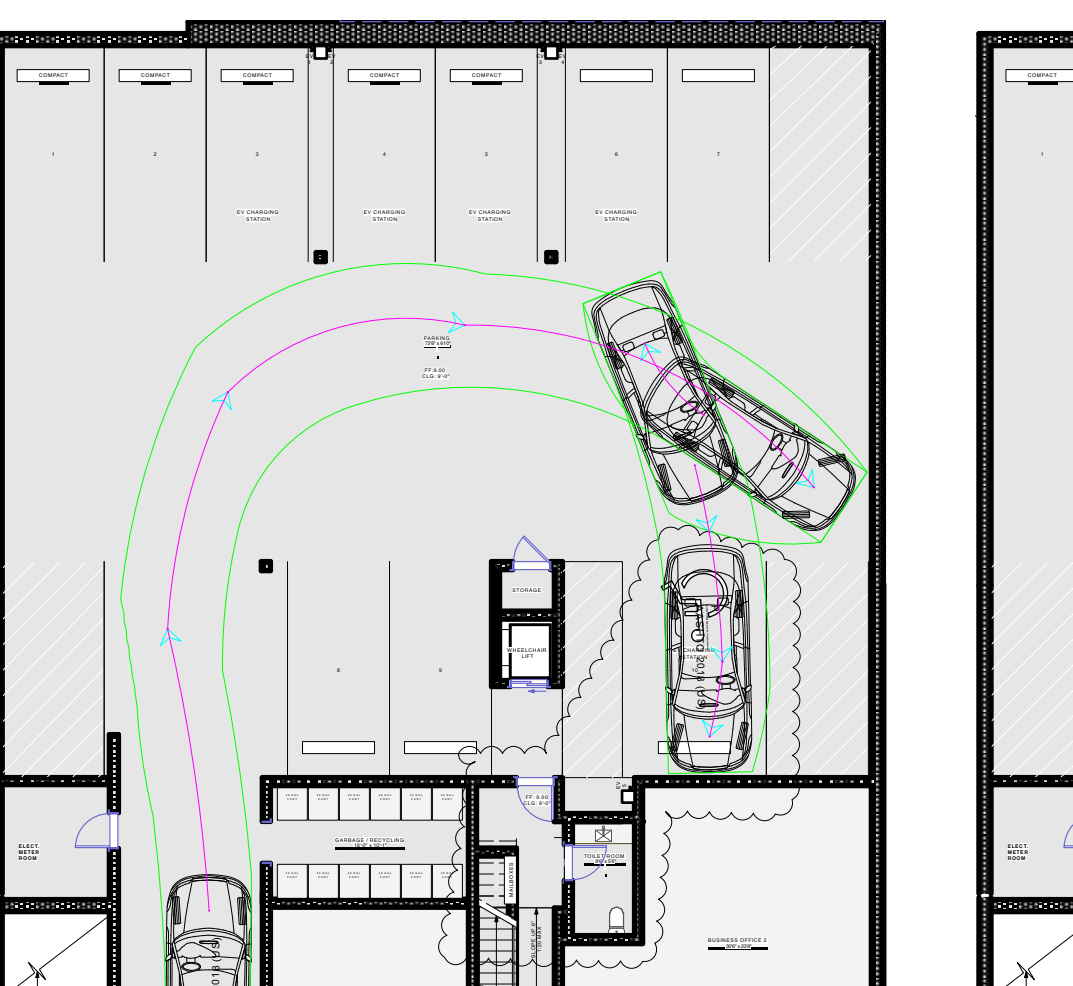
PARKING STALL 16 - OUT
AASHTO 2018 (US): P

COMPLIES



Scale: 1/16" = 1'-0"

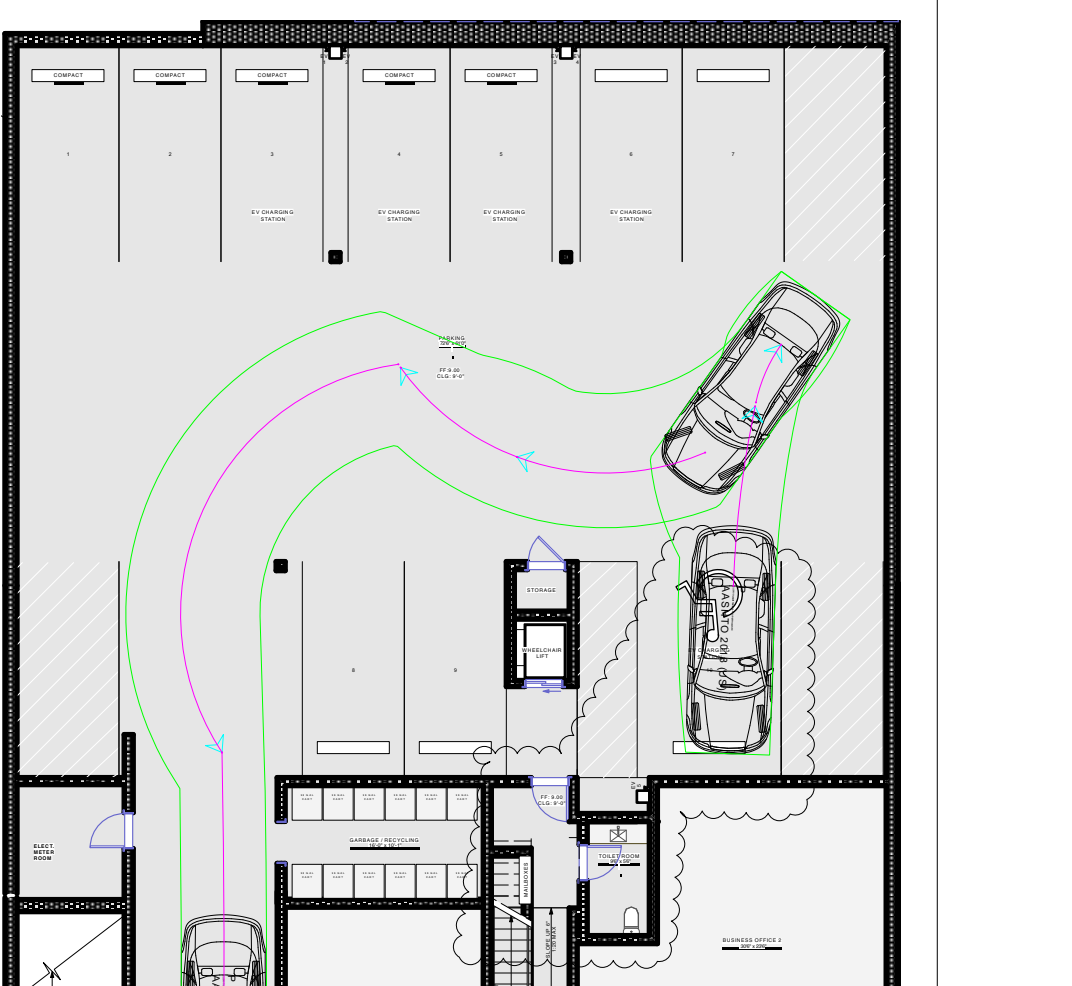
PARKING STALL 10 (ACCESSIBLE VAN SPACE) - IN
AASHTO 2018 (US): P



Scale: 1/16" = 1'-0"

PARKING STALL 10 (ACCESSIBLE VAN SPACE) - OUT
AASHTO 2018 (US): P

COMPLIES



Scale: 1/16" = 1'-0"

AASHTO 2018 (US): P VEHICLE DIMENSIONS

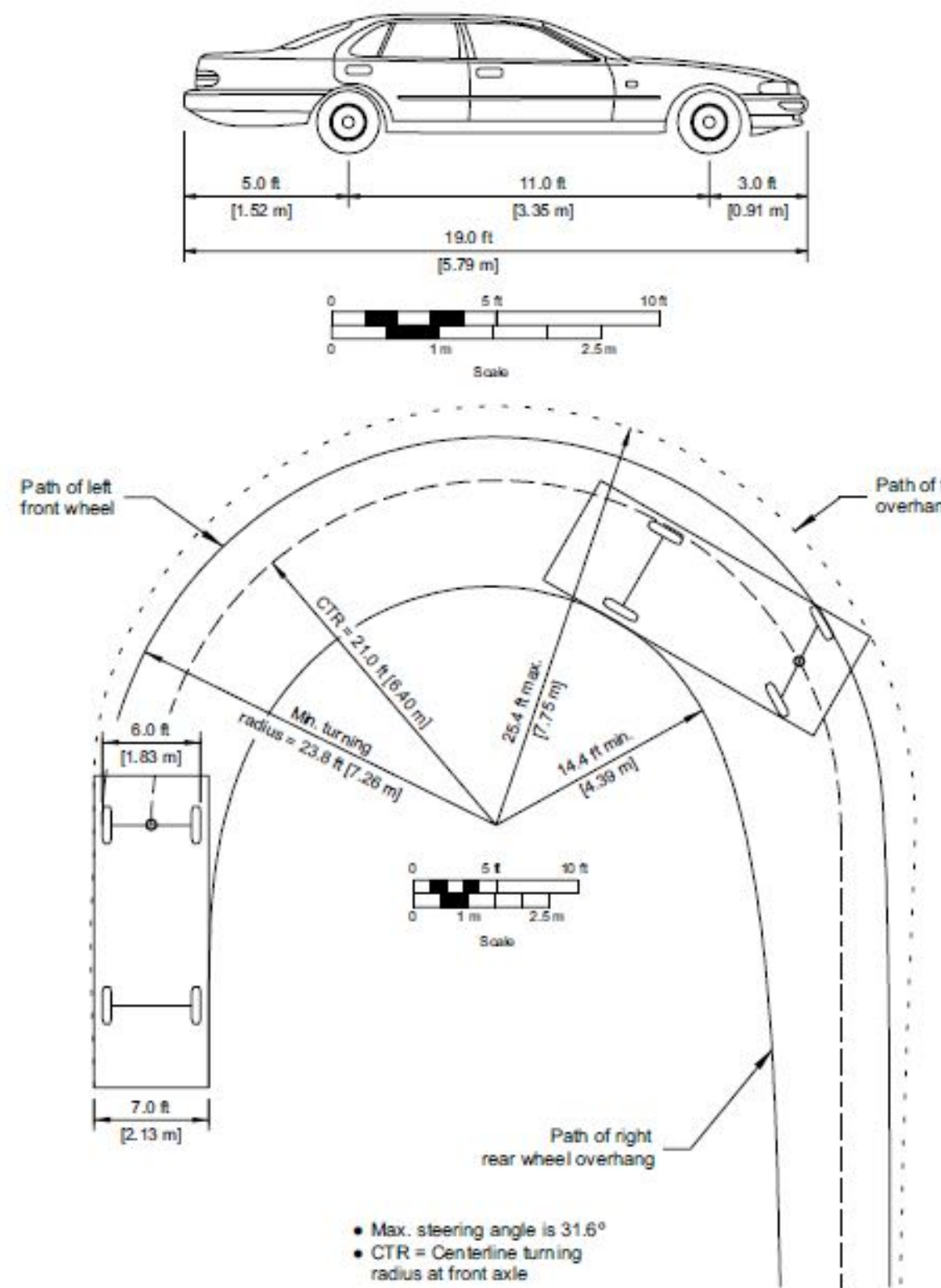
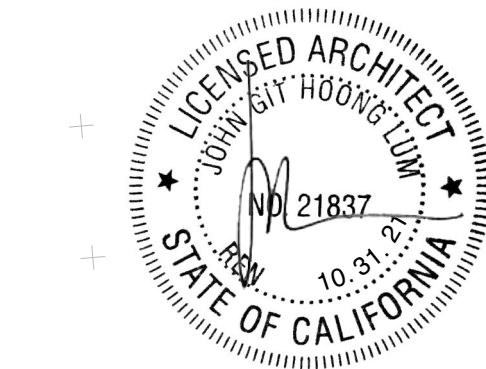


Figure 2-10. Minimum Turning Path for Passenger Car (P) Design Vehicle



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CALIFORNIA
APN: 183-061-03

bolinas community/land trust
6 Wharf Road
bolinas, ca 94924

client:

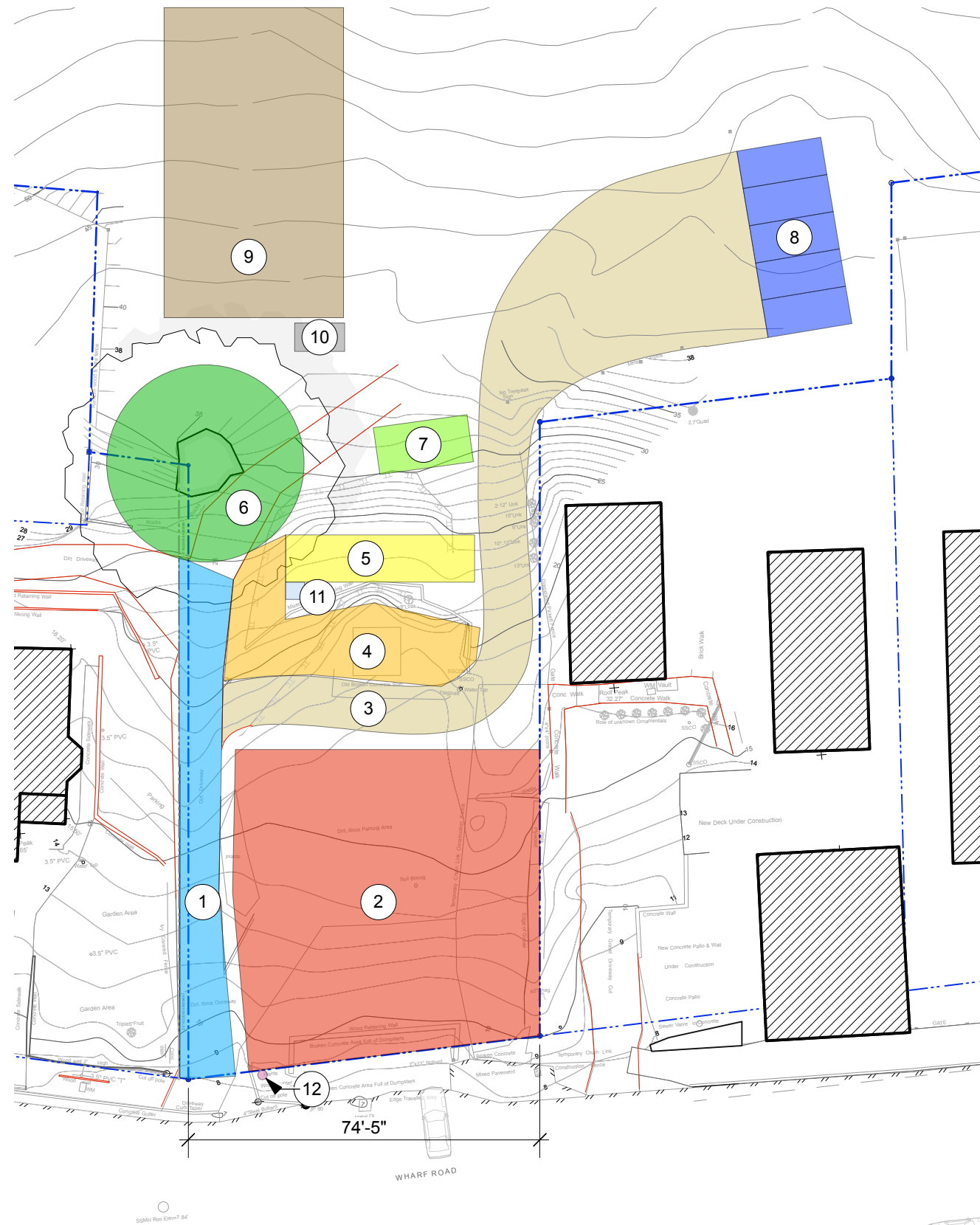
PARKING DIAGRAMS

A0.14

filename: BCLT-DOWNTOWN_DD_201202_2019_vwx

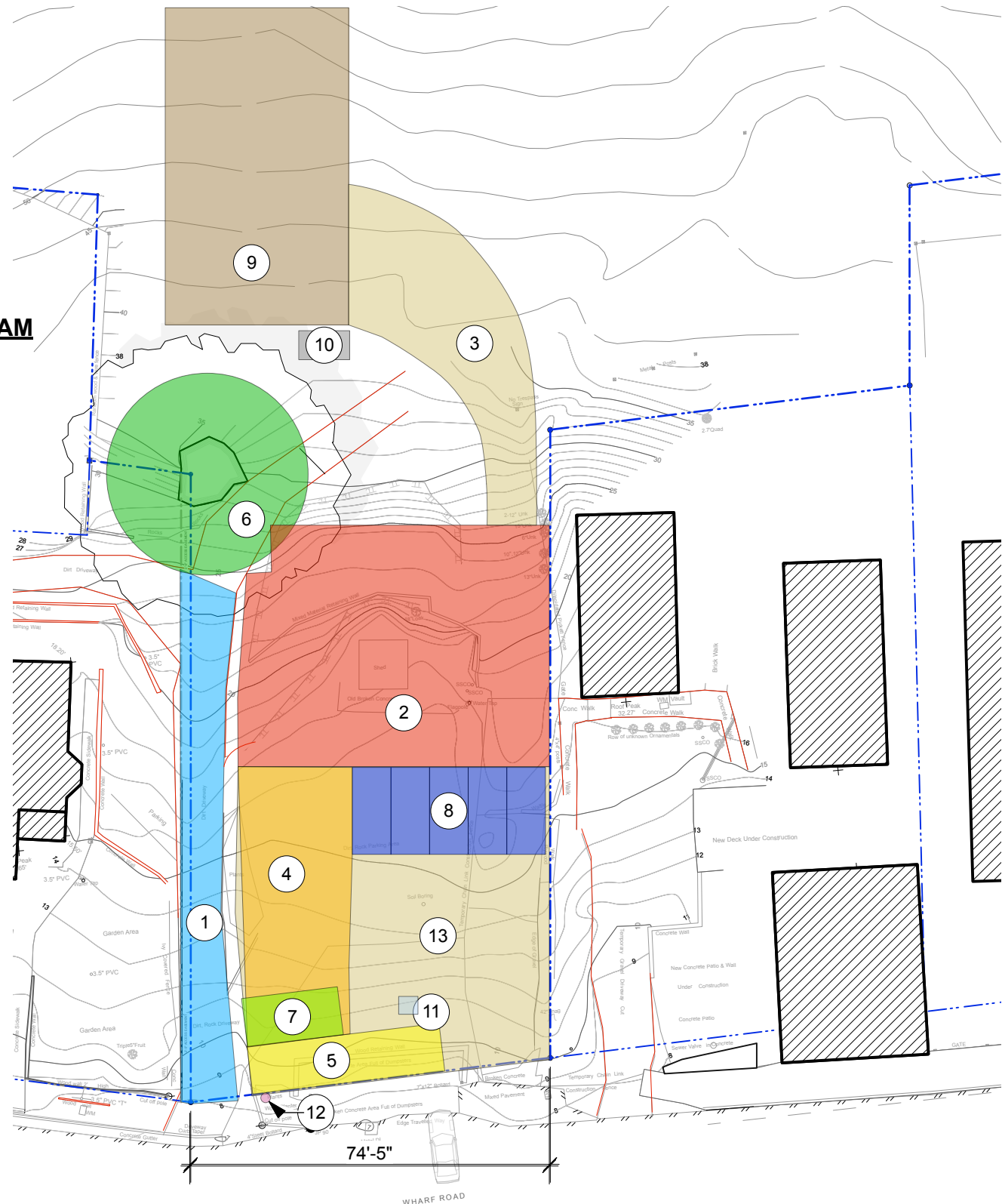
PHASE 1 CONSTRUCTION PROGRAM

- SITE PLAN**
- 1) EXISTING DIRT DRIVEWAY TO REMAIN
 - 2) AREA OF INITIAL EXCAVATION
 - 3) NEW GRADED DRIVEWAY AND PARKING AREA
 - 4) BUILDING MATERIAL STORAGE AREA
 - 5) CONSTRUCTION OFFICE TRAILER
 - 6) TREE PROTECTION ZONE
 - 7) SECURE STORAGE TRAILER
 - 8) WORKER PARKING
 - 9) SOIL STOCKPILE AREA
 - 10) AREA FOR POWER GENERATORS (IF NEEDED)
 - 11) PORTABLE TOILET
 - 12) TEMPORARY POWER POLE
- DUST REDUCTION**
CONSISTENT WITH THE BAY AIR QUALITY MANAGEMENT DISTRICT'S BASIC CONTROL MEASURES
- EROSION CONTROL AND/OR STORM WATER POLLUTION PREVENTION PLAN**
SEE PLANS ON SHEET C4 AND C5
- TREE PROTECTION**
REFER TO ARBORIST'S REPORT ON SHEET A0.11
- CONSTRUCTION SCHEDULING**
•DELIVERIES TO SITE AND OPERATION OF HEAVY MACHINERY TO OCCUR WEEKDAYS BETWEEN 8:00 AM AND 5:00 PM, AND WEEKENDS 9:00 AM AND 5:00 PM.
•EXCAVATION TO BE DONE IN ACCORDANCE WITH MARIN CO. EXCAVATION GUIDELINES.



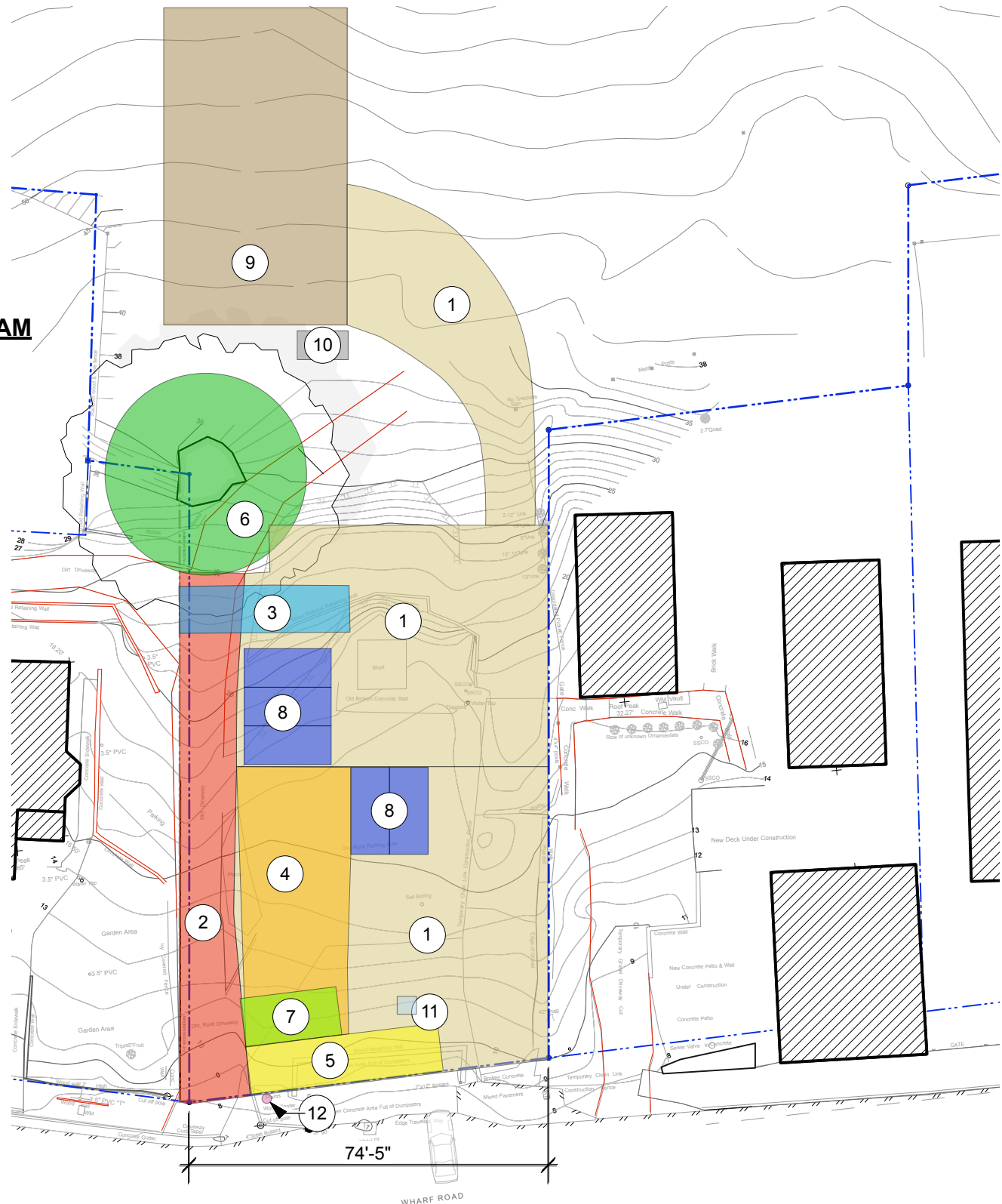
PHASE 2 CONSTRUCTION PROGRAM

- SITE PLAN**
- 1) EXISTING DIRT DRIVEWAY TO REMAIN
 - 2) AREA OF SECOND EXCAVATION
 - 3) NEW GRADED DRIVEWAY
 - 4) BUILDING MATERIAL STORAGE AREA
 - 5) CONSTRUCTION OFFICE TRAILER
 - 6) TREE PROTECTION ZONE
 - 7) SECURE STORAGE TRAILER
 - 8) WORKER PARKING
 - 9) SOIL STOCKPILE AREA
 - 10) AREA FOR POWER GENERATORS (IF NEEDED)
 - 11) PORTABLE TOILET
 - 12) TEMPORARY POWER POLE
 - 13) GRADED AREA
- DUST REDUCTION**
CONSISTENT WITH THE BAY AIR QUALITY MANAGEMENT DISTRICT'S BASIC CONTROL MEASURES
- EROSION CONTROL AND/OR STORM WATER POLLUTION PREVENTION PLAN**
SEE PLANS ON SHEET C4 AND C5
- TREE PROTECTION**
REFER TO ARBORIST'S REPORT ON SHEET A0.11
- CONSTRUCTION SCHEDULING**
•DELIVERIES TO SITE AND OPERATION OF HEAVY MACHINERY TO OCCUR WEEKDAYS BETWEEN 8:00 AM AND 5:00 PM, AND WEEKENDS 9:00 AM AND 5:00 PM.
•EXCAVATION TO BE DONE IN ACCORDANCE WITH MARIN CO. EXCAVATION GUIDELINES.



PHASE 3 CONSTRUCTION PROGRAM

- SITE PLAN**
- 1) GRADED AREA
 - 2) AREA OF THIRD EXCAVATION
 - 3) NEW GRADED TEMPORARY DRIVEWAY
 - 4) BUILDING MATERIAL STORAGE AREA
 - 5) CONSTRUCTION OFFICE TRAILER
 - 6) TREE PROTECTION ZONE
 - 7) SECURE STORAGE TRAILER
 - 8) WORKER PARKING
 - 9) SOIL STOCKPILE AREA
 - 10) AREA FOR POWER GENERATORS (IF NEEDED)
 - 11) PORTABLE TOILET
 - 12) TEMPORARY POWER POLE
- DUST REDUCTION**
CONSISTENT WITH THE BAY AIR QUALITY MANAGEMENT DISTRICT'S BASIC CONTROL MEASURES
- EROSION CONTROL AND/OR STORM WATER POLLUTION PREVENTION PLAN**
SEE PLANS ON SHEET C4 AND C5
- TREE PROTECTION**
REFER TO ARBORIST'S REPORT ON SHEET A0.11
- CONSTRUCTION SCHEDULING**
•DELIVERIES TO SITE AND OPERATION OF HEAVY MACHINERY TO OCCUR WEEKDAYS BETWEEN 8:00 AM AND 5:00 PM, AND WEEKENDS 9:00 AM AND 5:00 PM.
•EXCAVATION TO BE DONE IN ACCORDANCE WITH MARIN CO. EXCAVATION GUIDELINES.



OPERATIONAL CHARACTERISTICS (FOR NON-RESIDENTIAL USES)

1) USE OF NON-RESIDENTIAL SPACES:	RETAIL 1	BUSINESS OFFICE 2
2) MAX. NUMBER OF STAFF ON SITE	2	3
3) HOURS OF OPERATION	9:00 AM-6:00 PM	9:00 AM-6:00 PM
4) PEAK HOURS OF OPERATION	10:00 AM-4:00 PM	10:00 AM-4:00 PM
5) NUMBER OF STAFF, CUSTOMERS, AND VISITORS DURING PEAK HOURS OF OPERATION	4	6
6) SCHEDULE , FREQUENCY, AND NATURE OF DELIVERIES	ONCE DAILY, SMALL PACKAGES	ONCE DAILY, SMALL PACKAGES
7) NOISE LEVELS ASSOCIATED WITH BUSINESS OPERATION	NEGLIGIBLE	NEGLIGIBLE
8) PATH OF TRAVEL FOR PEDESTRIANS AND VEHICLES	SEE A0.04	SEE A0.04

BCLT - DOWNTOWN BOLINAS

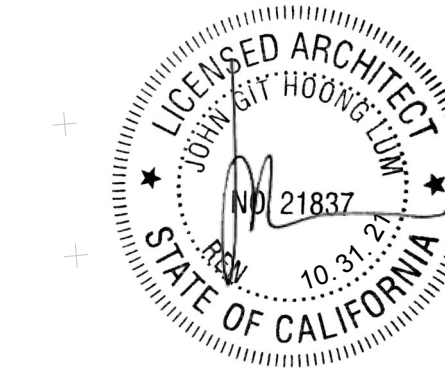
31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-061-03

client:

bolinas community land trust
6 Wharf Road
bolinas, CA 94924

CONSTRUCTION MGMT. PLAN

A0.15



JOHN LUM ARCHITECTURE
3246 SEVENTEENTH STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.9554

GENERAL NOTES

CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNERS AND DESIGN ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER AND DESIGN ENGINEER.

ALL MATERIAL, WORKMANSHIP, AND CONSTRUCTION SHALL CONFORM TO THE STATE OF CALIFORNIA STANDARD SPECIFICATIONS AND STANDARD PLANS (DATED JULY 1992) AND THE CURRENT MARIN COUNTY UNIFORM CONSTRUCTION STANDARDS AND SPECIFICATIONS.

CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING UNDERGROUND SERVICE ALERT (U.S.A.) CALL TOLL FREE (800) 642-2444 AT LEAST 48 HOURS PRIOR TO EXCAVATION. CONTRACTOR TO UNCOVER EXISTING BURIED UTILITIES WITH UTILITY OWNER TO VERIFY LOCATION AND ELEVATION. BURIED UTILITIES INCLUDE BUT ARE NOT LIMITED TO WATER, SEWER, GAS, ELECTRICAL AND TELEPHONE. ALL UTILITIES CONFLICTING WITH THE PROPOSED CONSTRUCTION SHALL BE RELOCATED PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION.

CONTRACTOR SHALL OBTAIN A TRENCH PERMIT FROM THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY PRIOR TO THE EXCAVATION OF ANY TRENCH GREATER THAN FIVE FEET IN DEPTH.

ALL UNDERGROUND IMPROVEMENTS SHALL BE INSTALLED AND APPROVED PRIOR TO ROAD SURFACING.

NOT TO BE USED AS BOUNDARY SURVEY, TOPOGRAPHIC SURVEY PROVIDED BY PACIFIC LAND SURVEYS, BODEGA CA 707-875-3208

ALL TREES TO REMAIN SHALL BE FENCED AS DIRECTED BY ARBORIST OR COUNTY PRIOR TO BEGINNING OF CONSTRUCTION. NO TREES OF PERMIT SIZE TO BE REMOVED WITHOUT OBTAINING A TREE REMOVAL PERMIT.

TRAFFIC CONTROL SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST CALTRANS MANUAL OF TRAFFIC CONTROL DEVICES AND AS DIRECTED BY ENGINEER.

PROPERTY LINES , RIGHT OF WAY AND EASEMENTS TO BE FLAGGED PRIOR TO CONSTRUCTION.

GRADING NOTES

CONTRACTOR SHALL ENLIST THE SERVICES OF A REGISTERED GEOTECHNICAL ENGINEER TO MONITOR THE PLACEMENT OF EMBANKMENTS. THE GEOTECHNICAL ENGINEER SHALL SUBMIT A FINAL SOILS REPORT THAT CERTIFIES THAT THE EMBANKMENTS WERE PLACED IN ACCORDANCE WITH THE PROJECT PLANS (AND AMENDMENTS THERETO, IF ANY), SPECIFICATIONS, AND SOUND GEOTECHNICAL PRACTICE. THE REPORT SHALL ADDRESS IN PARTICULAR THE SUSTAINABILITY OF THE NATIVE SOIL ENCOUNTERED AT THE TOE AND BASE OF ALL FILLS. FURTHER, THE REPORT SHALL CONTAIN ANALYSIS OF THE SOILS ENCOUNTERED AND A COMPILATION OF COMPACTION TESTS PERFORMED.

CUT SLOPES SHALL BE EQUAL TO OR LESS THAN 2:1. WITH A GEOTECHNICAL ENGINEER'S WRITTEN PERMISSION, WEATHERED ROCK CUTS MAY BE STEEPENED. FILL SLOPES SHALL BE EQUAL TO OR LESS THAN 2:1 OR REINFORCED WITH GEOGRID PER THE GEOTECHNICAL ENGINEER'S RECOMMENDATIONS IF STEEPER THAN 2:1 AND SHALLOWER THAN 1:1.

ALL EXCESS SOIL MATERIAL, STUMPS, AND BOULDERS SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH ANY ENVIRONMENTAL REGULATIONS AND THE PERMITING AGENCY'S GRADING ORDINANCE AS THEY MAY APPLY.

IF CONSTRUCTION IS PERFORMED EARLIER THAN MAY 1ST OR LATER THAN OCTOBER 1ST IN ANY GIVEN YEAR, AN APPROVED SILTATION CONTROL PLAN, DESIGNED BY A CIVIL ENGINEER OR APPROVED, COMPETENT INDIVIDUAL IS REQUIRED.

STRUCTURAL SECTION NOTES

ACTUAL DIMENSION OF THE DRIVEWAY PAVEMENT SECTION SHALL BE DESIGNED BY THE SOILS ENGINEER.

THE ROAD SUBGRADE WITHIN THE ROADWAY SECTION SHALL BE SCARIFIED TO A DEPTH OF SIX INCHES AND COMPACTED TO A TIGHT NON-YIELDING SURFACE WITH NO VISIBLE DISPLACEMENT TO AT LEAST 95% OF RELATIVE COMPACTION AND SHALL BE FREE OF LOOSE OR EXTRANEOUS MATERIAL.

THE CLASS II AGGREGATE BASE SHALL HAVE A RELATIVE COMPACTION OF AT LEAST 95%. SHALL BE FREE OF LOOSE OR EXTRANEOUS MATERIAL AND BE A TIGHT NON-YIELDING SURFACE WITH NO VISIBLE DISPLACEMENT.

THE ASPHALT CONCRETE SHALL HAVE A RELATIVE COMPACTION OF AT LEAST 95%.

A SOILS ENGINEER SHALL TEST, AND APPROVE THE CONSTRUCTION OF ROADS, AND, IF REQUIRED, PARKING AREAS. THE SOILS ENGINEER SHALL PROVIDE COPIES OF THE TEST RESULTS AND WRITTEN APPROVALS TO THE COUNTYS INSPECTOR WITHIN THREE WORKING DAYS OF TESTING OR APPROVAL. THE APPROVAL SHALL INCLUDE THE STABILITY AND RELATIVE DENSITY OF SUBGRADES AND BASE COURSES PRIOR TO THE PLACEMENT OF ASPHALT CONCRETE. A FINAL SOILS REPORT SHALL BE SUBMITTED BY THE PROJECT SOILS ENGINEER TO THE CONSTRUCTION INSPECTION SUPERVISOR OF THE PERMIT AND RESOURCE MANAGEMENT DEPARTMENT WHICH CONTAINS AN ANALYSIS OF THE SOILS ENCOUNTERED AND COMPILATION OF COMPACTION TESTS PERFORMED.

STORM DRAIN NOTES

TRENCH AND BACKFILL CONSTRUCTION SHALL BE IN ACCORDANCE WITH CALTRANS' STANDARD PLAN A62-D FOR CONCRETE CULVERTS AND A62-F FOR METAL AND PLASTIC CULVERTS, EXCEPT THAT THE TRENCH WDTN NEED ONLY BE ONE FOOT ON EACH SIDE OF THE PIPE.

ALL DRAINAGE PIPE IN DRIVEWAY SHALL HDPE TYPE N AASHTO RATED OR SDR35. GRATES IN DRIVEWAY AREA SHALL BE TRAFFIC RATED. DRAINAGE INLETS SHALL BE OF THE SIZE OR TYPE CALLED OUT AND CAN BE PRECAST OR POURED IN PLACE. CLEANOUTS REQUIRED EVERY 100 FEET AND AT ANY PIPE DIRECTION CHANGE 45 DEGREES OR GREATER.

ALL ROOF DRAINS SHALL BE TIED INTO STORM DRAINAGE SYSTEM AND SEPERATED FROM FOUNDATION DRAINS.. RAINWATER LEADERS SHALL HAVE AND FOUNDATION DRAINS SHALL HAVE CLEANOUTS AT ALL CHANGES IN DIRECTION GREATER THAN 44DEGREES.. FOUNDATION SUBDRAINS SHALL BE SEPERATE PIPING SYSTEMS TO OUTFALL FROM SURFACE DRAINS/ROOF DRAINS

MISCELLANEOUS NOTES

TESTING FOR RELATIVE DENSITIES SHALL BE IN ACCORDANCE WITH CALTRANS' TEST METHOD NO. 216 PART II OR ASTM 1557. THE USE OF SAND CONE METHODS - SUCH AS ASTM 1556 OR CALTRANS 216 PART I - SHALL NOT BE ALLOWED.

PLACEMENT OF MAIL BOXES IS TO BE COORDINATED WITH AND APPROVED BY THE LOCAL BRANCH OF THE UNITED STATES POST OFFICE.

DESIGN ENGINEER SHALL CERTIFY TO THE COUNTY IN WRITING THAT ALL GRADING, DRAINAGE, AND RETAINING WALL CONSTRUCTION WAS DONE IN ACCORDANCE WITH THE PLANS AND FIELD DIRECTIONS. ALSO NOTE THAT DRIVEWAY, PARKING AND OTHER SITE IMPROVEMENTS SHALL BE INSPECTED BY A DEPARTMENT OF PUBLIC WORKS ENGINEER.

ADDITIONAL GENERAL NOTES

UNAUTHORIZED CHANGES & USES: THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES TO THE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PUBLIC WORKS DEPARTMENTNT DEPARTMENT AND THE PREPARER OF THESE PLANS.

CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL.

THE LOCATIONS OF UNDERGROUND OBSTRUCTIONS SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND SHOULD NOT BE TAKEN AS FINAL OR ALL INCLUSIVE. THE CONTRACTOR IS CAUTIONED THAT THE PLANS MAY NOT INCLUDE ALL EXISTING UTILITIES AND THAT THE OWNER AND ENGINEER ASSUME NO RESPONSIBILITY FOR OBSTRUCTIONS WHICH MAY BE ENCOUNTERED.

THE CONTRACTOR MUST EXPOSE ALL EXISTING UTILITIES AS A FIRST ORDER OF WORK, INCLUDING SEWER AND STORM DRAINS, TO VERIFY DESIGN ASSUMPTIONS AND EXACT FIELD LOCATION.

EXISTING UTILITIES MAY REQUIRE RELOCATION AND/OR PROPOSED IMPROVEMENTS MAY REQUIRE GRADE OR ALIGNMENT REVISION DUE TO FIELD CONDITIONS.

THE CONTRACTOR IS CAUTIONED NOT TO ORDER PRECAST ITEMS OR INSTALL ANY IMPROVEMENTS UNTIL ALL CONFLICTS ARE RESOLVED. ALL IMPROVEMENTS INSTALLED OR ORDERED PRIOR TO CONFLICT RESOLUTION SHALL BE DONE SOLELY AT THE CONTRACTOR'S RISK AND AT NO EXPENSE TO THE OWNER.

THE CONTRACTOR SHALL RECOGNIZE THAT UNDERGROUND FACILITIES NOT SHOWN AS CIVIL IMPROVEMENTS (PG&E, TELEPHONE, TELEVISION, IRRIGATION, ETC.) SHALL BE COORDINATED AND CONSTRUCTED PRIOR TO PLACEMENT OF BASE ROCK AND PAVING.

ALL EARTHWORK AND SITE GRADING SHALL COMPLY WITH CHAPTER 33 AND APPENDIX CHAPTER 33 OF THE CALIFORNIA BUILDING CODE.

PRIOR TO BEGINING CONSTRUCTION, THE OWNER SHALL OBTAIN ALL NECESSARY AGREMENTS & EASEMENTS AS REQUIRED TO COMPLETE THE WORK.

ABBREVIATIONS

- AB

AC

BC

BOW

BVC

CL

C,G,C&G

CB

CO

CONC

DI

EC

EG

EVC

EX

EXIST

FL

FF

FG

FH

GB

HP

Ht.

INV

JT

MAX

MH

N/A

NTS

O/H

PCC

PCC

PRC

PUE

PVC

PVI

R/W

RCP

S

S/W

SD

SOG

SS

SSCO

SSMH

STA

STD

TC

TEMP

TOE

TOW

TW

TYP

W

WM
- AGGREGATE BASE

ASPHALT CONCRETE

BEGIN HORIZONTAL CURVE

BASE OF WALL

BEGIN VERTICAL CURVE

CENTERLINE

CURB AND GUTTER

CATCH BASIN

CLEANOUT

CONCRETE

DRAINAGE INLET

END HORIZONTAL CURVE

EXISTING GROUND

END VERTICAL CURVE

EXISTING

EXISTING

FLOWLINE

FINISHED FLOOR

FINISHED GRADE

FIRE HYDRANT

GRADE BREAK

HIGH POINT

EXPOSED FACE HEIGHT OF WALL

INVERT

JOINT TRENCH

MAXIMUM

MAN HOLE

NOT APPLICABLE

NOT TO SCALE

OVERHEAD

PORTLAND CEMENT CONCRETE

POINT ON CURVE

POINT OF REVERSE CURVATURE

PUBLIC UTILITY EASEMENT

POLYVINAL CHLORIDE

POINT OF VERTICAL INTERSECTION

RIGHT OF WAY

REINFORCED CONCRETE PIPE

SEWER

SIDEWALK

STORM DRAIN

SLAB ON GRADE

SANITARY SEWER

SANITARY SEWER CLEAN OUT

SANITARY SEWER MAN HOLE

STATION

STANDARD

TOP OF CURB

TEMPORARY

TOE OF WALL/SLOPE

TOP OF WALL

TOP OF WALL

TYPICAL

WATER

WATER METER

SHEET INDEX

- C1

C2

C3

C4

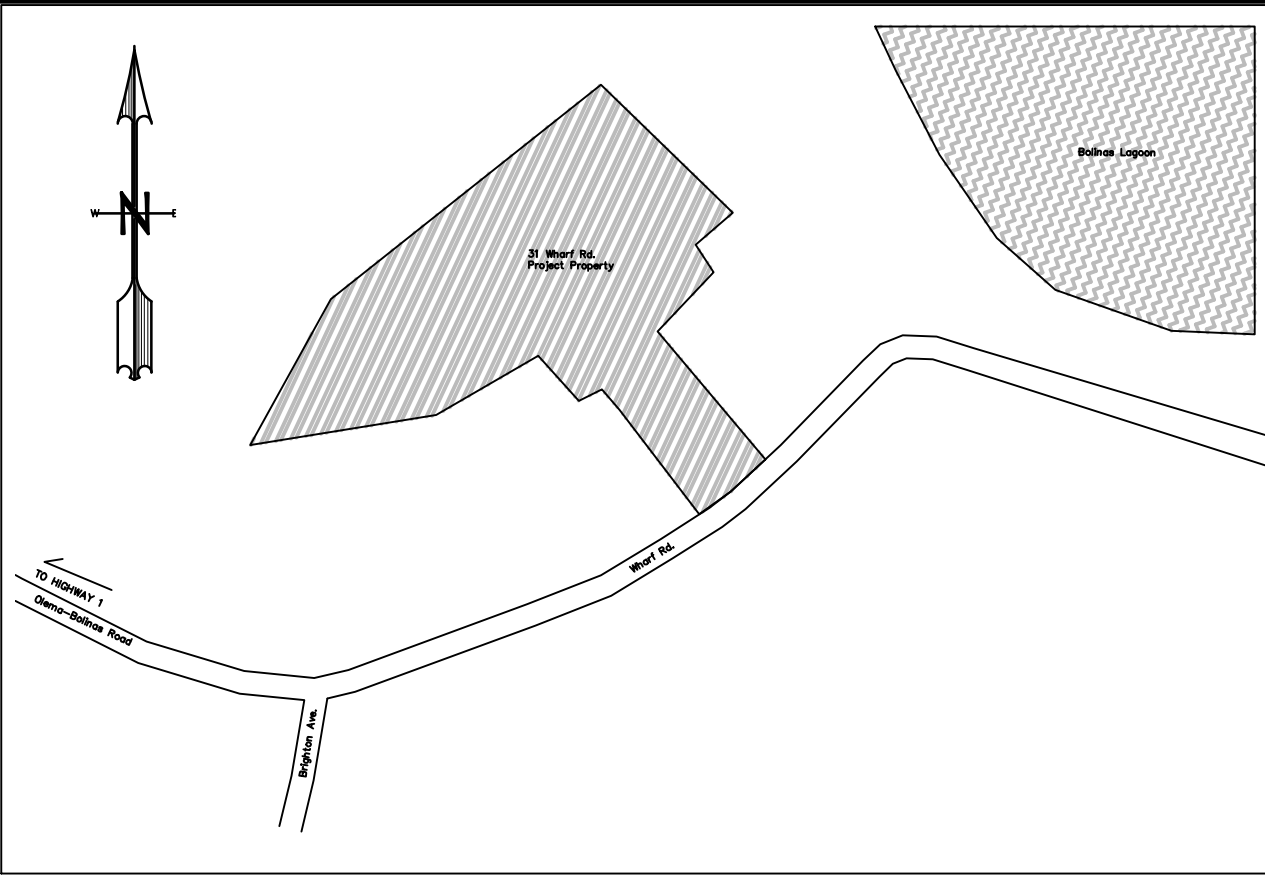
C5
- TITLE SHEET

- GRADING PLAN

- DRAINAGE PLAN

- EROSION AND SEDIMENT CONTROL PLAN

- STORM TREATMENT PLAN



VICINITY MAP

LEGEND

LINES:

CONTOUR-MJR

CONTOUR-MNR

APPROXIMATE BOUNDARY

RETAINING WALL

DAYLIGHT

STORM DRAIN/RAIN WATER COLLECTION

FOUNDATION/RET WALL DRAIN

RWL COLLECTION SYSTEM

WATER

SWALE

CLEANOUT

RAINWATER LEADER

BACKFLOW PREVENTER

SEWER

CABLE

ELECTRIC SERVICE

GAS SERVICE

TELECOM SERVICE

NO SCALE

EXISTING

PROPOSED

C.O.

DS

BWP

SS

X

E

GAS

EARTHWORK QUANTITIES

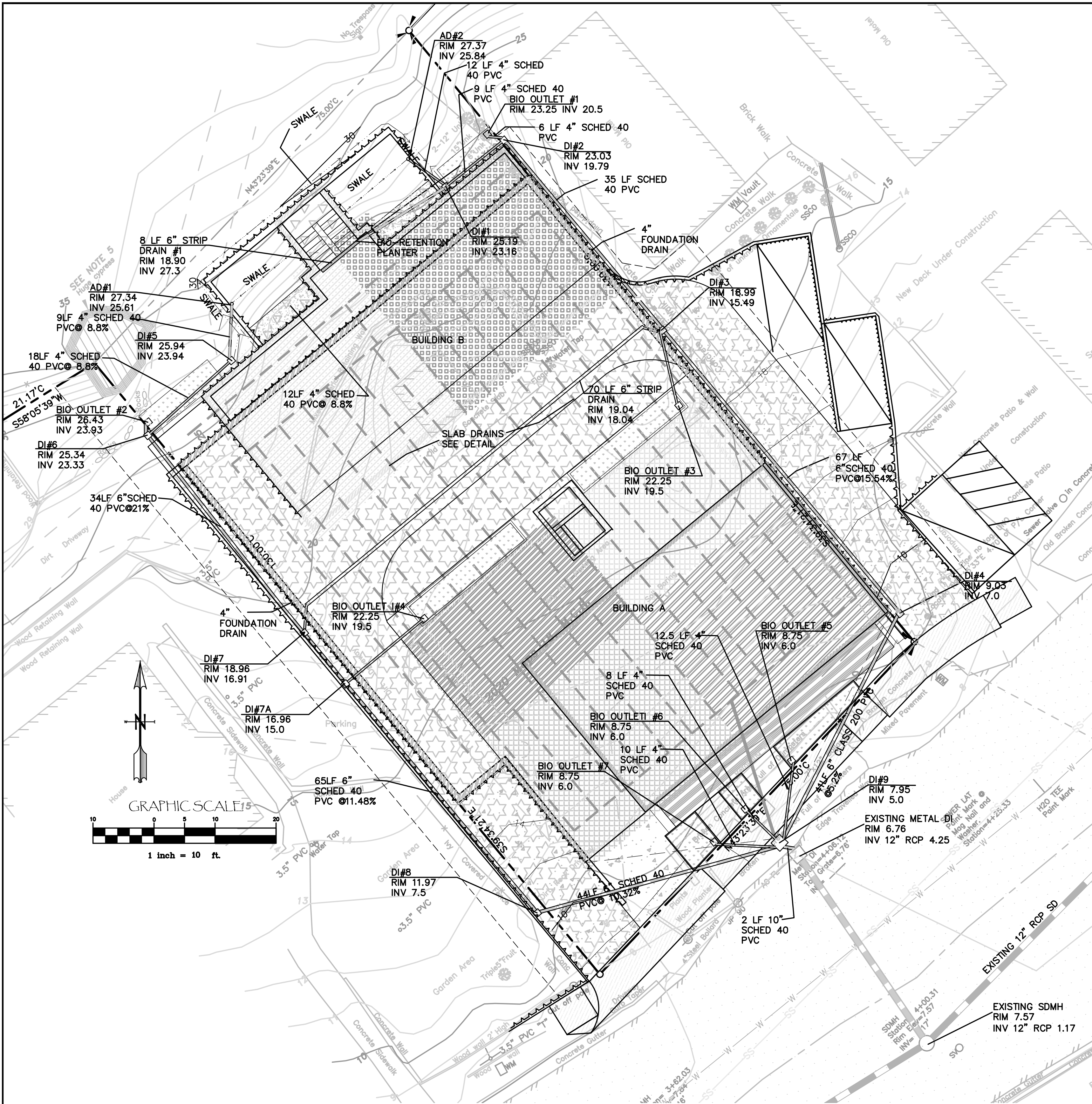
EARTHWORK QUANTITIES ARE APPROXIMATE AND FOR PLANNING PURPOSES ONLY. VOLUMES ARE APPROXIMATED FROM EXISTING AND FINISHED GRADE AND DO NOT INCLUDE TRENCH SPOILS, SITE STRIPPING, FOUNDATIONS, KEYWAY EXCAVATIONS OR EXPANSION/CONTRACTION OF SOIL. ACTUAL VOLUMES WILL VARY DEPENDING ON CONTRACTORS METHODS.

- ESTIMATED QUANTITIES:

CUT 2287 CY

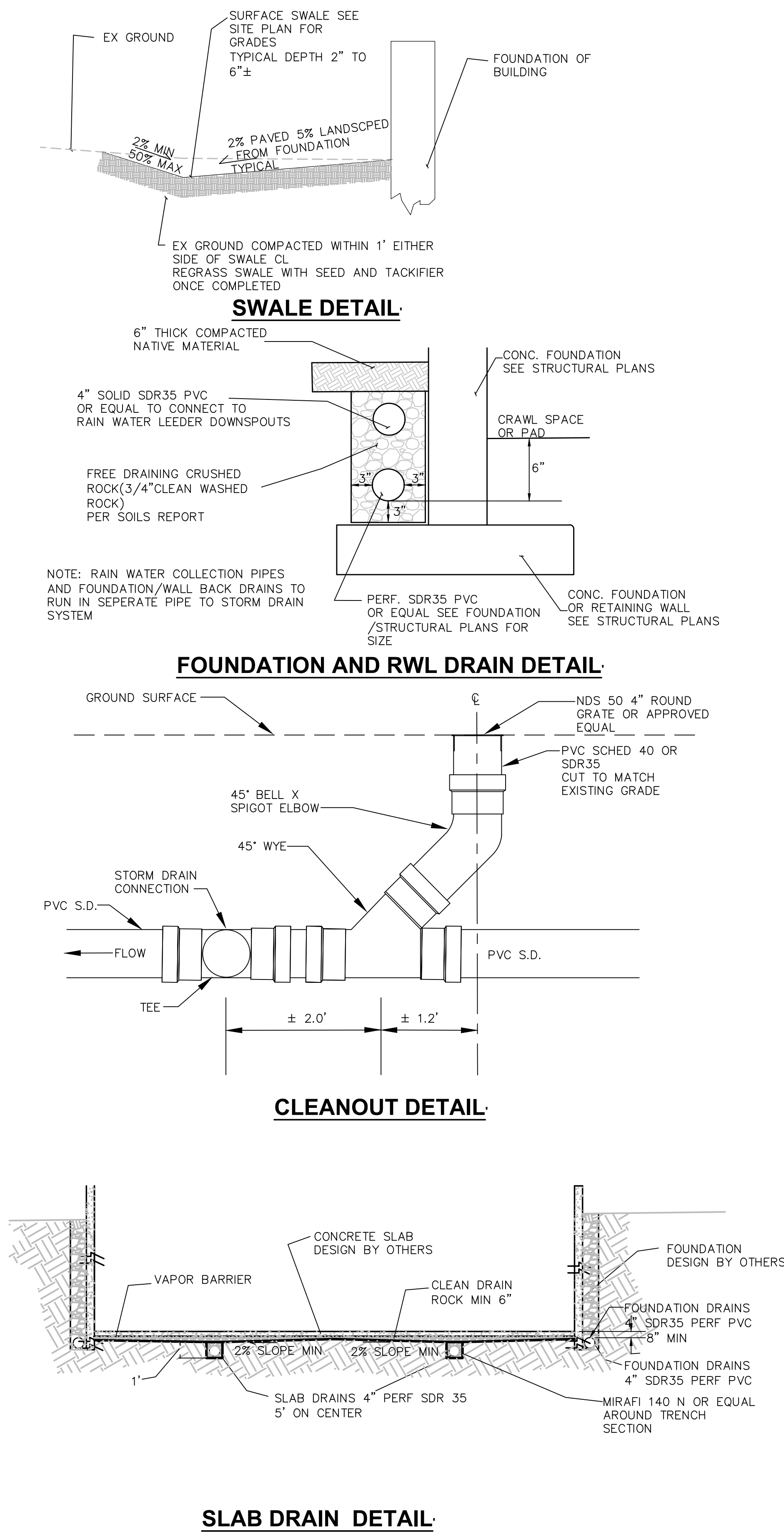
FILL 144.7 CY

NET 2142.3 CY CUT (EXPORT)



DRAINAGE PLAN

Scale 1"=10'





Know what's below.
Call before you dig.

PRELIMINARY
NOT FOR CONSTRUCTION
FOR DESIGN REVIEW ONLY



AYS Engineering Group, Inc PO Box 5693, Petaluma, CA 94955 Voice (707) 763-6620		IMPROVEMENT PLANS BCLT-Downtown Bolinas 31 Wharf Road, Bolinas Ca APN 193-061-03	PREPARED FOR: Bolinas Community Land Trust 6 Wharf Road Bolinas CA 94129	Revisions:																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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NOTES

- 1. Basis of bearings is N68°52'28"W 128.70' between Points "A" and "B", being a found 1.5" diameter iron pipe no tag with nail and a found 3/4" iron pipe no tag with bolt, bearing S48°12'16"W as calculated per the vesting deed for the property, D.N. 2019-0001893, O.R., M.C.R., (R1) per this map.
- 2. Distances and dimensions shown are in feet and decimals thereof. Offsets are perpendicular to lines.
- 3. Record data indicates that the right of way width of Wharf Road is variable.
- 4. Area of the subject property is 1.87 acres, more or less.
- 5. Historic property corner, a "spike driven in the root of a large cypress" per (R3)&(R5) - not found.

- 6. Extensive research was done on Wharf Road, aka Bond Road No 15. Marin Co DPW plans show centerline defined on Plan & Profile Sheet 20-98 Pages 1-4, but no right of way width is defined. Per DPW FB 271-101 field notes some centerline RRS, pins and monuments were set per Oglesby 1956 survey 2 OS 188, with no distances from centerline to ROW. 2-1/2" Brass Disk with punch "Marin County DPW" shown hereon was a found railroad spike and punch in 2003 when located by Pacific Land Surveys.
- 7. All fencelines around site are of mixed materials, ages, and meander.
- 8. Benchmark for site is NOS Tidal Station 4958 C 1979, a 3.5" Brass disk set in top of a concrete wall at the south end of Wharf Road calculated elevation 12.91 feet, NGVD 29 datum per NOAA datasheet. A survey tie to said benchmark is show hereon.
- 9. A Record of Survey map prepared by Pacific Land Surveys is currently in map check review at the County of Marin Surveyor's Office. Boundary information shown hereon is per that DRAFT Record of Survey map.
- 10. Roof peak locations were shot remotely with a reflectorless total station.

LEGEND

- ▲ Pacific Land Surveys Control point, #3 rebar & Cap PLS 6702 (typ.), or as noted
- Set 1/2" Iron pipe PLS 6702 or as noted
- ⊠ Found 1-1/2" square iron bolt, held as original corner
- Found 3/4" Iron pipe & cap LS 5814 per 2011 M 73, M.C.R.(R15)
- Found 3/4" Iron pipe wood plug, no tag per 8 OS 66, M.C.R.(R2)
- Found iron pipe as noted
- Found wood, iron or plastic post
- Found 2-1/2" Brass Disk with punch in wall monument "Marin County DPW"
- C Calculated M Measured PLS Pacific Land Surveys
- P/L Property Line M.C.R. Marin County Records UNO Unknown Origin
- D.N. Document Number A.P.N. Assessors Parcel Number N&T Nail & Tag
- SSMH Sanitary Sewer Manhole (SV) Sewer Valve ○ SSCO Sanitary Sewer Cleanout
- SDMH Storm Drain Manhole (WV) Water Valve (DI) Drop Inlet WM Water Meter
- JP Joint Pole (FH) Fire Hydrant
- DRAFT Boundary of subject property - not finalized
- Estimated Boundary of other lands
- Estimated Parcel line of record
- Existing fenceline
- Edge of Pavement
- Edge of unpaved driveway or track
- Top of bank or grade break
- 18" Look Live Oak tree, size as noted
- 6" Unk Unknown tree, size as noted
- 42" Snag or stump, size as noted
- 2.7' Quad Walnut tree, size as noted
- Five-foot contour interval
- One-foot contour interval

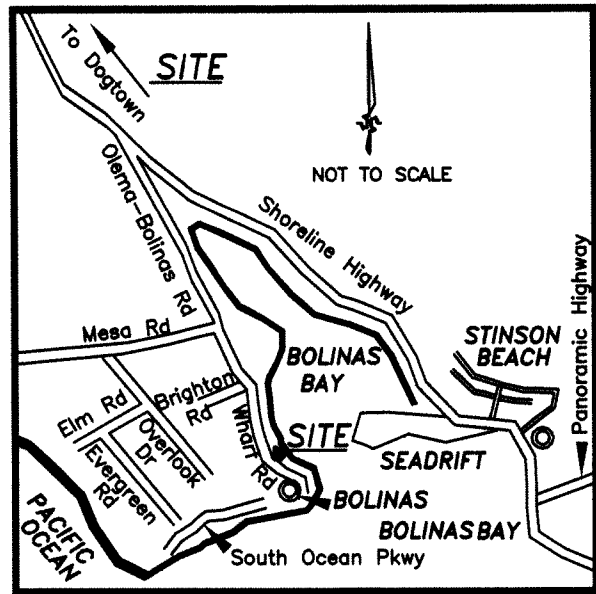
Pepper Trust
D.N. 2014-0019186

SURVEYOR'S STATEMENT

This map correctly represents a survey made by me or under my direction in conformance with the requirements of the Professional Land Surveyors' Act at the request of The Bolinas Community Land Trust in October 2019.

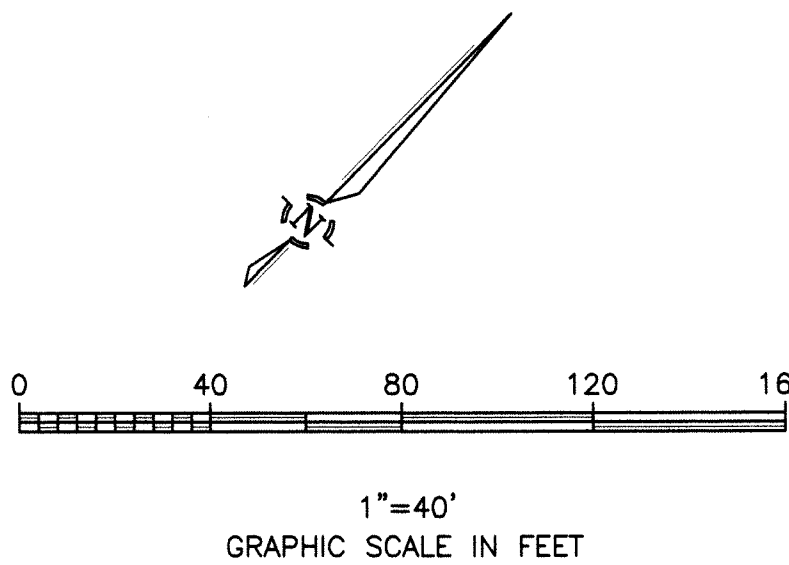
PACIFIC LAND SURVEYS, a sole proprietorship

Dated: Susan D. Ruschmeyer P.L.S. 6702
License Expires 06/30/20



VICINITY MAP

NOT TO SCALE



Ceniceros & McDougall
D.N. 2007-0019628

Topographic Map

Of a Portion of the Lands of
The Bolinas Community Land Trust
as described in Document No. 2019-0001893,
Official Records, Marin County Records.
31 Wharf Road Bolinas
A.P.N. 193-061-03
COUNTY OF MARIN • STATE OF CALIFORNIA
PACIFIC LAND SURVEYS
P.O. BOX 316 BODEGA, CA 94922 • 707.875.3308
SCALE: 1"= 40' • MARCH 2020 • SHEET 1 OF 1

A-2-MAR-21-0045

Exhibit 3

Page 22 of 34

filename: BCLT-DOWNTOWN_DD_210128.wxd



1
A1.01 'BUILDING A' - FIRST FLOOR PLAN
Scale: 1/4" = 1'-0"



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 183-061-03

bolinas community/land trust
6 Wharf Road
bolinas, ca 94924

client:

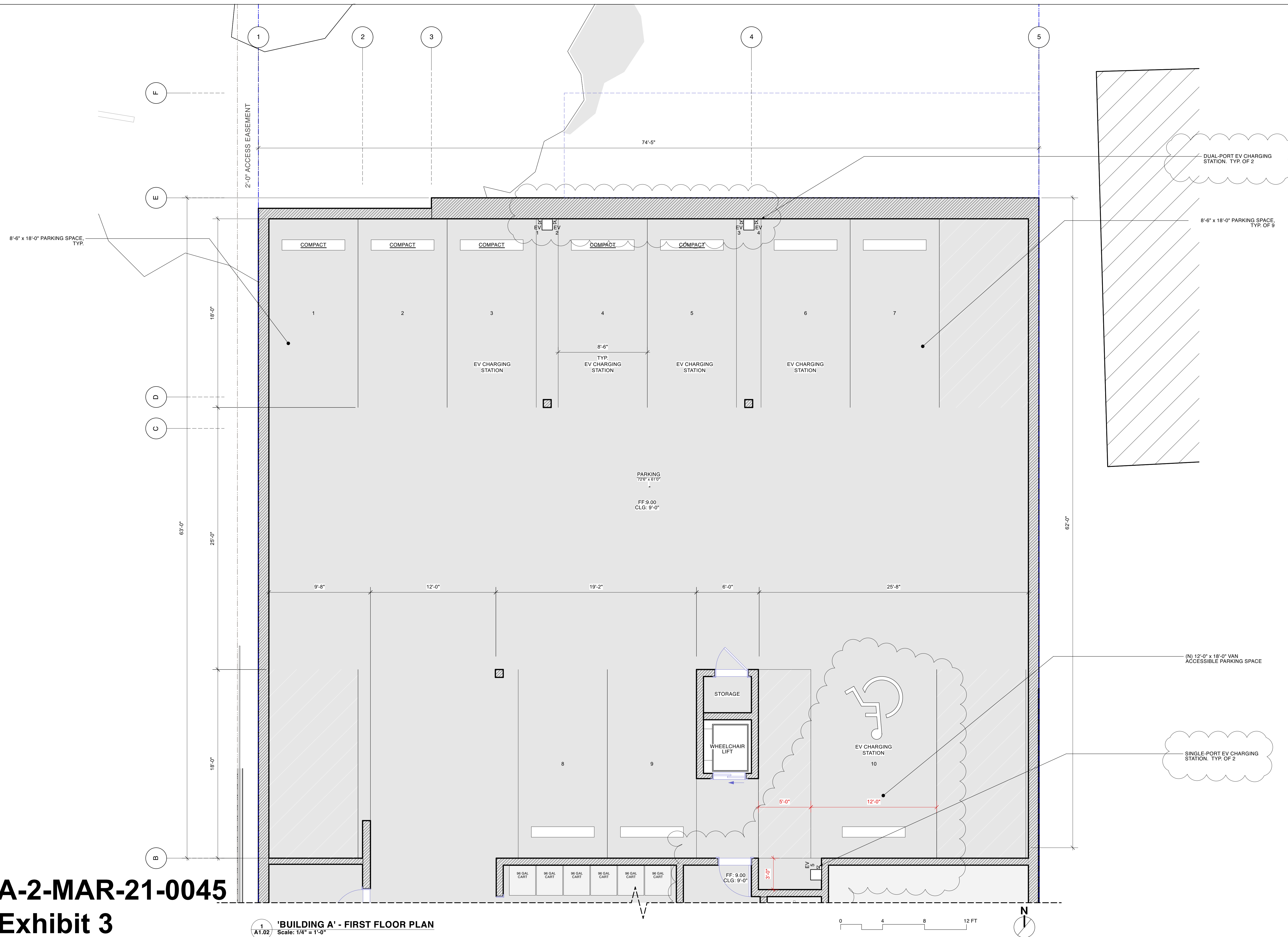
date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm
12.18.20	Planning Submittal update	di
02.01.21	Planning Submittal update	di



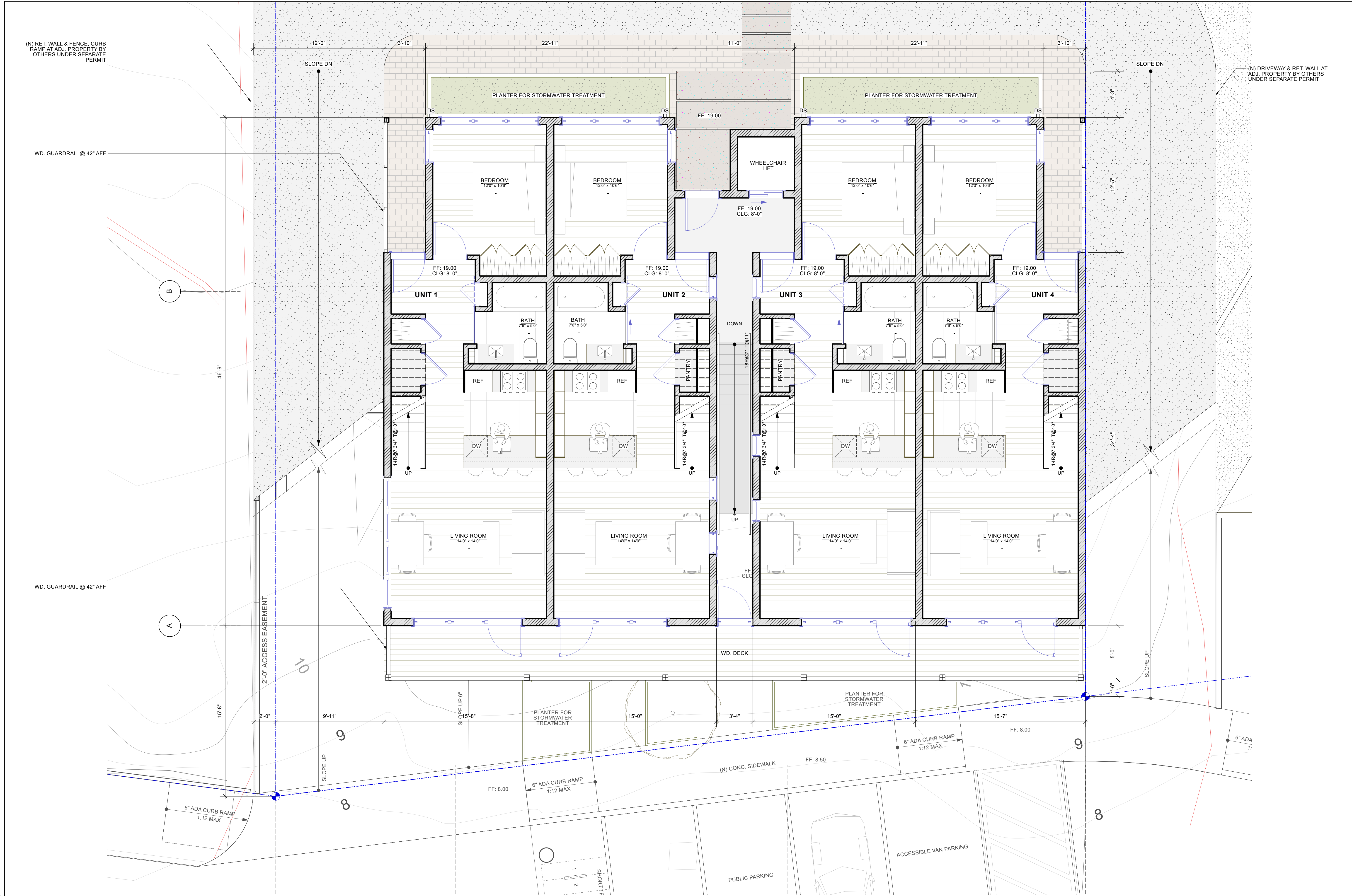
client: bolinas community land trust
6 wharf road
bolinas, ca 94924

FLOOR PLANS

A1.02

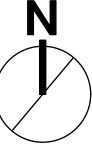


filename: BCLT-DOWNTOWN_DO_201123.wx



1
A1.03 'BUILDING A' - SECOND FLOOR PLAN
Scale: 1/4" = 1'-0"

0 4 8 12 FT



BCLT - DOWNTOWN BOLINAS
31 WHARF ROAD
BOLINAS, CA 94708
APN: 193-051-03

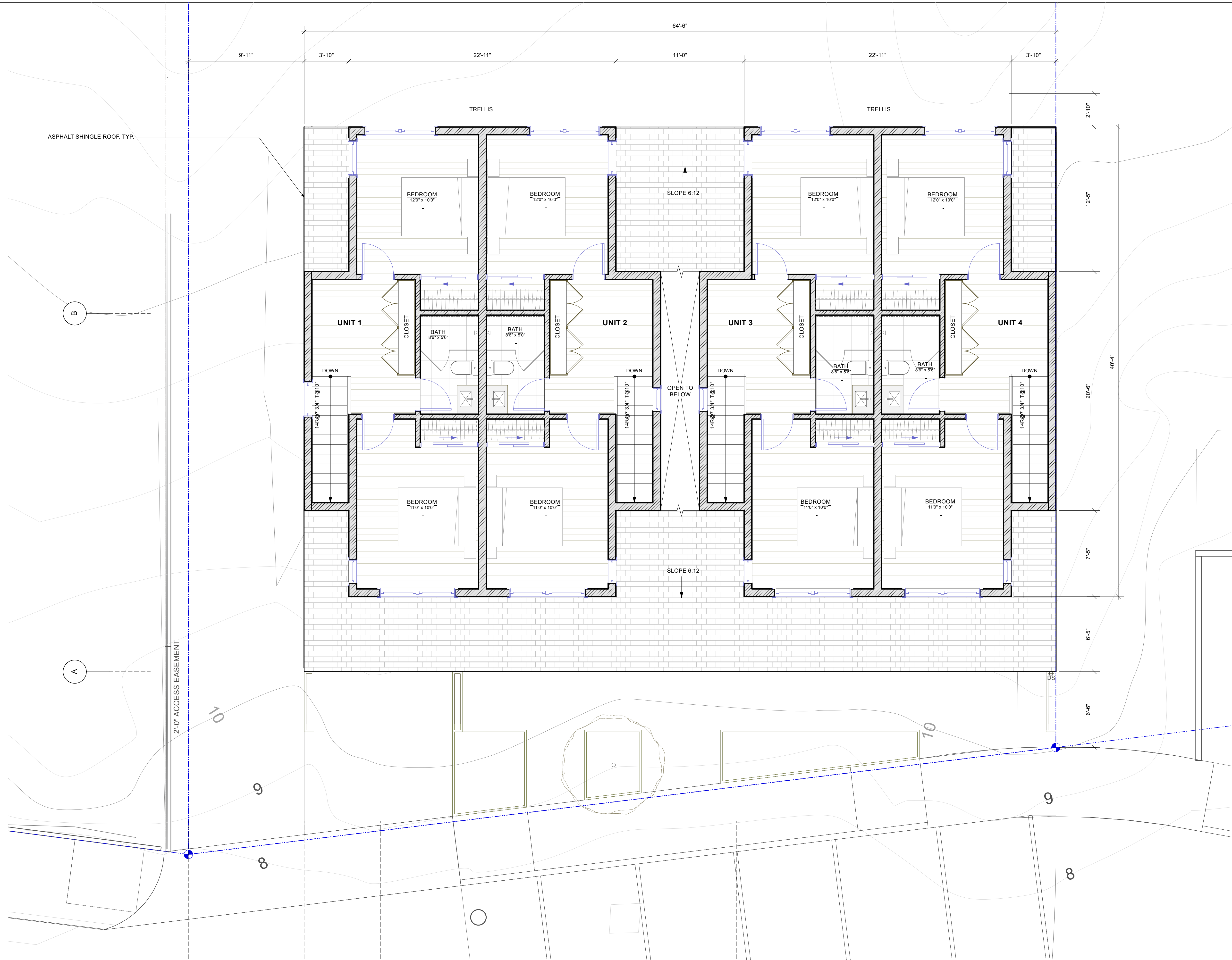
client: bolinas community/land trust
6 wharf road
bolinas, ca 94924

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
08.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

filename: BCLT-DOWNTOWN_DO_201123.vwx

1
A1.04 'BUILDING A' - THIRD FLOOR PLAN
Scale: 1/4" = 1'-0"

0 4 8 12 FT



BCLT - DOWNTOWN BOLINAS

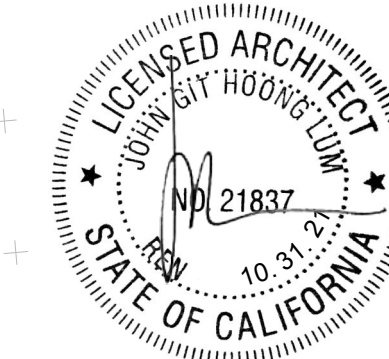
31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-051-03

client:
bolinas community/land trust
6 Wharf Road
bolinas, ca 94924

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
08.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

FLOOR PLANS

A1.04

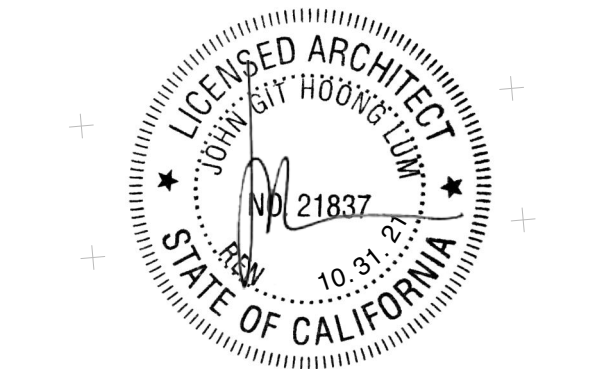
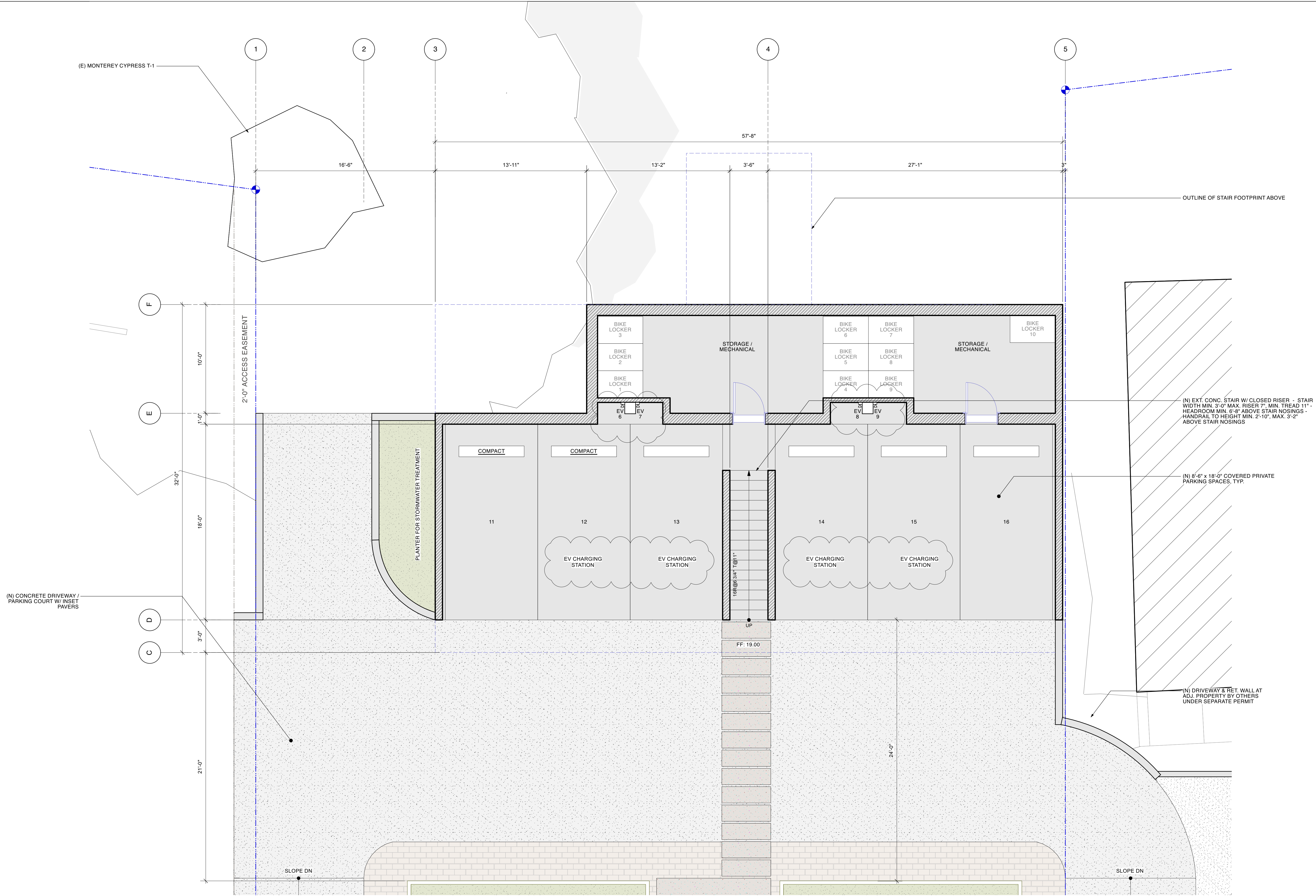
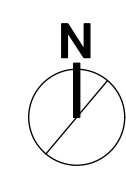
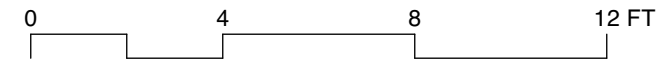


LUM

JOHN LUM ARCHITECTURE
3246 SEVENTEEN STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.0554

filename: BCLT-DOWNTOWN_DD_210128.wxw

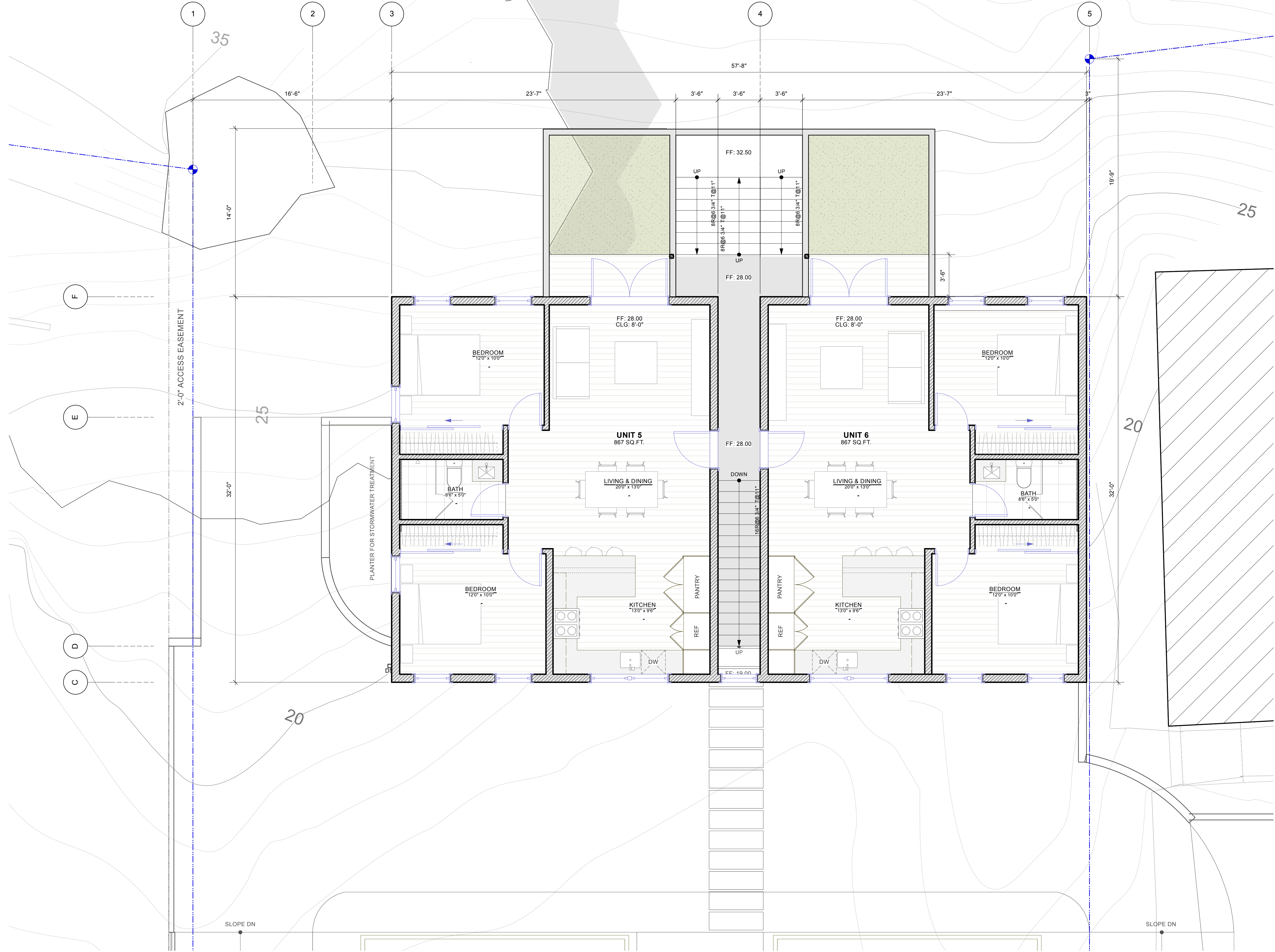
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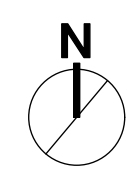
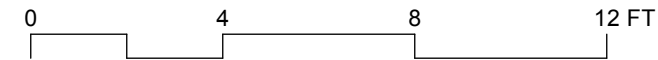
BCLT - DOWNTOWN BOLINAS
31 WHARF ROAD
BOLINAS, CA 94924
APR 18-2021-03
bolinas community/land trust
6 wharf road
bolinas, ca 94924
client:

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm
12.18.20	Planning Submittal update	di
02.01.21	Planning Submittal update	di

filename: BCLT-DOWNTOWN_DD_201123.vwx



1
A1.06 'BUILDING B' - SECOND FLOOR PLAN
Scale: 1/4" = 1'-0"

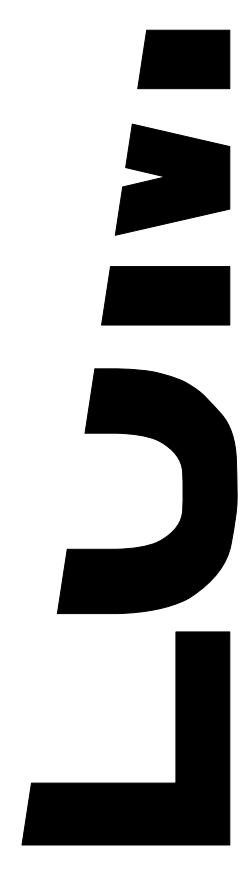
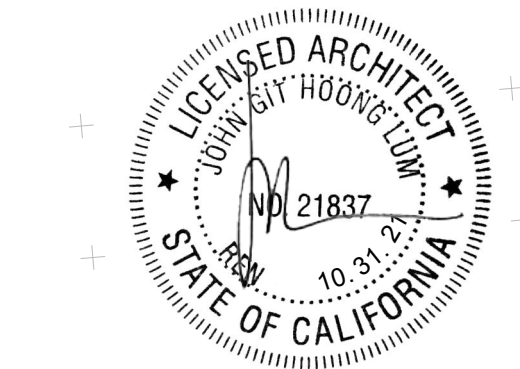


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06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

BCLT - DOWNTOWN BOLINAS
31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-061-03

client:

bolinas community/land trust
6 Wharf Road
bolinas, ca 94924



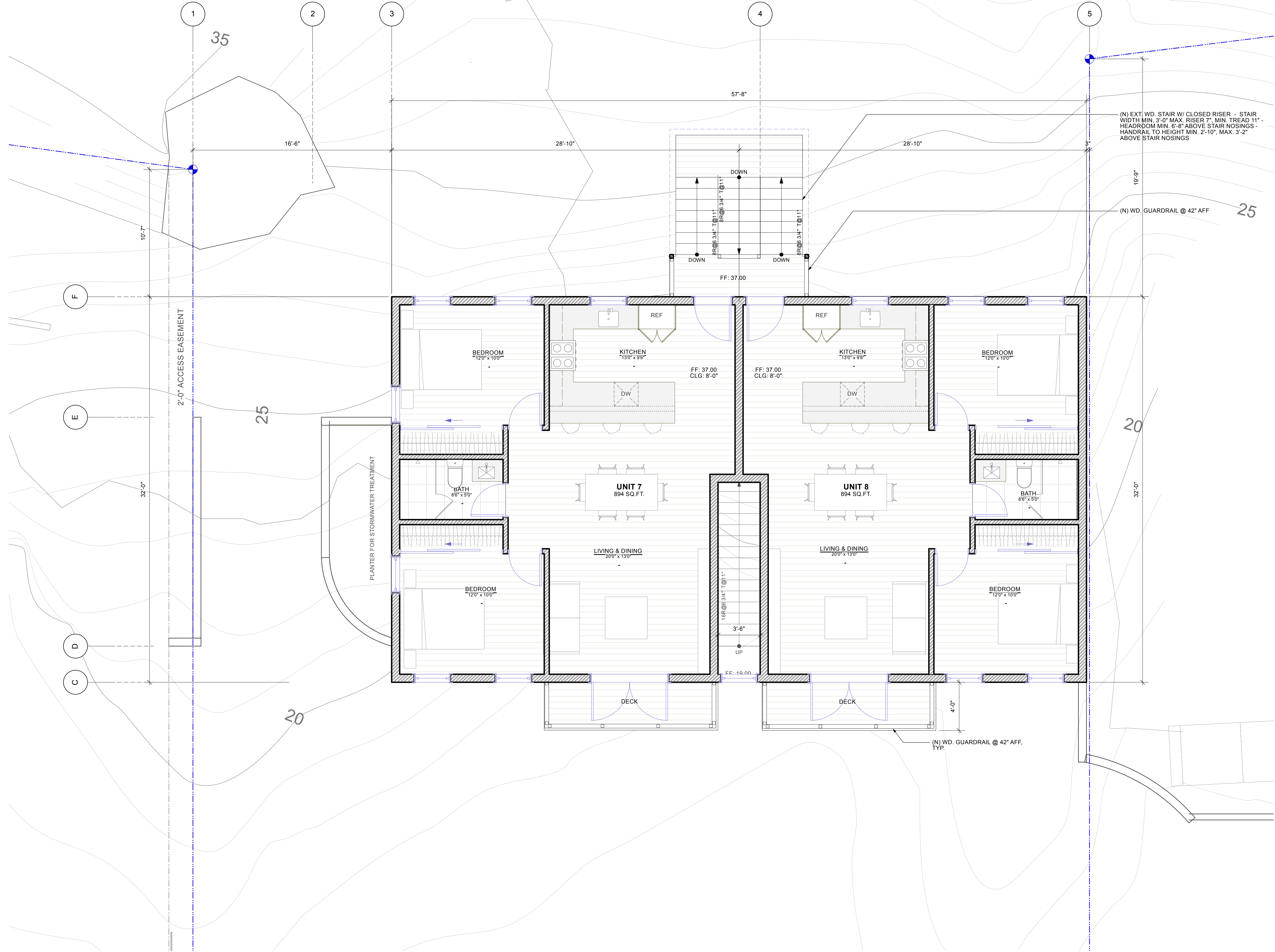
JOHN LUM ARCHITECTURE
3246 SEVENTEEN STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.0554

FLOOR PLANS

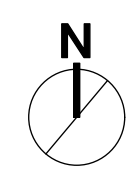
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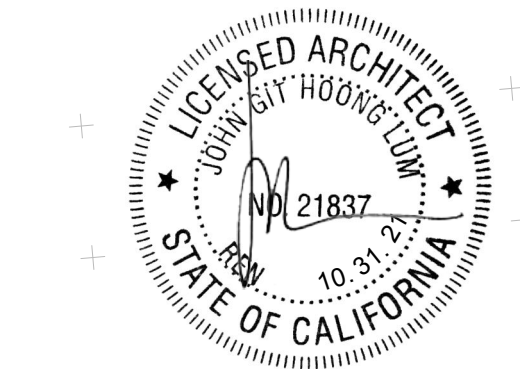


0 4 8 12 FT



BCLT - DOWNTOWN BOLINAS

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm



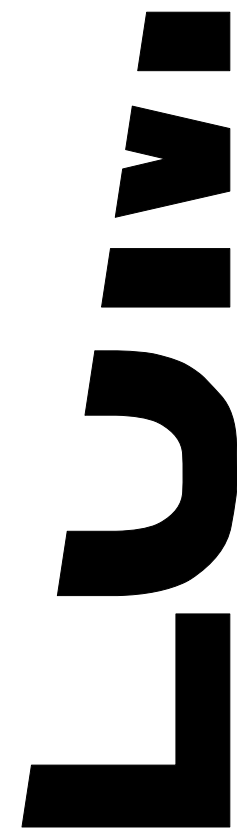
LUM
JOHN LUM ARCHITECTURE
3246 SEVENTEEN STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.0554

FLOOR PLANS

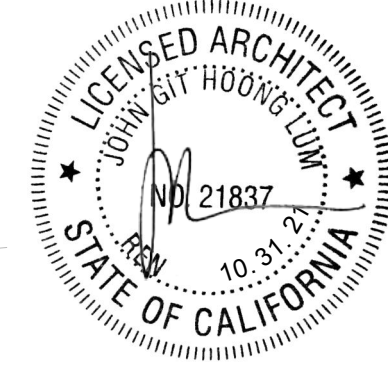
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client:
bolinas community/land trust
6 Wharf Road
bolinas, ca 94924

filename: BCLT-DOWNTOWN_DD_201123.wxw



JOHN LUM ARCHITECTURE
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BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 183-051-03

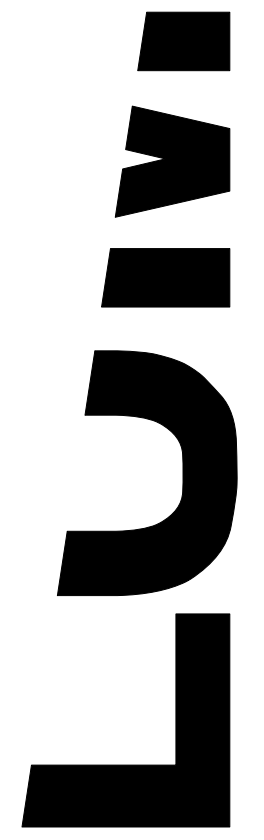
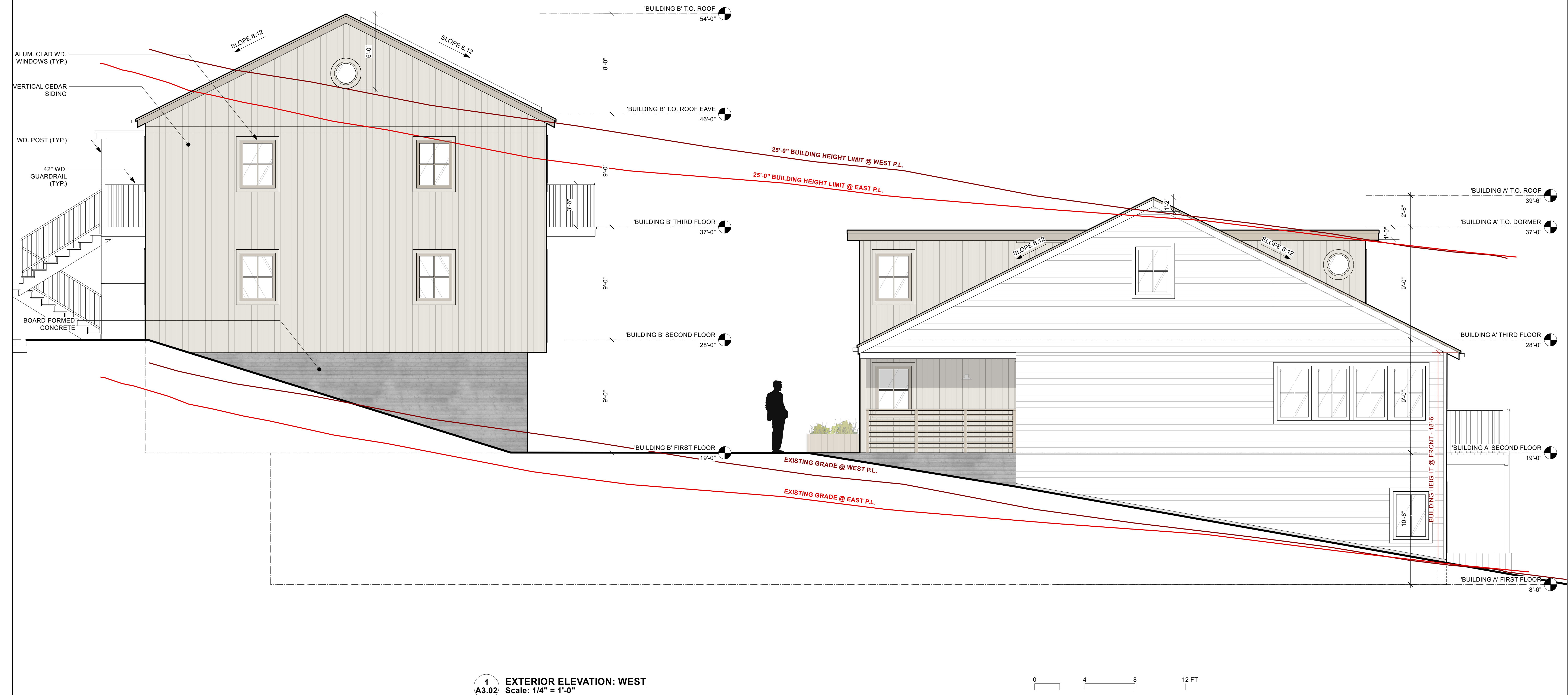
bolinas community/land trust
6 Wharf Road
bolinas, ca 94924
client:

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
08.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

EXTERIOR ELEVATIONS

A3.01

filename: BCLT-DOWNTOWN_DO_201123.vwx



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BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 193-051-03

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client:

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
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12.04.20	Planning Submittal	hm

EXTERIOR ELEVATIONS

A3.02

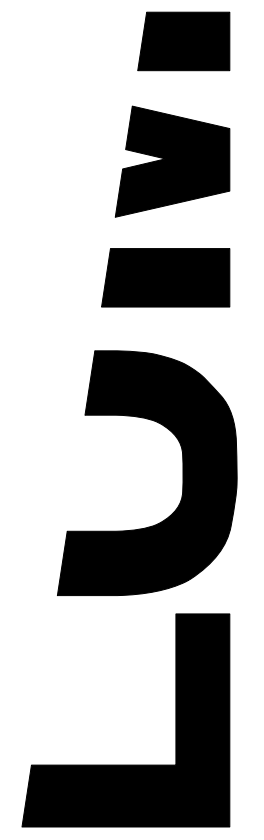
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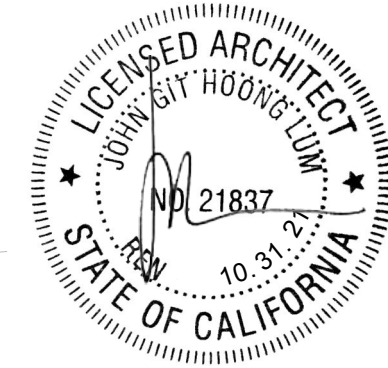
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EXTERIOR ELEVATION: 'BUILDING B' - SOUTH
Scale: 1/4" = 1'-0"



2
A3.03
EXTERIOR ELEVATION: 'BUILDING B' - NORTH
Scale: 1/4" = 1'-0"



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BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 183-051-03

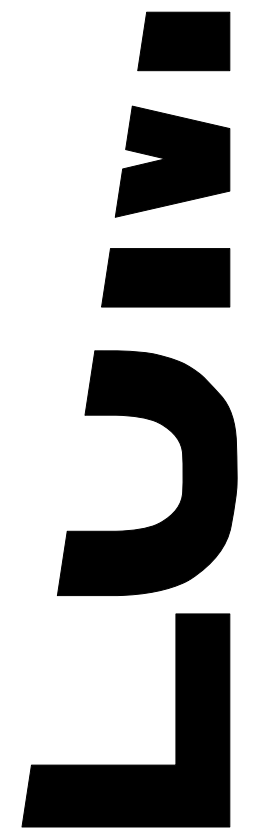
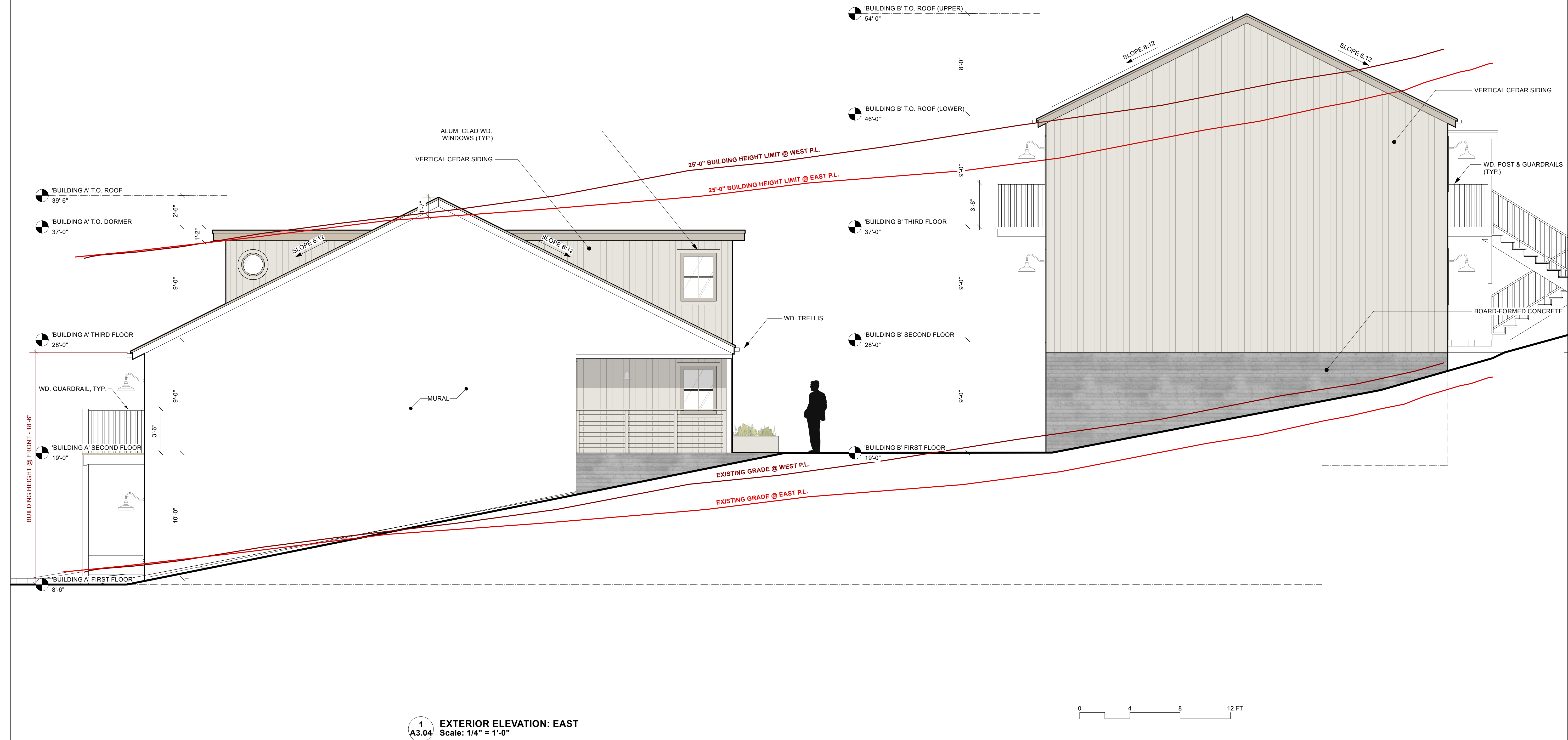
bolinas community/land trust
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bolinas, ca 94924
client:

date	issues / revisions	by
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08.18.20	Planning Submittal	hm
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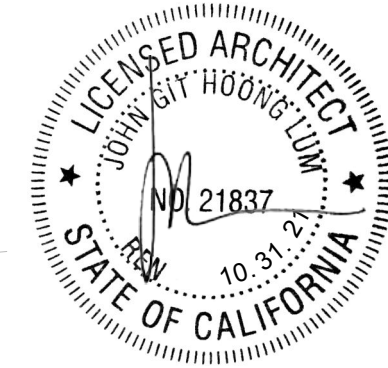
EXTERIOR ELEVATIONS

A3.03

filename: BCLT-DOWNTOWN_DO_201123.vwx



JOHN LUM ARCHITECTURE
3246 SEVENTEENTH STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.0554



BCLT - DOWNTOWN BOLINAS

31 WHARF ROAD
BOLINAS, CA 94924
APN: 182-051-03

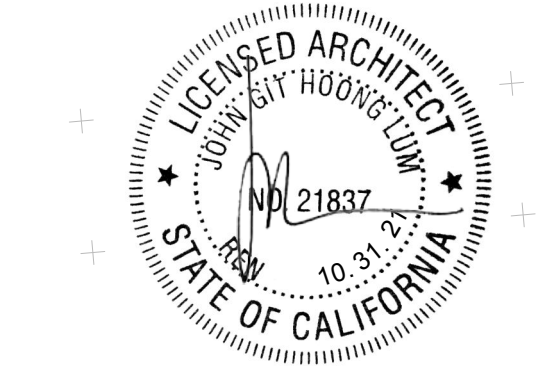
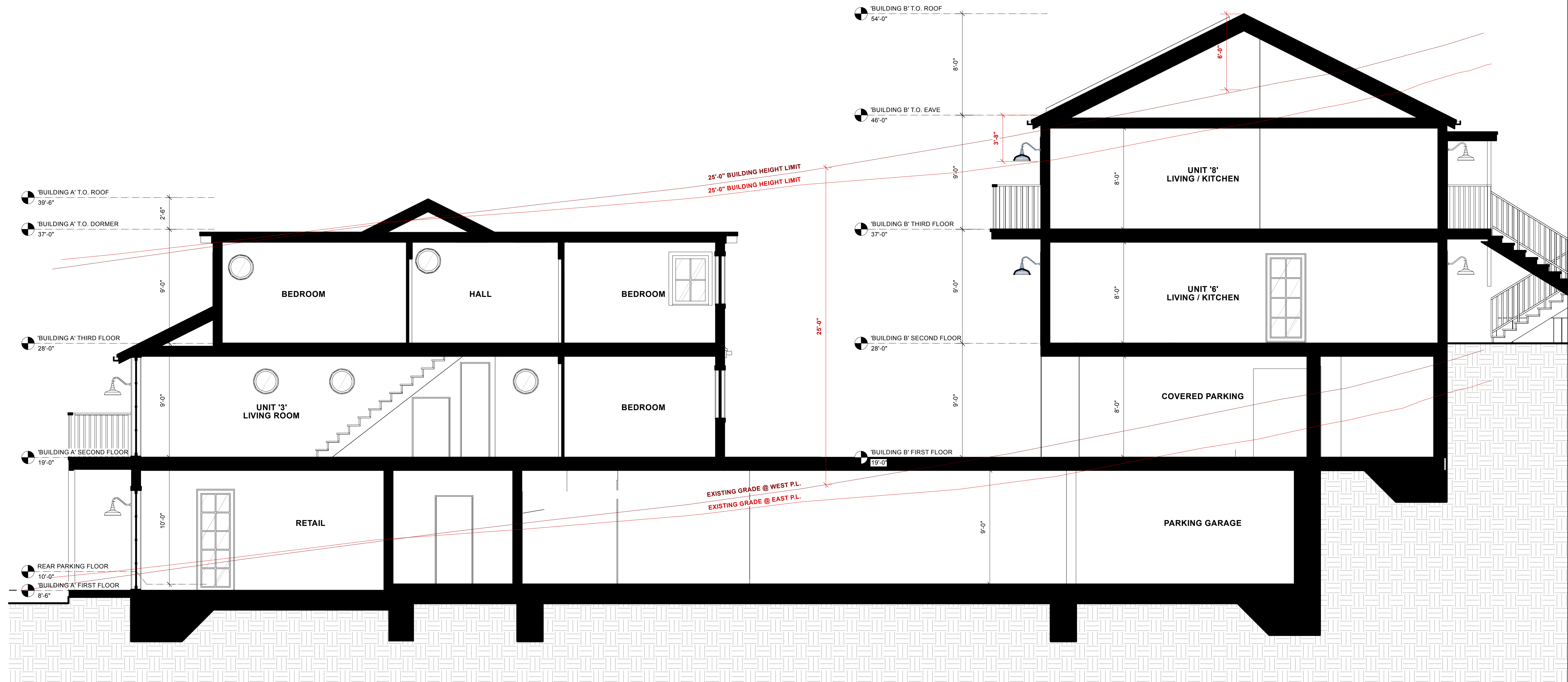
bolinas community/land trust
6 Wharf Road
bolinas, ca 94924
client:

date	issues / revisions	by
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06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

EXTERIOR ELEVATIONS

A3.04

filename: BCLT-DOWNTOWN_DO_201123.vwx



BCLT - DOWNTOWN BOLINAS

date	issues / revisions	by
06.10.20	Planning Pre-App Review	hm
06.18.20	Planning Submittal	hm
12.04.20	Planning Submittal	hm

BUILDING SECTIONS

A3.05

LUM

JOHN LUM ARCHITECTURE
3246 SEVENTEENTH STREET SAN FRANCISCO, CA 94110
TEL 415.558.9550 FAX 415.558.0554

client:
bolinas community/land trust
6 wharf road
bolinas, ca 94924

RECEIVED

JUN 15 2021

**MARIN COUNTY
COMMUNITY DEVELOPMENT AGENCY**

CALIFORNIA
COASTAL COMMISSION

THOMAS LAI, DIRECTOR

NOTICE OF FINAL LOCAL (BOARD OF SUPERVISORS) DECISION

Pursuant to Coastal Act Section 30603(d), Coastal Commission Regulations Section 13571, and LCP Policy and/or Implementation Plan.

June 9, 2021

**California Coastal Commission
455 Market Street, Suite 300
San Francisco, CA 94105**

Attention: Coastal Planner

Applicant's Name: **Bolinas Community Land Trust**

Coastal Permit Number: Coastal Permit [P2835]

Assessor's Parcel Number: 193-061-03

Project Location: 31 Wharf Road, Bolinas

Determination: Approved With Conditions
(Resolution of the June 8, 2021 Marin County Board of Supervisors' hearing is attached specifying action.)

Decision Date: June 8, 2021

County Appeal Period: N/A – Final appeal to the Board of Supervisors

Local review is now complete.

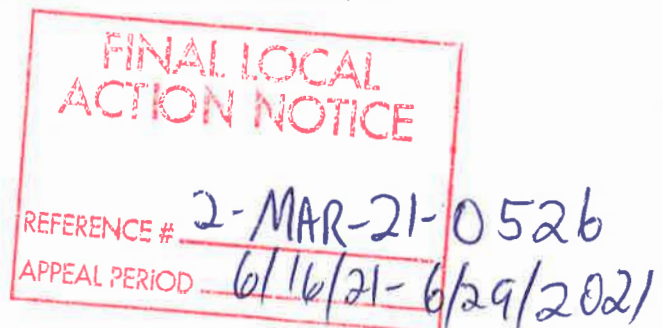
This permit IS appealable to the California Coastal Commission (see Marin County Code Section 22.56.080 attached); please initiate the California Coastal Commission appeal period.

Any correspondence concerning this matter should be directed to Michelle Levenson, Sr. Planner at (415) 473-3615.

Sincerely,


Michelle Levenson
Sr. Planner

Attachment1- Resolution



RESOLUTION NO. 2021-42
RESOLUTION OF THE MARIN COUNTY BOARD OF SUPERVISORS
DENYING THE H. ROLAND CROTTS, TRUSTEE OF THE CROTTS 2002 TRUST, APPEAL
AND APPROVING THE BOLINAS COMMUNITY LAND TRUST (BCLT) COASTAL PERMIT,
DESIGN REVIEW AND USE PERMIT (P2835)
31 WHARF ROAD, BOLINAS
ASSESSOR'S PARCEL: 193-061-03

SECTION I: FINDINGS

1. WHEREAS, the Bolinas Community Land Trust (BCLT), requests Coastal Permit, Design Review and Use Permit approval to construct a new, 10,268-square-foot mixed-used development consisting of 8,629 square feet of affordable housing units (a total of eight units) and 1,639 square feet of commercial area on a vacant lot in Bolinas. The proposed development would result in floor area ratio of 10.15-percent on the 101,150-square-foot lot (2.32 acres) and would reach a maximum height of 33 feet above surrounding grade. The proposed residential component of the project would consist of eight units, all of which would be available for rent to low and very-low income households. The development would be contained in two, two-story buildings, with the commercial component located on the first floor of the structure fronting Wharf Road. The residential units would range in size from 847 square feet to 1,174 square feet with the smaller units containing two bedrooms and the larger units containing three bedrooms. A total of four, three bedroom units and four, two bedroom units are proposed.

The exterior walls of the development would have the following setbacks: over 100 feet from the rear, north property line; 5.5 feet from the south, front property line; 0 feet from the east, side property line; and 9.5 feet from the west, side property line. Other site improvements include the construction of 6,934 square feet of on-site, project-related parking. On-site parking would be contained in an underground parking structure as well as in above ground carports. A total of 16 on-site parking spaces are proposed. In addition, four off-site parking spaces along Wharf Road directly in front of the site would be retained/created with the project. The project site is located at 31 Wharf Road, Bolinas and is further identified as APN 193-061-03.

2. WHEREAS, on April 12, 2021, the Marin County Planning Commission held a duly noticed public hearing to take public testimony and consider the project. The Planning Commission approved the project by a vote of four to one.

3. WHEREAS, on April 19, 2021, H. Roland Crotts, Trustee of the Crotts 2002 trust, submitted a timely appeal of the Planning Commission's approval.

4. WHEREAS, on June 8, 2021, the Marin County Board of Supervisors held a duly noticed public hearing to consider the merits of the project and consider the bases of the appeal, and to take public testimony related to the project.

5. WHEREAS, the Board of Supervisors finds that the appeal lacks sufficient merit to overturn the Planning Commission's decision approving the Bolinas Community Land Trust (BCLT) Coastal Permit, Design Review and Use Permit for the reasons discussed below.

C. Ingress/Egress has not been adequately addressed, and project parking and driveway result in encroachments

Response

The County does not regulate or enforce easements between private properties, and there is presently no recorded access easement between the appellant and the applicant. The appellant has used the applicant's property for access over the years and claims to have a vested access right. The applicant has agreed to allow the appellant to continue to use the project site for access by utilizing the one-way driveway proposed with the project. With the exception of the driveway easement proposed along the eastern side of the site, the project is entirely contained within the site owned by the applicant. The County's Department has reviewed and will continue to review the access proposed with the project prior to the issuance of a building permit.

6. WHEREAS, the project is statutorily exempt from the requirement to prepare an environmental document pursuant to Public Resources Code Section 21159.25- exemption for multi-family and mixed-used housing projects related to environmental quality. The project qualifies for exemption as follows: (a) The project is consistent with the current Marin Countywide Plan and the applicable zoning designation and regulations; (b) the residential portion of the project is a multi-family housing development that contains six or more residential units; (c) the project is less than 5 acres in area, is located in unincorporated Marin County and is substantially surrounded by qualified urban uses; (d) the project site has no habitat value for endangered, threatened or rare species; (d) approval of the project would not result in significant effects to transportation, noise, air quality, and greenhouse gas emissions; (e) the site can be adequately served by public utilities and services; (f) the project site is a legal parcel; and (g) none of the conditions specific in Public Resources Code Section 21159.25(c) apply to the project.

7. WHEREAS, Government Code Section 65589.5 (the State Housing Accountability Act) states that the lack of housing in the state of California is a "critical problem...that threatens the economic, environmental and social quality of life..." in the State. The Act further states that, "California has a housing supply and affordability crisis of historic proportions...the absence of meaningful and effective policy reforms to significantly enhance the approval and supply of housing affordable to Californians of all income levels is a key factor...."

Section 65589.9 (L)(3)(d) of the Act states that, "A local agency shall not disapprove a housing development project...for very low, low- or moderate-income households...including through the use of development review standards, unless it makes written findings, based upon a preponderance of the evidence in the record..." that one of the provisions outlined in Sections 65589.9(L)(3)(d)(1) through (5) applies. The project does not meet the threshold for denial under the Act as follows:

- A. Section 66589.9(L)(3)(d)(1)-While currently in progress, the County has yet to adopt a housing element pursuant to the Act and has not met its share of the regional housing need allocation pursuant to Government Code Section 65584;
- B. Section 66589.9(L)(3)(d)(2). As discussed in more detail below, the project would not result in a "specific, adverse impact upon the public health or safety";
- C. Section 66589.9(L)(3)(d)(3). The development would comply with State and Federal law.
- D. Section 66589.9(L)(3)(d)(4). The site is not zoned for agriculture or resource preservation. Water and sewer service for the project would be provided by the Bolinas Community Public

Therefore, the County finds that the proposed development is a qualifying project under State Density Bonus Law and as such the requested waivers to development standards must be granted as the project does not meet the threshold to deny such waivers under Section 65915(e)(1) of the Government Code.

9. WHEREAS, the project is consistent with the goals and policies of the Marin Countywide Plan for the following reasons:

- A. While much of the portion of the site where development would occur is vegetated with non-native species, one coast live oak tree would be removed to construct the proposed project. The northern portion of the project site would remain undeveloped and existing trees would remain intact with the project. Therefore, the project is consistent with the CWP woodland preservation policy (BIO-1.3) because the project would not entail the irreplaceable removal of a substantial number of mature, native trees.
- B. The applicant provided a Biological Site Assessment prepared by WRA Associates and dated May 6, 2020 that describes the biological conditions of the project site and assesses the potential for the presence of special-status species and associated habitats. The assessment states that the southern portion of the site where development is proposed has been previously disturbed and is characterized by disturbance-adapted, non-native plant species.

The assessment states that the northern portion of the site that would remain undeveloped contains non-native grassland and a coast live oak woodland, both habitat types that are not considered environmentally sensitive. The northern portion of the project contains a 0.01 acre seasonal wetland characterized by herbaceous hydrophytic vegetation such as Pacific rush, common rush and tall; this wetland area is a jurisdictional wetland under the Clean Water Act and is considered environmentally sensitive.

The assessment states that while a total of five special-status plant species have the potential to occur on the project site, none of these plant species were observed in the field during an April 9, 2020 site visit conducted by a botanist. In addition, the project would be located over 100 feet from the wetland, consistent with environmental buffers required under the Local Coastal Program for Unit 1 and the Marin CWP policies regarding wetland conservation areas (WCAs).

According to the assessment, a total of 6 wildlife species as well as nesting birds protected under the Migratory Bird Treaty Act have the potential to occur on the project site. These species include special-status bats, overwintering monarch butterfly and nesting birds. The assessment recommended that tree removal occur outside of the nesting and roosting seasons, and concluded that the project would not result in any potentially significant adverse biological impacts to the environment.

The application would be required to implement roosting bat and nesting bird protection measures pursuant to Marin County Development Code Sections 22.20.040(F) and 22.20.040(G). The proposed project would not result in potentially significant adverse effects to special-status plant and animal species, therefore, the project is consistent with the CWP special-status species protection policy (BIO-2.2).

land use policy map. However, Policy CWP CD-8.7 states that the FAR in the C-NC land use designation may be exceeded to accommodate affordable units at the low to very-low affordable categories, such as those proposed with the project. Policy CD-8.7 also states that for parcels larger than 2 acres, no more than 50-percent of the new floor area may be developed for commercial uses, and the remaining floor area shall be developed for new housing. As the development would include 1,639 square feet of commercial uses of the 10,268 square feet of floor area proposed for non-parking structures, the project would comply with this requirement. Therefore the project is consistent with the CWP community development policies and programs, in particular policy CD-8.7.

10. WHEREAS, the project is consistent with Bolinas Community Plan, as discussed below.

The project is within the Bolinas Community Plan area and is consistent with the plan's provisions regarding the need for low income housing, promoting commercial and residential uses in the downtown planning area and preserving sensitive biological resources.

11. WHEREAS, the project is consistent with the mandatory findings for Coastal Permit approval (Marin County Code Section 22.56.130I).

A. Water Supply.

The site is currently served by the Bolinas Community Public Services District (BCPSD). The District provided a letter dated March 30, 2021, indicating that the applicant must apply to the District to transfer or stack water meter use allocations to adequately serve the proposed project. The applicant intends to apply for such permissions at the next scheduled BCPSD meeting of April 21, 2021.

B. Septic System Standards.

The project is connected to sewer service; a septic system is not proposed with the project.

C. Grading and Excavation.

It is estimated that the project would require the excavation of 2,287 cubic yards of material and would result in the placement of 144.7 cubic yards of material. A total of 2,142.3 cubic yards of excavated material would be hauled off-site for disposal. Grading activities would be the minimum necessary to construct the proposed project in accordance with relevant building codes and regulations. The applicant has provided a Construction Management Plan that indicates that all grading would conform to DPW requirements. As described above, the applicant has prepared a SWPPP that provides for the use of straw wattles during construction that would protect soils that have been exposed during grading activities. Prior to the issuance of a building permit for the project, review and approval of a grading plan will be required by the Department of Public Works.

D. Archaeological Resources

A review of the Marin County Archaeological Sites Inventory indicates that the site is located within a "High Sensitivity" archeological area. The applicant provided an Archaeological Survey Report for the property which indicates that a records search of the database maintained by the Northwestern Information Center at Sonoma State University

oak and non-native plant species. The northern portion of the site would remain undisturbed with construction of the project.

The proposed project is not located adjacent to the shoreline or within a bluff erosion zone.

K. Geologic Hazards.

As described in the geotechnical investigation prepared for the project, there are no known active faults that cross the project site. The Marin County Community Development Agency- Building and Safety Division will require seismic compliance with the California Building Code prior to issuance of a project building permit.

L. Public Works Projects.

The proposed project will not affect existing or proposed local public works projects in the area. The project site is located along an existing road, and would not entail the expansion of a public road, flood control project or utility services.

M. Land Division Standards.

No land division or property line adjustment is proposed as part of this project.

O. Visual Resources and Community Character.

The project would exceed the 25-foot maximum height limit for primary structures as the rear building would reach a maximum height of 33 feet. However, because the project is a qualifying project under State Density Bonus Law, the applicant has applied for a waiver to this development standard. While the maximum height of the structure would exceed 25 feet, buildings in the downtown area, in particular Smiley's located adjacent to the project site, contain two-story structures of varying heights which contribute to the unique character of the area. The building massing and siting has been designed to limit height and vertical elements to the extent feasible by stepping the massing up the hillside. The project has been sited in the southern portion of the site fronting Wharf Street to avoid effects to sensitive resources, concentrating the development in this area consistent with other development in downtown Bolinas.

The proposed project would not impair or obstruct coastal views from a public street or public viewing location and the site is located a considerable distance from public roads that provide coastal viewing opportunities in the area.

P. Recreational/Commercial/Visitor Facilities.

The project would provide a mixture of commercial and residential uses, consistent with the C-VCR zoning designation for the site. While the Interim Zoning Code provisions for the C-VCR district state that residential uses shall be permitted only if such uses are incidental to the commercial the Interim Zoning Code allows for additional residential uses to be permitted through a Use Permit. The applicant has applied for a Use Permit and associated findings are provided below.

requirements for the residential components of the project. As a qualifying project, the applicant has applied for relief from the east side setback requirement under State Density Bonus Law. The design, scale and massing of the project would be consistent with development in the downtown Bolinas area which consists of a variety of structures that contribute to the unique character of the downtown area. As designed the project would not impair or interfere with the use or enjoyment of other properties in the vicinity, including public lands and rights-of-way.

- D. It will not directly, or in a cumulative fashion, impair, inhibit or limit further investment or improvements in the vicinity, on the same or other properties, including public lands and rights-of-way;**

The proposed project would be located entirely on the subject site that is under private ownership. Implementation of the project would not affect further investment or improvements in the vicinity, including public lands and rights-of-way.

- E. It will be properly and adequately landscaped with maximum retention of trees and other natural material;**

As discussed above, landscaping is proposed with the project that would harmonize with the surrounding environment while complying with fire standards. Development has been concentrated in the southern portion of the site avoiding excessive tree removal and ensuring the maximum retention of trees.

- F. It will minimize or eliminate adverse physical or visual effects which might otherwise result from unplanned or inappropriate development, design or juxtaposition. Adverse effects may include, but are not limited to, those produced by the design and location characteristics of:**

- 1. The scale, mass, height, area and materials of buildings and structures,**

While the maximum height of the structure would exceed 25 feet, such an exceedance is allowed under State Density Bonus Law. Even with the increased height, the proposed project would be compatible with the architectural style and character of the surrounding downtown Bolinas area which contain a mixture of two-story buildings of varying heights and massing. As discussed above, the proposed project would incorporate colors and materials that would harmonize with the built and natural environments. In addition, the selected exterior materials are earth-tone, non-reflective and unobtrusive. All exterior lighting proposed with the project would be directed downward and shielded so as not to cast glare on nearby properties.

- 2. Drainage systems and appurtenant structures,**

Conceptual project plans have been reviewed by DPW and found to be acceptable. Approval of project specific drainage plans would be required by DPW prior to the issuance of a project building permit.

- 3. Cut and fill or the reforming of the natural terrain, and structures appurtenant thereto such as retaining walls and bulkheads,**

Required Parking Spaces as the project, "includes the maximum percentage of extremely-low, very-low, or low-income units...and is located within one-half mile of a major transit stop...and there is unobstructed access to the major transit stop from the development...then upon the request of the developer, the vehicular parking ratio...shall not be required to exceed 0.5 spaces per bedroom...". In addition, Section 24.04.340(B) of the County Code requires a total of 1 parking space for every 200 square feet of non-residential use.

The project site is located 400 feet (0.076 mile) from a Golden Gate Transit stop and proposes a total of 20 bedrooms, therefore a minimum of 10 on-site parking spaces is required under Section 24.04.340 of the County Code for the residential component of the project. The non-residential component of the project requires a total of 6 parking spaces.

The applicant proposes a total 16 on-site parking spaces as well as four off-site parking spaces directly in front of the development on Wharf Road and meets the County's parking requirements.

In addition to parking, DPW reviewed the drive aisle widths and turning radius' proposed with the project and determined that the project meets County requirements.

The applicant provided a memorandum entitled, "Transportation Conditions Related to 31 Wharf Road in Bolinas" prepared by Parisi Transportation Consulting and dated June 15, 2020. The consultant provided an analysis of the maximum (or worst case) number of weekday vehicle trips that may be generated by the project. The analysis indicated that a total of 88 daily trips may be generated under this worst-case scenario. For the purposes of assessing whether a project may have a potentially significant effect on transportation under the California Environmental Quality Act (CEQA), vehicle miles traveled (VMT) that exceeds an applicable threshold of significance may indicate a potentially significant impact. According to the State's Technical Advisory on Evaluating Transportation Impacts in CEQA, projects that generate or attract fewer than 110 vehicle trips per day generally may be assumed to cause a less-than-significant impact on transportation. As the project would potentially generate 88 daily trips, the project would not conflict or be inconsistent with CEQA Guidelines for transportation.

It is not anticipated that the additional residential units proposed with project above those considered incidental to the commercial use would be detrimental to the public welfare or injurious to property or improvements in the neighborhood as the project has been sited in the most accessible portion of the site, would minimize potential environmental effects by focusing development on the disturbed portion on the site, would provide on-site parking consistent with County requirements and would not generate transportation effects under CEQA.

SECTION II: ACTION

NOW THEREFORE, BE IT RESOLVED that the project described in condition of approval 1 is authorized by the Marin County Board of Supervisors and is subject to the conditions of project approval.

This decision certifies the proposed project's conformance with the requirements of the Marin County Development Code and in no way affects the requirements of any other County, State, Federal, or local agency that regulates development. In addition to a Building Permit, additional

deemed vested until the permit holder has actually obtained any required Building Permit or other construction permit and has substantially completed improvements in accordance with the approved permits, or has actually commenced the allowed use on the subject property, in compliance with the conditions of approval.

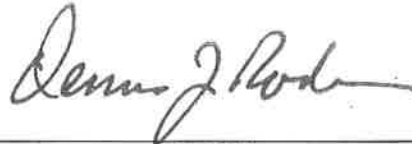
SECTION V: ADOPTION

PASSED AND ADOPTED at a regular meeting of the Board of Supervisors of Marin County held on this 8th of June 2021.

AYES: SUPERVISORS Damon Connolly, Katie Rice, Stephanie Moulton-Peters,
Judy Arnold, Dennis Rodoni

NOES: NONE

ABSENT: NONE



PRESIDENT, BOARD OF SUPERVISORS

ATTEST:



CLERK

CALIFORNIA COASTAL COMMISSION

NORTH CENTRAL COAST DISTRICT OFFICE
455 MARKET ST., SUITE 228
SAN FRANCISCO, CA 94105-2420
(415) 904-5260
NORTHCENTRALCOAST@COASTAL.CA.GOV

**APPEAL FORM****Appeal of Local Government Coastal Development Permit**

Filing Information (STAFF ONLY) District Office: North Central Coast

Appeal Number: A-2-MAR-21-0045

Date Filed: 6/29/2021

Appellant Name(s): H. Roland Crotts

APPELLANTS

IMPORTANT. Before you complete and submit this appeal form to appeal a coastal development permit (CDP) decision of a local government with a certified local coastal program (LCP) to the California Coastal Commission, please review the appeal information sheet. The appeal information sheet describes who is eligible to appeal what types of local government CDP decisions, the proper grounds for appeal, and the procedures for submitting such appeals to the Commission. Appellants are responsible for submitting appeals that conform to the Commission law, including regulations. Appeals that do not conform may not be accepted. If you have any questions about any aspect of the appeal process, please contact staff in the Commission district office with jurisdiction over the area in question (see the Commission's contact page at <https://coastal.ca.gov/contact/#/>).

Note regarding emailed appeals. Please note that emailed appeals are accepted ONLY at the general email address for the Coastal Commission district office with jurisdiction over the local government in question. For the North Coast district office, the email address is NorthCentralCoast@coastal.ca.gov. An appeal emailed to some other email address, including a different district's general email address or a staff email address, will be rejected. It is the appellant's responsibility to use the correct email address, and appellants are encouraged to contact Commission staff with any questions. For more information, see the Commission's contact page at <https://coastal.ca.gov/contact/#/>.

A-2-MAR-21-0045

Exhibit 5

1 of 23

Appeal of local CDP decision
Page 2

1. Appellant information:

Name: H. Roland Crotts, T'ee of the Crotts 2002 Trust

Mailing address: C/o Law Offices of John E. Sharp, 24 Professional Center Parkway, Suite 110, San Rafael, CA 94903

Phone number: 415.479.1645

Email address: john@johnsharpplaw.com

How did you participate in the local CDP application and decision-making process?

Did not participate

Submitted comment

Testified at hearing

Other

Describe: Appellant was the appellant of the decision by the Marin County Planning Commission of April 12, 2021.
Appellant appealed the Planning Commission's approval of the project to the Board of Supervisors
which heard the appeal on 6/8/21.

If you did *not* participate in the local CDP application and decision-making process, please identify why you should be allowed to appeal anyway (e.g., if you did not participate because you were not properly noticed).

Describe: _____

Please identify how you exhausted all LCP CDP appeal processes or otherwise identify why you should be allowed to appeal (e.g., if the local government did not follow proper CDP notice and hearing procedures, or it charges a fee for local appellate CDP processes).

Describe: Appellant appealed the Marin County Planning Commission's determination to the Marin County
Board of Supervisors and was heard at the hearing and submitted written material, attached hereto
and submitted as part of this appeal, which was heard on 6/8/21. Appellant is informed that the
County provided Notice to the Commission on 6/15/21.

1 If there are multiple appellants, each appellant must provide their own contact and participation information. Please attach additional sheets as necessary.

Appeal of local CDP decision
Page 3

2. Local CDP decision being appealed2

Local government name:	<u>County of Marin</u>
Local government approval body:	<u>Community Development Agency/Board of Supervisors</u>
Local government CDP application number:	<u>P2835, Board of Supervisors Resolution # 2021-42</u>
Local government CDP decision:	<u>CDP approval</u> CDP denial ³
Date of local government CDP decision:	<u>June 8, 2021 (Notice to Coastal Commission June 14, 2021).</u>

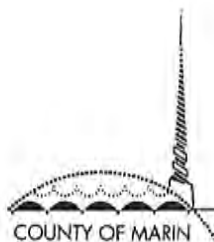
Please identify the location and description of the development that was approved or denied by the local government.

Describe: 31 Wharf Road, Bolinas, CA. Please see attached Notice (including project description)
from the Board of Supervisors hearing of 6/8/21, identified as Attachment A.

2 Attach additional sheets as necessary to fully describe the local government CDP decision, including a description of the development that was the subject of the CDP application and decision.

3 Very few local CDP denials are appealable, and those that are also require submittal of an appeal fee. Please see the [appeal information sheet](#) for more information.

ATTACHMENT A



COMMUNITY DEVELOPMENT AGENCY PLANNING DIVISION

NOTICE OF MARIN COUNTY BOARD OF SUPERVISORS HEARING
H. Roland Crotts, Trustee of the Crotts 2002 Trust, Appeal of the
Bolinas Community Land Trust (BCLT)
Coastal Permit, Design Review and Use Permit
Project ID P2835

NOTICE IS HEREBY GIVEN that the Marin County Board of Supervisors will consider issuing a decision on the H. Roland Crotts, Trustee of the Crotts 2002 Trust, appeal of the Bolinas Community Land Trust (BCLT) Coastal Permit, Design Review and Use Permit during a public hearing scheduled on Tuesday, June 8, 2021. The applicant for the project is the Bolinas Community Land Trust (BCLT), and the property is located at 31 Wharf Road, in Bolinas, further identified as Assessor's Parcel 193-061-03.

On July 7, 2020, the applicant submitted a Planning Application requesting Coastal Permit, Design Review and Use Permit approval to construct a 10,628-square-foot mixed use development consisting of 8,649 square feet of affordable housing units (a total of 8 units) and 1,639 square feet of commercial area on a vacant lot in Bolinas. The proposed development would result in floor area ratio of 10.15-percent on the 101,150-square-foot lot (2.32 acres) and would reach a maximum height of 33 feet above surrounding grade. The proposed residential component of the project would consist of eight units, all of which would be available for rent to low and very-low income households. The development would be contained in two, two-story structures, with the commercial component located on the first floor of the structure fronting Wharf Road. The residential units would range in size from 847 square feet to 1,174 square feet with the smaller units containing two bedrooms and the larger units containing three bedrooms. A total of four, three bedroom units and four, two bedroom units are proposed.

The exterior walls of the development would have the following setbacks: over 100 feet from the rear, north property line; 5.5 feet from the south, front property line; 0 feet from the east, side property line; and 9.5 feet from the west, side property line. Other site improvements include the construction of 6,934 square feet of on-site, project-related parking. On-site parking would be contained in an underground parking structure as well as in above ground carports. A total of 16 on-site parking spaces are proposed. In addition, four off-site parking spaces along Wharf Road directly in front of the site would be retained/created with the project.

The following approvals are required for the project: (1) Use Permit approval is required because the development consists of multi-family residential housing in the C-VCR zoning district (Marin County Interim Zoning Code Section 22.57.123(I)); (2) Design Review approval is required because new buildings are proposed in the C-VCR zoning district (Marin County Interim Zoning Code Section 22.82.020(I)); and (3) Coastal Permit approval is required because the project requires Use Permit and Design Review approval (Marin County Interim Zoning Code Section 22.42.020(I)).

For more information about the application, please visit the Planning Division's project webpage at:
https://www.marincounty.org/depts/cd/divisions/planning/projects/bolinas/bolinas_community_land_trust_cp_dr_up_p2835_bo

Project plans and other documents related to the application are available on the project's webpage, where you can subscribe to receive email notifications and updates.

A-2-MAR-21-0045

In compliance with local and state shelter-in-place orders, and as allowed by Governor
Exhibit 5
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correspondence delivered to the Community Development Agency, Planning Division during or prior to the public hearing. (Government Code Section 65009(b)(2).)

May 26, 2021

Michelle Levenson
Senior Planner



All public meetings and events sponsored or conducted by the County of Marin are held in accessible sites. Requests for accommodations may be made by calling (415) 473-4381 (Voice) 473-3232 (TDD/TTY) or by e-mail at disabilityaccess@marincounty.org at least four work days in advance of the event. Copies of documents are available in alternative formats, upon request.

Appeal of local CDP decision
Page 4

3. Applicant information

Applicant name(s): H. Roland Crotts, Tee of the Crotts 2002 Trust

Applicant Address: 31 Wharf Rd, Bolinas, CA 94924

4. Grounds for this appeal⁴

For appeals of a CDP approval, grounds for appeal are limited to allegations that the approved development does not conform to the LCP or to Coastal Act public access provisions. For appeals of a CDP denial, grounds for appeal are limited to allegations that the development conforms to the LCP and to Coastal Act public access provisions. Please clearly identify the ways in which the development meets or doesn't meet, as applicable, the LCP and Coastal Act provisions, with citations to specific provisions as much as possible. Appellants are encouraged to be concise, and to arrange their appeals by topic area and by individual policies.

Describe: Please see attached summary: Attachment B.

Appellant attaches hereto, as Attachment C and incorporates herein by reference,
the attachment to his appeal to the Marin County Board of Supervisors, dated April
19, 2021 and Appellant's counsel letter of 6/4/21 to the Board of Supervisors, in
support of that appeal, which was heard on 6/8/21.

⁴ Attach additional sheets as necessary to fully describe the grounds for appeal.

ATTACHMENT B

4. Grounds for this appeal

For appeals of a CDP approval, grounds for appeal are limited to allegations that the approved development does not conform to the LCP or to Coastal Act public access provisions. For appeals of a CDP denial, grounds for appeal are limited to allegations that the development conforms to the LCP and to Coastal Act public access provisions. Please clearly identify the ways in which the development meets or doesn't meet, as applicable, the LCP and Coastal Act provisions, with citations to specific provisions as much as possible. Appellants are encouraged to be concise, and to arrange their appeals by topic area and by individual policies.

Describe:

SEA LEVEL RISE: STORMWATER TREATMENT AND DRAINAGE: The approved development contains little information, solving the technical challenges of building such a large project on this sloping site. The geotechnical report prepared by Romic Engineers points out several areas that will need attention should the project go forward as planned. Because the project is on a slope, the expectation is that there will be a need for careful placement, and extensive monitoring, of a drainage system to collect, treat and discharge storm water. Special attention must be given to construction design and materials to ensure that there is adequate "damp proofing" of the proposed underground parking facility underneath both buildings at the bottom of the site. In addition, the amount of impermeable project hardscape will increase the flow of drainage. The project drains and storm water will flow directly into the Bolinas Lagoon, designated as a world heritage site in 1998. Appellant is concerned that sea level rise has not been addressed.

Further, the project lies 277.29 feet from Bolinas Lagoon, according to the attached Google Earth photo. Furthermore, it is disingenuous to say that the project "doesn't lie between the coast and the first road" (as stated in the staff report for the 6/8/21 Board of Supervisors appeal hearing), inasmuch as Wharf Road is the road bordering Bolinas Lagoon and, and its westward terminus, the beach and shoreline.

Further, the Applicant's Geotechnical Report raises questions regarding the function and sustainability of the excavated subterranean parking garage, insofar as various potential for flooding associated with the magnitude of said excavated subterranean parking areas, considering in the context of sea level rise, which has been frequent subject in Marin County's local coastal plan amendment efforts.

OTHER CALIFORNIA COASTAL ACT POLICIES

Article 6 Development

Section 30253

Minimize risk to life and property – geology, flood, fire

The project lies close to the San Andreas fault, the Bolinas Lagoon and the Pacific Ocean. The downtown is flooded from time to time, especially in the winter when high tides converge with heavy storm events. Though there are excellent volunteer fire departments in West Marin, there are many pockets of accumulated dead and dying vegetation that represent a buildup of highly flammable debris around the downtown. Emergency vehicle access to the downtown to minimize risk to life and property will be negatively impacted by the proposed project.

No structures requiring alteration of land forms

The project design includes a below ground parking structure that engineers hired by the BCLT advise will need increased support and monitoring because of its location at the bottom of a hillside. This aspect of the plan, along with the increase in site hardscape will definitely alter the land form. We would ask that the project be modified to reduce the number of units to remove the need for the subterranean structure. The increased hardscape of the site relates to the need for a driveway between the current set of two buildings, creating traffic congestion within the project, which would also be obviated if the project were reduced to a scale that is more in keeping with the historic downtown.

Section 30254

Where existing public works can accommodate only a limited amount of new development, must keep public recreation a priority

The on-going drought has brought home how water-constrained Bolinas is. The public utility district is struggling to meet anticipated demand through the summer and fall to the hopeful beginning of the next water year. Public restrooms are now closed and temporary facilities have been brought in to serve visitors. We are learning the hard lesson that we cannot assume that the public utility district can accommodate our current lifestyle desires for the number of water services already in place. Adding so many additional units will exacerbate the situation and reduce our ability to accommodate visitors' needs further.

Article 4 Marine Resources

Section 30230

Specially protected areas

The Bolinas Lagoon, a world heritage site, will be negatively impacted by run off generated by daily activities at a project of such proportions as is currently proposed.

Sections 30231 and 30232

Public health – control discharges and protection against spillage

Again, there is concern for on-going storm drain use coming from daily activities at the project site, as well as the ability of project to contain and treat the discharges coming from a large storm event.

Protect special communities and neighborhoods

As currently designed, the project does not offer protection to the Bolinas downtown community and impacts the larger community by increasing traffic in the area.

Section 30251

Protect scenic and visual qualities, protect views

The project will reduce scenic and visual qualities given its footprint that is out of proportion with the rest of the downtown buildings that are either 19-century clapboard buildings with characteristic false fronts or smaller, newer buildings that are generally one story (dentist's office) or low two story (Bolin's garage).

At least one house, 23 Wharf Road next to the project, will lose its long-standing view of the Bolinas Ridge.

Section 30252

Have access to public transportation, commercial facilities, non-auto circulation, adequate parking, assure potential for public transit, recognize the needs of new residents, don't overload nearby coastal roads

Public transportation is minimal during the week (four round-trips to and from Bolinas to Marin City), but does increase during the weekend (eight round trips from Sausalito) to accommodate visitors.

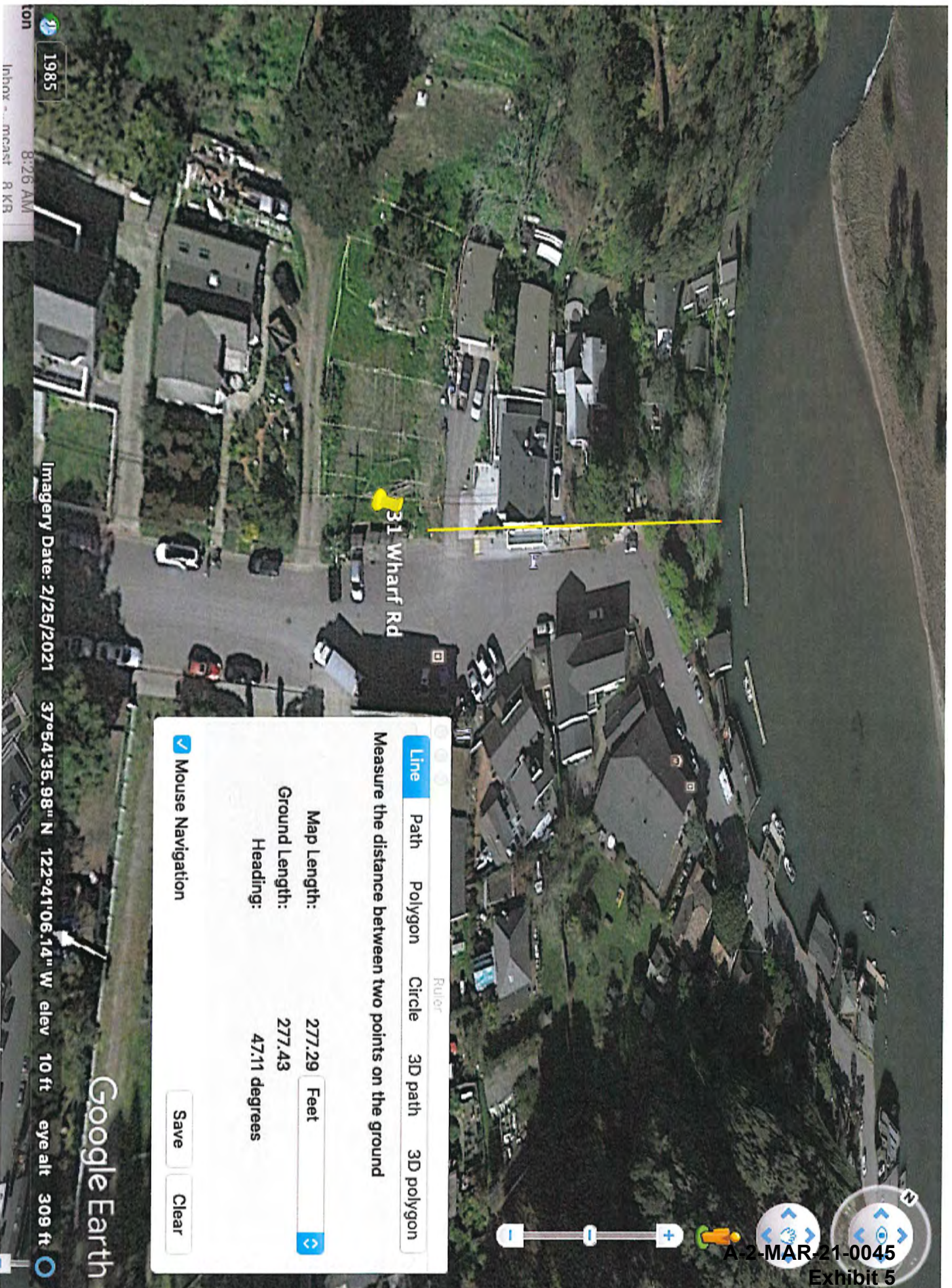
Circulation of automobiles on is congested in the small downtown area any day of the week, but especially on the weekends. Double parking and vehicles circling are common place. Parking is inadequate; visitors routinely park their vehicles in areas clearly marked "No Parking Anytime," creating dangerous conditions for all people coming to the downtown area. Residents now try to minimize their downtown trips by car as they wish to avoid the unpleasant urban experience of trying to find parking. Projects to work with California agencies to increase public transportation throughout West Marin would be most welcome.

Be consistent with air pollution control

The project will increase an already congested downtown, contributing to air pollution as well as to indoor air pollution from the subterranean structure below the lower building.

Minimum energy consumption and vehicle miles traveled

The project does not call out energy-conserving measures in the buildings, though one may assume they will be included in more detailed construction plans. On the other hand, energy consumption in vehicle miles traveled will certainly be increased, as inadequate public transportation will require residents to have one or more personal vehicles per apartment.



Ruler

Line	Path	Polygon	Circle	3D path	3D polygon
------	------	---------	--------	---------	------------

Measure the distance between two points on the ground

Map Length:	277.29	Feet
Ground Length:	277.43	
Heading:	47.11	degrees

☒ Mouse Navigation

Google Earth

Imagery Date: 2/25/2021

37°54'35.98" N 122°41'06.14" W elev 10 ft eye alt 309 ft

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Exhibit 5
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ATTACHMENT C 4/19/21

Attachment to Petition for Appeal to Board of Supervisors: H. Roland Crotts

Appellant's property, located at 23 Wharf Rd., is immediately adjacent to that of the Applicant. As such, Appellant's property is negatively affected by the height, bulk and mass of Applicant's project. The height of the improvements immediately facing Appellant's access, yard, and interior living areas, is out of character with the neighborhood.

The project encroached over Appellant's driveway and parking. Ingress and egress have not been addressed, in light of Appellant's easement rights, with which the project conflicts.

The proximity of the proposed development to Appellant's boundary as well as the improvements, including the home at 23 Wharf Road, are affected to such a degree that Appellant believes certain of the proposed Design Review findings are unsupported by substantial evidence as required by law. (See *Topanga Association for a Scenic Community v. County of Los Angeles* (1974) 11 Cal. 3d 506 and Code of Civil Procedure 1945, et seq.)

The findings set forth at, for example, Page 9, C, and F. 1. of the draft resolution, are unsupported by substantial evidence. The purported evidence is simply a restatement of the finding itself, and is prohibited by the reasoning in *Topanga*, supra. With particular reference to finding C, the recitals re "removal of onsite vegetation" and "design, scale and massing" reflect little or no attention to the tangible impacts of the Project upon Appellant's property.

In addition, the Planning Commission's approval of the project fails to address the fact that both water supply and sewage have been historically overly-burdened in Bolinas.

DESIGN REVIEW, COASTAL PERMIT AND USE PERMIT FINDINGS

All requisite findings require attention to "Sense of Openness." From Appellant's perspective, when one views the proposed development from Appellant's property, one is faced with unarticulated height, mass and bulk, creating a looming effect, such that privacy and the quiet use and enjoyment of Appellant's property are destroyed. As such, the findings, with reference to, in particular, mass and bulk (Design Guidelines D-1.1 through D-1.5) are unsupported. Contrary to the substantial evidence standard set forth at Code of Civil Procedure Section 1094.5, et seq, all the evidence here demonstrates that design review findings are unsupportable.

The current water and sewer system constraints constitute serious surcharging of the already

strained water supply and sewer/septic capacity in Bolinas. (Appellant will expand upon this in more detail in further correspondence to the Board).

THE STATE HOUSING AND ACCOUNTABILITY ACT.

The Staff Report and Draft Resolution fails to address the multitude of evidence in the administrative record, establishing that the Project is, in fact, burdened by “a preponderance of evidence” that the Project will result in a specific, adverse impact upon the public health and safety, will not be effectively serviced for water and sewer, is not consistent with the CWP, such that, pursuant to, without limitation, Gov. Code Sec. 65589.5, the project can, and should, be denied.

ADDITIONAL CONCERNS.

Common to all the entitlements sought by Applicant are the impacts stated above. These impacts negatively affect not only Appellant, but significantly change the character of the downtown Bolinas community. As such, the findings suffer from a lack of evidence to support them, such that Design Review approval, as well as that of the Coastal Permit and Use Permit should be set aside, and the project denied, or continued, pending further review consistent with the above.

Appellant reserves the right to supplement this Appeal and to submit such other and further documentation as may be appropriate up to and including at the time of hearing.

Law Offices of
JOHN E. SHARP
24 Professional Center Parkway, Suite 110
San Rafael, CA 94903

John E. Sharp
john@johnsharplaw.com

Telephone: (415) 479-1645
Facsimile: (415) 295-7020

June 4, 2021

VIA US. MAIL AND E-MAIL - DRodoni@marincounty.org;
BOS@marincounty.org;MLevenson@marincounty.org

Dennis Rodoni
Board of Supervisors
3501 Civic Center Drive, Suite 329
San Rafael, CA 94903

Re: Project 2835: 31 Wharf Road Bolinas CA 94924, Bolinas Community Land Trust (BCLT)

Dear President Rodoni and Members of the Board:

This office represents Appellant, Roland Crofts, Trustee of the Crofts Family Trust, owner of the property at 23 Wharf Road, immediately adjacent to the proposed project at 31 Wharf Road ("the Project"). The following supplements our appeal, filed on April 18, 2021. Please include this correspondence, and attachment, in the administrative record.

The Project lacks adequate access.

For 80 years, a right of way has existed between 23 and 31 Wharf Road, lying partially on each property. Applicant's own survey, dated March 26, 2020, performed by Ms. Ruschmeyer, shows the driveway. Mr. Crofts will testify that the driveway has been used for two-way access to and from 23 Wharf Rd. for the 80+ years it has existed. While we recognize that rights of way and easements are not enforced by the County as a planning matter, it is axiomatic that, for a variety of reasons including, but not limited to public safety concerns, a project, particularly of this magnitude, must have adequate ingress and egress. As demonstrated below, this project has no such ingress and egress.

A court has the power to "grant such equitable relief as may be proper under the circumstances of the case". We believe that the equities, namely, without limitation, exclusive use by the owners of 23 Wharf Rd. of the area in dispute for generations as part of the living area of 23 Wharf Road strongly compels exercise of the court's powers to adjudicate, at minimum, an equitable easement in favor of my Appellant and his successors. Also see *Hirshfield v. Schwartz* (2001) 91 Cal. App. 4th 749, in which the Court of Appeal, in the face of a survey revealing encroachments which had been built over the property in dispute over a period of 17 years, recognized the propriety of the trial court's having applied the relative hardship doctrine. Without exhaustively detailing the facts of the *Hirshfield* case here, we believe that it squarely applies to the matter at hand. We believe that the fact of use of the disputed driveway weighs heavily in

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favor of, at minimum, a court granting an equitable easement. As such, the Applicant cannot, as a planning matter, demonstrate access.

Moreover, in order to access the back of the Project, there is mention of an easement on the property at 41 Wharf Road. The coming and going of vehicles through the project will impede parking for Smiley's Saloon, the business on the other side of the project and would make an unacceptable demand on Appellant to drive through the project to get to his house. Put another way, the inability of this lot to accept a project of this magnitude, and Applicant's apparent attempt to require Appellant to access his property via a tiny driveway adjacent to Smiley's, creates an unacceptable and illegal shifting of Applicant's burden of providing access to at least 2 adjoining property owners.

Appellant further opposes this 8-unit multi-apartment project for 31 Wharf Road for the following reasons:

Government Code Section 65915 is misapplied and does not abrogate the County of its authority or responsibilities.

The Project relies upon recent amendments to California affordable housing law as identified in, without limitation, Government code Section 65915.

Although Appellant recognizes that, to some degree, the County is constrained by Section 65915, the County is not completely robbed of its discretion and sound judgment. Indeed, due to significant health and safety concerns with the proposed Project, as recognized in both state and local law, Appellant suggests that further review is required before the pending application can be deemed approved. (Government Code Sec. 65915 (e)(1)).

The following issues are unresolved, and compel further review:

WATER SUPPLY: A primary issue is the supply of water and sewer service specific to the property. The BCPUD letter in the planning file affirms that the property has an existing water and sewer meter. Those meters served a single-family residence which is no longer on the site. The project proposes to construct buildings that will house up to 40 residents. There is presently a cap on water use on site for new housing of 2700 cu ft of water per quarter, which, while adequate for a single family residence, stretches the limit beyond reason for 40 residents and two commercial enterprises.

Two possibilities for alternative water supply have been presented by the BCLT. One involves exchanging the project meters with the existing Gibson House meters, a BCLT-owned multiple dwelling building downtown that has a higher water allotment, but there is no mention of this or the fact of the town's limited water supply in the project description. Another possibility, that was not going to be used as stated in the description, is relying on water from a recently dug well on the property. This possibility has excited project proponents, and has led the BCPUD to request that it be contracted as an emergency

additional town water source. Both the BCLT and the BCPUD are overlooking the fact that the well water flow rate depends ultimately on rainwater, which is in such short supply this year, as well as the fact that it is close to the Bolinas Lagoon and Pacific Ocean and will likely become contaminated by salt water intrusion if relied on as a regular water source for the project..

We are in a drought, with rainfall at about one-third of average. The BCPUD has put the community on notice that unless water deliveries to the entire community remain below 76,000 g/p/d averaged over 7 days, it will have to impose a mandatory limit of 100 g/p/d per household. This limit may be lowered, as water use has recently increased, according to the BCPUD daily water delivery readings. While this dire situation may not preclude building a multi-family project, it underscores the urgency not to overbuild where the most crucial resource, water, is so limited.

SEWER CAPACITY: Household water use inevitably entails sewer use. While the site has a sewer meter, it is not obvious that the existing downtown sewer and treatment system has the capacity to accept the sewerage expected from the proposed project, regardless of whether or not its water use remains within the BCPUD limit.

Two homeowners outside the sewer district have recently requested that they be added to the system as their on-site systems no longer meet performance standards. In order to determine how to respond, the BCPUD must conduct a treatment system survey. Given the uncertainty about adding sewerage to the system, the BCLT downtown project should certainly be delayed until this matter is clarified.

STORMWATER TREATMENT AND DRAINAGE: There is little information or even mention in the project description of the technical challenges of building such a large project on the sloping site. The geotechnical report prepared by Romic Engineers points out several areas that will need attention should the project go forward as planned. Because the project is on a slope, the expectation is that there will be a need for careful placement, and extensive monitoring, of a drainage system to collect, treat and discharge storm water. Special attention must be given to construction design and materials to ensure that there is adequate "damp proofing" of the proposed underground parking facility underneath both buildings at the bottom of the site. In addition, the amount of impermeable project hardscape will increase the flow of drainage; there is no mention of this in the project description. The project drains and storm water will flow directly into the Bolinas Lagoon, designated as a world heritage site in 1998. It is therefore of utmost importance that an effective treatment and monitoring system be in place for this project.

PARKING: The project has been granted a variance for the number of parking spaces it must include based its location within 400 ft of a transit hub. The Bolinas-Mill Valley shuttle service schedule provides four buses to and from Bolinas each day during the week. The weekend schedule, eight trips back and forth to Sausalito, is focused on visitor, not resident, use. This does not fit the intended definition of a transit hub. Thus, the project should have the usual required number of parking spaces, which is not feasible given the size of the footprint. Anticipating, then, that most residents would park on the street, the already congested downtown situation will be increased beyond reasonable safety and public health considerations.

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Attached hereto is a photograph depicting actual use of the 4 proposed parking spaces relied upon as on-street parking to support the project. This photo shows use of the referenced parking area on a recent typical day. Clearly, parking remains unresolved and, in fact, will be exacerbated if the project is approved as-is.

Proposed Findings Do Not Meet Legal Requirements:

Pursuant to California code of Civil Procedure 1094.5, et seq, and applicable case law, proper findings, supported by substantial evidence, must be made in order to approve a project. As stated in *Topanga Ass'n for a Scenic Community v. County of Los Angeles* (1974) 11 Cal. 3d 506, 511 (emphasis added), an administrative grant must be supported by administrative findings. A court reviewing that grant must determine whether substantial evidence supports the findings and whether the findings support the conclusion that **all** applicable legislative requirements for a variance have been satisfied. Here, as more particularly set for the below, such findings are not properly made. Rather, the findings are circular in nature, thus failing to bridge the analytic gap between mere recitation of the findings and actual evidence to support them.

HEIGHT, BULK and MASS: The project lies immediately adjacent to Appellant's property. The expanse of unarticulated mass, shown in the project plans in the elevations depicted at Sheets A3.02, et seq, clearly demonstrates that the quality-of-life values sought to be protected by, without limitation, Marin County Code, Ch 20.82.040, are not being respected.

In addition, the project will be out of proportion with the rest of the downtown buildings. The project as described requires a variance as the front building wall is somewhat above the 27' allowance. The plans show that the height is 27.6'. According to the building plans, the height of the front building will be 40' and the back building, 46'6". A variance is requested according to guidelines for building affordable housing, but it is very much out of proportion with the rest of the downtown architecture.

Lest the review process devolve to an attempt to simply dichotomize whether one is for or against affordable housing (Appellant is NOT against a sustainable, community-serving project), Appellant points out that there is room for compromise in the design, while still addressing the affordable housing needs of the Bolinas community. By way of example, consensus could be built around a project with a smaller footprint, fewer units, consisting of a single structure without subsurface parking. This would also address Appellant's access concerns.

Conclusion

In summary, the approval granted by the Planning Commission is, at best, premature. While Appellant recognizes that affordable housing creates mandates which change the face of land use planning in local jurisdictions, said jurisdictions, including the County, are not divested of their abilities and requirements

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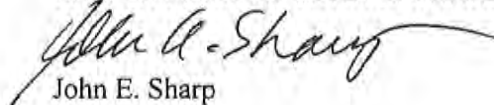
to ensure that existing neighborhoods and zoning districts are not impacted in such a way as to place in jeopardy the health and safety of neighbors of any given project. For the above stated reasons, Appellant respectfully requests that the County sustain this appeal. Alternatively, the matter should be continued for further study consistent with the matters identified herein and expressed by others in the community similarly concerned.

Finally, Appellant reserves the right to submit such further materials and testimony as may be relevant up to and including at the Planning Commission Meeting on the Administrative Review.

If you have any questions or comments, please feel free to be in touch.

Very truly yours,

LAW OFFICES OF JOHN E. SHARP



John E. Sharp

JES/AEA

cc: Roland Crotts

Appeal of local CDP decision
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5. Identification of interested persons

On a separate page, please provide the names and contact information (i.e., mailing and email addresses) of all persons whom you know to be interested in the local CDP decision and/or the approved or denied development (e.g., other persons who participated in the local CDP application and decision making process, etc.), and check this box to acknowledge that you have done so.

Interested persons identified and provided on a separate attached sheet

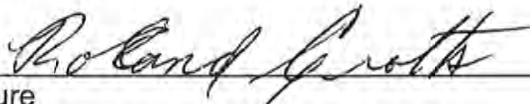
6. Appellant certification⁵

I attest that to the best of my knowledge, all information and facts in this appeal are correct and complete.

Print name Roland Crofts

X

Signature



Date of Signature 6/25/2021

7. Representative authorization⁶

While not required, you may identify others to represent you in the appeal process. If you do, they must have the power to bind you in all matters concerning the appeal. To do so, please complete the representative authorization form below and check this box to acknowledge that you have done so.

I have authorized a representative, and I have provided authorization for them on the representative authorization form attached.

⁵ If there are multiple appellants, each appellant must provide their own certification. Please attach additional sheets as necessary.

⁶ If there are multiple appellants, each appellant must provide their own representative authorization form to identify others who represent them. Please attach additional sheets as necessary.

ATTACHMENT D

5. Identification of interested persons

On a separate page, please provide the names and contact information (i.e., mailing and email addresses) of all persons whom you know to be interested in the local CDP decision and/or the approved or denied development (e.g., other persons who participated in the local CDP application and decision making process, etc.), and check this box to acknowledge that you have done so.

Interested persons identified and provided on a separate attached sheet

1) **Supporters of Appellant's appeal to the County Board of Supervisors and to the Coastal Commission (partial list)**

Genie McNaughton (geniemcnaughton@gmail.com);
Jannine Aroyan (beauxgardens@aol.com);
Jennifer Pfeiffer (jenniepfeifferr@gmail.com);
Rudi Ferris (ruferris@msn.com)

2) **County of Marin Community Development Agency**

3501 Civic Center Drive, Ste 308, San Rafael, CA 94903
Michelle Levenson (MLevenson@marincounty.org); Senior Planner

3) **Bolinas Community Land Trust: Applicant**

22 Brighton Ave, Bolinas, CA 94924
info@bolinaslandtrust.org

4) **County of Marin Board of Supervisors**

3501 Civic Center Drive, Suite 329, San Rafael, CA 94903
BOS@marincounty.org

CALIFORNIA COASTAL COMMISSION

455 MARKET STREET, SUITE 300
SAN FRANCISCO, CA 94105-2219
VOICE (415) 904-5200
FAX (415) 904-5400

**DISCLOSURE OF REPRESENTATIVES**

If you intend to have anyone communicate on your behalf to the California Coastal Commission, individual Commissioners, and/or Commission staff regarding your coastal development permit (CDP) application (including if your project has been appealed to the Commission from a local government decision) or your appeal, then you are required to identify the name and contact information for all such persons prior to any such communication occurring (see Public Resources Code, Section 30319). The law provides that failure to comply with this disclosure requirement prior to the time that a communication occurs is a misdemeanor that is punishable by a fine or imprisonment and may lead to denial of an application or rejection of an appeal.

To meet this important disclosure requirement, please list below all representatives who will communicate on your behalf or on the behalf of your business and submit the list to the appropriate Commission office. This list could include a wide variety of people such as attorneys, architects, biologists, engineers, etc. If you identify more than one such representative, please identify a lead representative for ease of coordination and communication. You must submit an updated list anytime your list of representatives changes. You must submit the disclosure list before any communication by your representative to the Commission or staff occurs.

Your Name H. Roland Crotts, Tee of the Crotts 2002 Trust

CDP Application or Appeal Number 2-MAR-21-0526

Lead Representative

Name John E. Sharp, Law Offices of John E. Sharp

Title Attorney

Street Address 24 Professional Center Parkway, Suite 110

City San Rafael

State, Zip California, 94903

Email Address john@johnsharpplaw.com

Daytime Phone 415.479.1645

Your Signature Roland Crotts

Date of Signature 6/25/2021

A-2-MAR-21-0045**Exhibit 5****20 of 23**

ATTACHMENT B

4. Grounds for this appeal

For appeals of a CDP approval, grounds for appeal are limited to allegations that the approved development does not conform to the LCP or to Coastal Act public access provisions. For appeals of a CDP denial, grounds for appeal are limited to allegations that the development conforms to the LCP and to Coastal Act public access provisions. Please clearly identify the ways in which the development meets or doesn't meet, as applicable, the LCP and Coastal Act provisions, with citations to specific provisions as much as possible. Appellants are encouraged to be concise, and to arrange their appeals by topic area and by individual policies.

Describe:

SEA LEVEL RISE: STORMWATER TREATMENT AND DRAINAGE: The approved development contains little information, solving the technical challenges of building such a large project on this sloping site. The geotechnical report prepared by Romic Engineers points out several areas that will need attention should the project go forward as planned. Because the project is on a slope, the expectation is that there will be a need for careful placement, and extensive monitoring, of a drainage system to collect, treat and discharge storm water. Special attention must be given to construction design and materials to ensure that there is adequate "damp proofing" of the proposed underground parking facility underneath both buildings at the bottom of the site. In addition, the amount of impermeable project hardscape will increase the flow of drainage. The project drains and storm water will flow directly into the Bolinas Lagoon, designated as a world heritage site in 1998. Appellant is concerned that sea level rise has not been addressed.

Further, the project lies 277.29 feet from Bolinas Lagoon, according to the attached Google Earth photo. Furthermore, it is disingenuous to say that the project "doesn't lie between the coast and the first road" (as stated in the staff report for the 6/8/21 Board of Supervisors appeal hearing), inasmuch as Wharf Road is the road bordering Bolinas Lagoon and, and its westward terminus, the beach and shoreline.

Further, the Applicant's Geotechnical Report raises questions regarding the function and sustainability of the excavated subterranean parking garage, insofar as various potential for flooding associated with the magnitude of said excavated subterranean parking areas, considering in the context of sea level rise, which has been frequent subject in Marin County's local coastal plan amendment efforts.

OTHER CALIFORNIA COASTAL ACT POLICIES

Article 6 Development

Section 30253

Minimize risk to life and property – geology, flood, fire

The project lies close to the San Andreas fault, the Bolinas Lagoon and the Pacific Ocean. The downtown is flooded from time to time, especially in the winter when high tides converge with heavy storm events. Though there are excellent volunteer fire departments in West Marin, there are many pockets of accumulated dead and dying vegetation that represent a buildup of highly flammable debris around the downtown. Emergency vehicle access to the downtown to minimize risk to life and property will be negatively impacted by the proposed project.

No structures requiring alteration of land forms

The project design includes a below ground parking structure that engineers hired by the BCLT advise will need increased support and monitoring because of its location at the bottom of a hillside. This aspect of the plan, along with the increase in site hardscape will definitely alter the land form. We would ask that the project be modified to reduce the number of units to remove the need for the subterranean structure. The increased hardscape of the site relates to the need for a driveway between the current set of two buildings, creating traffic congestion within the project, which would also be obviated if the project were reduced to a scale that is more in keeping with the historic downtown.

Section 30254

Where existing public works can accommodate only a limited amount of new development, must keep public recreation a priority

The on-going drought has brought home how water-constrained Bolinas is. The public utility district is struggling to meet anticipated demand through the summer and fall to the hopeful beginning of the next water year. Public restrooms are now closed and temporary facilities have been brought in to serve visitors. We are learning the hard lesson that we cannot assume that the public utility district can accommodate our current lifestyle desires for the number of water services already in place. Adding so many additional units will exacerbate the situation and reduce our ability to accommodate visitors' needs further.

Article 4 Marine Resources

Section 30230

Specially protected areas

The Bolinas Lagoon, a world heritage site, will be negatively impacted by run off generated by daily activities at a project of such proportions as is currently proposed.

Sections 30231 and 30232

Public health – control discharges and protection against spillage

Again, there is concern for on-going storm drain use coming from daily activities at the project site, as well as the ability of project to contain and treat the discharges coming from a large storm event.

Protect special communities and neighborhoods

As currently designed, the project does not offer protection to the Bolinas downtown community and impacts the larger community by increasing traffic in the area.

Section 30251

Protect scenic and visual qualities, protect views

The project will reduce scenic and visual qualities given its footprint that is out of proportion with the rest of the downtown buildings that are either 19-century clapboard buildings with characteristic false fronts or smaller, newer buildings that are generally one story (dentist's office) or low two story (Bolin's garage).

At least one house, 23 Wharf Road next to the project, will lose its long-standing view of the Bolinas Ridge.

Section 30252

Have access to public transportation, commercial facilities, non-auto circulation, adequate parking, assure potential for public transit, recognize the needs of new residents, don't overload nearby coastal roads

Public transportation is minimal during the week (four round-trips to and from Bolinas to Marin City), but does increase during the weekend (eight round trips from Sausalito) to accommodate visitors.

Circulation of automobiles on is congested in the small downtown area any day of the week, but especially on the weekends. Double parking and vehicles circling are common place. Parking is inadequate; visitors routinely park their vehicles in areas clearly marked "No Parking Anytime," creating dangerous conditions for all people coming to the downtown area. Residents now try to minimize their downtown trips by car as they wish to avoid the unpleasant urban experience of trying to find parking. Projects to work with California agencies to increase public transportation throughout West Marin would be most welcome.

Be consistent with air pollution control

The project will increase an already congested downtown, contributing to air pollution as well as to indoor air pollution from the subterranean structure below the lower building.

Minimum energy consumption and vehicle miles traveled

The project does not call out energy-conserving measures in the buildings, though one may assume they will be included in more detailed construction plans. On the other hand, energy consumption in vehicle miles traveled will certainly be increased, as inadequate public transportation will require residents to have one or more personal vehicles per apartment.