

CALIFORNIA COASTAL COMMISSION

South Coast District Office
301 E Ocean Blvd., Suite 300
Long Beach, CA 90802-4302
(562) 590-5071



W17a

P-3-20-78-2978-A1 (2918 3rd St, LLC)

SEPTEMBER 8, 2021

EXHIBITS

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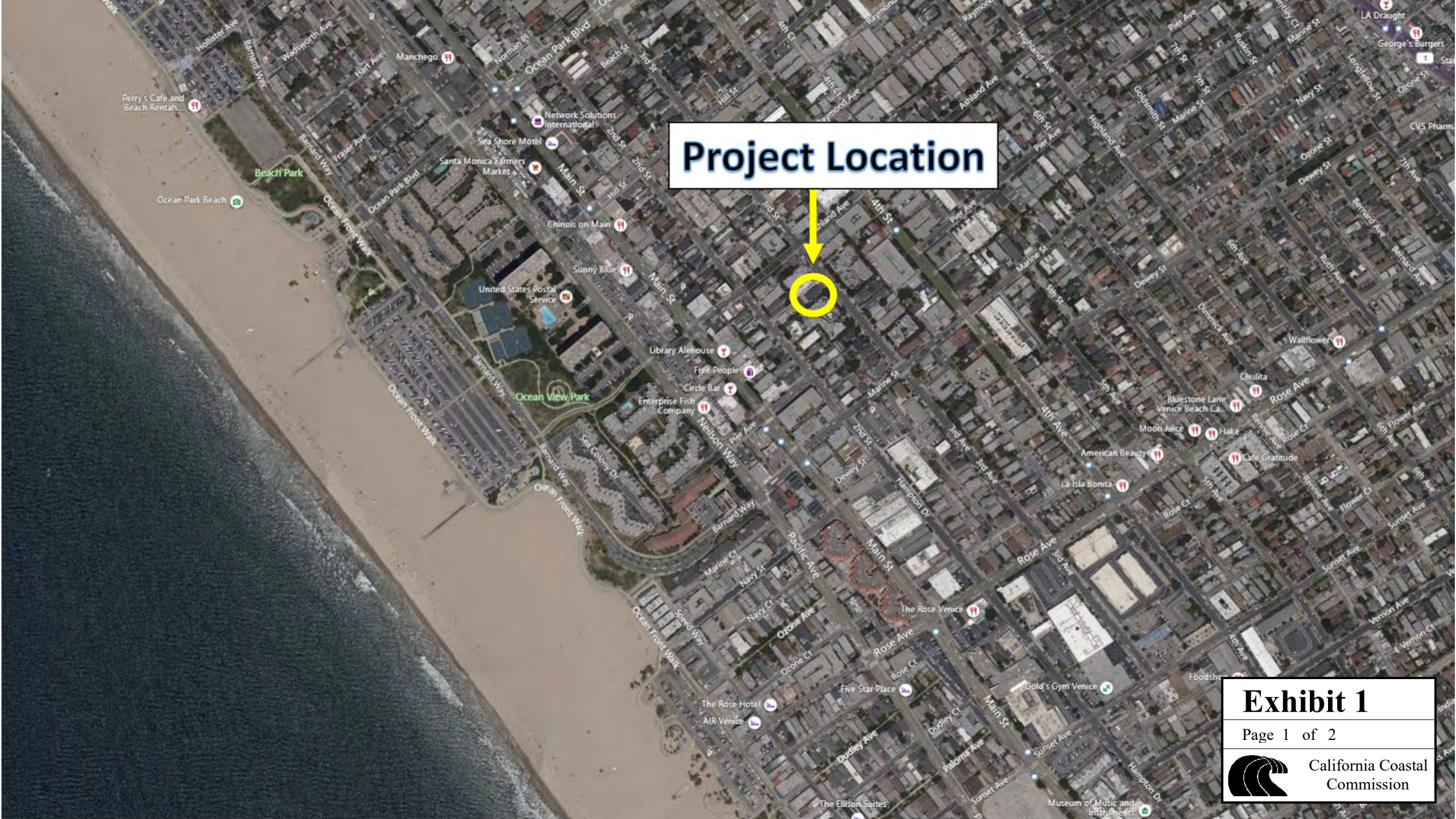
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Project Location



Project Site

2918 3rd St

3rd Street

Exhibit 1

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California Coastal
Commission

2918 3RD ST - ADU

SANTA MONICA, CA 90405

REVISION AND/OR ISSUE DATE
10/19/2020
COASTAL COMMISSION
SUBMITTAL



NOTES

ARCHITECT:
BALLENTINE ARCHITECTS STUDIO
315 WASHINGTON BOULEVARD
VENICE, CA 90292
PH: 424-835-4338
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PROJECT FILE AND ADDRESS:
2918 3RD ST - ADU
SANTA MONICA, CA
90405

DRAWING SCALE
N.T.S.

SHEET NAME

COVER SHEET

Exhibit 2

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California Coastal
Commission

& < C O _l # (E)	And Angle At Centerline Diameter or Round Perpendicular Number Existing	D.D. D.F. D.B. DET. D.F. DIA. DIM. DISP. DN. D.O. DR. D.S. D.S.P. DWG. DWR. DWP. D.W. DX. E. EA. E.J. ELEV. ELEC. ELEV. EMER. ENCL. E.O.S. EQ. EQUIP. E.S.H.A. ESC. E.W. E.W.P. BLK. BLKG. BM. BRG. BOT. B. B.R. B.T. B.I. BSMT. BS.BN. B.U.R. B.L. B.V.L.	Driveway Drain, Deck Drain Drinking Fountain Double Detail Drinking Fountain Diameter Dimension Dispenser/ Disposal Down Door Opening Door Downspout Dry Standpipe Drawing Drawer Drywall Painted Dish Washer Duplex East Each Expansion Joint Elevation Electrical Elevator Emergency Enclosure Edge of Slab Equal Equipment Environmentally Sensitive Habitat Area Escalator Each Way Electric Water Heater (E)EXIST. EXP. EXPO. EXT. Exit F.A. F.A.R. F.D. FDN. F.E. F.F. F.E.C. F.G. F.G.L. F.H.C. F.H. FIN. FLASH. FLR. FLUOR. F.O.C. F.O.F. F.O.M. F.O.S. FRF. FR. F.R.T. F.S. F.P.L. FT. FTG. FURN. FUT. FIX. G. GAL. G.C. GTR. GA. GALV. G.B. GALV. G.B.	Glass, Glazing Ground Grade Gypsum Board H. H.B. H.C. HCP. HDR. HDWR. HDWD. H.M. H.P. HORIZ. HR. HT. H.V. H.W. H.WY. I.D. IN. INFO. INCL. INSUL. INT. INSTL. I.P. JAN. J.BOX. JST. JT. Joint KIT. K.S. KO. Knockout LAM LAV. L.F. L.H. LKR. LNDG. L.R. L.R. L.T. LVR. LCL. LMS. LV. MATL. MAR. MAX. M.B. M-BOX. MECH. MEMB. MEZZ. MTL. MFR. MH. MIN. MIR. MISC. M.O. MONO. M.O. MTO. MUL. MWK. N. NAT. N. N.J. Not Included	N.I.C. NO. NOM. N.S. N.T.S. Not to scale O/ O.A. OBS. O.C. O.DIA. O.D. O.F.C.I. O.H. O.H.D. OFF. O.H. O.VHD. OPNG. OPP. OUT. OR. P.C. P.C.P. PED. P.G. PERIM. P.D. P.L. P.L. P.LB.G. PLAM. PLAS. PLYWD. PL. P.H. PH. PT. P.T. P.T.D. PTN. PKG. P.U.E. QUAL. QUANT. Q.T. R. RAD. R.D. REF. RET.G. REFR. REINF. REQ. REM. RESIL. REV. RFG. R.H. RM. R.O. R.O.W. Redwood S. S.A.C.T. S&S S&V S.C. SCUP. SCHED. S.D. S.G. SECT. SECT. SE. SH.	Not in Contract Number Nominal No Scale Not to scale Over Overall Obscure On Center Outside Diameter Overflow Drain (Roof Plan) Owner Furnish Contractor Installed Over Head Over Head Door Office Overhang Overhead Opening Opposite Outlet Ornamental P.C. P.C.P. PED. P.G. PERIM. P.D. P.L. P.L. P.LB.G. PLAM. PLAS. PLYWD. PL. P.H. PH. PT. P.T. P.T.D. PTN. PKG. P.U.E. QUAL. QUANT. Q.T. R. RAD. R.D. REF. RET.G. REFR. REINF. REQ. REM. RESIL. REV. RFG. R.H. RM. R.O. R.O.W. Redwood S. S.A.C.T. S&S S&V S.C. SCUP. SCHED. S.D. S.G. SECT. SECT. SE. SH.	Shower Sheet Sheathing Similar Sliding Specification Square Square Foot Square Steel Service Sink STD. Standard Steel STL. STOR. STRUCT. STWY. SKL. Sink SUSP. Switch SYM. SYS. System T. T.B. T&G Tongue & Groove Top of Curb T.D. Top of Drain TEL. TEMP. TERR. Terrazzo THK. Thickness THR. Threshold TOIL. Toilet T.O.P. Top Of Pad T.O.S. Top Of Sheathing T.P. Top of Pavement T.P.D. Toilet Paper Dispenser T.S. Tube Steel T.V. Television T.O.W. Top of Wall TYP. Typical T&B Top & Bottom T.O.C. Top of Curb T.O.M. Top of Masonry T.O.P.P. Top of Parapet T.O.R.D. Top of Roof Deck UNF. Unfinished U.O.N. Unless Otherwise Noted UR. Urinal UP. Unpainted UT. Utility VARN. Varnish V.B. Vapor Barrier VERT. Vertical VEST. Vestibule V.I.F. Verify in Field VOL. Volume VIN. Vinyl V.P. Vent Pipe V.T.R. Vent through Roof W. West W/ With W/O Without WIN. Window W.H. Water Heater W/O Without W.C. Water Closet WD. Wood WP. Waterproof WPM. Waterproof Membrane W.M. Water Meter WCST. Wainscot W.S.P. Wet Stand Pipe WT. Weight W.I. Wrought Iron
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PROJECT LOCATION:	2918 3RD ST, SANTA MONICA, CA 90405
BLOCK:	B
LOT:	4
TRACT:	SANTA FE TRACT
APN:	4287-024-005
ZONE:	SM OP2
CONSTRUCTION TYPE:	TYPE V-B (NON-SPRINKLERED)
OCCUPANCY TYPE:	R2
LOT SIZE:	6,547 SQ. FT.
DETACHED ADU:	
STORIES:	2
MAXIMUM STORY LIMIT:	2
BUILDING HEIGHT:	19'-2"
BUILDING HEIGHT LIMIT:	23'-0" PER OP2 ZONING CODE
SETBACKS:	
FRONT:	4'-0"
REAR:	4'-0"
FLOOR AREA:	
EXISTING CARPORT TO BE DEMOLISHED:	900 SQ. FT.
ADU A (GROUND FLOOR):	582 SQ. FT.
ADU B (SECOND FLOOR):	582 SQ. FT.
ADD. PARCEL COVERAGE:	687 SQ. FT.
ATTACHED ADU:	
STORIES:	2
MAXIMUM STORY LIMIT:	2
BUILDING HEIGHT:	19'-2"
BUILDING HEIGHT LIMIT:	23'-0" PER OP2 ZONING CODE
SETBACKS:	
FRONT:	10'-0"
SIDE:	4'-0"
REAR:	4'-0"
FLOOR AREA:	
DETACHED ADU:	
LAUNDRY:	150 SQFT
STORAGE:	232 SQFT
GARAGE:	241 SQFT
(N) FLOOR AREA:	
ATTACHED ADU:	700 SQFT
PARKING:	
(E) PARKING PROVIDED:	5
(N) PARKING PROVIDED:	2 PER CA GOVT. CODE 65852.2 (b) (6) (CARPORT. PUBLIC TRANSIT IS LOCATED WITHIN 0.1 MILES OF PROPERTY, SEE VICINITY MAP)
ALL CONSTRUCTION TO COMPLY WITH 2020 SANTA MONICA MUNICIPAL CODE, TITLE 24 & 2019 CALIFORNIA GREEN BUILDING CODE, 2019 CALIFORNIA BUILDING CODE (CBC), 2019 CALIFORNIA MECHANICAL CODE (CMC), CALIFORNIA PLUMBING CODE (CPC), CALIFORNIA ELECTRICAL CODE (CEC), AND CALIFORNIA ENERGY CODE (CENC).	

ARCHITECTURAL DRAWINGS	
A1.01	TITLE SHEET
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GEOTECHNICAL PRELIMINARY GEOTECHNICAL REPORT (DATED JULY 13, 2020)	
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CATALOG OF REPORTS	
3	
DEMOLITION OF (E) 900 SQ. FT. 5-CAR CARPORT AND CONSTRUCTION OF (2) DETACHED ADUS PER CA GOVT. CODE 65852.2. CONVERSION OF (B) LAUNDRY, STORAGE, AND GARAGE (623 SQFT) INTO (N) 700 SQFT ATTACHED ADU.	
SCOPE OF WORK	
2	

DETAIL	AREA DETAIL	DOOR AND WINDOW CALLOUTS	SECTION	WORK POINT CONTROL POINT OR DATUM POINT
DETAIL NO. SHEET NO.	DETAIL NO. SHEET NO.	WINDOW SYMBOL DOOR SYMBOL	SECTION NO. SHEET NO.	
INTERIOR ELEVATION	EXT. ELEVATION	CONSTRUCTION TYPE SYMBOL	WALL SECTION	NORTH ARROW
GRID SPACE NO. ELEVATION NO. SHEET NO.	ELEVATION NO. SHEET NO.	SECTION NO. SHEET NO.	SECTION NO. SHEET NO.	

SYMBOLS 7



VICINITY MAP 5

THESE DRAWINGS, DESIGN CONCEPTS, AND IDEAS ARE THE PROPERTY OF THE ARCHITECT BENJAMIN BALLENTINE AND SHALL NOT BE USED FOR ANY PURPOSE OTHER THAN THE CONSTRUCTION OF THE SPECIFIC PROJECT INDICATED HEREIN, AND SHALL NOT BE USED UNLESS PERSONALLY ENDORSED ON EACH DRAWING.

- | | | | |
|------------|---|---------------|---|
| SITE NOTES | 3 | SITE KEYNOTES | 2 |
|------------|---|---------------|---|

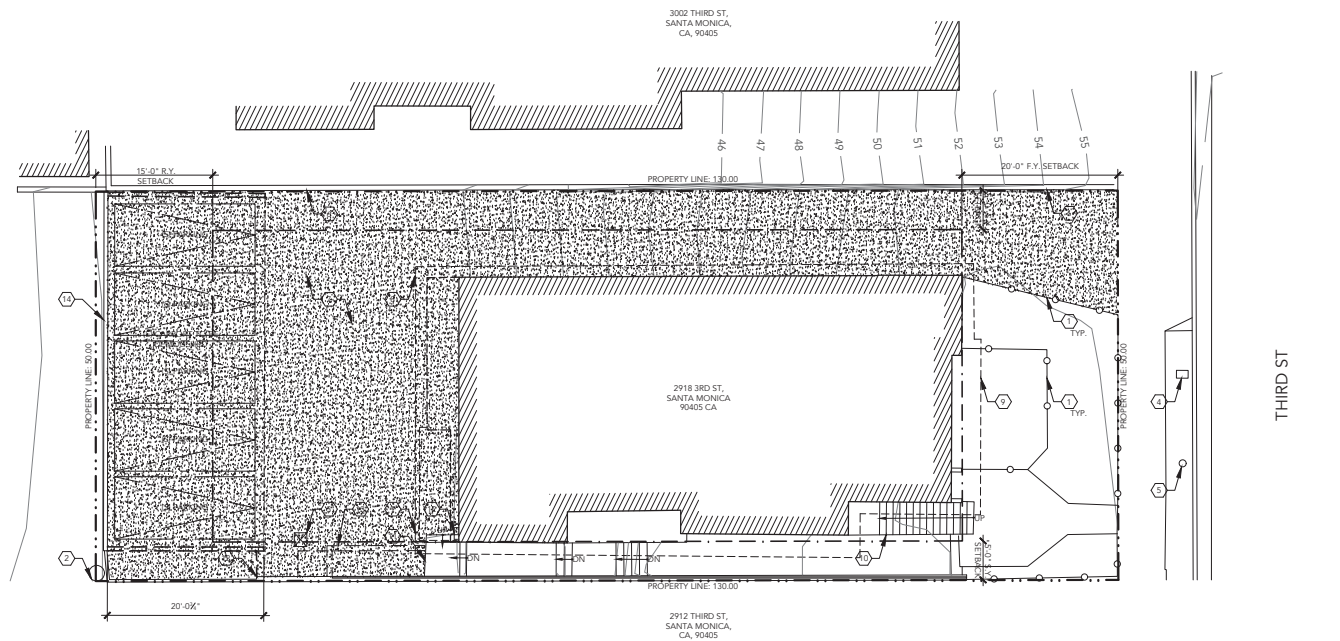
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ARCHITECTS • PLANNING • INTERIOR DESIGN

DEMOLITION
SITE PLAN

California Coastal
Commission

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DEMOLITION S

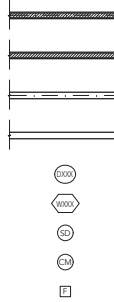



OUTDOOR LIVING AREA

- | | |
|---------------|---|
| SITE KEYNOTES | 2 |
|---------------|---|

SITE PLAN



<ol style="list-style-type: none"> GLAZING WITHIN 12" OF DOORWAY, WITHIN 18" OF FLOOR IN SHOWER, FOR BATHTUB DOORS AND ENCLOSURES SHALL BE TEMPERED GLASS. EXHAUST FANS SHALL BE CAPABLE OF PERFORMING ONE COMPLETE AIR CHANGE EVERY 5 MINUTES. ACES AND SIMILAR OPENINGS WHICH AFFORD A PASSAGE FOR FIRE AT CEILING AND FLOOR LEVELS, WITH NONCOMBUSTIBLE MATERIALS. (718.2.5) PROVIDE MIN. 32" WIDE DOORS TO ALL ROOMS. ALL TOILETS SHALL BE LOW FLOW UNITS. ALL FAUCETS AND SHOWER HEADS SHALL BE EQUIPPED WITH FLOW CONTROL DEVICES TO LIMIT FLOW. PROVIDE 15" MINIMUM CLEAR BETWEEN THE CENTER OF WATER CLOSET TO ANY SIDE WALL AND 24" CLEAR SPACE IN FRONT OF ANY WATER CLOSET. SOUND INSULATION REQUIRED IN ALL TOILET ROOM WALLS. PENETRATIONS IN A FIRE-RATED WALL SHALL BE PROTECTED BY AN APPROVED FIRE STOP MATERIAL IN ACCORDANCE WITH SECTION 714.3.12 INCHES NOMINAL LUMBER FIREBLOCKING SHALL BE PROVIDED IN CONCEALED SPACES OF STUD WALLS AND PARTITIONS, INCLUDING FURRED SPACES AT THE CEILING AND FLOOR LEVELS AND AT 10-FOOT INTERVALS BOTH VERTICAL AND HORIZONTAL. STEEL COPPER OR FERROUS PIPES OR CONDUITS MAY PENETRATE CONCRETE OR MASONRY WALLS WHERE THE PENETRATING ITEM IS A MAXIMUM 6- INCH DIAMETER AND THE AREA OF THE OPENING THROUGH THE WALL DOES NOT EXCEED 144 SQUARE INCHES. MEMBRANE PENETRATIONS OF MAXIMUM 2- HR. FIRE-RESISTANCE RATED WALL AND PARTITIONS BY STEEL ELECTRICAL OUTLET BOXES NOT EXCEEDING 16 SQUARE INCHES ARE PERMITTED PROVIDED OPENINGS DO NOT EXCEED 100 SQUARE INCHES FOR ANY 100 SQUARE FEET OF WALL AREA. OUTLET BOXES ON OPPOSITE SIDES OF WALLS OR PARTITIONS MUST BE SEPARATED BY A HORIZONTAL DISTANCE OF 24 INCHES. (714.3.2) AN APPROVED SMOKE ALARM SHALL BE INSTALLED IN EACH SLEEPING ROOM & HALLWAY OR AREA GIVING ACCESS TO A SLEEPING ROOM, AND ON EACH STORY AND BASEMENT FOR DWELLINGS WITH MORE THAN ONE STORY. SMOKE ALARMS SHALL BE INTERCONNECTED SO THAT THE ACTUATION OF ONE ALARM WILL ACTIVATE ALL THE ALARMS WITHIN THE INDIVIDUAL DWELLING UNIT. IN NEW CONSTRUCTION SMOKE ALARMS SHALL RECEIVE THEIR PRIMARY POWER SOURCE FROM THE BUILDING WIRING AND SHALL BE EQUIPPED WITH BATTERY BACK-UP AND LOW BATTERY SIGNAL. AN APPROVED CARBON MONOXIDE ALARM SHALL BE INSTALLED IN DWELLING UNITS AND IN SLEEPING UNITS WITHIN WHICH FUEL-BURNING APPLIANCES ARE INSTALLED AND IN DWELLING UNITS THAT HAVE ATTACHED GARAGES. CARBON MONOXIDE ALARM SHALL BE PROVIDED OUTSIDE OF EACH SEPARATE DWELLING UNIT IN THE IMMEDIATE VICINITY OF THE BEDROOM AND ON EVERY LEVEL OF DWELLING UNIT INCLUDING BASEMENTS. ALL INTERIOR AND EXTERIOR STAIRWAYS SHALL BE ILLUMINATED NOT LESS THAN 1 FOOT-CANDLE (1 LUX). BATHROOMS, WATER CLOSET COMPARTMENTS AND OTHER SIMILAR ROOMS SHALL BE PROVIDED NATURAL VENTILATION OR WITH MECHANICAL EXHAUSTION CAPABLE OF 50CFM EXHAUSTED DIRECTLY TO THE OUTSIDE R303.3. EACH APPLIANCE PROVIDED AND INSTALLED MEETS ENERGY STAR IF AN ENERGY STAR DESIGNATION IS APPLICABLE FOR THAT APPLIANCE. (4.59.1) THE CONSTRUCTION SHALL NOT RESTRICT A FIVE FOOT CLEAR AND UNOBSTRUCTED ACCESS TO ANY WATER OR POWER DISTRIBUTION FACILITIES (POWER POLES, PULL-BOXES, TRANSFORMERS, VAULTS, PUMPS, VALVES, METERS, APPURTENANCES, ETC.) OR TO THE LOCATION OF THE HOOK-UP. THE CONSTRUCTION SHALL NOT BE WITHIN TEN FEET OF ANY POWER LINES WHETHER OR NOT THE LINES ARE LOCATED ON THE PROPERTY. FAILURE TO COMPLY MAY CAUSE CONSTRUCTION DELAYS AND/OR ADDITIONAL EXPENSES. AN APPROVED SEISMIC GAS SHUT-OFF VALVE WILL BE INSTALLED ON THE FUEL GAS LINE ON THE DOWNSTREAM SIDE OF THE UTILITY METER AND BE RIGIDLY CONNECTED TO THE EXTERIOR OF THE BUILDING OR STRUCTURE CONTAINING THE FUEL GAS PIPING. (PER ORDINANCE 170158) (SEPARATE PLUMBING PERMIT IS REQUIRED). PLUMBING FIXTURES ARE REQUIRED TO BE CONNECTED TO A SANITARY SEWER OR TO AN APPROVED SEWAGE DISPOSAL SYSTEM (R306.3). KITCHEN SINKS, LAVATORIES, BATHTUBS, BIDETS, LAUNDRY TUBS AND WASHING MACHINE OUTLETS SHALL BE PROVIDED WITH HOT AND COLD WATER AND CONNECTED TO AN APPROVED WATER SUPPLY (R306.4). BATHTUB AND SHOWER FLOORS, WALLS ABOVE BATHTUBS WITH A SHOWHEAD, AND SHOWER COMPARTMENTS SHALL BE FINISHED WITH A NONABSORBENT SURFACE. SUCH WALL SURFACES SHALL EXTEND TO A HEIGHT OF NOT LESS THAN 6 FEET ABOVE THE FLOOR (R307.2). PROVIDE ULTRA-LOW FLUSH WATER CLOSETS FOR ALL NEW CONSTRUCTION. EXISTING SHOWER HEADS AND TOILETS MUST BE ADAPTED FOR LOW WATER CONSUMPTION. EACH PANEL OF SAFETY GLAZING INSTALLED IN HAZARDOUS LOCATIONS SHALL BE IDENTIFIED BY A MANUFACTURER'S DESIGNATION SPECIFYING WHO APPLIED THE DESIGNATION, THE MANUFACTURER OR INSTALLER AND THE SAFETY GLAZING STANDARD. THE FOLLOWING SHALL BE CONSIDERED SPECIFIC HAZARDOUS LOCATIONS FOR THE PURPOSES OF SAFETY GLAZING: GLAZING IN SECTION 2406 A. SWING DOORS. B. FIXED AND SLIDING PANELS OF SLIDING DOOR ASSEMBLIES AND PANELS IN SLIDING AND BI-FOLD CLOSET DOOR ASSEMBLIES. C. STORM DOORS. D. UNFRAMED SWINGING DOORS. E. DOORS AND ENCLOSURES FOR HOT TUBS, WHIRLPOOLS, SAUNAS, STEAM ROOMS, BATHTUBS, AND SHOWERS. F. FIXED OR OPERABLE PANELS ADJACENT TO A DOOR WHERE THE NEAREST EXPOSED EDGE OF THE GLAZING IS WITHIN 24 INCHES (610 MM) ARC OF EITHER VERTICAL EDGE OF THE DOOR IN A CLOSED POSITION AND WHERE THE BOTTOM EXPOSED EDGE OF THE GLAZING IS LESS THAN 60 INCHES (1525 MM) ABOVE THE WALKING SURFACE. READ CODE FOR EXCEPTIONS. G. FIXED OR OPERABLE PANEL, OTHER THAN DESCRIBED IN ITEMS E AND F, WHICH MEETS ALL OF THE FOLLOWING CONDITIONS: I. EXPOSED AREA OF AN INDIVIDUAL PANE GREATER THAN 9 SQUARE FEET (834 M2) II. EXPOSED BOTTOM EDGE LESS THAN 18 INCHES (457 MM) ABOVE THE FLOOR. III. EXPOSED TOP EDGE GREATER THAN 36 INCHES (914 MM) ABOVE THE FLOOR. IV. ONE OR MORE WALKING SURFACES WITHIN 36 INCHES (914 MM) HORIZONTALLY OF THE PLANE OF THE GLAZING. WATER HEATER MUST BE STRAPPED TO WALL. (SEC. 507.3, LAR) MECHANICALLY VENTILATED BUILDINGS SHALL PROVIDE REGULARLY OCCUPIED AREAS OF THE BUILDING WITH A MERV 13 FILTER FOR OUTSIDE AND RETURN AIR. FILTERS SHALL BE INSTALLED PRIOR TO OCCUPANCY AND RECOMMENDATIONS FOR MAINTENANCE WITH FILTERS OF THE SAME VALUE SHALL BE INCLUDED IN THE OPERATION AND MAINTENANCE MANUAL. SMOKE DETECTORS SHALL BE PROVIDED FOR ALL DWELLINGS INTENDED FOR HUMAN OCCUPANCY, WHERE A PERMIT IS REQUIRED FOR ALTERATIONS, REPAIRS, OR ADDITIONS. (R314.2) WHERE A PERMIT IS REQUIRED FOR ALTERATIONS, REPAIRS OR ADDITIONS, EXISTING DWELLINGS OR SLEEPING UNITS THAT HAVE ATTACHED GARAGES OR FUEL-BURNING APPLIANCES SHALL BE PROVIDED WITH A CARBON MONOXIDE ALARM IN ACCORDANCE WITH SECTION R315.2. CARBON MONOXIDE ALARMS SHALL ONLY BE REQUIRED IN THE SPECIFIC DWELLING UNIT OR SLEEPING UNIT FOR WHICH THE PERMIT WAS OBTAINED. (R315.2.1) EVERY SPACE INTENDED FOR HUMAN OCCUPANCY SHALL BE PROVIDED WITH NATURAL LIGHT BY MEANS OF EXTERIOR GLAZED OPENINGS IN ACCORDANCE WITH SECTION R303.1 OR SHALL BE PROVIDED WITH ARTIFICIAL LIGHT THAT IS ADEQUATE TO PROVIDE AN AVERAGE ILLUMINATION OF 6 FOOT-CANDLES OVER THE AREA OF THE ROOM AT A HEIGHT OF 30 INCHES ABOVE THE FLOOR LEVEL. (R303.1) A COPY OF THE EVALUATION REPORT AND/OR CONDITIONS OF LISTING SHALL BE MADE AVAILABLE AT THE JOB SITE. BUILDINGS SHALL HAVE APPROVED ADDRESS NUMBERS, BUILDING NUMBERS OR APPROVED BUILDING IDENTIFICATION PLACED IN A POSITION THAT IS PLAINLY LEGIBLE AND VISIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY. (R319.1) FANS SHALL BE ENERGY STAR COMPLIANT AND BE DUCTED TO TERMINATE TO THE OUTSIDE OF THE BUILDING. FANS, NOT FUNCTIONING AS A COMPONENT OF A WHOLE HOUSE VENTILATION SYSTEM, MUST BE CONTROLLED BY A HUMIDITY CONTROL. 4.506.1 	<ol style="list-style-type: none"> ENGINEERED HARDWOOD FLOORING FLOOR TILE TO EXTEND UP WALL 4" MIN. CLOSET ORGANIZER, O.F.C.I LINE OF FULL HEIGHT BUILT-IN CABINETRY 36" STONE COUNTERTOP, W/ 1-1/2" NOSING O/P.G. WOOD CABINETS LINE OF KNEE SPACE BELOW 24" GAS RANGE, O.F.C.I PROVIDE TELEVISION OUTLET @ 66" A.F.F. 1/2" FRAMELESS GLASS SHOWER ENCLOSURE W/ CR LAURENCE PATCH FITTINGS LINE OF RANGE HOOD ABOVE 20" HIGH CAESARSTONE BENCH W/ 4" NOSING LINEAR SHOWER DRAIN UNDERSIDE OF STAIRS AND WALLS TO HAVE 5/8" TYPE 'X' GYP. BD. LOCATION OF (N) ELECTRICAL METER 12" H. FULL WIDTH STONE SHOWER NICHE LOCATION OF (N) TANKLESS WH LOCATION OF EGRESS WINDOW STEEL AWNING PER STRUCT. 	 <p>(N) 1-HR RATED 2X4 WOOD STUD WALL, W/ 5/8" TYPE 'X' GYP. BOARD TYPICAL BOTH SIDES</p> <p>(N) 2X4 WALL PER STRUCTURAL</p> <p>(E) 1-HOUR FIRE RATED WALL</p> <p>(E) WALL TO REMAIN</p> <p>DOOR</p> <p>WINDOW</p> <p>CEILING MOUNTED SMOKE DETECTOR - HARD WIRED W/ BATTERY BACK-UP, SEE NOTE 10.24/A3.11</p> <p>CEILING MOUNTED CARBON MONOXIDE DETECTOR, SEE NOTE 11.15/A3.11</p> <p>CEILING MOUNTED EXHAUST FAN, SEE NOTE 28/A3.11</p>	<div> REVISION AND/OR ISSUE DATE 1 10/19/2020 COASTAL COMMISSION SUBMITTAL </div> <div>  </div> <div> NOTES </div>
PLAN NOTES	5	PLAN KEYNOTES	4
		PLAN LEGEND	3

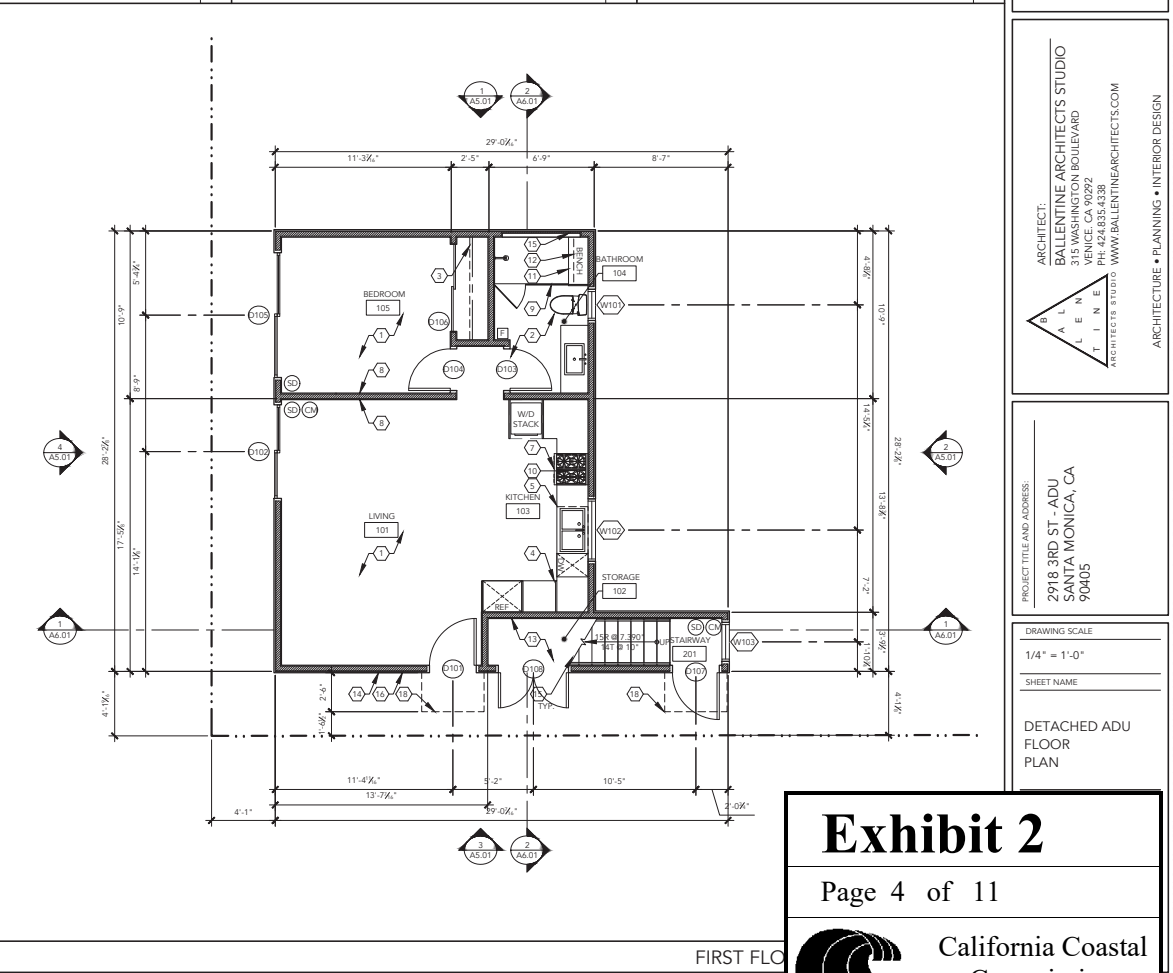
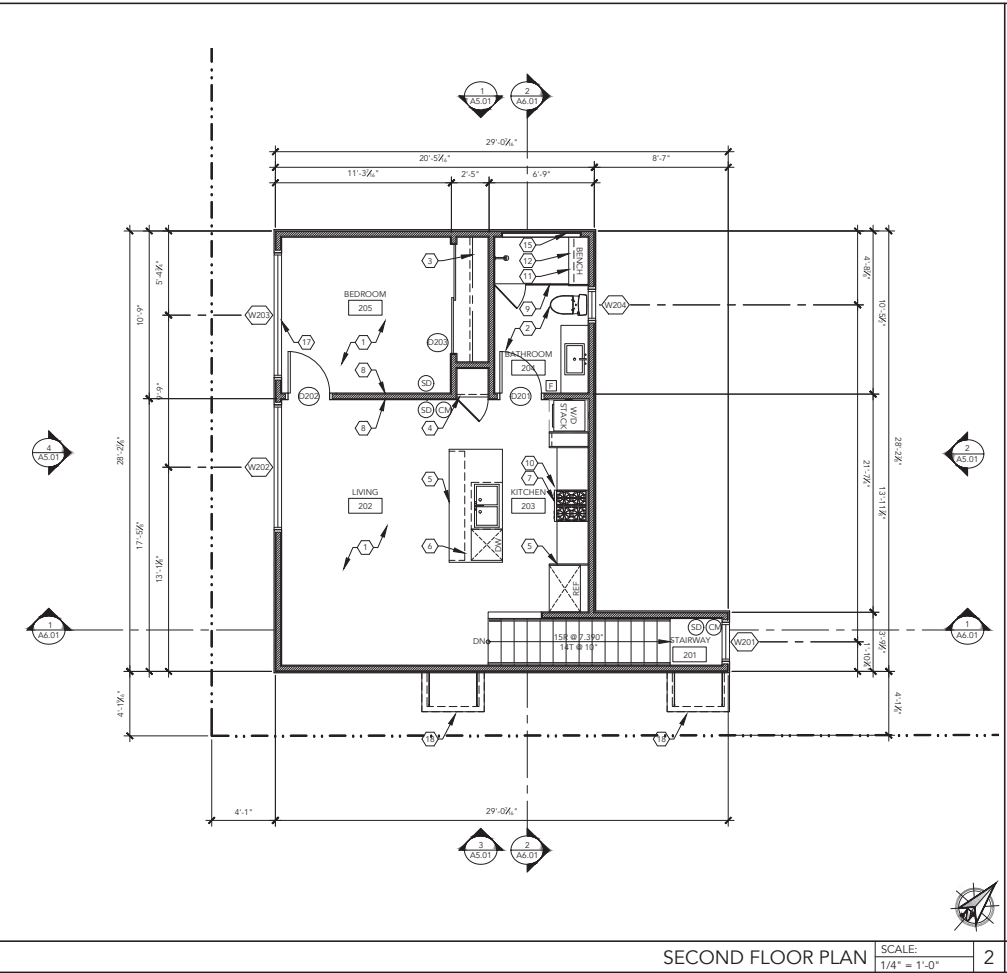


Exhibit 2

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California Coastal Commission

1. GENERAL CONTRACTOR SHALL PROVIDE ALL NECESSARY BLOCKING AND NAILING FOR WATERTIGHT AND WATERPROOF INSTALLATION.
2. LOCATIONS OF NEW ROOFING AND CRICKETING ARE SHOWN FOR DESIGN INTENT ONLY. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE PRECISE LOCATION AND PROVIDE THESE ELEMENTS AS REQUIRED TO PROVIDE A PROPERLY SLOPED AND WATER TIGHT SYSTEM IN WHICH WATER IS NOT RETAINED ON THE ROOF.
3. ALL CONCENTRATED DRAINAGE, INCLUDING ROOF WATER, SHALL BE CONDUCTED, VIA GRAVITY, TO THE STREET OR AN APPROVED LOCATION AT A 2% MINIMUM.

1. GAF RUBEROID ENERGYCAP TORCH ROOFING OFF-WHITE COLOR.
2. 12" H PARAPET WALL.
3. LINE OF CRICKET SLOPE 2% MIN PER PLAN.
4. SHEET METAL SCUPPER & DOWNSPOUT.
5. LINE OF RIDGE.
6. 30"x48" AWNING.

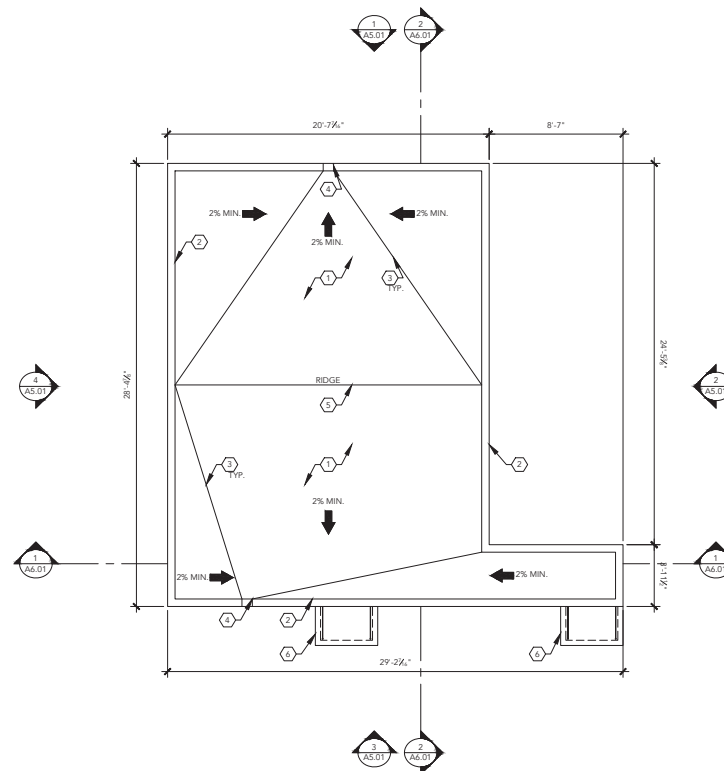
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NOTES

ROOF NOTES 4

ROOF KEYNOTES 3



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ARCHITECTS STUDIO

ARCHITECTURE • PLANNING • INTERIOR DESIGN

PROJECT TITLE AND ADDRESS:
2918 3RD ST - ADU
SANTA MONICA, CA
90405

DRAWING SCALE

1/4" = 1'-0"

SHEET NAME

ROOF
PLAN

Exhibit 2

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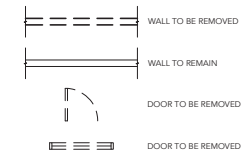


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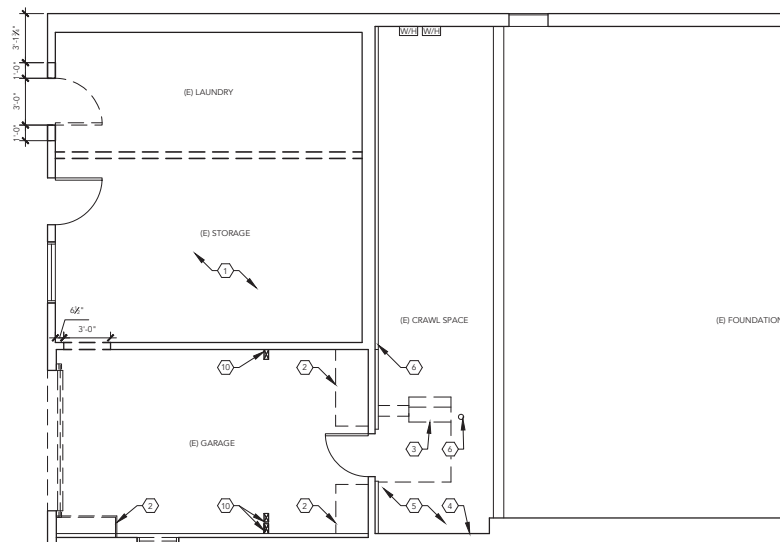
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- | | |
|---|--|
| 1 | REMOVE TILE TO SUBFLOOR |
| 2 | (E) GARAGE/STORAGE CABINETS TO BE REMOVED |
| 3 | (E) BRICKWORK TO BE REMOVED TO SUBFLOOR |
| 4 | (E) COMMUNICATIONS EQUIPMENT TO BE RELOCATED BY CONTRACTOR |
| 5 | (E) WOOD STAIRS TO BE REMOVED |
| 6 | (E) PLUMBING TO BE RELOCATED |
| 7 | (E) STRUCTURAL FRAMING TO BE REMOVED, SEE STRUCTURAL |



DEMOLITION KEYNOTES 4

DEMOLITION PLAN LEGEND	3
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WWW.BALLENTINEARCHITECTS.COM

PROJECT TITLE AND ADDRESS:
22918 3RD ST - ADU
SANTA MONICA, CA

DRAWING SCALE

 $1/4" = 1'-0"$

SHEET NAME

DEMOLITION
PLAN

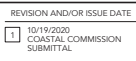
Page 6 of 11

California Coastal
Commission

DEMOLITION

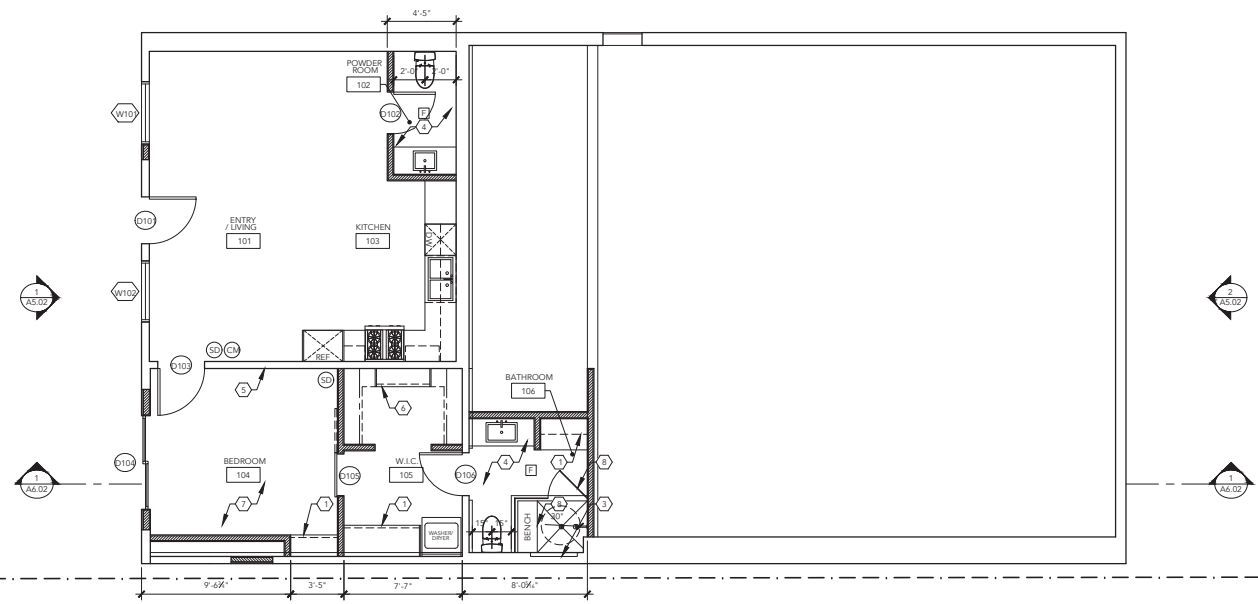
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- | | |
|---|--|
| 1 | REMOVE TILE TO SUBFLOOR |
| 2 | (E) GARAGE/STORAGE CABINETRY TO BE REMOVED |
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NOTES

PLAN NOTES	5	PLAN KEYNOTES	4	PLAN LEGEND	3
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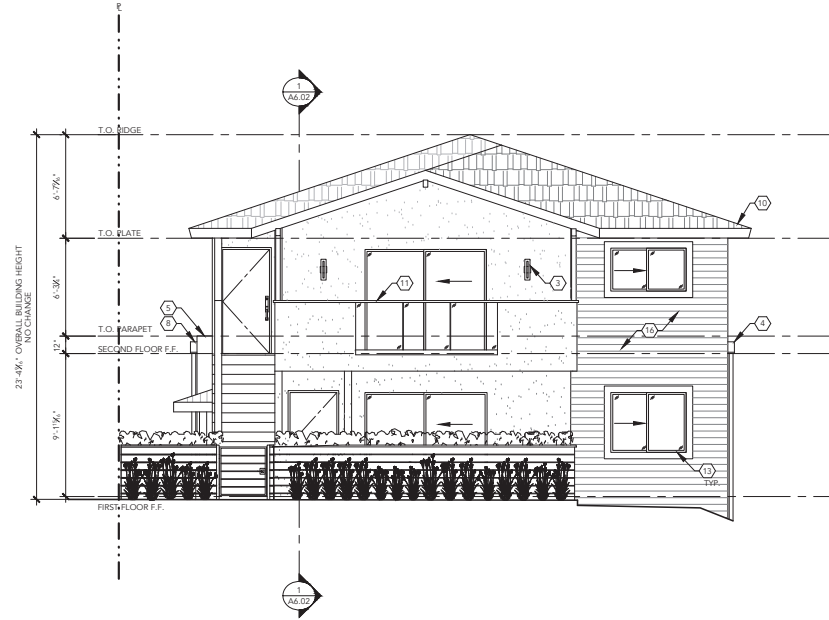


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90405

DRAWING SCALE
$1/4" = 1'-0"$
SHEET NAME

ATTACHED ADU
FLOOR
PLAN



EAST ELEVATION (FRONT)

SCALE:
1/4" = 1'-0"

2

ELEVATION KEYNOTES

- 1 SMOOTH STUCCO, PAINT TO MATCH MAIN BUILDING
- 2 72" H STEEL GATE WITH NATURAL CEDAR BOARDS
- 3 16" H WALL SCONCE TO BE MOUNTED +66" A.F.F.
- 4 50" W STEEL AWNING
- 5 12" H PARAPET WALL
- 6 STAINED CEDAR SIDING
- 7 AREA OF VINE CLIMBING WALL
- 8 SHEET METAL CONDUCTOR HEADS & DOWNSPOUTS
- 9 WOOD BOARDS TO BE SANDING & STAINED
- 10 ASPHALT SHINGLE HIP ROOF TO REMAIN
- 11 42" H TEMPERED GLASS GUARD RAIL
- 12 SMOOTH STUCCO TO BE PAINTED
- 13 ALL (B) WINDOW TRIM TO BE PAINTED BLACK
- 14 (B) STUCCO @ RECESSES TO BE PAINTED LIGHT GREY
- 15 STUCCO BUILDING EXTERIOR TO BE PAINTED WHITE
- 16 WOOD BOARDS TO BE SANDING & STAINED

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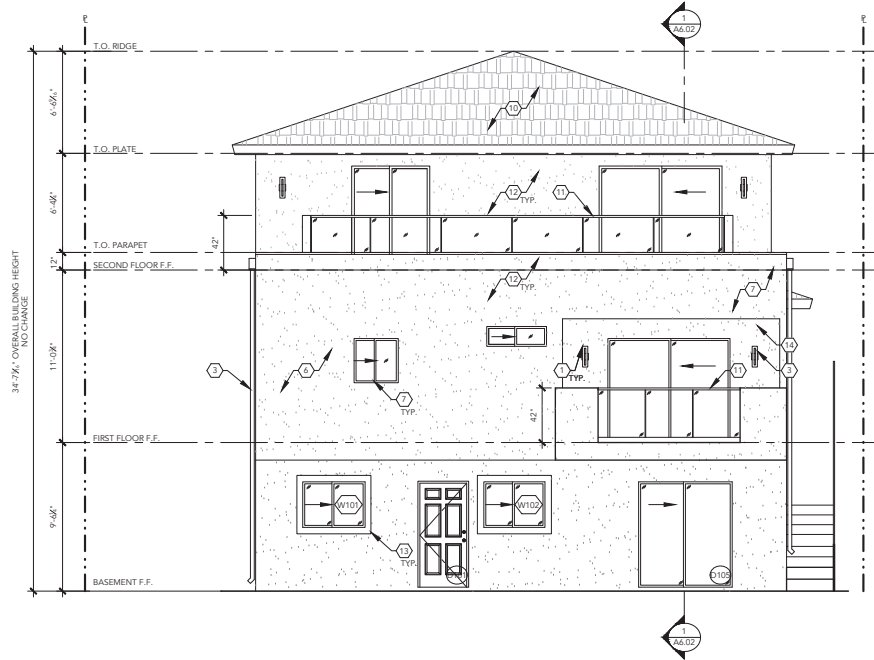
PROJECT TITLE AND ADDRESS:
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SANTA MONICA, CA
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DRAWING SCALE

1/4" = 1'-0"

SHEET NAME

ELEVATIONS



WEST ELEVATION

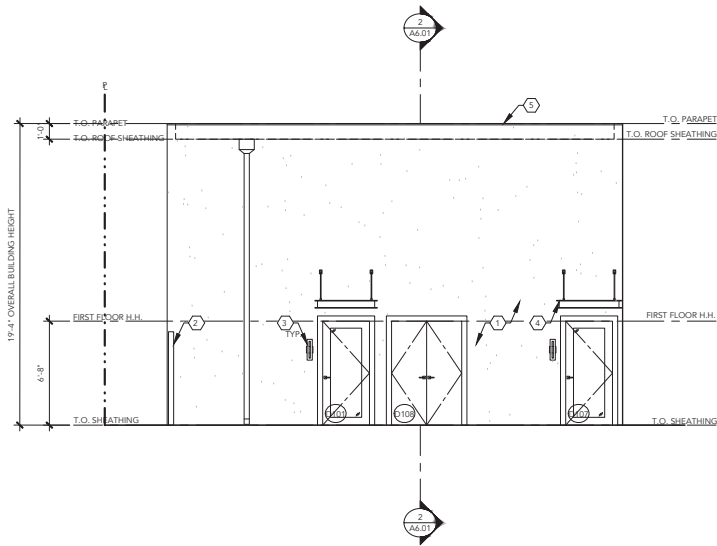
Exhibit 2

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California Coastal
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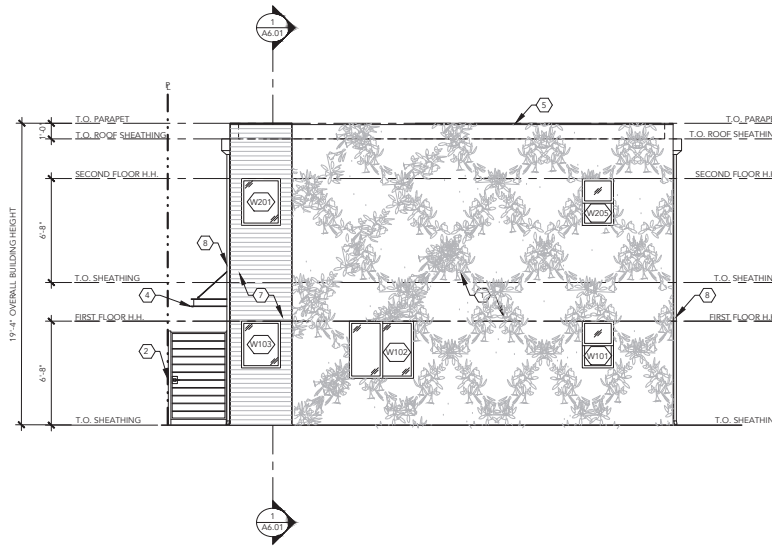
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NORTH WEST ELEVATION

SCALE:
1/4" = 1'-0"

4



SOUTH WEST ELEVATION (FRONT)

SCALE:
1/4" = 1'-0"

2

ELEVATION KEYNOTES

- 1 SMOOTH STUCCO, PAINT TO MATCH MAIN BUILDING
- 2 72" H STEEL GATE WITH NATURAL CEDAR BOARDS
- 3 16" H WALL SCONCE TO BE MOUNTED +66" A.F.F.
- 4 50" W STEEL AWNING
- 5 12" H PARAPET WALL
- 6 STAINED CEDAR SIDING
- 7 AREA OF VINE CLIMBING WALL
- 8 SHEET METAL CONDUCTOR HEADS & DOWNSPOUTS
- 9 (B) WOOD BOARDS TO BE SANDBLASTED & STAINED
- 10 (B) ASPHALT SHINGLE HIP ROOF TO REMAIN
- 11 42" H TEMPERED GLASS GUARD RAIL
- 12 (B) SMOOTH STUCCO TO BE PAINTED
- 13 ALL (B) WINDOW TRIM TO BE PAINTED BLACK
- 14 (B) STUCCO RECESSES TO BE PAINTED LIGHT GREY
- 15 (B) STUCCO BUILDING EXTERIOR TO BE PAINTED WHITE
- 16 (B) WOOD BOARDS TO BE SANDBLASTED & STAINED

REVISION AND/OR ISSUE DATE
1 10/19/2020
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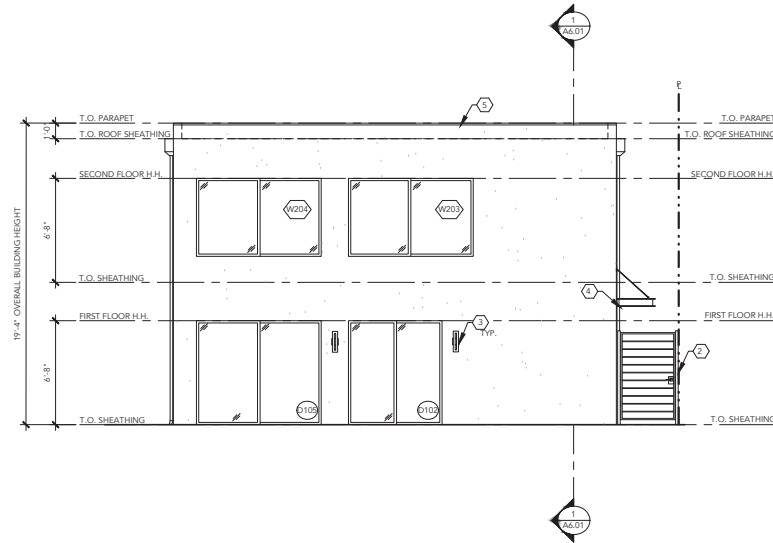
PROJECT TITLE AND ADDRESS:
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SANTA MONICA, CA
90405

DRAWING SCALE

1/4" = 1'-0"

SHEET NAME

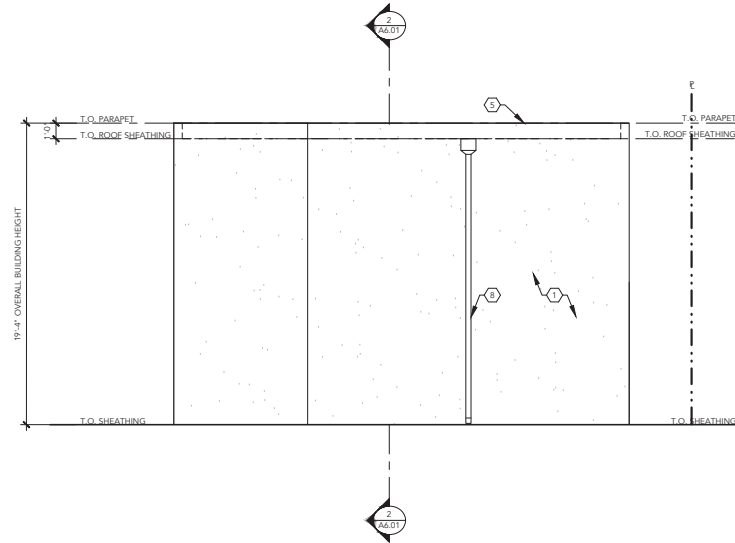
ELEVATIONS



NORTH EAST (REAR) ELEVATION

SCALE:
1/4" = 1'-0"

3



SOUTH EAST ELEVATION

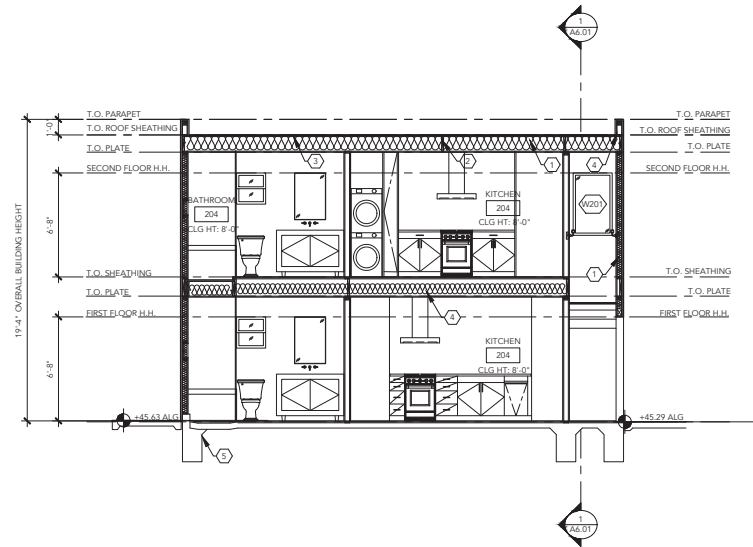
Exhibit 2

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LONGITUDINAL SECTION

SCALE:
1/4" = 1'-0"

2

SECTION KEYNOTES

- 1 BATT INSULATION - CEILING (R-38), WALL (R-15) FLOOR (R-19)
- 2 ROOF RAFTER PER STRUCTURAL
- 3 ROOF AND FLOOR SHEATHING PER STRUCTURAL
- 4 12" H STUCCO PARAPET WALL
- 5 IN SLAB ON GRADE FOUNDATION & FOOTINGS PER STRUCTURAL DRAWINGS
- 6 DECK FRAMING PER STRUCTURAL DRAWINGS
- 7 FLOOR SHEATHING PER STRUCTURAL, DECK TO HAVE TILE FINISH
- 8 12" H STUCCO PARAPET WALL
- 9 IN-42" TEMPERED GLASS OR LAURENC RAILING SYSTEM

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1 10/19/2020
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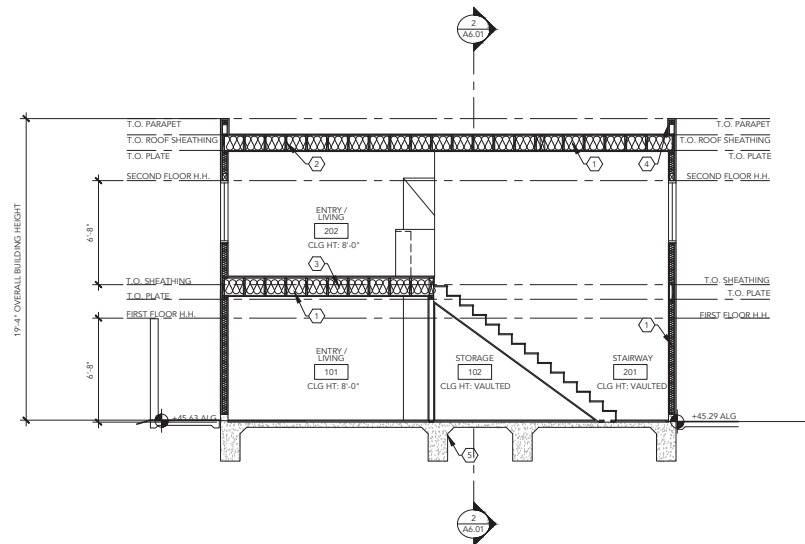
PROJECT TITLE AND ADDRESS:
2918 3RD ST - ADU
SANTA MONICA, CA
90405

DRAWING SCALE

1/4" - 1'-0"

SHEET NAME

SECTIONS



LATERAL

Exhibit 2

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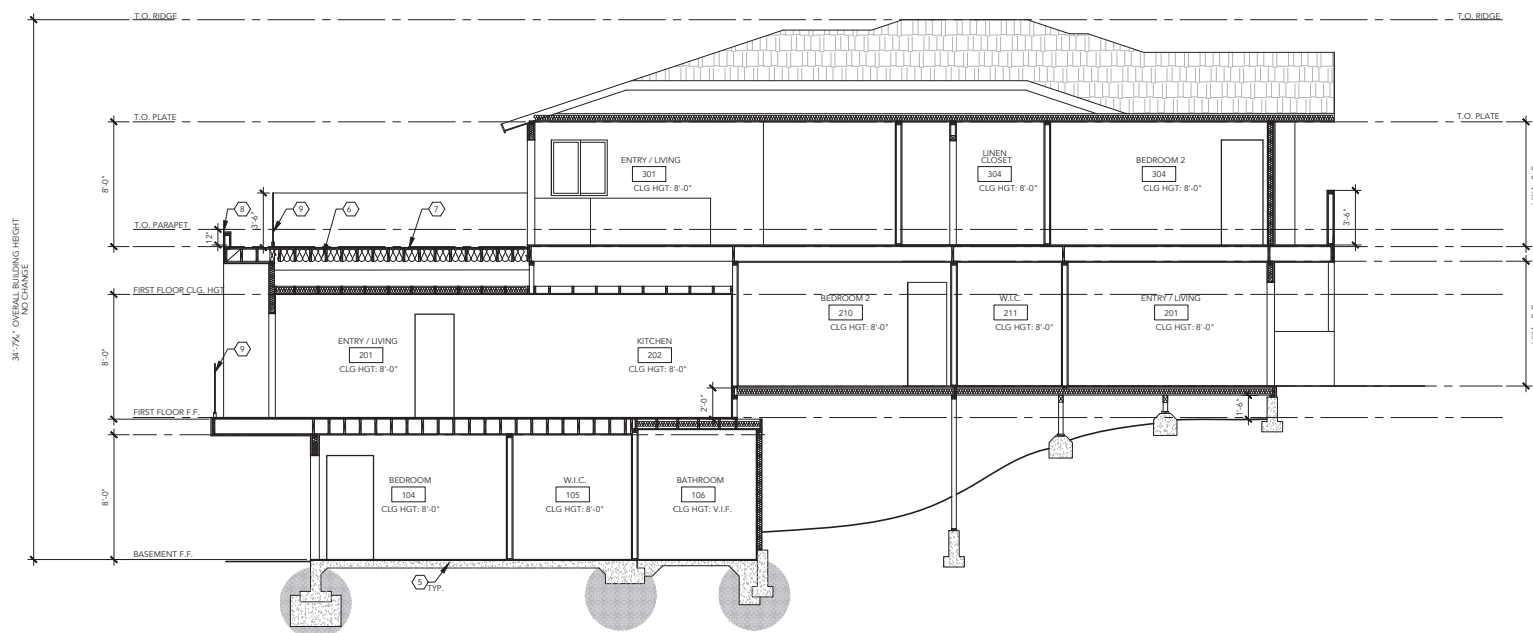
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SECTION KEYNOTES



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32918 3RD ST - ADU
SANTA MONICA, CA

DRAWING SCALE
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SHEET NAME

SECTIONS

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California Coastal
Commission

LONGITUDINAL

ARCHITECTURE • PLANNING • INTERIOR DESIGN



January 12th, 2021

California Coastal Commission - South Coast Area Office
301 E Ocean Blvd., Suite 300, Long Beach, CA 90802
Application Number: 5-20-0607
Project Address: 2918 3rd St, Santa Monica, CA 90405

Dear Vince,

In response to your notice of incomplete application letter, we wish to respond to the following additional information to help your review of the ADU development located at 2918 3rd St. Santa Monica.

Consistency with State ADU Laws:

The ADUs are consistent with all aspects of the State ADU Laws. Gov. Code § 65852.2 permits "at least one accessory dwelling unit within an existing multifamily dwelling and shall allow up to 25 percent of the existing multifamily dwelling units." The proposed development includes one new ADU in an existing storage/laundry space. Additionally, the State ADU Laws permit "Not more than two accessory dwelling units that are located on a lot that has an existing multifamily dwelling, but are detached from that multifamily dwelling..." The proposed development includes two detached ADUs at the rear of the lot. This area previously was partially occupied by a carport, which will be removed as part of the development. The state law notes that local agencies "shall not impose parking standards for an accessory dwelling unit" when "The accessory dwelling unit is located within one-half mile walking distance of public transit." The proposed development is 0.1 miles from the nearest bus station at Main and Marine Street, and there are seven (7) Santa Monica Big Blue Bus lines within a 0.5 mile walk, including a Rapid line.

Lastly, the project is in compliance with the development standards imposed by the State ADU Laws. The unit sizes are 516 SQ.FT. and 565 SQ.FT., which is below the 850 SQ.FT. maximum. The building height is 21'-1", which is below the 23'-0" maximum. Other guidelines, setbacks, etc. are in compliance with State ADU laws. .

Project Alternatives Analysis:

There are numerous aspects of this site makes it nearly impossible to meet the legal standards required to maintain the existing number of parking stalls while adding new ADUs. The existing multi-family building occupies the front of the parcel, leaving the rear portion of the property as the only area available for either detached ADUs or parking. Unfortunately because access to the rear of the property is provided via an existing driveway a





side of the property, turning radius and backup distance make it technically impossible to maintain the same number of legal parking stalls. In fact, current parking standards are so rigid that even the existing parking spaces would not meet the current code, which has changed since the buildings construction in 1979. Existing parking is legal non-conforming – it does not meet the current code. Further, setback and building separation requirements only reduce the available buildable area. In addition, SMMC 9.21.130 imposes Resource Recovery and Recycling Standards, which require an extraordinarily large enclosure at the rear the property.

Please note that while the analysis above applies to new, legal parking spaces that are in compliance with Santa Monica City code, in practice the tenants of the building will be parking an additional 3 cars in the area in front of the trash enclosure and an area in the front yard. These additional 3 spaces do not qualify as legal spaces due to the backup distance, but in practice they will function similar to the existing spaces on-site. Including these spaces, there will be 5 usable parking spaces on site.

Although the State ADU laws do not require replacement of any of the parking stalls being demolished to make room for the new Detached ADU, we have worked closely with the Mobility and Planning departments at the City of Santa Monica to facilitate two legal parking spaces. This is the maximum feasible amount, given SMMC chapter 9.28, Off-Street Parking Requirements.

Santa Monica is a city that thrives on public transportation and according to the City of Santa Monica Community Development Department, "The City of Santa Monica has one of the most extensive transit networks among Cities of its size. 17% of residents ride the Big Blue Bus at least a few times a week and 12% ride some other transit service as often." In addition to public transportation, the city has miles and miles of bike lanes, the closest being two blocks away. For off-site parking, there is abundant permit parking for residents on the street as well as public parking lots 0.2 miles away. Prior residents at 2918 3 rd Street have elected to use off-site permit parking in lieu of on-site parking because despite its coastal location, there is an overabundance of parking available in this particular area.

Kind regards,

Clayton Young



February 3, 2021

California Coastal Commission – South Coast Area office
301 E Ocean Blvd., Suite 300, Long Beach, CA 90802

Application Number: 5-20-0607

Project Address: 2918 3rd Street, Santa Monica, CA 90405

Re: Response to Request for Additional Parking Information, Exploration of Alternatives

Dear Vince,

This memorandum has been prepared in response to your request for further relating to parking in support Application No. 5-20-0607. The California Coastal Commission (“CCC”) commission has raised a concern regarding the reduction of legal parking spaces from five to two. We hope this memorandum and the associated exhibits highlight the thoughtful, detailed, and extensive efforts put forth by ownership, Ballentine Architects and our engineering team. Below and attached you will find details regarding the numerous planning, geometric, geographic and other constraints that limit the size and shape of the detached ADU footprint as well as the number of legal parking spaces. We are confident that our proposed plan represents the maximum number of viable parking spaces while maximizing the number of livable, affordable ADU units

Background

Ownerships interest are aligned with those of the CCC – it is our strong preference to maximize parking wherever possible to improve marketability of the property to prospective tenants and to improve tenants’ quality of life.

The existing project contains five (5) legal parking spaces within a detached carport that complied with the City of Santa Monica’s development standards at the time of construction in 1979. Because the City of Santa Monica considers the carport to be an “existing structure”, new “detached” ADUs cannot be added to the carport structure.

California’s new ADU laws permit the removal of parking spaces without limitation in the development of ADUs. This technically allows ownership to remove all five parking spaces in favor of building the largest permitted detached ADU building. While this would be the most profitable strategy for the ownership, we believe that maintaining usable parking on-site is a benefit to both tenants and to the community.

As this memorandum and attached exhibits will show, there are a total of five (5) parking spaces on site, however three of the spaces are not considered “legal” by the City. These spaces are viable, but non-conforming for reasons detailed below. Unfortunately, despite nearly a year of land surveys, exhaustive massing studies, extensive and detailed discussions with numerous divisions of the City of Santa Monica (including mobility, planning, and waste management), there were a few specific constraints that required us to reduce the **legal** parking count from five to three. Those issues are as follows:

- **Site topography.** The property is situated on a slope, with more than 15’ of height differential from the eastern to western property line.
- **Site geometry.** At 130’ x 50’, the property is long and narrow. The driveway ramp is approximately 10’11” wide, which makes the usable space even more narrow. This geometry makes parking difficult due to limited back up distance and limited turning radii.
- **Location of existing structure.** The existing residential structure is located in the center of the lot, limiting the size of the rear yard to approximately 46’ x 50’. This rear yard is extremely tight given that this must contain the detached ADU footprint, all parking stalls, rear and side yard setbacks, open space for required backup distance, an oversized trash enclosure to accommodate the City of Santa Monica, and open space in front of the trash enclosure. The location of the detached ADU is also limited by the required 10’ of separation between new and existing structures.

Exhibit 3

Page 3 of 35



California Coastal
Commission

- **Restrictive parking standards.** A standard parking stall is 8' wide, meaning that maintaining five legal stalls would require 40' of width. This increases to 43' of width after including two 18" separations as required by the City when a parking stall is adjacent to a wall. The City of Santa Monica requires a backup distance of 40' in order for a parking stall to be legal. Put differently, maintaining five legal parking stalls requires a minimum 43' x 40' open space in the rear yard. **This equates to almost 75% of the entire rear yard (approximately 50' x 46'), leaving no viable building footprint after factoring in setbacks, trash requirements, and required building separation.**
- **Trash enclosure requirements.** This was the most surprising and difficult constraint to address in the development of our plans. For a 6-unit building the City of Santa Monica requires a minimum trash enclosure dimension of 21' x 7.5', which is enough to fit three 4' x 6' bins. This is an exorbitantly oversized trash enclosure relative to the number of residents we expect at the property – it is the same size that the City would require for a 10-unit building comprised of all 3-bedrooms, yet it is imposed on this proposed ADU development, which is only 6 units, 1-2 bedrooms. In reality, the waste and recycling generated by the Property will be collected in four smaller 95-gallon bins (less than 3' x 3' each), which will be rolled to the street by hand once per week. This is how waste collection is done now, albeit with slightly smaller trash cans. Our proposed plan includes two parking spaces in front of one potential storage area for our 95-gallon bins, which is within the trash enclosure area. However, because the City assumes that Ownership may contract with the City to roll out waste bins at some point in the future, **the city will not consider these legal as they encroach on the trash enclosure clearance.** This is an unrealistic assumption given the limited waste production of our project. Note: Ownership has negotiated a slightly reduced enclosure size (from 21' down to 18'), which is already reflected in the plans submitted to Coastal.
- **Trash Staging requirements.** In addition to the extraordinary size of trash enclosure that must be added to our plans, this enclosure cannot exist in the front yard due the required enclosure exceeding the maximum height requirement of 42" in the front setback. As a result, the City also requires a "trash staging area" in the front setback, which is an area that bins can be temporarily stored during collection. Again, this area will not be used as the project does not generate enough waste to necessitate larger bins. Our proposed plan includes one parking stall in the trash staging area. **Unfortunately, even if parking dimensions are met, the City will not consider it legal if it encroaches on the staging area.**

Response 1. The Property Contains Additional Viable, Non-Conforming On-Site Parking.

For the reasons above, the City considers our proposed plans to have only two (2) **legal** parking stalls. However, we have intentionally designed the site in such a way that an additional three (3) cars are able to comfortably park on-site, for a total of five stalls – two legal, three non-conforming. Please reference the attached exhibit for a diagram.

Response 2. We have explored many other parking configurations, but none provide a viable alternative.

Please see the attached exhibit, which contains many alternative parking diagrams and their disadvantages. There are only a select few parking configurations that allow for the construction of any ADU whatsoever. Of those options, our proposed option is the best combination of total number of viable parking spaces (including legal and non-conforming) and livable space for future occupants of the ADUs.

Response 3. Subterranean Parking is not viable.

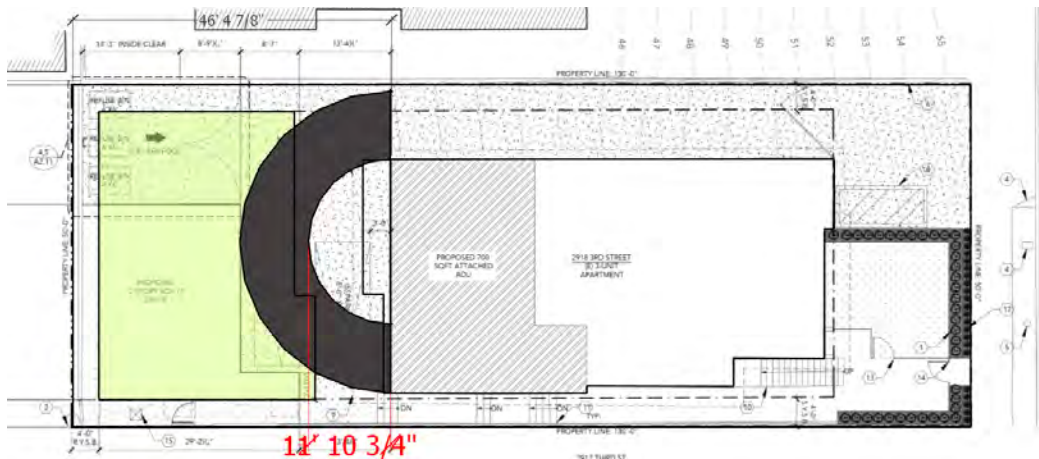
We briefly explored the idea of subterranean parking. Unfortunately, the slope and location of existing structure would require us to have access to the subterranean garage in the rear yard. Given the location of the driveway and the required turning radius, we do not have enough room to access a subterranean garage in the rear yard; we can only provide an 11' 10" turning radius, which is not viable (typically need 15' or more). If the property was hypothetically larger and able to accommodate the required turning radius, such a garage would require a completely new foundation in the rear of the building, major structural modification throughout the building (including in occupied units), relocation of the underground electrical service, and relocation of plumbing; our general contractor, who has built dozens of subterranean garages in the City of Santa Monica, indicated that it would add more than \$500,000 of cost to the project, rendering it unprofitable.

Exhibit 3

Page 4 of 35



California Coastal
Commission



Response 4. Reducing number of ADUs would not affect on-site parking.

The proposed project will include two (2) detached ADUs and one (1) attached ADU. The attached ADU does not affect parking counts. Reducing the number of detached ADUs from two (2) to one (1) would allow us to increase the number of legal parking spaces from two to three, but it would reduce our non-conforming spaces from three to one. Put differently, we would end up with more “legal” spaces, but practically less parking overall. Further, this would have a devastating impact on the financial viability of this project as the rent associated with the detached building would be reduced by 50%, rendering the project unprofitable and not worth pursuing. Lastly, this would be inconsistent with the intent of the California ADU laws and with the Coastal Commissions mission of increasing the amount of affordable housing in coastal zones.

Response 5. Alternative parking is available.

The Property is located in close proximity to Main street in Santa Monica, which is a low-rise retail and restaurant district with significant public parking. The following lots are located within a 5-minute walk of the Property:

- Lot 9 – 110 Hill Street
- Lot 10 – 111 Hill Street
- Lot 11 – 170 Hollister Street
- Lot 26 – 150 Strand
- Lot 5S – 2600 Barnard Way
- Lot 4S – 2030 Ocean Avenue

A City Parking Map is shown as an Exhibit to this memo. Ownership has contacted the City of Santa Monica for parking lot occupancy and traffic data, but the City did not provide a response. Please also note that preferential permit parking is also available on 3rd and 4th Street and is widely available at all times of day.

Please reach out if you have any questions or if we can provide any other helpful support.

Best,

Bryan Gortikov, Manager
 Bean Third Street, LLC
 bg@gortikovinc.com / 310-979-5721

Exhibit 3

Page 5 of 35



California Coastal
 Commission

ADU AND PARKING PLAN AS SUBMITTED TO COASTAL

These slides have been prepared for the Coastal Commission to support our Coastal Development Permit, Application No 5-20-0607. The commission has raised a concern regarding the reduction of legal parking spaces from five to two. These slides outline the numerous planning, geometric, geographic and other constraints that limit the size and shape of the detached ADU footprint as well as the number of legal parking spaces. We are confident that our proposed plan represents the maximum number of viable parking spaces while maintaining two livable detached ADU units.

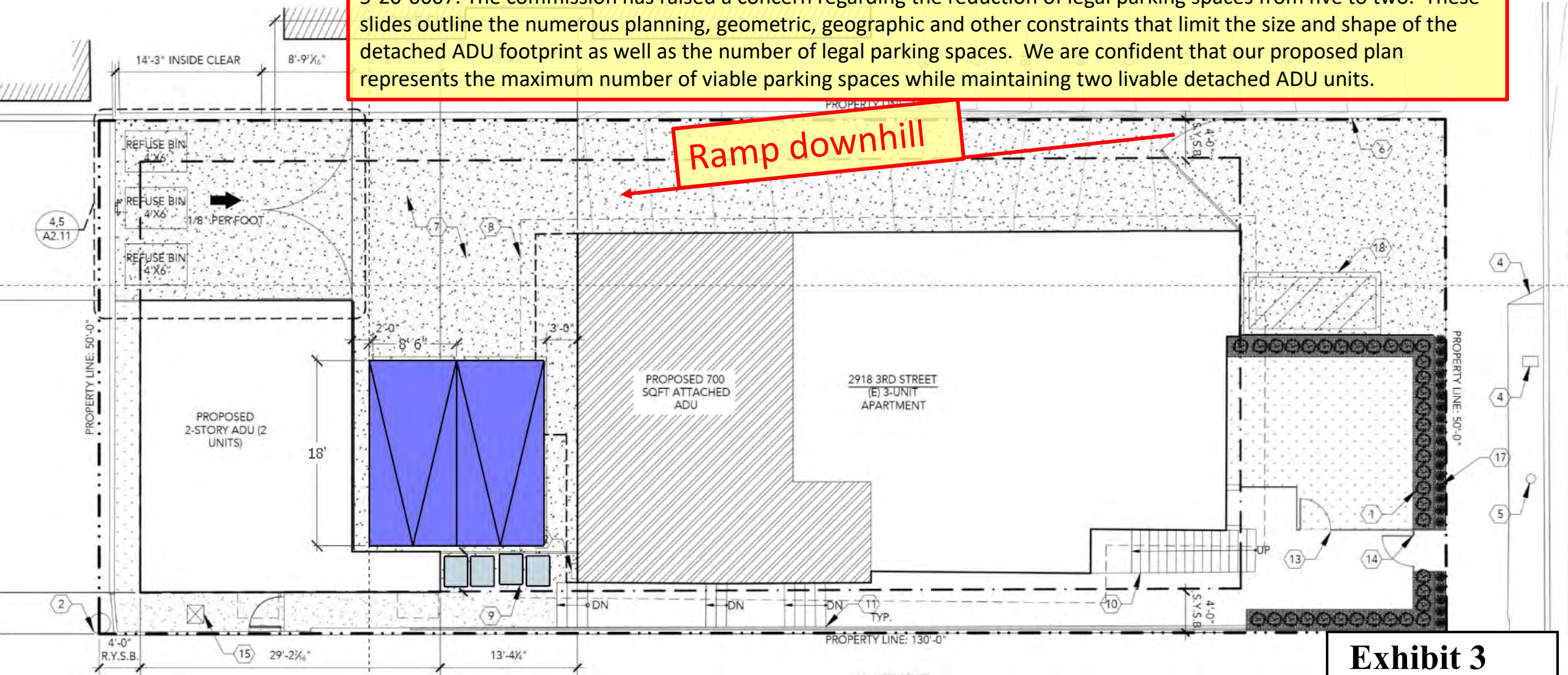


Exhibit 3

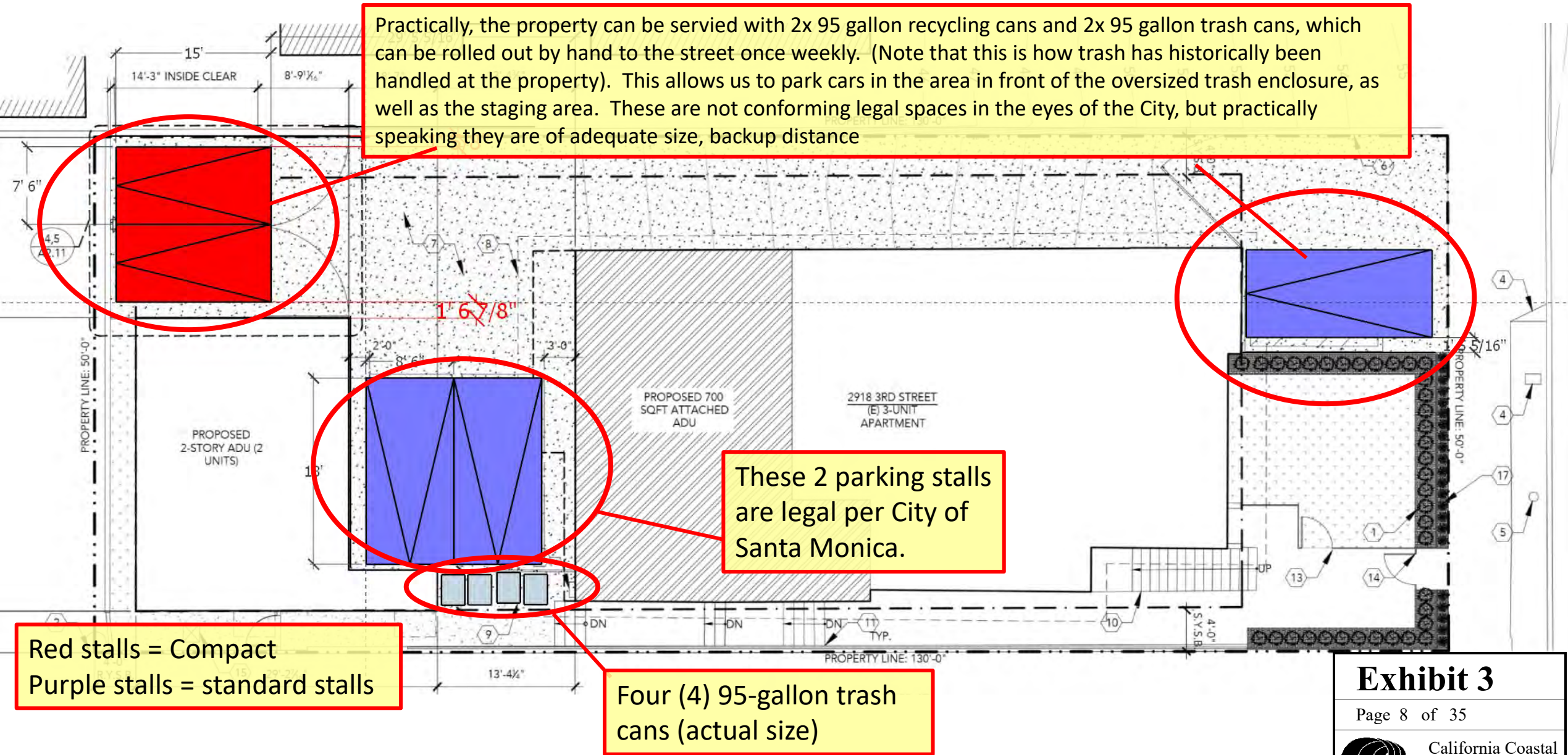
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California Coastal
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2918 THIRD STREET, SANTA MONICA, CA 90405



ADU AND PARKING PLAN AS SUBMITTED TO COASTAL –
INCLUDING NON-CONFORMING BUT VIABLE STALLS



ALTERNATIVE PARKING EXPLORATION – BUILDING ENVELOPE

During the six month development of our architectural plans, we explored a wide variety of ADU locations, ADU shapes parking configurations in an attempt to maximize parking onsite. The green box below indicates the building envelope for ADUs based on a 4' side yard setback, 4' rear yard setback, and a 10' required distance between the ADU structure and the existing structure. The ADU footprint can only exist within the green shape below.

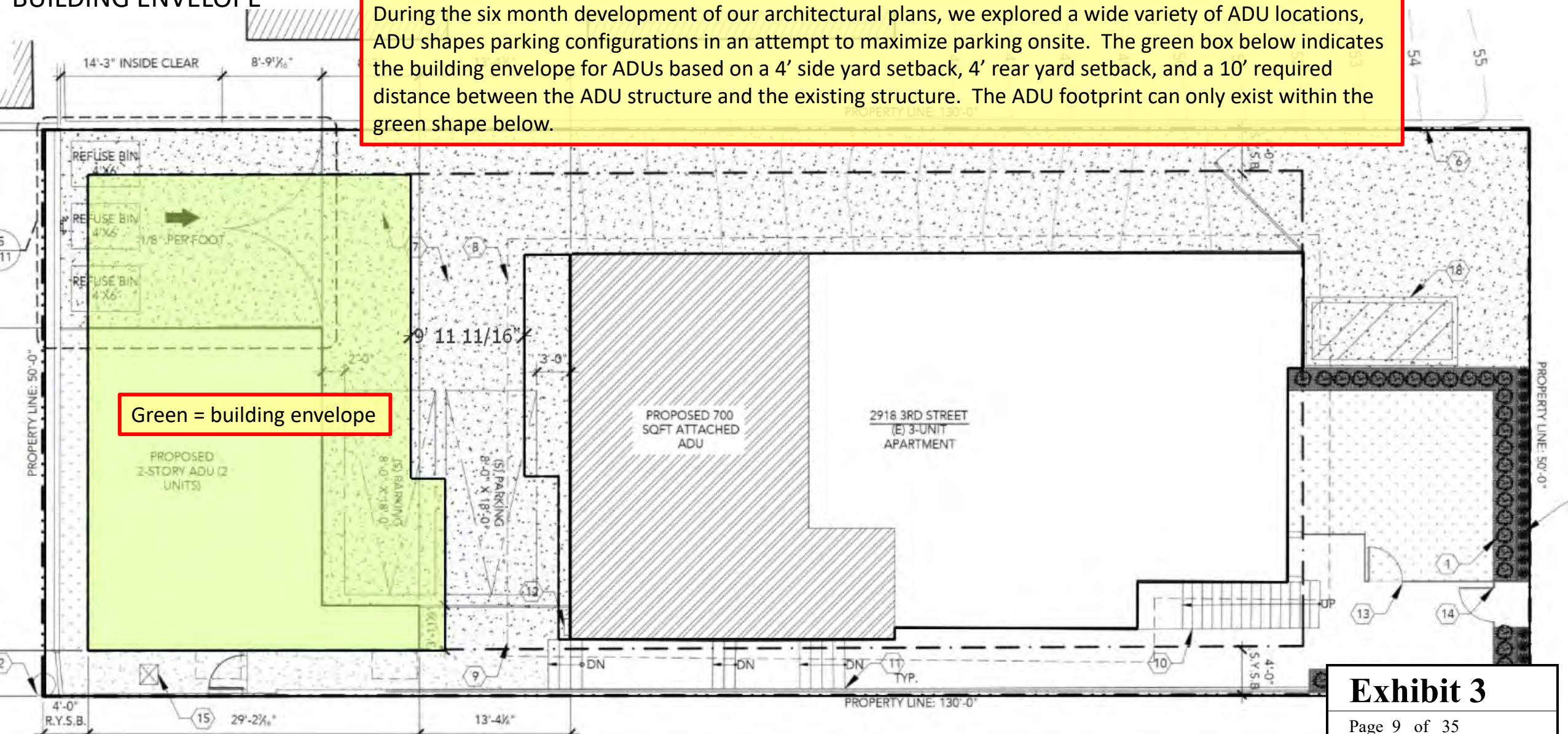


Exhibit 3

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California Coastal
Commission

ALTERNATIVE PARKING EXPLORATION –
BUILDING ENVELOPE

As previously described, the building envelope is significantly reduced due to the required trash enclosure, which must be in the rear yard setback and must be directly accessible along the long side of the enclosure. The City will not allow building in front of the enclosure and will not consider a parking stall in front of the enclosure a “legal” space.

Yellow = Trash enclosure

Green = building envelope

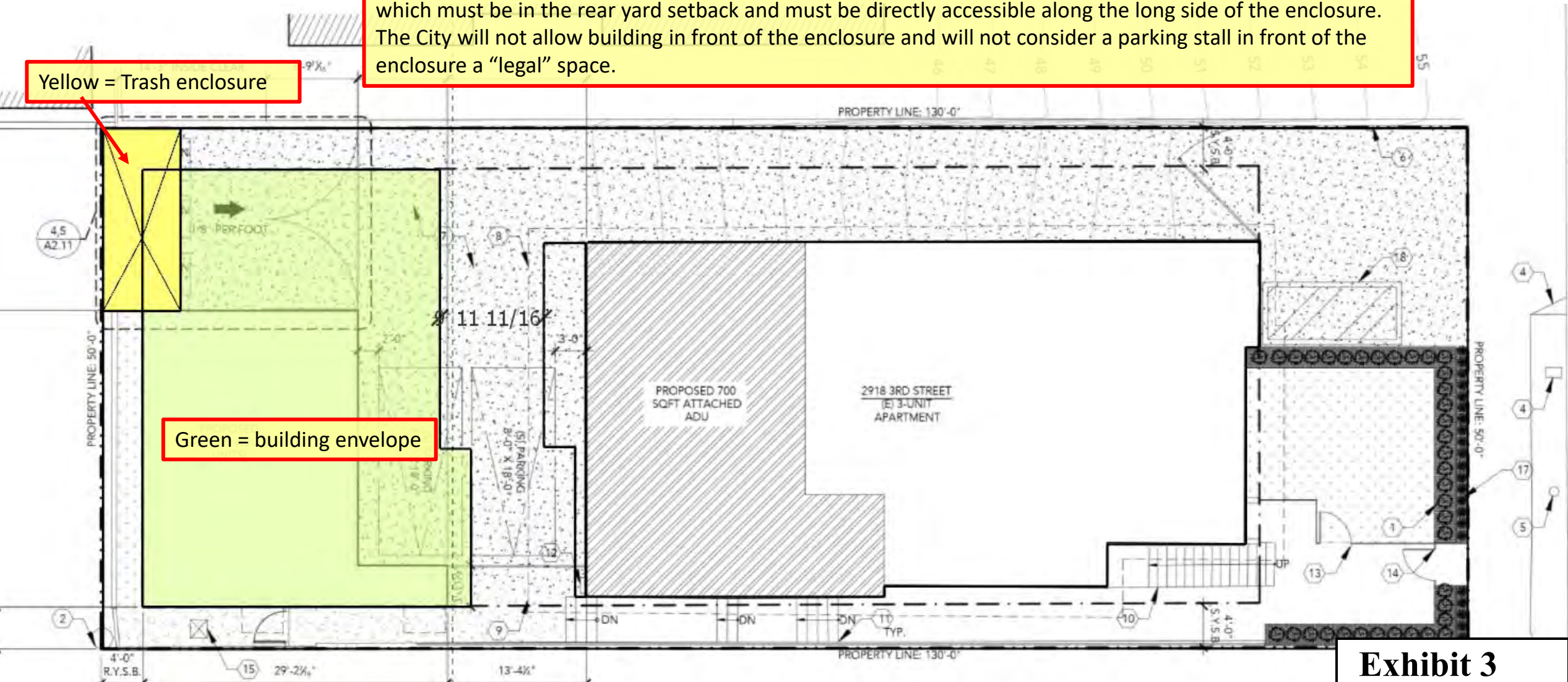
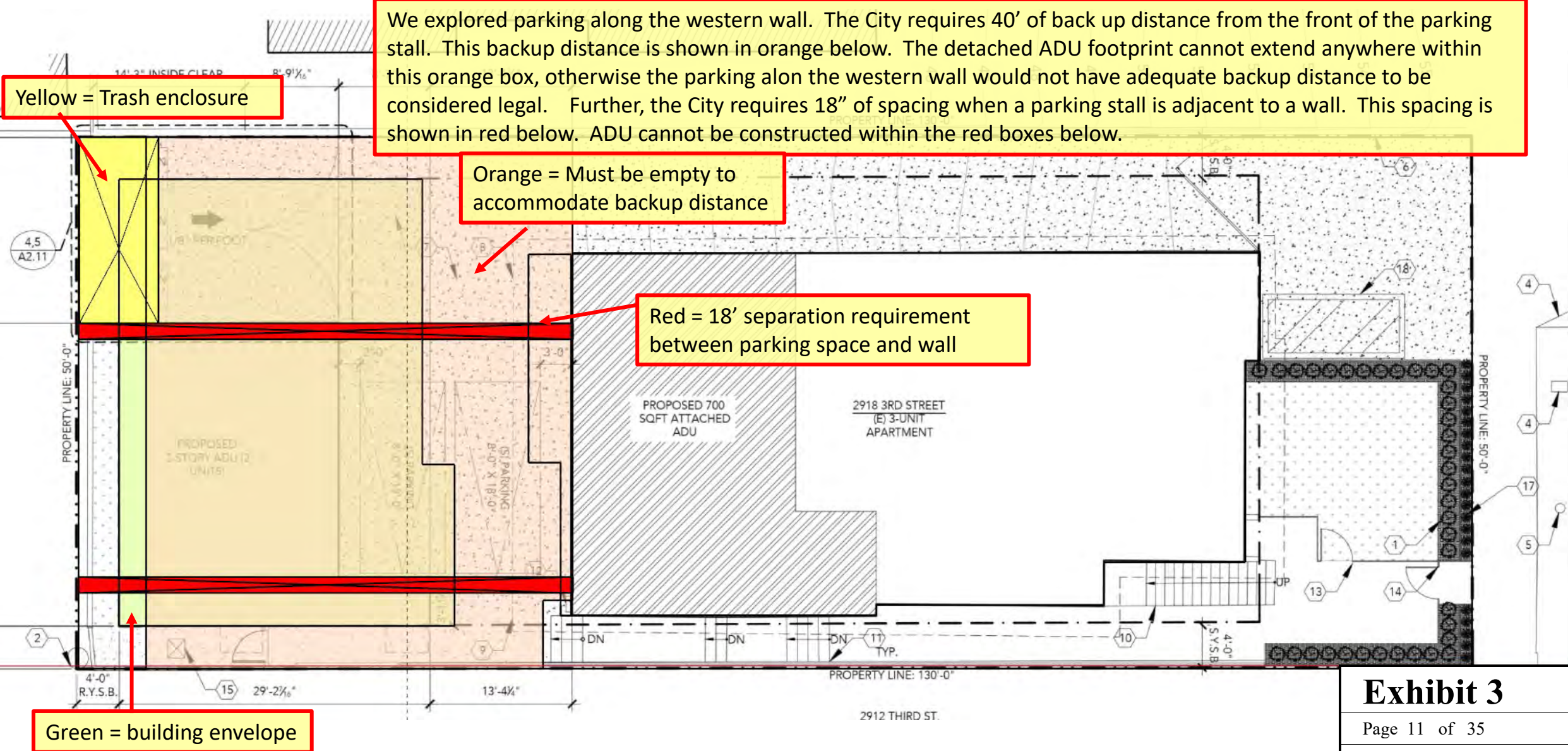


Exhibit 3

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 California Coastal Commission

ALTERNATIVE PARKING EXPLORATION – PARKING ALONG WESTERN WALL – OPTION 1



ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG WESTERN WALL – OPTION 1

Yellow = Trash enclosure

The Pink area indicates the only buildable ADU footprint under this scenario. This is the only footprint given the constraints of i) the trash enclosure, ii) the back up distance, iii) the required 18" separation between parking spaces and adjacent walls, iv) the rear and sideyard setbacks, and v) the required 10' distance from the existing building.

Orange = Must be empty to accommodate backup distance

Red = 18' separation requirement between parking space and wall

Pink = permissible building envelope

Purple = three (3) legal compact stalls

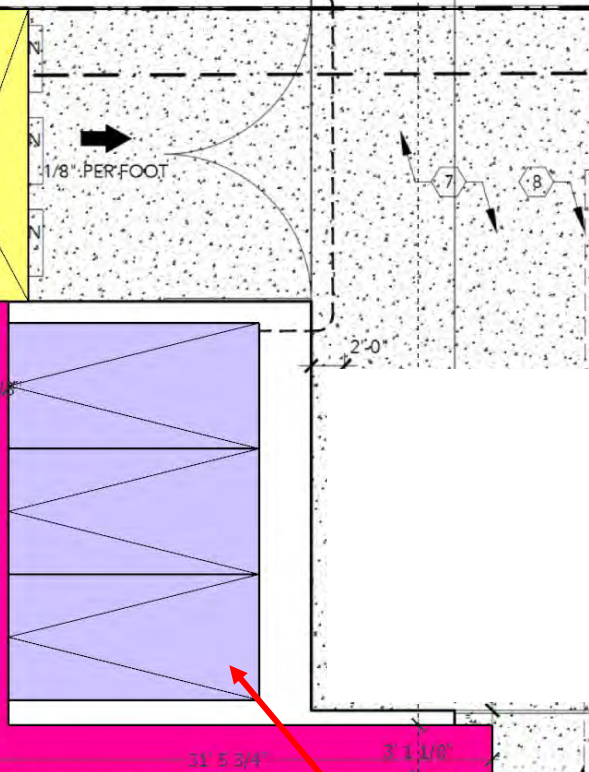
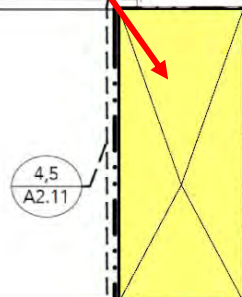
2918 THIRD STREET, SANTA MONICA, CA 90405

ADU DEVELOPMENT P

ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG WESTERN WALL – OPTION 1

A clearer picture of the permissible ADU building footprint, in pink. Clearly, this shape is not viable.

Yellow = Trash enclosure



Pink = permissible
building envelope

Purple = three (3) legal
compact stalls

ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG WESTERN WALL – OPTION 2

This diagram shows a second alternative for parking along the western wall, this time with the parking enclosure at the southeast of the rear yard.

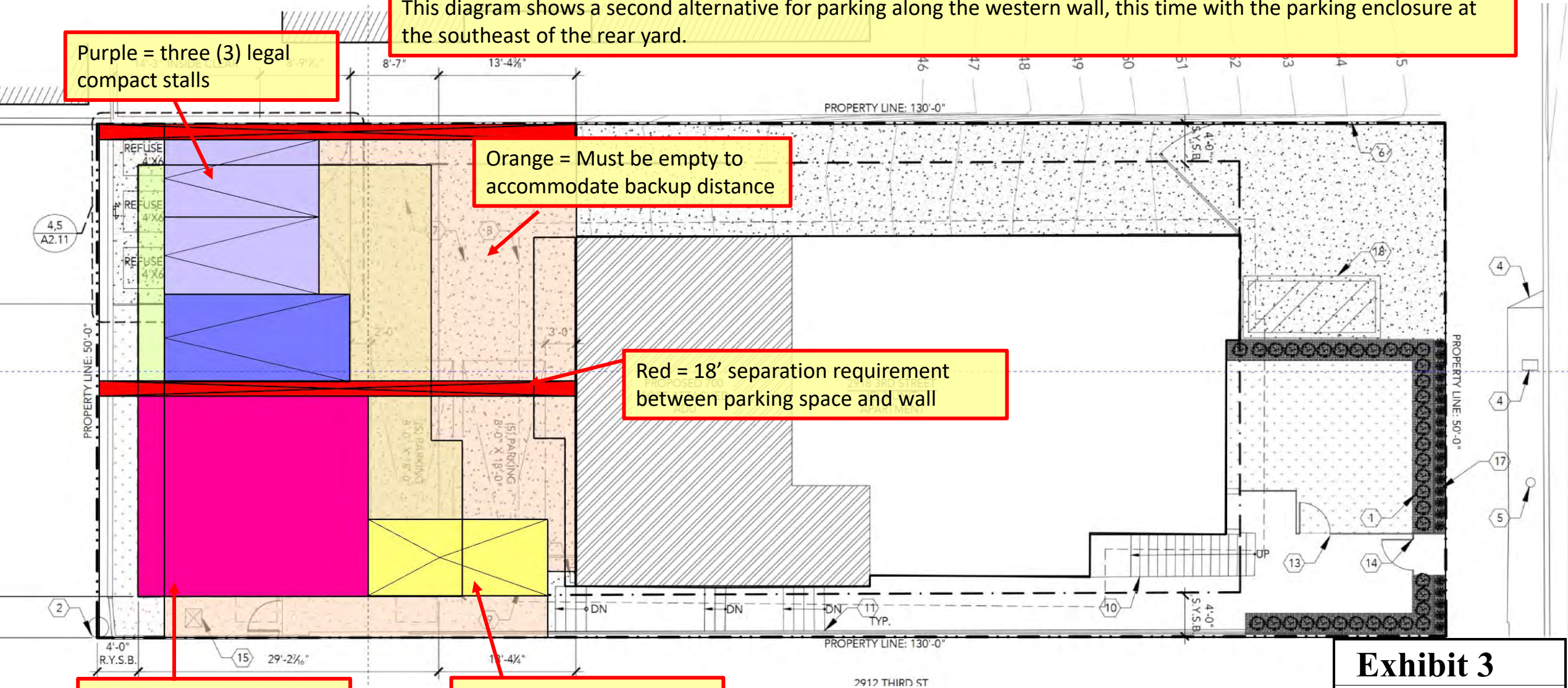
Purple = three (3) legal compact stalls

Orange = Must be empty to accommodate backup distance

Red = 18' separation requirement between parking space and wall

Pink = permissible building envelope

Yellow = Trash enclosure



ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG WESTERN WALL – OPTION 2

A clearer version of the second option. We explored this thoroughly as this appeared to be a viable alternative – although the ADUs would be dramatically smaller and less livable, it allowed 3 legal parking stalls, and 2 non-conforming but viable parking stalls in front of the trash enclosure. However, the City of Santa Monica considers a trash enclosure to be a structure, which cannot exist within 10’ of another existing structure, rendering this plan impermissible.

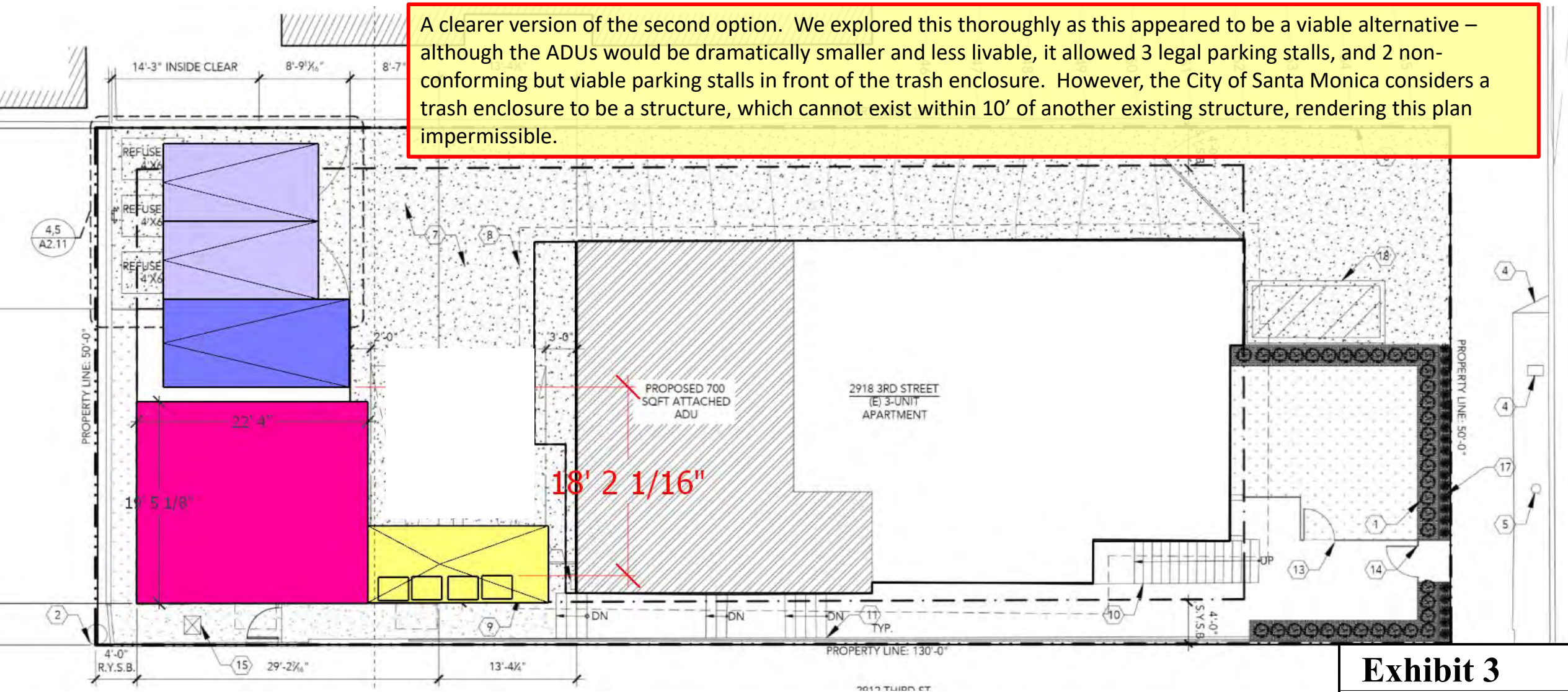


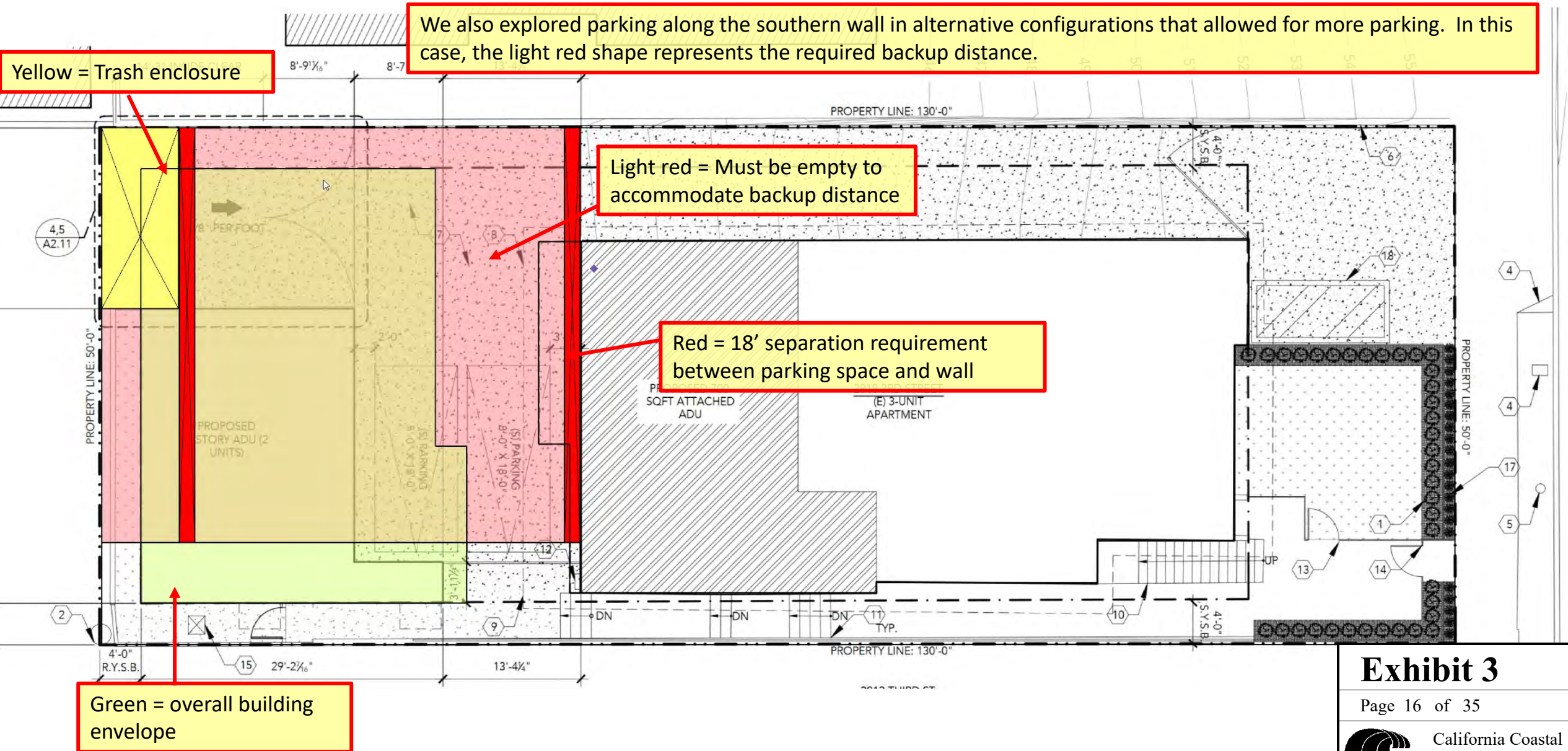
Exhibit 3

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ALTERNATIVE PARKING EXPLORATION – PARKING ALONG SOUTHERN WALL – OPTION 1



ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG SOUTHERN WALL – OPTION 1

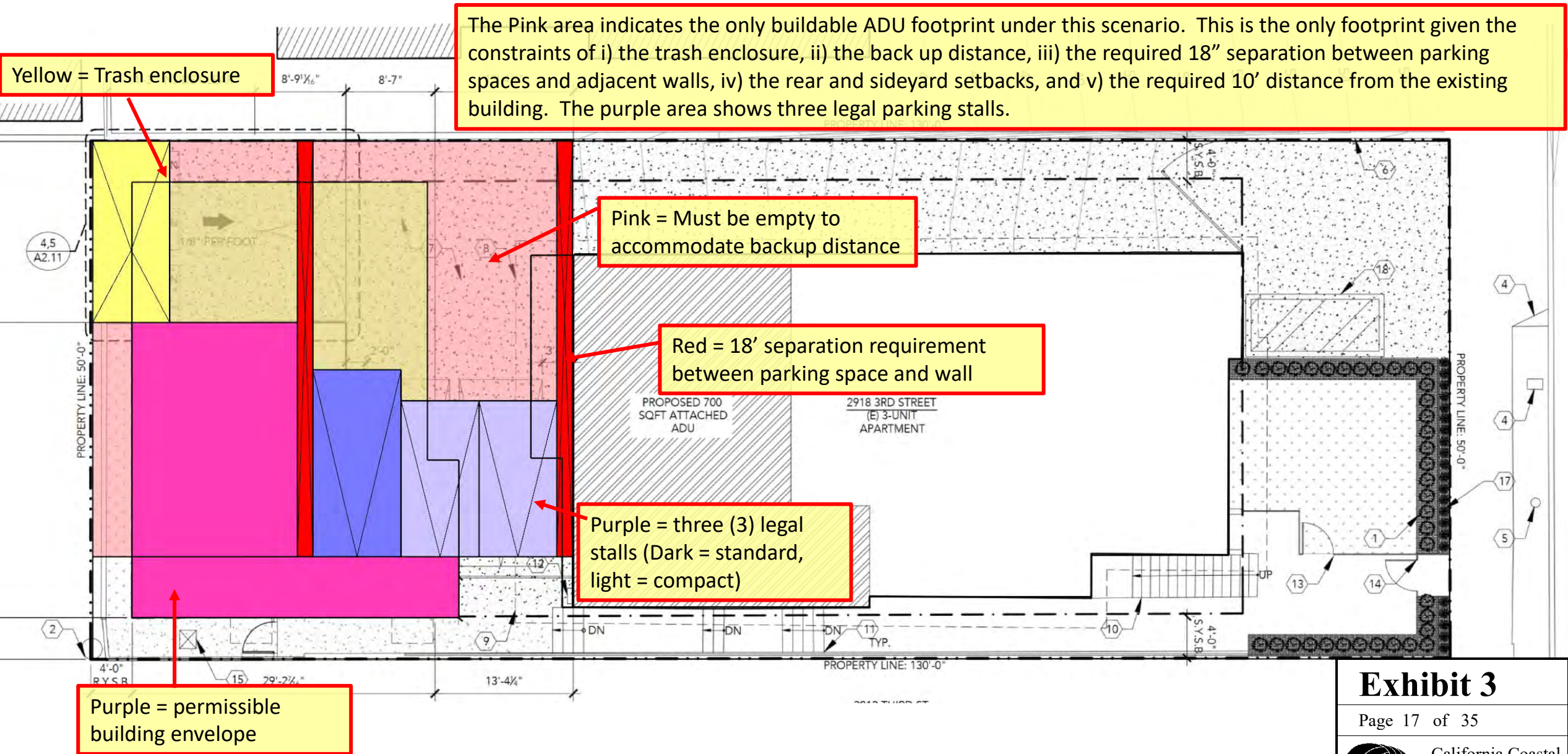
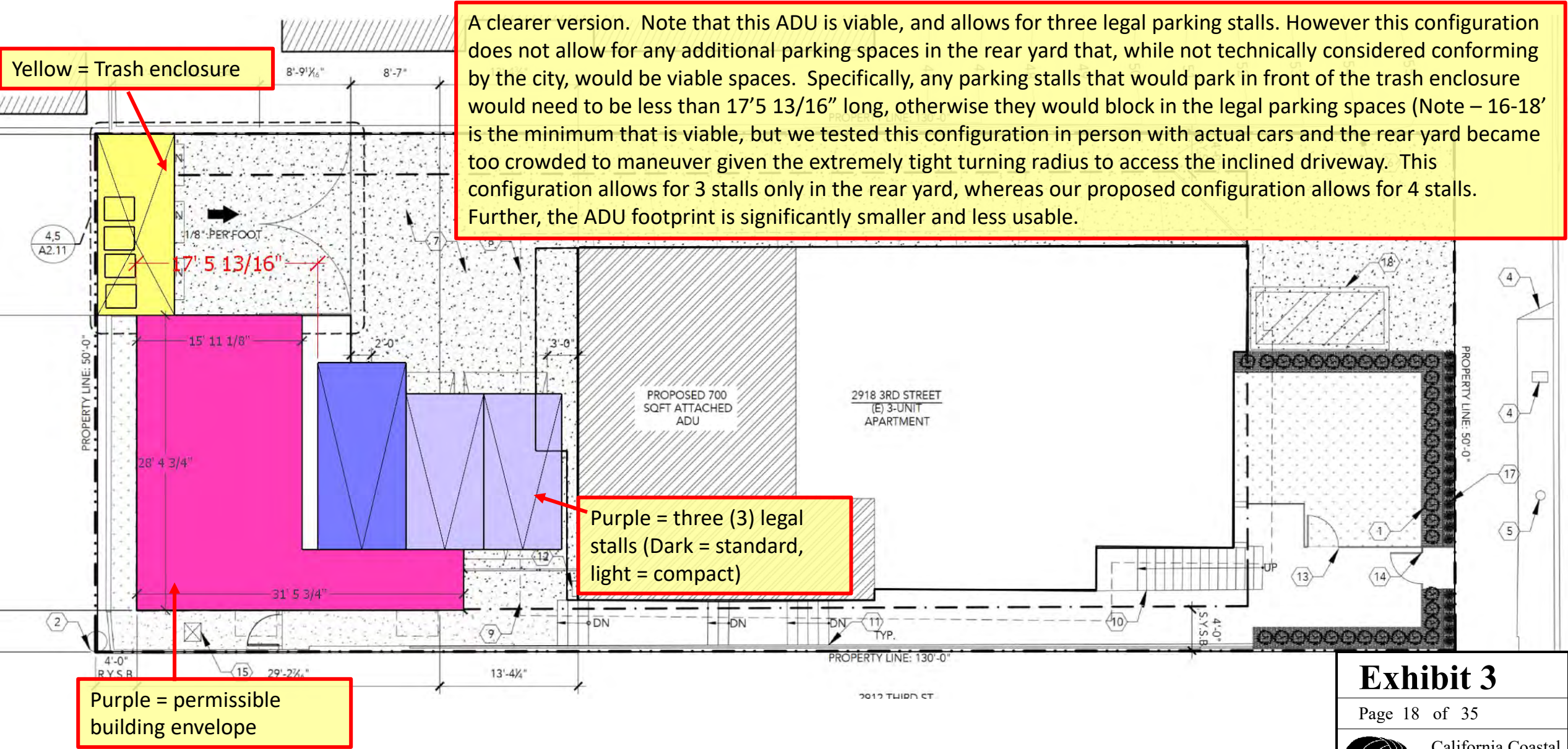


Exhibit 3

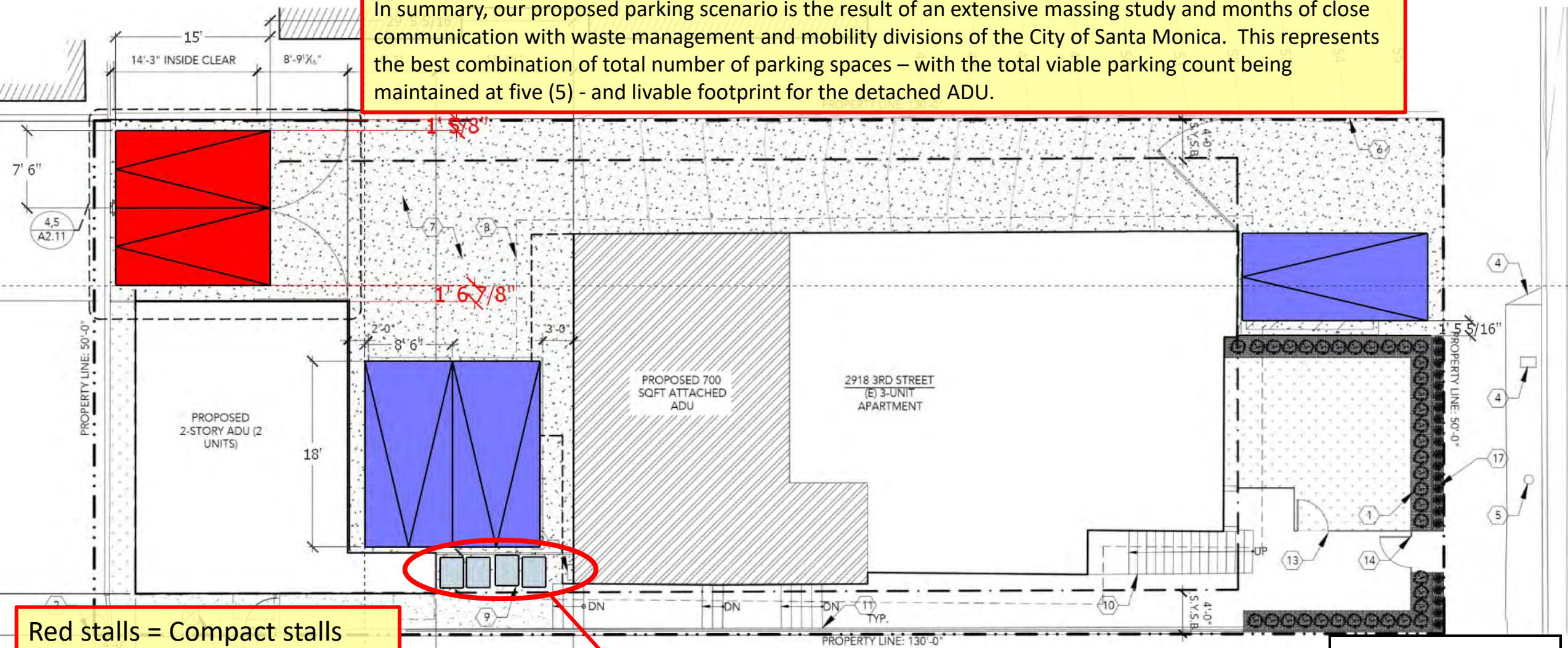
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ALTERNATIVE PARKING EXPLORATION –
PARKING ALONG SOUTHERN WALL – OPTION 1



ADU AND PARKING PLAN AS SUBMITTED TO COASTAL –
INCLUDING NON-CONFORMING BUT VIABLE STALLS

In summary, our proposed parking scenario is the result of an extensive massing study and months of close communication with waste management and mobility divisions of the City of Santa Monica. This represents the best combination of total number of parking spaces – with the total viable parking count being maintained at five (5) - and livable footprint for the detached ADU.



Red stalls = Compact stalls
Purple stalls = standard stalls

Four (4) 95-gallon trash
cans (actual size)

Exhibit 3

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PARKING STANDARDS

Mobility Division
1685 Main Street, Room 115, PO Box 2200
Santa Monica, CA 90407-2200
Tel: 310/458-8291
transportation.planning@smgov.net

Parking Stall Dimensions (see separate handout for R-1 Single Family)

- Standard (S): 8'6" x 18' x 7' (width x length x vertical clear)
- Compact (C): 7'6" x 15' x 7'
- Accessible: 14' (9'+5') x 18' x 8'2"
- Accessible Van: 17' (12'+5') or (9'+8') x 18' x 8'2"
- Parallel Parking Stall: 8'6" x 26' x 7' (one) 8'6" x 22' x 7' (multiple)
- Loading Space: See SMMC 9.28.080 for additional details
- Corner or End Stall: Commercial: (S) 12' (C) 11'
Residential: (S) 11'6" (C) 10'6"
- Wall Stall: (S) 8'6" + 1'6" for each wall and (C) 7'6" + 1'6"
- Vanpool Stall: 7'2" Vertical Clearance
- Bicycle stalls: 2' x 6' x 6' (7' for parking garages)

Wheel Stops

(H) = 36" for standard head-in parking, 30" for compact head-in parking

Striping

4" white stripes for parking and loading spaces. Blue stripes for accessible stalls.

Encroachments Upon Stalls

Wall-Mounted Storage. Cannot encroach more than 3'6" into the length and must be a minimum 4'6" off the ground. Includes shelves, racks and cabinets but not ducts or any mechanical, electrical and plumbing equipment.

Columns. A column ≤ 12" wide placed entirely within the non-shaded area (see diagram to right) is permitted without requiring any stall widening.

Rear Setbacks

15' from the centerline of the alley for garages, spaces, ramps, loading zones etc.

Hazardous Visual Obstruction Areas (HVOs)

HVOs provide the driver with a line of sight a minimum of 5 feet along the property and five feet back from the intersection of the property line and the driveway. See Santa Monica Municipal Code (SMMC) section 9.21.180.

Three Maneuvers

Per SMMC Section 9.28.120.B.8, use of a required parking space shall not require more than three vehicle maneuvers. To comply with this requirement, some parking spaces may require additional width and/or aisle space. Notwithstanding the above, for all uses with 20 or more parking spaces, up to 5% of the total number of parking spaces, with a maximum of 10 spaces, may require four turning maneuvers. Such spaces shall be distributed around the parking area(s) on the parcel.

Standard Aisle Widths and Angled Parking Design Dimensions

(A) PARKING ANGLE	(B) WIDTH COMP./STAND.	(C) DEPTH COMP./STAND.	(D) AISLE ¹ COMP./RESID./COMM.	(E) SINGLE BAY COMP./RESID./COMM.	(F) DOUBLE BAY COMP./RESID./COMM.
90	7'-6"	8'-6"	15'-0" 18'-0"	20'-0" 22'-0" 25'-0"	35'-0" 40'-0" 43'-0"
85	7'-7"	8'-6"	15'-7" 18'-8"	19'-0" 21'-0" 24'-0"	34'-7" 39'-8" 42'-8"
80	7'-8"	8'-7"	16'-1" 19'-2"	18'-0" 20'-0" 23'-0"	34'-1" 39'-2" 42'-2"
75	7'-9"	8'-10"	16'-5" 19'-7"	17'-0" 19'-0" 22'-0"	33'-5" 38'-7" 41'-7"
70	8'-0"	9'-0"	16'-9" 19'-10"	16'-0" 18'-0" 21'-0"	32'-9" 37'-10" 40'-10"
65	8'-4"	9'-4"	16'-10" 19'-11"	15'-0" 17'-0" 20'-0"	31'-10" 36'-11" 39'-11"
60	8'-8"	9'-10"	16'-9" 19'-10"	14'-0" 16'-0" 19'-0"	30'-9" 35'-10" 38'-10"
55	9'-1"	10'-4"	16'-7" 19'-7"	13'-0" 15'-0" 18'-0"	29'-7" 34'-7" 37'-7"
50	9'-10"	11'-1"	16'-4" 19'-2"	12'-0" 14'-0" 17'-0"	28'-4" 33'-2" 36'-2"
45	10'-7"	12'-0"	15'-11" 18'-8"	11'-0" 13'-0" 16'-0"	26'-11" 31'-8" 34'-8"
40	11'-8"	13'-2"	15'-5" 18'-0"	10'-0" 12'-0" 15'-0"	25'-5" 30'-0" 33'-0"
35	13'-1"	14'-10"	14'-8" 17'-2"	10'-0" 11'-0" 14'-0"	24'-8" 28'-2" 31'-2"
30	15'-3"	17'-0"	14'-0" 16'-2"	10'-0" 10'-0" 13'-0"	24'-0" 26'-2" 29'-2"

¹ The Mobility Division may approve a reduced aisle when the parking stall width is widened using a one to three ratio; for example, in some commercial developments, a 90-degree parking space could be widened to 9'6" (8'6" + 1') with a reduced drive aisle of 22' (25'-3').

² These dimensions may be used only if all spaces in the aisle are compact and may be used equally in commercial and residential districts.

This sheet is only a summary of the City's basic parking requirements. More information on parking design can be found in SMMC chapter 9.28, Off-street Parking Requirements, or by directly calling the Mobility Division.

Revised Jan 2020

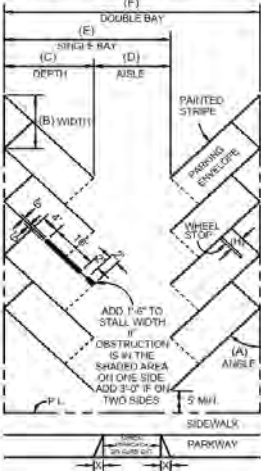


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Commission

Design Standards for On-Site Refuse and Recycling
Rooms or Outdoor Enclosures
Per Municipal Code Section 9.21.130

<u>NON-RESIDENTIAL OR MIXED-USE</u>	<u>REFUSE/RECYCLING DIMENSIONS OR AREAS</u>
Less than 5,000 sq ft	21' x 7'5" x 10'
5,001 – 20,000 sq ft	21' x 14' x 10'
20,001 – 40,000 sq ft	28' x 20' x 10'
<u>RESIDENTIAL DEVELOPMENT</u>	<u>REFUSE/RECYCLING DIMENSIONS OR AREAS</u>
Single Family Residence	On private property, screened from public
3 – 10 Units	21' x 7'5" x 10'
11 – 20 Units	21' x 14' x 10'
21 – 40 Units	28' x 20' x 10'

*Note: An outdoor enclosure must have walls at least 6' high and an opening at least 8' wide.

Large Residential, Nonresidential and Mixed-Use Development: Any development, whether residential, nonresidential, or mixed-use with more than 40 residential units, or with more than 40,000 square feet of floor area shall be reviewed by the Director of Public Works, who shall require the design and placement of a refuse and recycling room or outdoor enclosure consistent with the purpose of this Section to provide adequate and accessible areas for the storage and collection of the refuse and recyclable materials.

*

Revised 11/24/15

Diagram illustrating the design standards for a trash enclosure. The plan view shows a rectangular enclosure with a concrete floor, roof column, roof outline, and steel angle on all exposed corners. The enclosure is situated adjacent to an alley. Dimensions include a minimum width of 8'-0" and a total width of 21'-0". The enclosure is 10'-0" high. The diagram also shows a concrete curb with 3/4" chamfers at all corners. The property line is indicated. The diagram is labeled 'PLAN'.

GENERAL NOTES:

1. THE ENCLOSURE MUST BE LOCATED ADJACENT TO THE ALLEY IF AN ADJACENT ALLEY EXISTS. AN ADDITIONAL MONTHLY FEE MAY BE REQUIRED BY THE CITY IF THE ENCLOSURE IS LOCATED MORE THAN 10 FEET FROM THE STREET OR ALLEY OR IF THE ENCLOSURE IS LOCATED IN A SUBTERRANEAN AREA.
2. PROVIDE: HOSE BIB WITHIN 20' OF ENCLOSURE LIGHTING (MOTION SENSOR SWITCH) DRAIN (CONNECTED TO SEWER) VENTILATION
PLEASE NOTE: HOSE BIB MUST BE LOCATED OUTSIDE OF THE ENCLOSURE.
3. TYPICAL REFUSE BIN SIZE IS 6' W. 4' L. FURNISHED BY THE CITY.
4. MATERIALS TO BE USED FOR CONSTRUCTING THE BIN ENCLOSURES INCLUDE SOLID OPAQUE AND IMPACT RESISTANT WALLS. ALL SURFACES MUST BE SEALED WITH AN ANTI-GRAFFITI COATING.
5. DOORS TO BE SLIDING OR MOTORIZED ROLL-UP (SEE SHEET 2). INCLUDE 10-KEY ACCESS PAD ON THE OUTSIDE. NO ENCROACHMENTS INTO THE PUBLIC RIGHT-OF-WAY. TRASH ENCLOSURE, INCLUDING DOORS, SHALL NOT INTERFERE WITH THE 5' x 5' H.V.O TRIANGLE.
6. ALL MAINTENANCE INCLUDING BUT NOT LIMITED TO GRAFFITI REMOVAL, CLEANLINESS AND REPLACING BROKEN OR MISSING PLASTIC SLATS SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER.
7. ALL WORK SHALL BE DONE IN ACCORDANCE WITH CURRENT EDITIONS OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, CALIFORNIA BUILDING CODE, AND THIS DETAIL.
8. CONTRACTOR SHALL RESTORE ALL EXISTING CONSTRUCTION THAT IS CUT INTO, ALTERED, DAMAGED OR LEFT UNFINISHED.
9. ROOF ENCLOSURES SHALL BE CONSTRUCTED WITH A SOLID ROOF MEETING ARCHITECTURAL & STRUCTURAL DESIGN CRITERIA FROM THE CITY PLANNING AND BUILDING & SAFETY DIVISIONS.
10. TRASH ENCLOSURES SHOULD BE ACCESSIBLE FROM INSIDE THE PROPERTY.
11. INTERIOR FACE OF WALLS SHALL BE CLEAR OF ANY OBSTRUCTIONS.
12. REFUSE AND RECYCLING ENCLOSURE IS FOR THE SOLE PURPOSE OF STORING REFUSE, RECYCLING, AND ORGANIC CONTAINERS PROVIDED BY CITY OF SANTA MONICA.

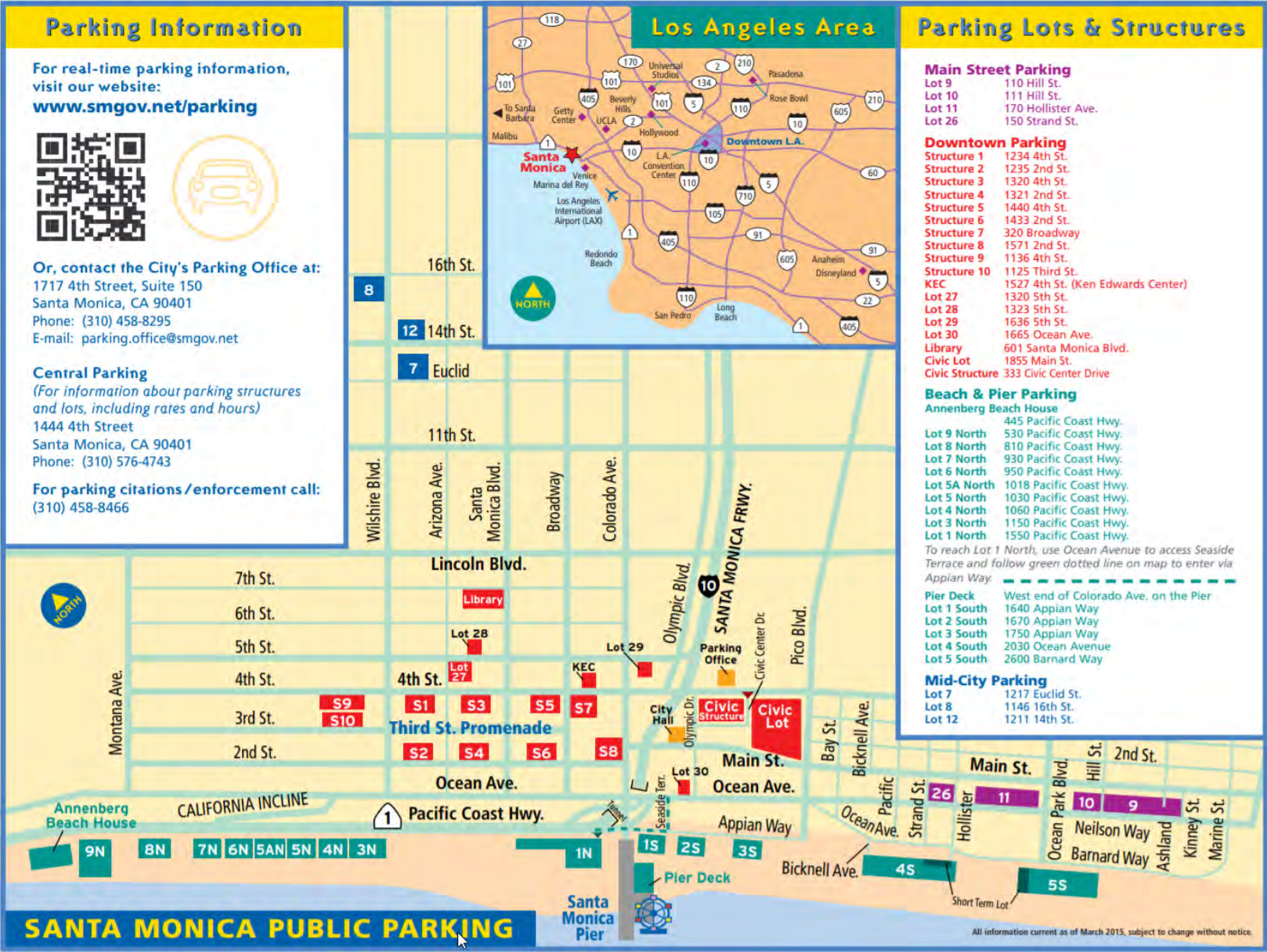
THIS DESIGN GUIDELINE IS FOR THREE BINS ONLY, PLEASE REFER TO THE NEW DESIGN STANDARD PER SANTA MONICA MUNICIPAL CODE 9.21.130 FOR RESIDENTIAL, NON-RESIDENTIAL, AND MIXED USE DEVELOPMENT.

City of Santa Monica
Department of Public Works
1437 4th Street, Suite 300 Santa Monica, CA 90401
TEL. (310) 458-8721 FAX. (310) 393-4425
e-mail : amengineering@smgov.net

GUIDELINES
ON-SITE REFUSE B

SUBMITTED BY : Robert Zek, P.E.
APPROVED BY : [Signature]
Rick T. Volk, P.E., City Engineer

REVISD 05/08/2018



Bryan Gortikov
2918 3rd Street, LLC
2633 Lincoln Boulevard

March 26, 2021

California Coastal Commission - South Coast Area Office (the "Commission")
301 E Ocean Blvd., Suite 300, Long Beach, CA 90802
Application Number: 5-20-0607
Project Address: 2918 3rd St, Santa Monica, CA 90405

Re: Response to Request for Additional Information – 2918 3rd Street, Santa Monica

Dear Amber & Vince,

Following our conference call on March 23rd at 12:00PM PST, we wish to provide the following additional information and project design updates to assist with your review of the ADU development (the "Project") located at 2918 3rd Street in Santa Monica (the "Subject"). Note that we have expedited our design and response in an effort to address the Commission's requests this week per our discussion. The plans shown in the attached exhibits are conceptual in nature and for illustrative purposes only. We will forward the formal architectural plans once they are complete. Once we have your approval, we will pursue a revision to our Concept Approval from the City of Santa Monica.

Consistency with State ADU Laws

Assembly Bill No. 68 (the "State ADU Law" or "AB68") was approved on October 9, 2019 and went into effect January 1, 2020. Among other things, AB68 (specifically Sec 1.1, which amends Government Code Section 65852.2) requires that local agencies permit the development of both *Attached ADUs* and *Detached ADUs*, which permission shall supersede any restrictions that may exist in local agency's code. The specific provisions are excerpted below::

*Sec 1.1(e)(1). Notwithstanding subdivisions (a) to (d), inclusive, a local agency shall ministerially approve an application for a building permit within a residential or mixed-use zone to **create any of the following**:*

- (A) *[Omitted for brevity – refers to single family]*
- (B) *[Omitted for brevity – refers to single family]*
- (C)
 - (i) *Multiple accessory dwelling units within portions of existing multifamily dwelling structures that are not used as livable space, including, but not limited to, storage rooms, boiler rooms, passageways, attics, basements, or garages, if each unit complies with state building standards for dwellings.*
 - (ii) *A local agency shall allow **at least one accessory dwelling unit within an existing multifamily dwelling** and shall allow up to 25 percent of the existing multifamily dwelling units.*

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Commission

(D) Not more than two accessory dwelling units that are located on a lot that has an existing multifamily dwelling, but are detached from that multifamily dwelling and are subject to a height limit of 16 feet and four-foot rear yard and side setbacks.

Per the code excerpted above, the State ADU law permits “any of” (as opposed to “only one of”) the types of ADUs detailed in Section 1.1(e)(1). Subsection (C) describes “Attached ADUs”, which are permitted in number up to 25% of the total unit count but shall be no less than one. Because 2918 3rd Street contains only three units, it qualifies for only one Attached ADU. This ADU is in the location of a former laundry and storage area, which qualifies as “non-livable space” per AB68.

Additionally, Subsection (D) describes “Detached ADUs”, which are permitted up to a maximum of two with the condition that they are detached from the multifamily dwelling, are limited to 16’ in height, and abide by four-foot rear and side yard setbacks. Note that the City of Santa Monica permits a maximum height of 23 feet in the Subject’s “OP2” zoning district. The two proposed Detached ADUs at 2918 3rd Street comply with the applicable restrictions.

Please note that there are additional development standards detailed within the State ADU Law, including but not limited to unit size. In each case, we have carefully reviewed the restrictions and proposed a development that is compliant with the applicable provisions of Santa Monica Municipal Code and the State ADU Laws. The City of Santa Monica has reviewed and indicated its concurrence, as evidenced by its Concept Approval.

Parking Counts

In the time since our last formal response to the Commission, we have come to understand that the Commission would like us to prioritize maximizing legal, conforming spaces as opposed to spaces that are viable but non-conforming. We were also notified that the City Attorney of Santa Monica has advised its planning staff that it can no longer impose the trash enclosure requirements outlined in the Santa Monica Municipal Code on ADU projects within the City; the requirements proved unreasonably difficult to comply with, which is not consistent with the spirit of AB68. This change in messaging from the City is helpful to our ability to maintain as much parking as possible following the Development.

Per our March 23rd conversation, we understand that the Commission is willing to recommend the Subject for approval if we are able to provide at a minimum three legal, conforming parking spaces. Since that conversation, we have spoken with Peter Dzewaltowski, Transportation Planning Associate, City of Santa Monica Mobility Division, and James Combs, Planner, City of Santa Monica Planning Division.

With only a superficial drafting change, our original plans would be approved by the City as providing three legal parking spaces. We would simply remove the markings for the two compact parking spaces in the Northwest corner – each of which is 3” narrower than permissible – and replace them with markings indicating one Standard space with an abundance of spacing on either side as shown on **Exhibit A**. This revised space would comply with Santa Monica’s parking standards, and the overall parking count would comply with the Commissions stated minimum parking goal.

However, in an effort to please the Commission we have explored a redesign that would allow us to meet the Commissions “stretch” target of four parking spaces. To accommodate this, the northern wall of the

Exhibit 3

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detached ADU must be moved 18 inches south, creating an additional 18 inches of width for the side-by-side parking spaces in the Northwest corner. This would enable us to park one standard and one compact side-by-side in the Northwest corner, which would equate to a total of four spaces (3:1 standard to compact ratio) overall, as shown on **Exhibit B**. Moving this wall would adversely impact the livability of the interior of the unit. Specifically, it would require us to reduce the already limited existing kitchen counter space and storage by approximately 20%. While it is a secondary concern, it is worth mentioning that Ownership will incur significant cost in implementing this change as it would require material revisions to the existing structural, mechanical, electrical, plumbing, and architectural plans. *Note that the City rejected our request to approve a 1:1 standard to compact ratio, which would have required a less significant modification to our plans. The City would consider approving this request subject to our pursuing a minor entitlement (a "Minor Modification Permit"), but this would be time- and cost-prohibitive.*

Despite the adverse impacts stated above, if the Commission believes that providing these four spaces is necessary in order to recommend our project for approval, we will proceed with the redesign and seek immediate conceptual approval from the City of a revised site layout with four legal parking spaces.

Given the timing constraints of our Coastal Development Permit approval deadline, we would appreciate your prompt feedback. I am available by phone at (818) 613-7960 or via email at bg@gortikovinc.com

Kind regards,



Bryan Gortikov

Exhibit 3

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EXHIBIT A – THREE STANDARD PARKING SPACES – NO CHANGE REQUIRED TO PLANS

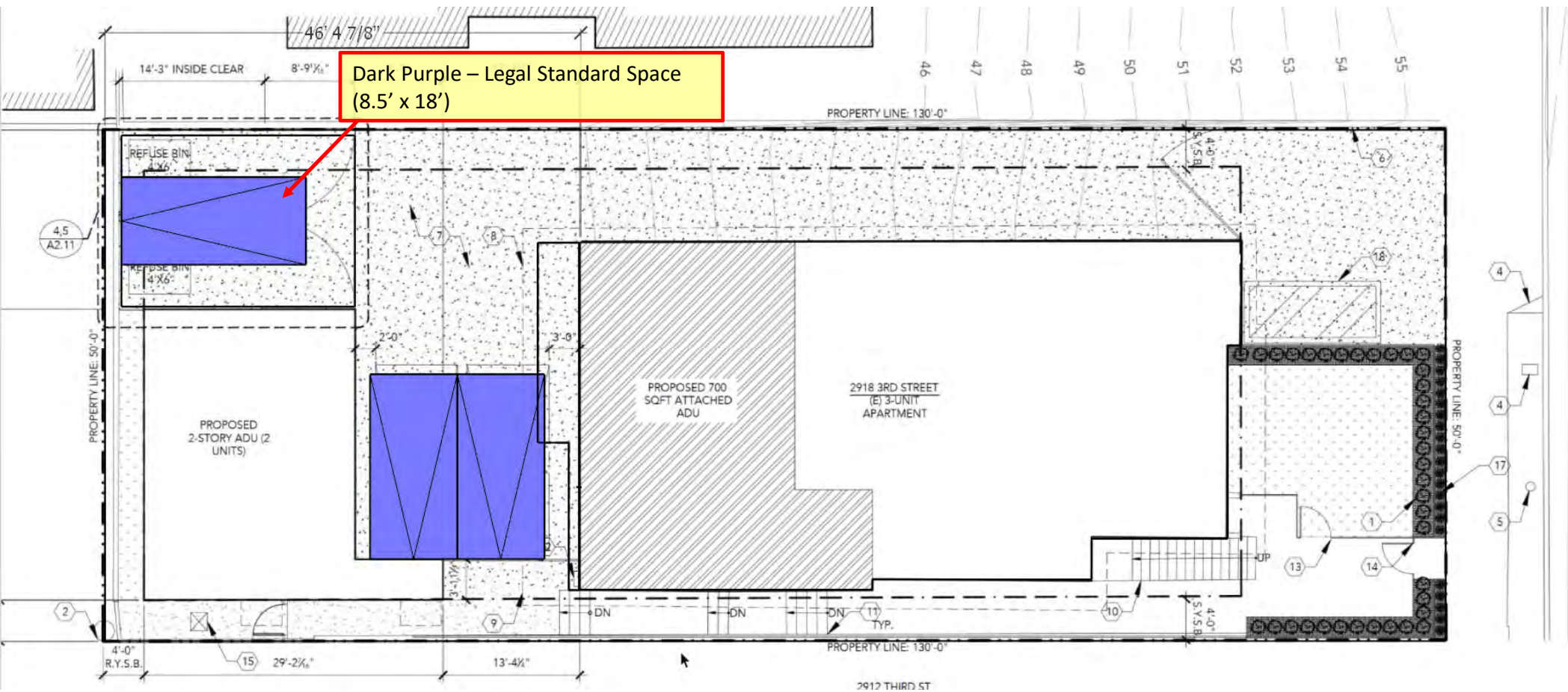


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EXHIBIT B – FOUR PARKING SPACES (3:1) – CHANGE REQUIRED

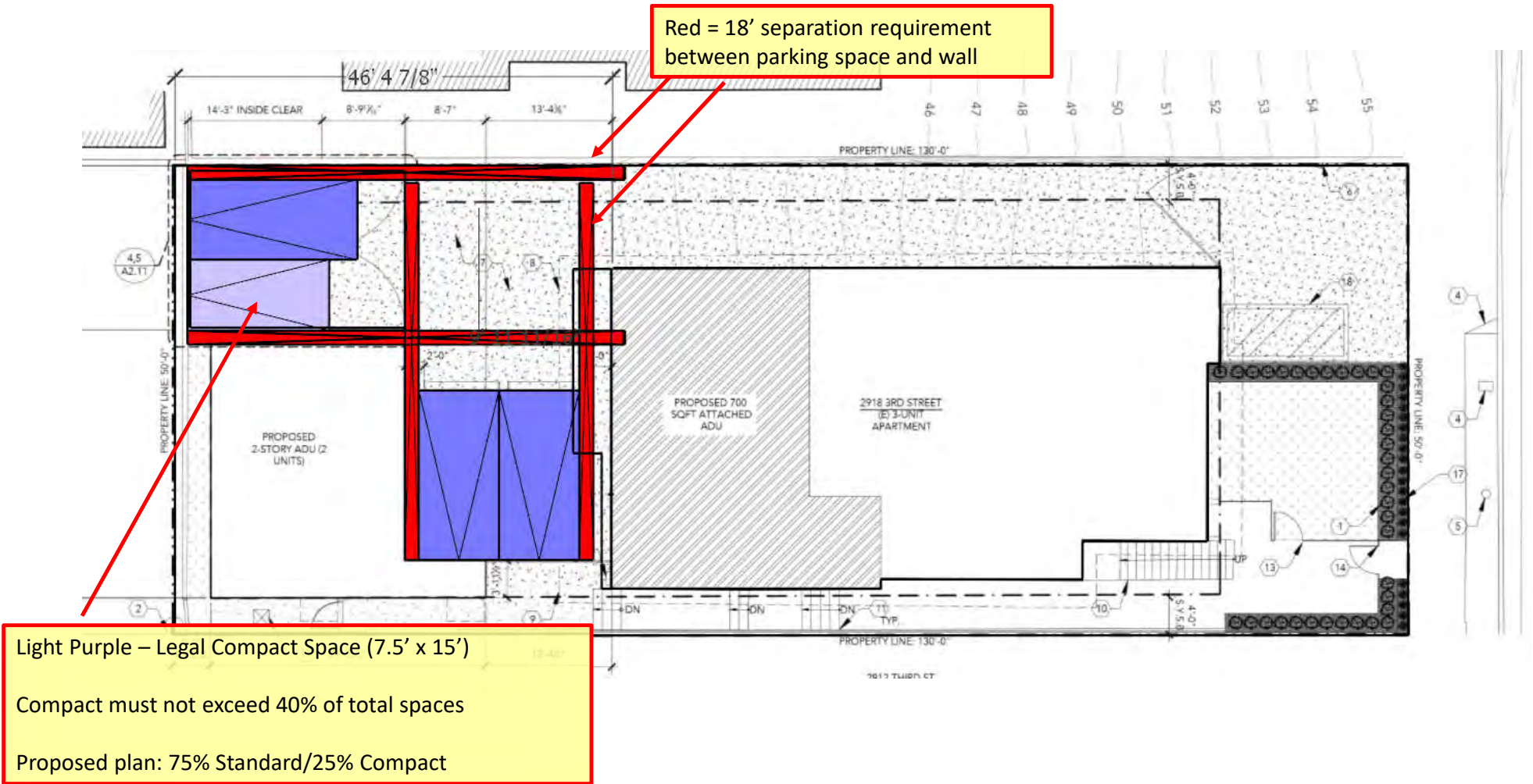


EXHIBIT B – FOUR PARKING SPACES (3:1) – CHANGE REQUIRED

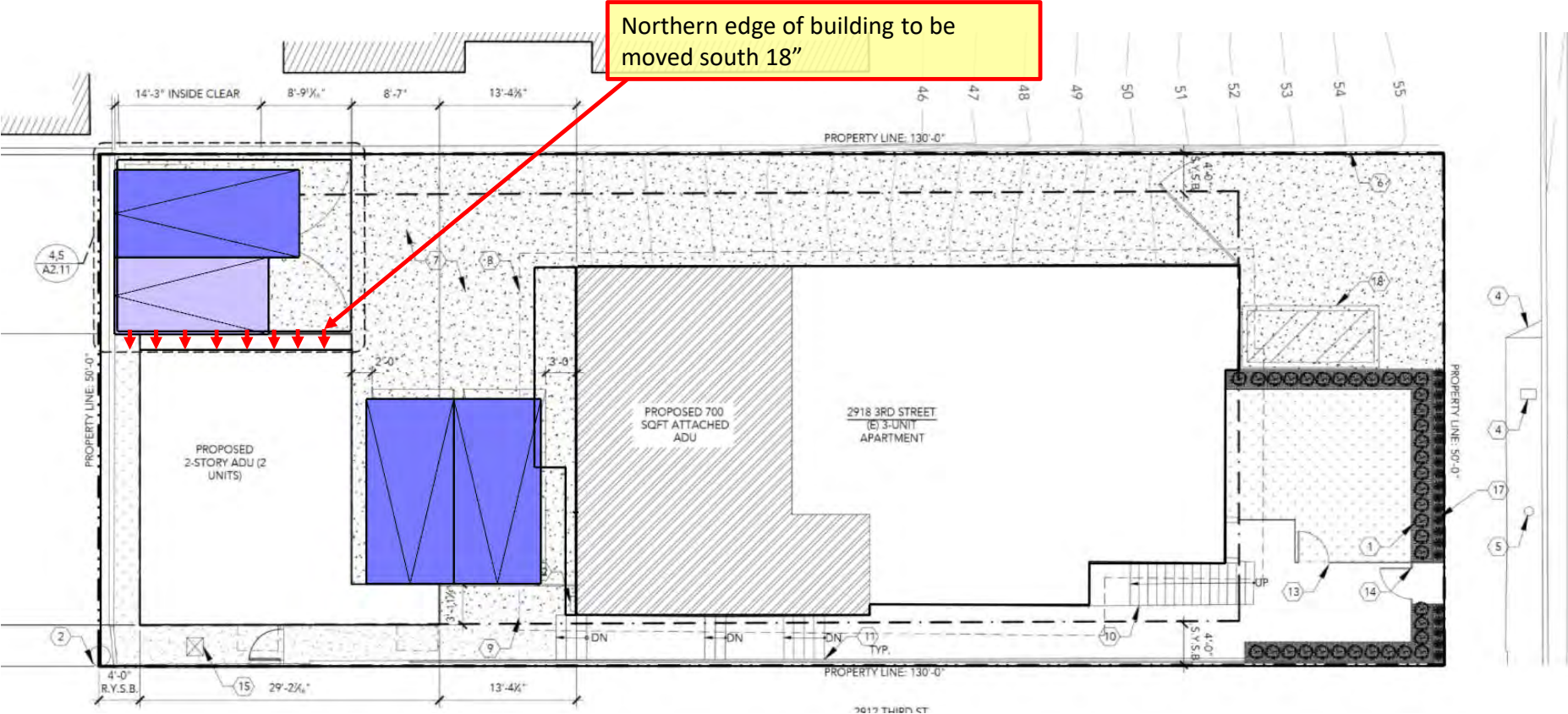
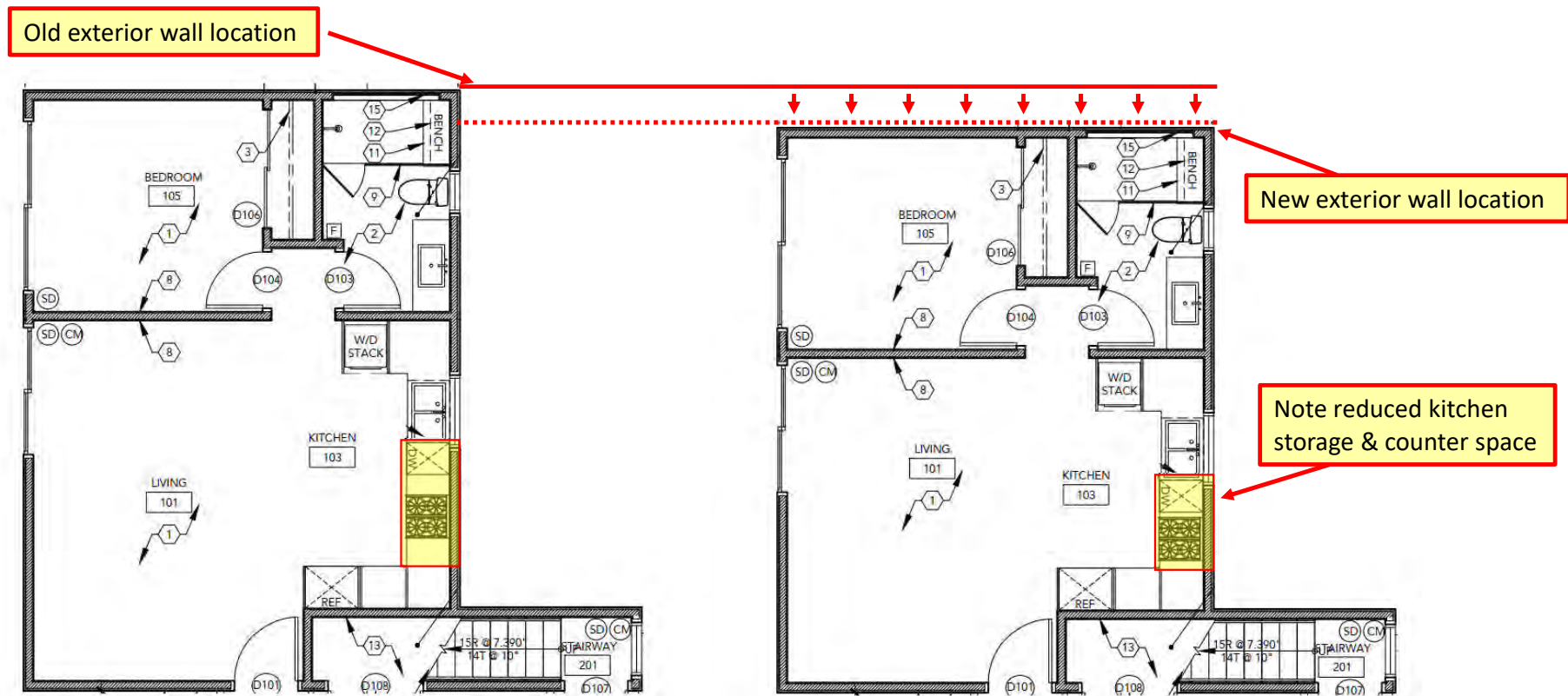
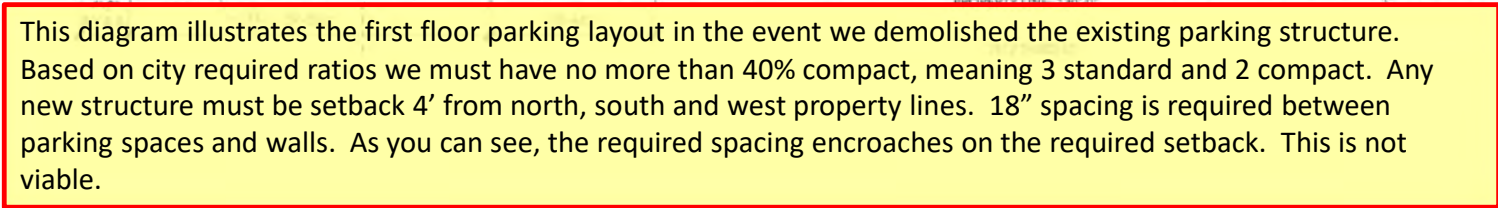


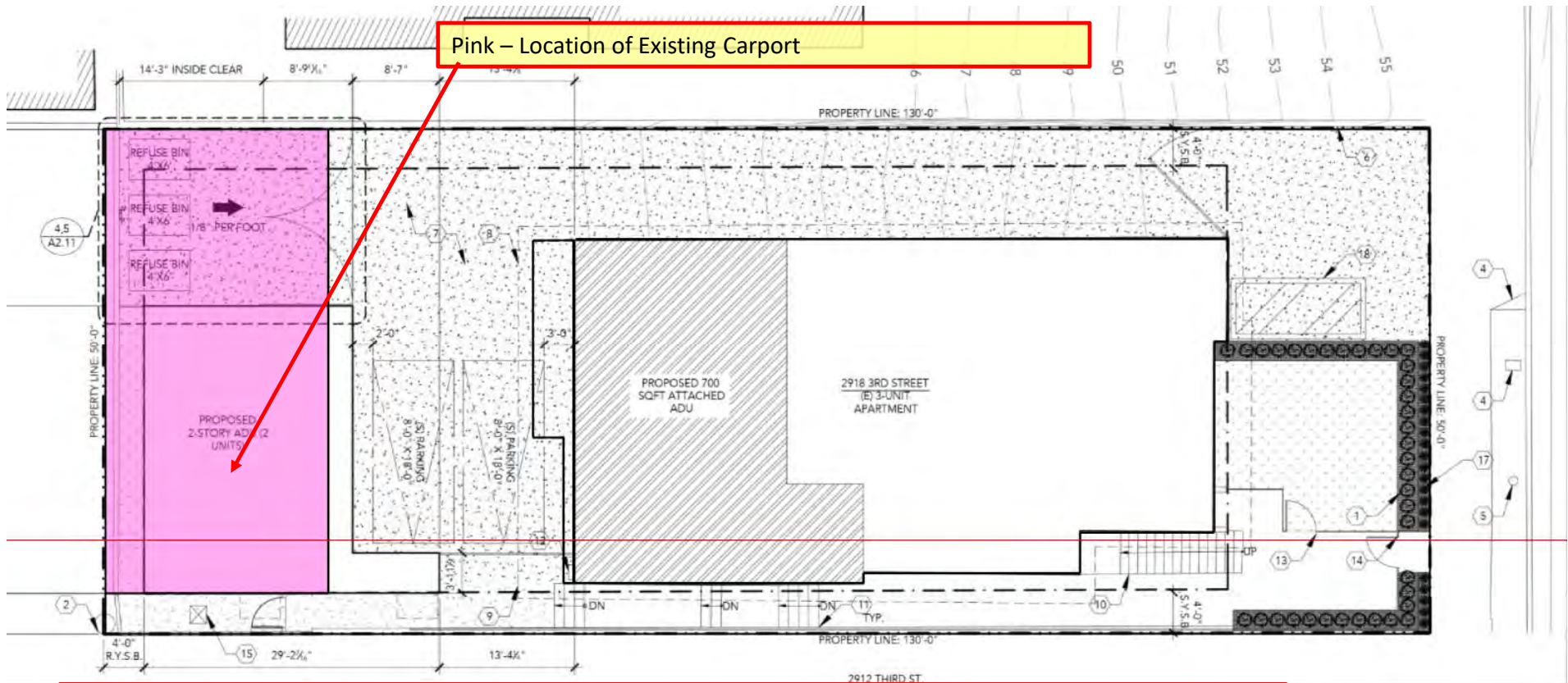
EXHIBIT B – FOUR PARKING SPACES (3:1) – CHANGE REQUIRED



Interior of detached ADU is shown here to illustrate the consequence of moving the exterior wall. We will lose 18" along the north-south dimension in each of the two ADUs. The bedrooms and bathrooms are already sized as small as they can be. The length will be eliminated from the kitchen, which eliminates most of the counter and storage space.

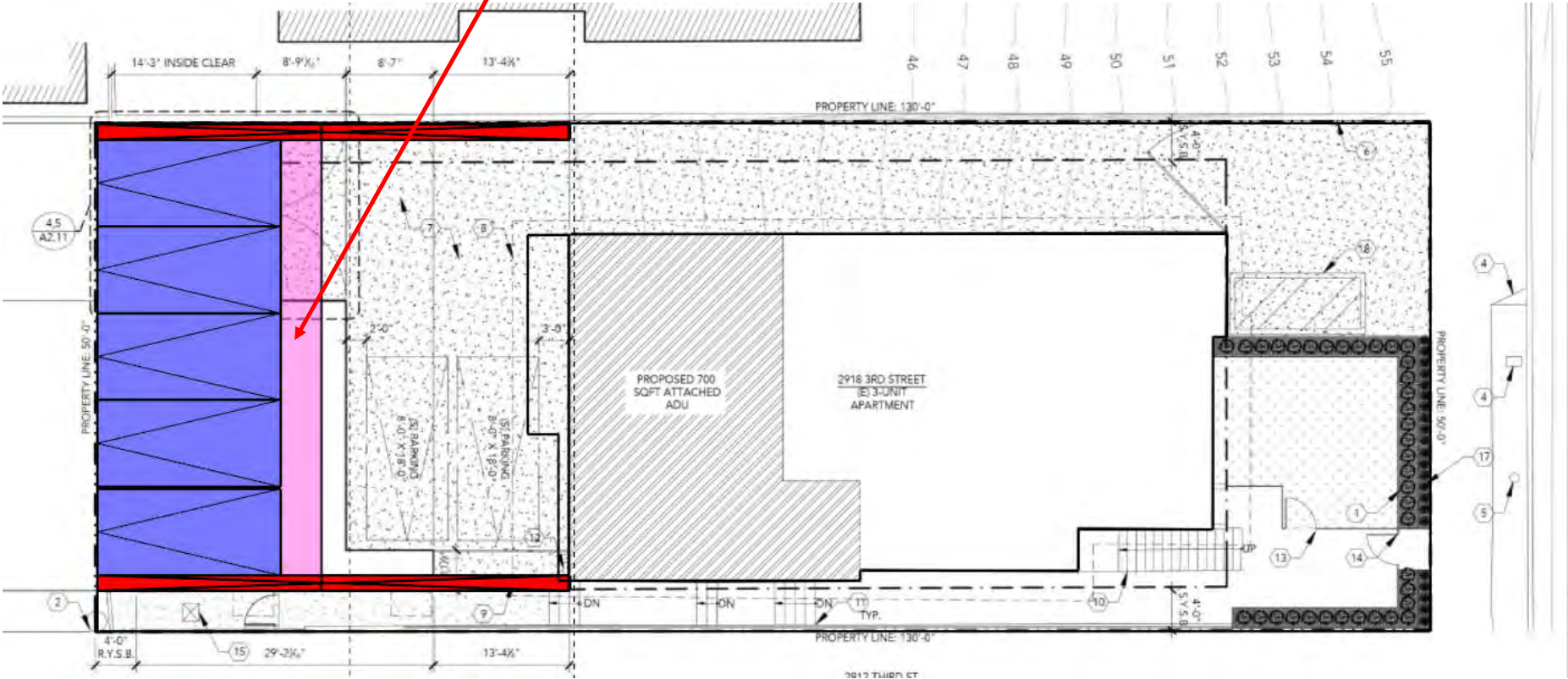
APRIL 20 2021





In order to accommodate the width of five parking stalls, we must keep the existing carport unmodified. A second story may be constructed, but no portion of the second story may exist within the setbacks (4' from any side), and be a minimum of 10' from the existing building.

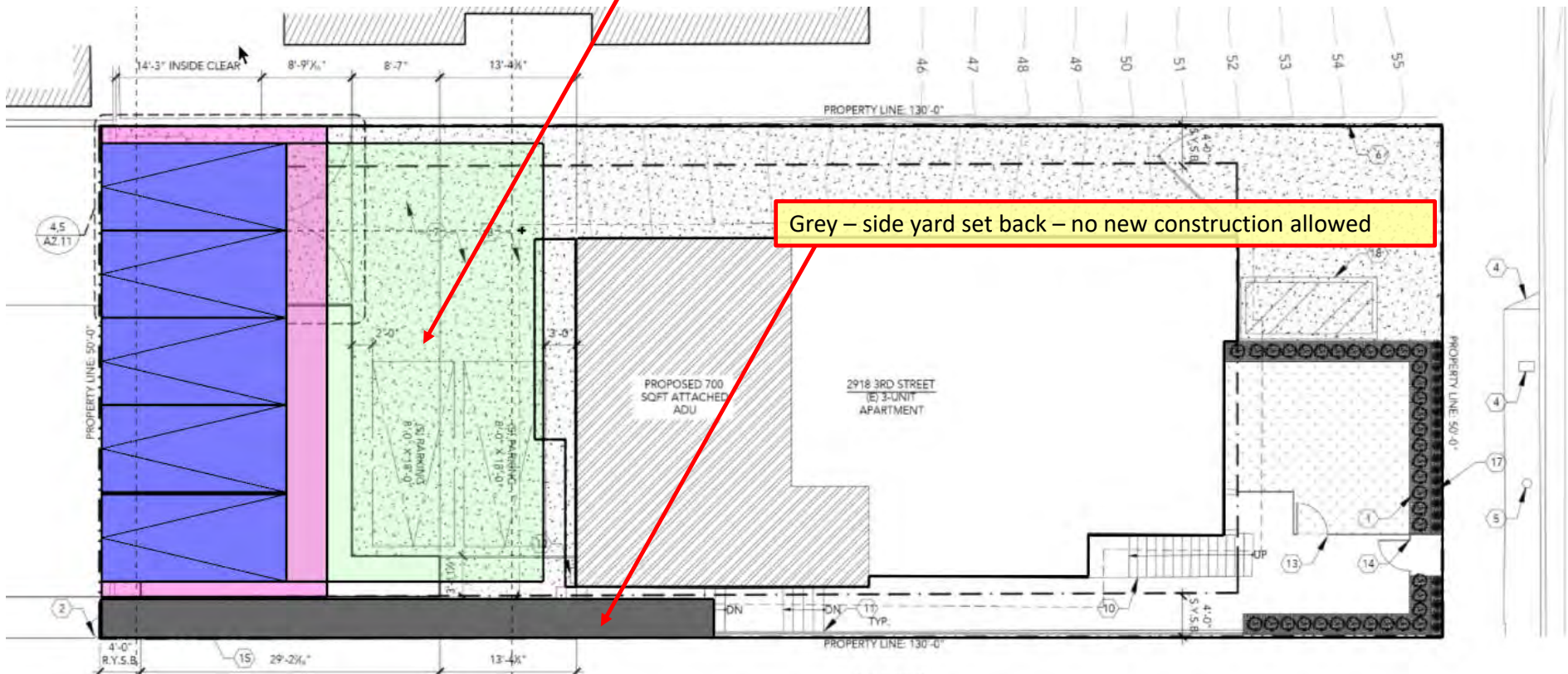
Pink – Location of Existing Carport



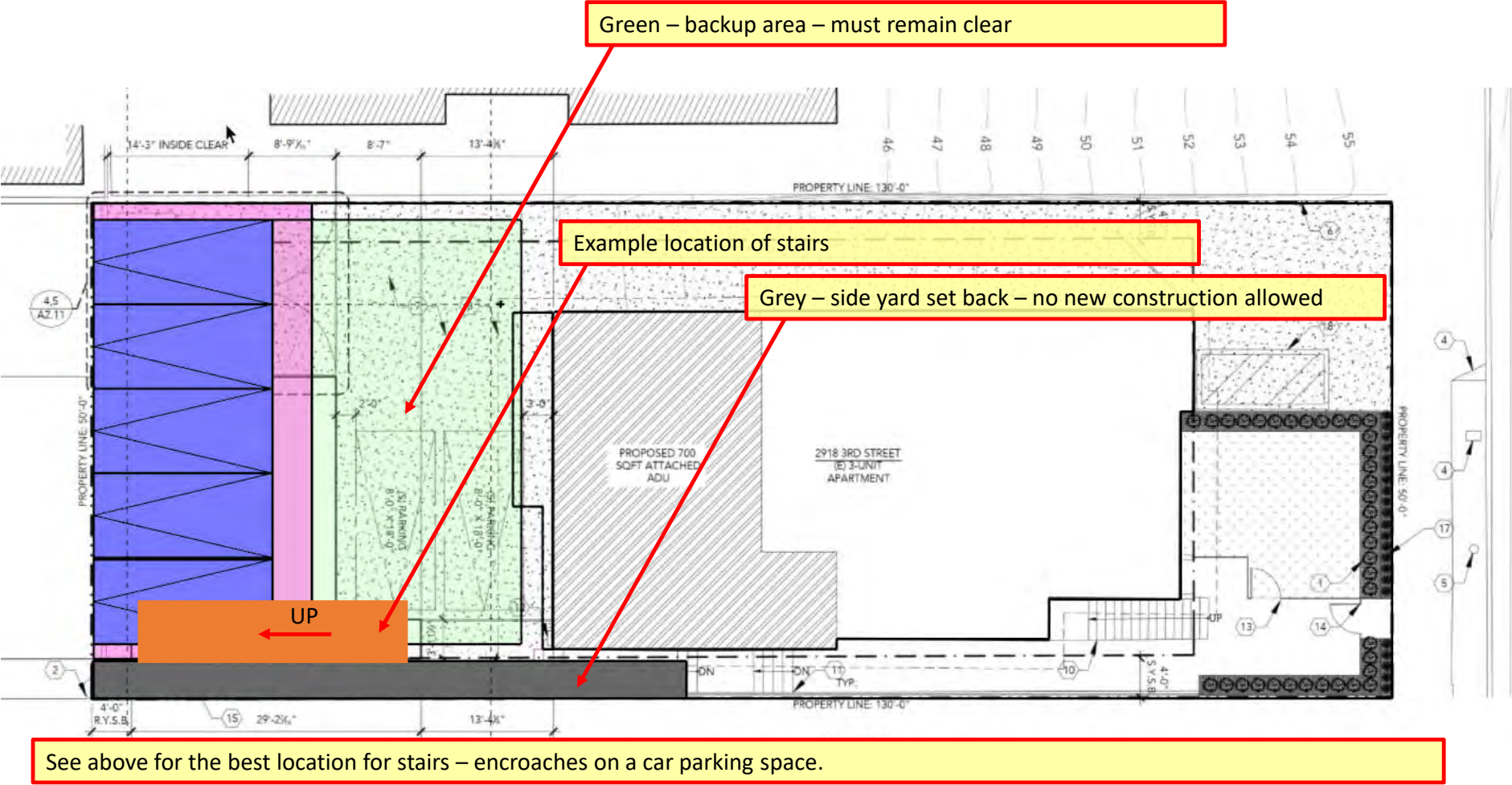
As you can see, the existing carport currently accommodate five standard parking stalls

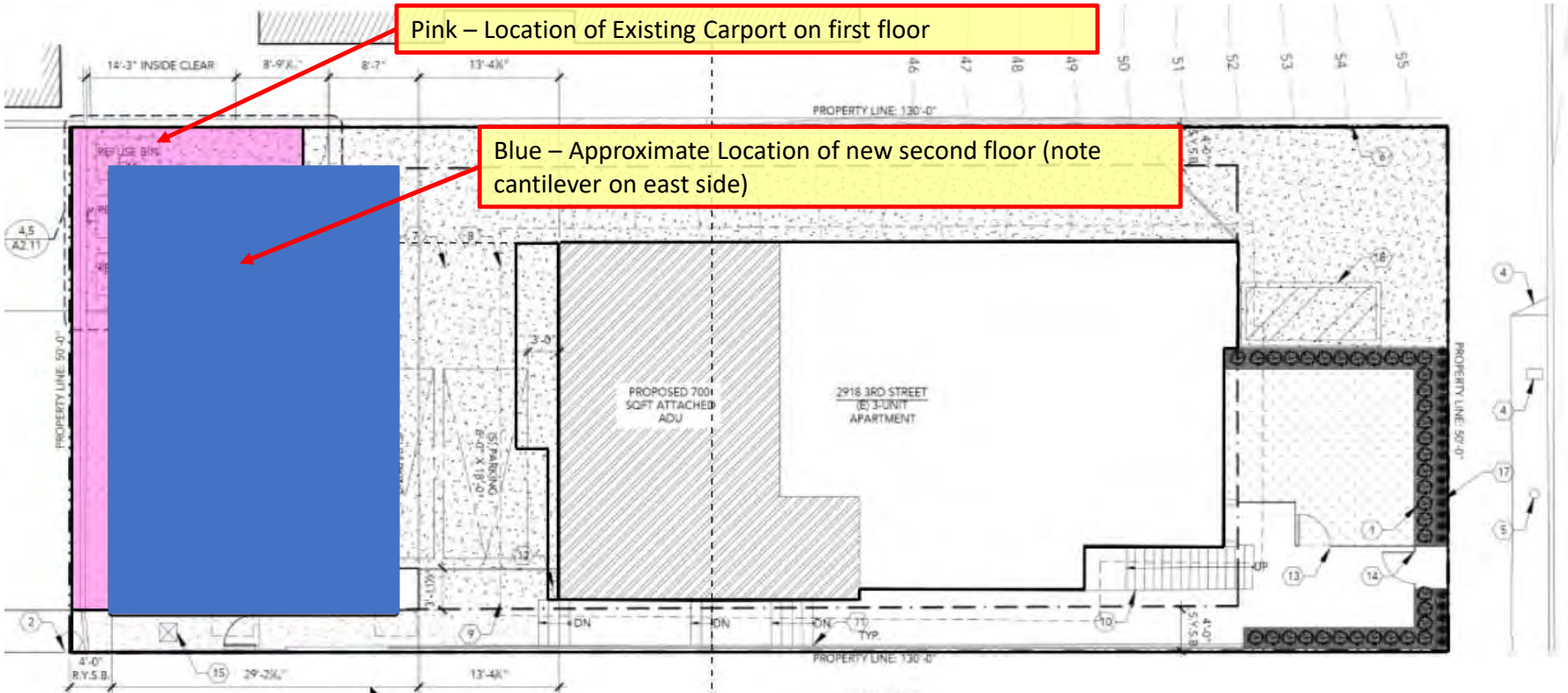
APRIL 20 2021

Grey – side yard set back – no new construction allowed



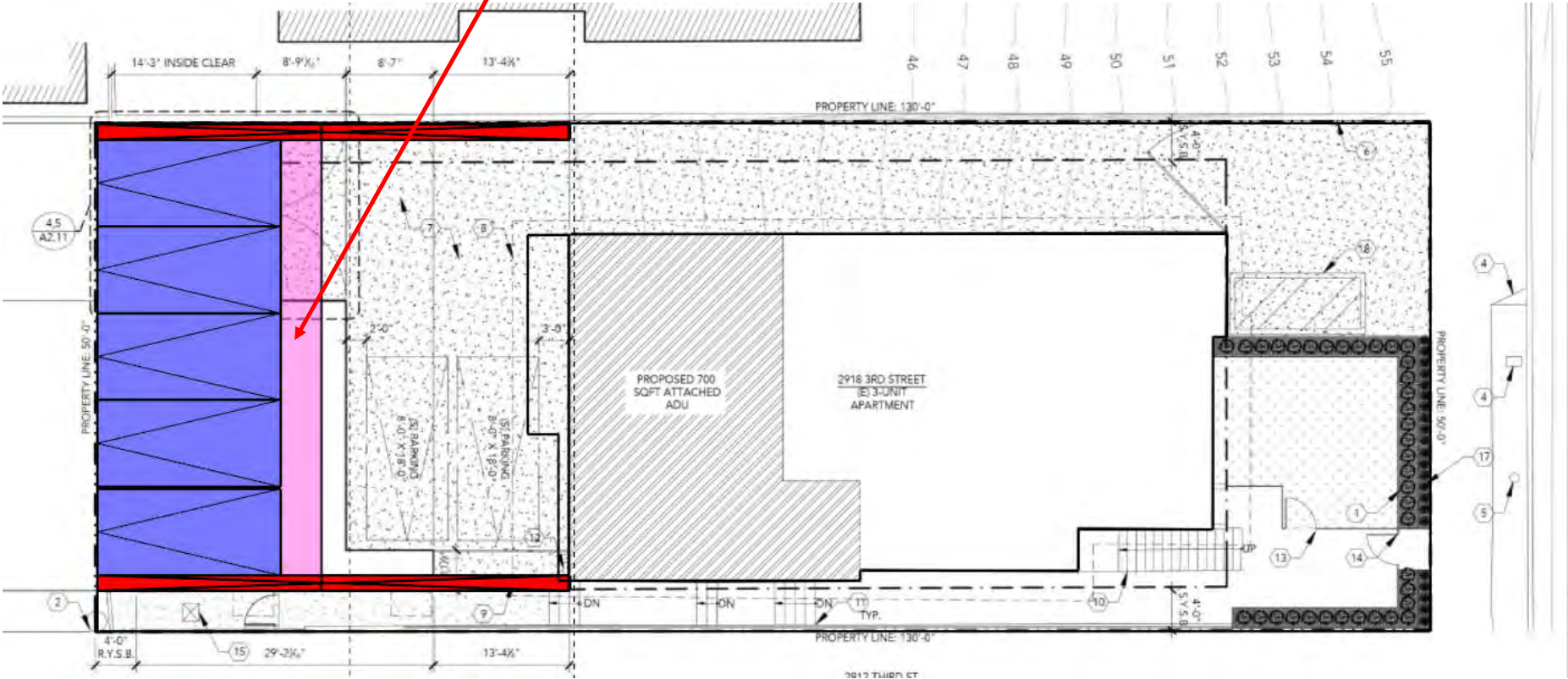
The problem with this layout is that we need to build stairs to access the upper floor. We cannot build these stairs in the side yard setback (grey area above) – we have confirmed this with the city. The only available place to build the stairs would be *behind* the car parking, which would block the backup path for at least one car. If the backup path is blocked, the City would not acknowledge it as a legal space, which would reduce our legal parking count back down to 4 spaces – which is what we already have.



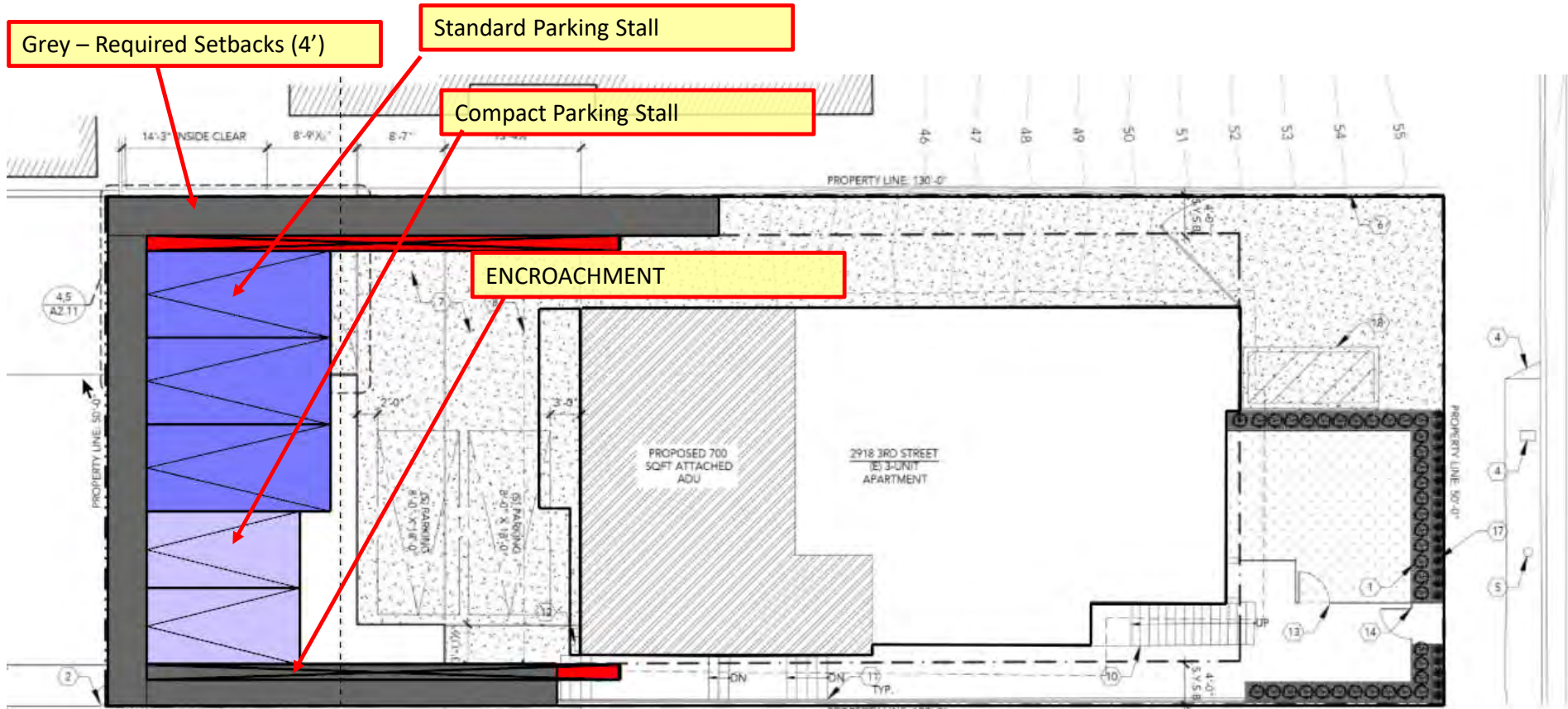


Although the location of the stairs makes this second floor only location non-viable, it is worth mentioning certain secondary issues. The second story would need to cantilever more than 8’ off of the eastern end of the carport in order to create a livable space (550 SF per unit) on the second floor. This adds considerable structural cost. Additionally, with the first floor being parking only we would be designing a “soft-story condition” which is a major seismic risk. To mitigate this, we would need to incorporate additional structural members to add shear strength – grade beams, moment frames, etc. We have discussed this with our contractor, who specializes in earthquake retrofits. He believes these factors could add \$50,000 or more to the budget of the building. Combined with the reduced livable square footage (and lower associated market rent), even after adding back in the value of creating an additional parking space this would materially impact the financial viability of the development.

Pink – Location of Existing Carport



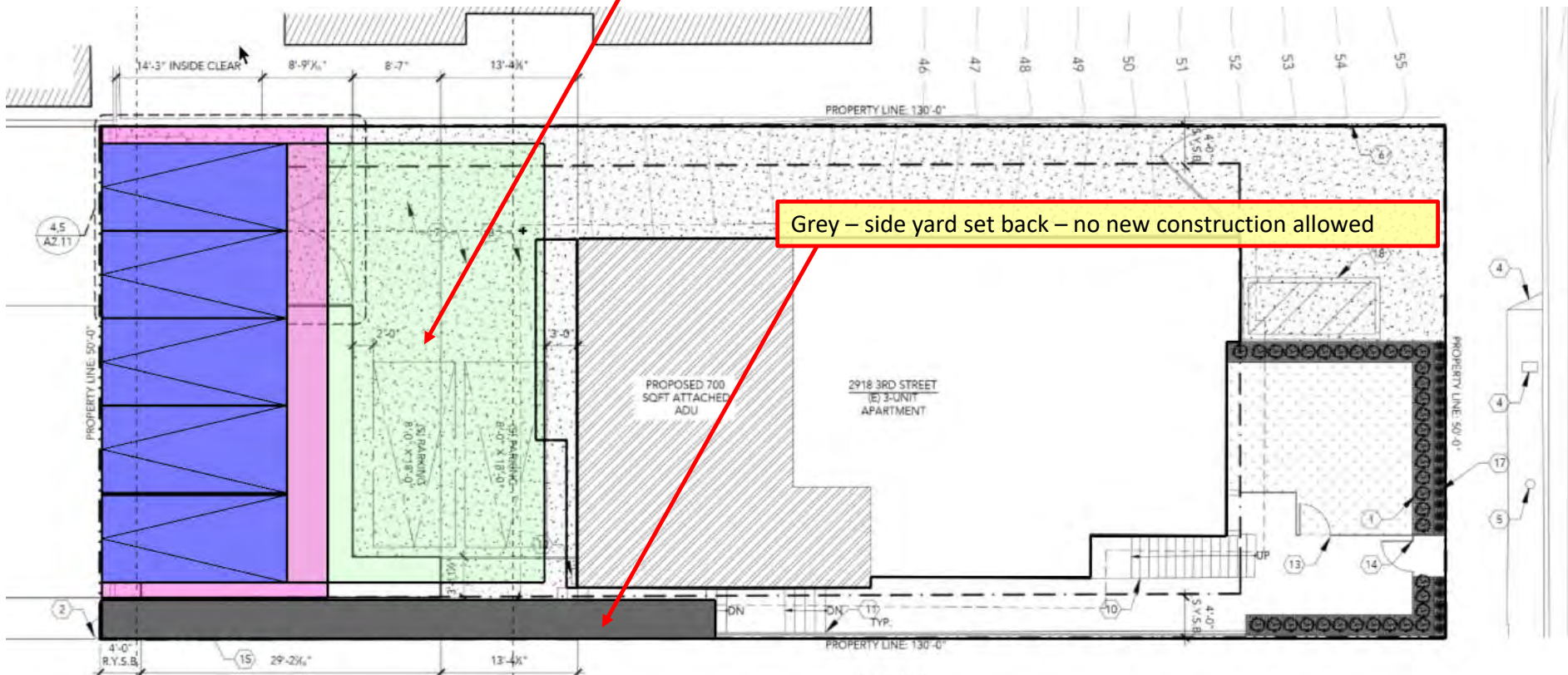
As you can see, the existing carport currently accommodate five standard parking stalls



This diagram illustrates the first floor parking layout in the event we demolished the existing parking structure. Based on city required ratios we must have no more than 40% compact, meaning 3 standard and 2 compact. Any new structure must be setback 4' from north, south and west property lines. 18" spacing is required between parking spaces and walls. As you can see, the required spacing encroaches on the required setback. This is not viable.

APRIL 20 2021

Grey – side yard set back – no new construction allowed



The problem with this layout is that we need to build stairs to access the upper floor. We cannot build these stairs in the side yard setback (grey area above) – we have confirmed this with the city. The only available place to build the stairs would be *behind* the car parking, which would block the backup path for at least one car. If the backup path is blocked, the City would not acknowledge it as a legal space, which would reduce our legal parking count back down to 4 spaces – which is what we already have.

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Commission

2918 3RD STREET PARKING ANALYSIS – SCENARIO 2 – BUILD ON TOP OF EXISTING CARPORT

APRIL 20 2021

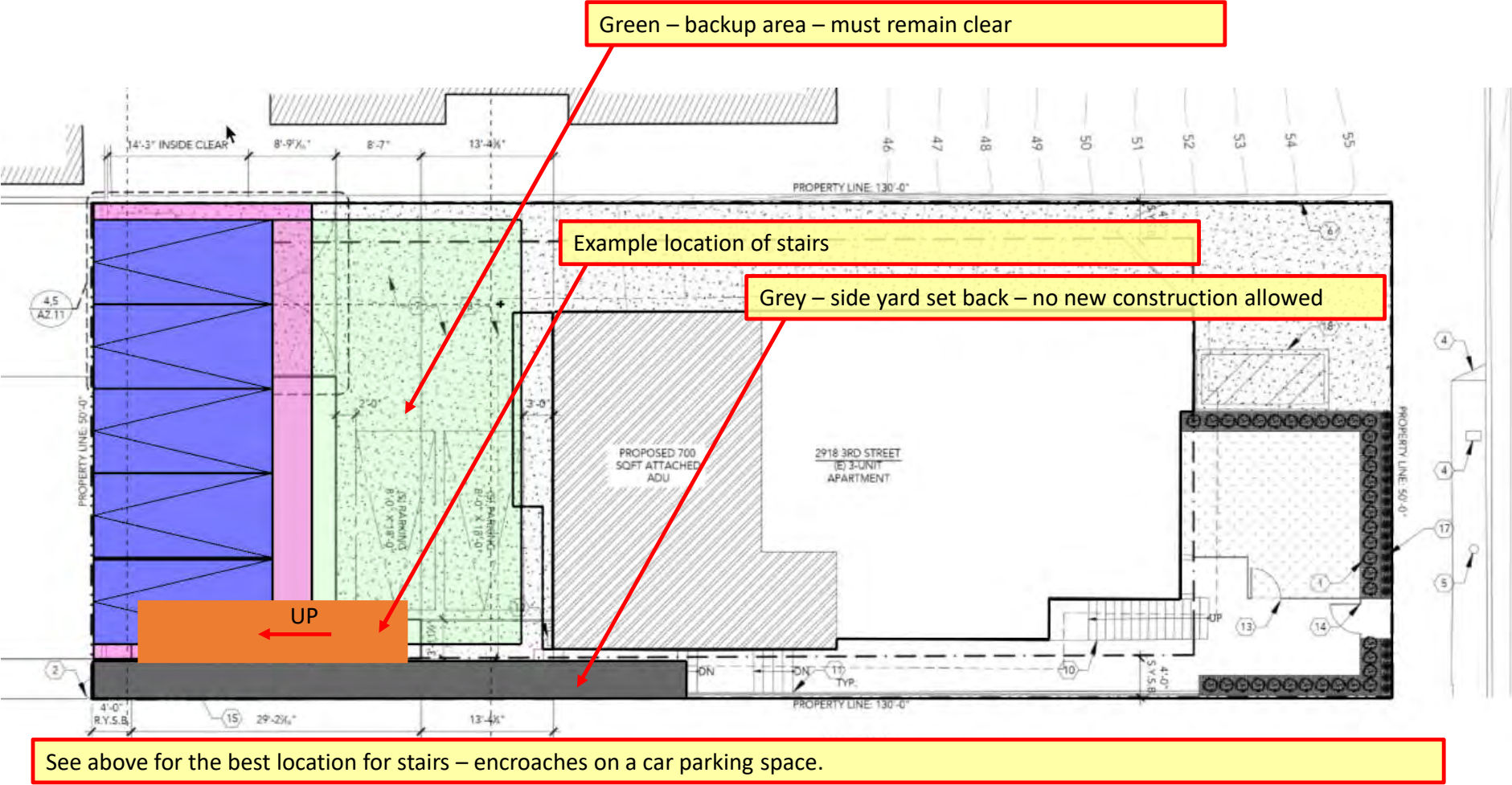
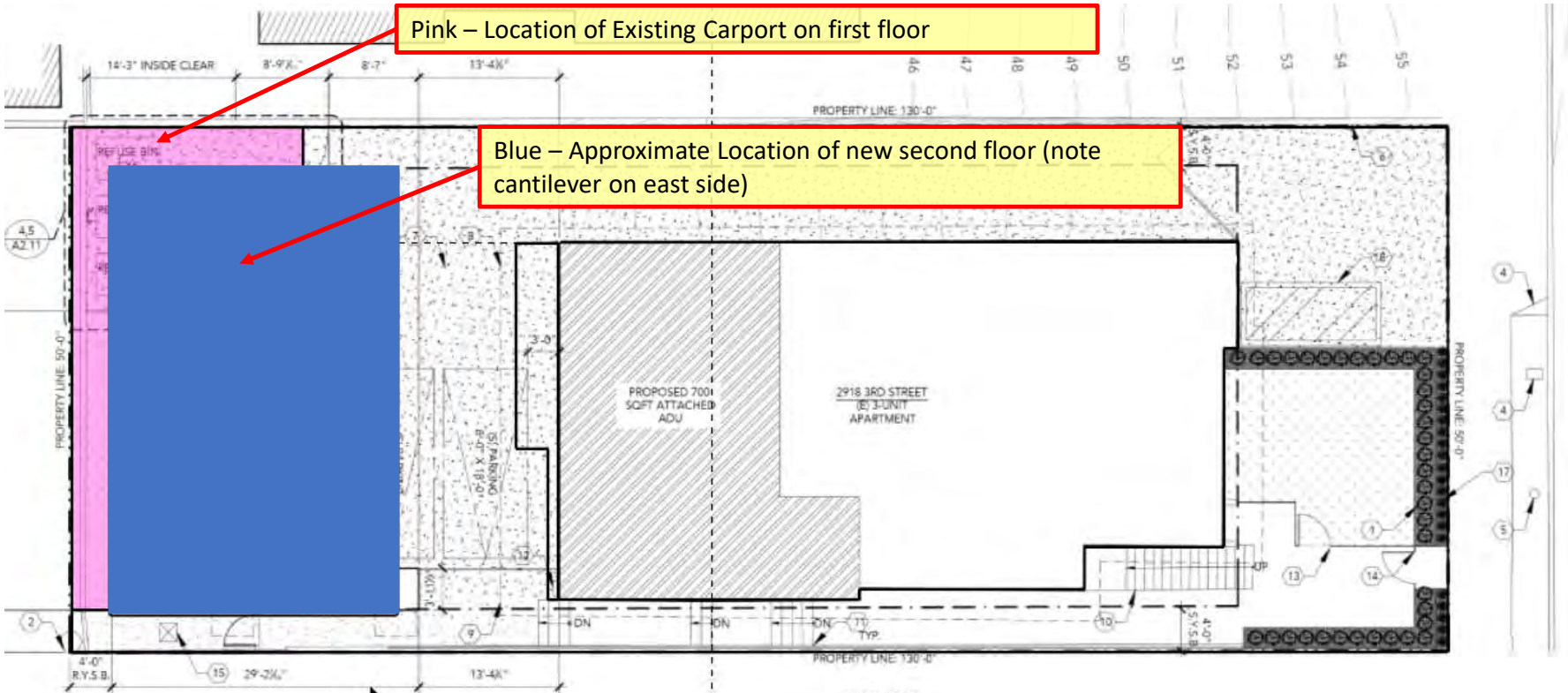


Exhibit 5

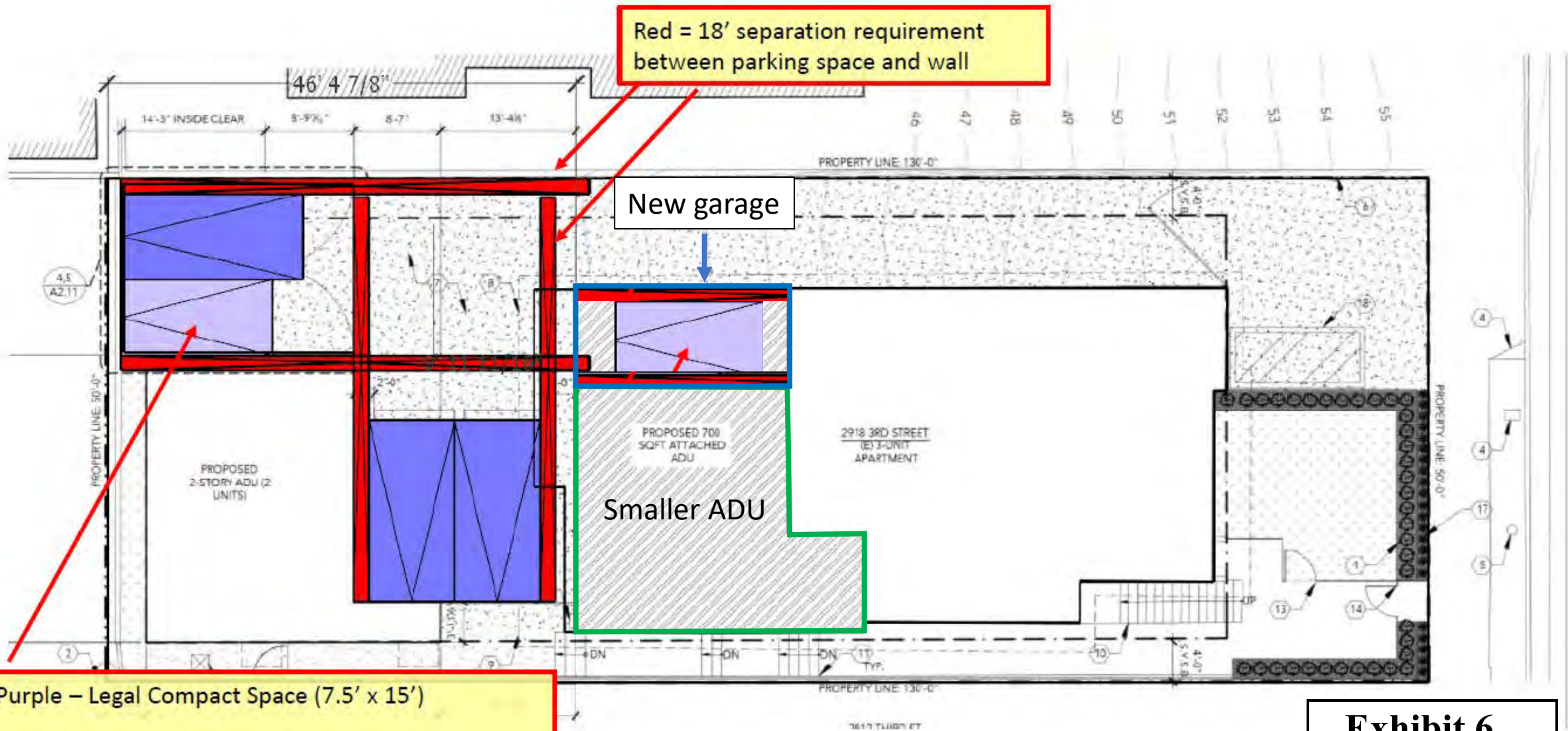
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Although the location of the stairs makes this second floor only location non-viable, it is worth mentioning certain secondary issues. The second story would need to cantilever more than 8’ off of the eastern end of the carport in order to create a livable space (550 SF per unit) on the second floor. This adds considerable structural cost. Additionally, with the first floor being parking only we would be designing a “soft-story condition” which is a major seismic risk. To mitigate this, we would need to incorporate additional structural members to add shear strength – grade beams, moment frames, etc. We have discussed this with our contractor, who specializes in earthquake retrofits. He believes these factors could add \$50,000 or more to the budget of the building. Combined with the reduced livable square footage (and lower associated market rent), even after adding back in the value of creating an additional parking space this would materially impact the financial viability of the development.



Red = 18' separation requirement
between parking space and wall

New garage

PROPOSED 700
SQFT ATTACHED
ADU

Smaller ADU

PROPOSED
2-STORY ADU (2
UNITS)

Light Purple – Legal Compact Space (7.5' x 15')

Compact must not exceed 40% of total spaces

Proposed plan: 75% Standard/25% Compact

CALIFORNIA COASTAL COMMISSION
SOUTH COAST REGIONAL COMMISSION
666 E. OCEAN BOULEVARD, SUITE 3107
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LONG BEACH, CALIFORNIA 90801
(213) 590-5071 (714) 846-0648

COASTAL DEVELOPMENT PERMIT

Application Number: P-3-20-78-2978

Name of Applicant: Livio De Rossi

3115 - 6th Street, Santa Monica, CA 90405

Permit Type: ☐ Emergency
☒ Standard
☐ Administrative

Development Location: 2918 Third Street, Santa Monica, CA

Development Description: Demolition of existing single-family dwelling
and construct a three-unit apartment building, 24 feet above centerline
of frontage road , six on-site parking spaces with conditions.

I. The proposed development is subject to the following conditions imposed pursuant to the California Coastal Act of 1976:

See attached Page 3 for conditions.

Condition/s Met On 7.18.78 By _____

Exhibit 7

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II. The South Coast Commission finds that:

A. The proposed development, or as conditioned;

1. The developments are in conformity with the provisions of Chapter 3 of the California Coastal Act of 1976 and will not prejudice the ability of the local government to prepare a local coastal program that is in conformity with the provisions of Chapter 3 of the California Coastal Act of 1976.
2. If located between the nearest public road and the sea or shoreline of any body of water located within the coastal zone, the development is in conformity with the public access and public recreation policies of Chapter 3 of the California Coastal Act of 1976.
3. There are no feasible alternatives, or feasible mitigation measures, as provided in the California Environmental Quality Act, available for imposition by this Commission under the power granted to it which would substantially lessen any significant adverse impact that the development, as finally proposed may have on the environment.

III. Whereas, at a public hearing, held on June 5, 1978 at Torrance by a 7 to 0 vote permit application number P-3-20-78-2978 is approved.

IV. This permit may not be assigned to another person except as provided in Section 13170, Coastal Commission Rules and Regulations.

V. This permit shall not become effective until a COPY of this permit has been returned to the Regional Commission, upon which copy all permittees or agent(s) authorized in the permit application have acknowledged that they have received a copy of the permit and have accepted its contents.

VI. Work authorized by this permit must commence within two years from the date of the Regional Commission vote upon the application. Any extension of time of said commencement date must be applied for prior to expiration of the permit.

VII. Issued on behalf of the South Coast Regional Commission on

July 18, , 197 8.


M. J. Carpenter
Executive Director

I, _____, permittee/agent, hereby acknowledge receipt of Permit Number P-3-20-78-2978 and have accepted its contents.

JULY 18-1978

(date)

(signature)

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Conditions for P-78-2978

Prior to issuance of permit, applicant shall submit:

1. a deed restriction for recording limiting the use of the structure to three rental units only, and
2. evidence that public notice has been placed in a newspaper of general circulation, such as the Evening Outlook for a period of ten (10) working days; such notice will describe the structure and announce the availability of a structure for removal and relocation; the availability period will commence the day of the first notice and last 60-consecutive calendar days thereafter and shall be stated as such in the public notice, if after the 60th day there are no requests for relocation the permit will be issued (providing all other conditions are satisfied) allowing demolition and construction of the approved project; if relocation occurs prior to expiration of the 60-day period, evidence shall be supplied to verify that fact and the permit can be issued (providing other conditions are met).

* * *

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