

CALIFORNIA COASTAL COMMISSION

SOUTH COAST DISTRICT OFFICE
301 E. OCEAN BLVD, SUITE 300
LONG BEACH, CA 90802-4325
PH (562) 590-5071



F11a

A-5-LGB-22-0060 (City of Laguna Beach)

June 14, 2024

EXHIBITS

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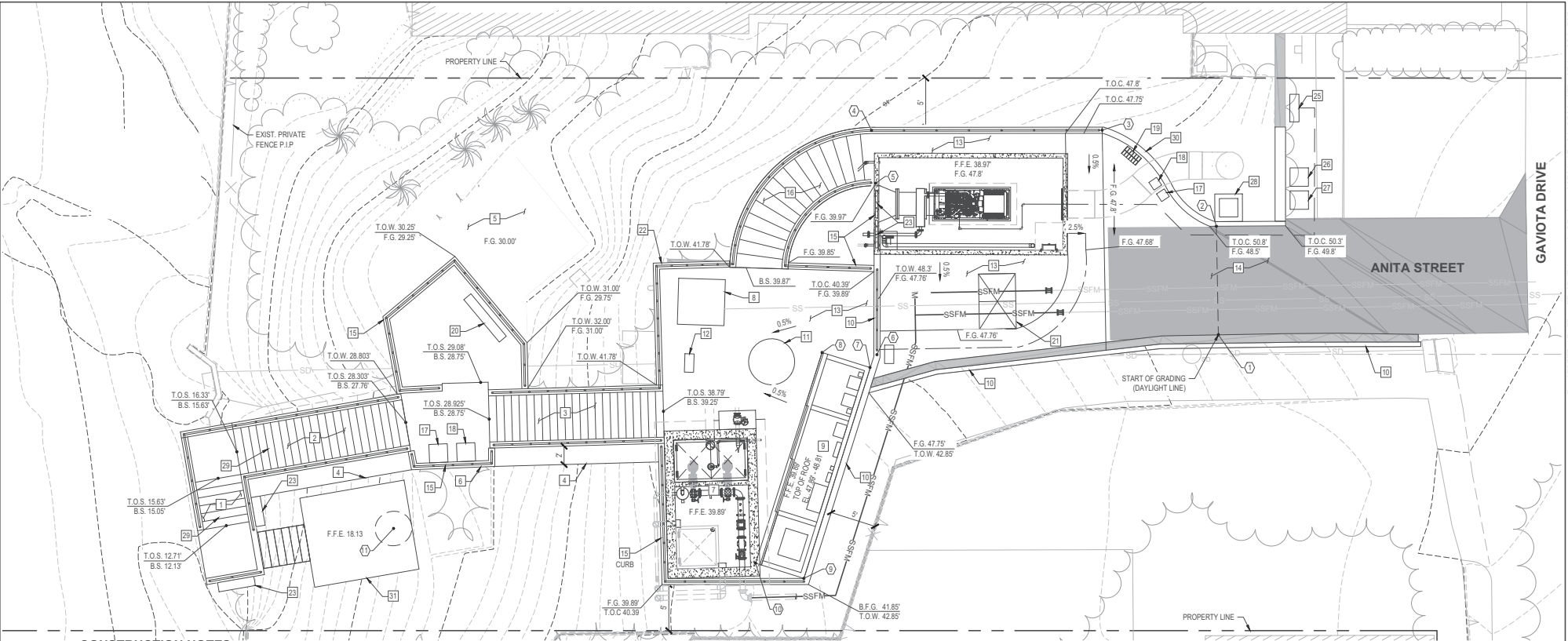
EXHIBIT 4 — Existing Site Photos

EXHIBIT 1 — Vicinity Map





EXHIBIT 2 — Proposed Project Plans and Renderings



- CONSTRUCTION NOTES:**
1. INSTALL NEW 7"(H) X 5"(W) X 10"(D) REINFORCED CONCRETE STAIRS PER SHEET SF#.
 2. MODIFY EXIST. 7"(H) X 4-6"(W) X 10"(D) CONCRETE STAIRS TO BE 7"(H) X 5"(W) X 10"(D) PER SHEET SF#.
 3. MODIFY EXIST. 7"(H) X 4-6"(W) X 10"(D) CONCRETE STAIRS TO BE 7"(H) X 5"(W) X 10"(D) PER SHEET SF#.
 4. INSTALL 2"(W) RIBBON GUTTER PER DETAIL.
 5. INSTALL LIFT STATION CONCRETE FLOOR REPAIRS PER SHEET SF#.
 6. INSTALL REINFORCED CONCRETE RETAINING WALL PER DETAIL.
 7. ANITA STREET SEWER LIFT STATION WET WELL AND VALVE VAULT. SEE SHEET MM#.
 8. EXIST. SS MANHOLE TO BE REFURBISHED PER DETAIL.
 9. INSTALL ABOVE GROUND ELECTRICAL SHED PER SHEETS #A-#B-#.
 10. INSTALL REINFORCED CONCRETE RETAINING WALL PER SHEETS SF#-SF#.
 11. INSTALL S/D HYDRODYNAMIC SEPARATOR (CONTECH CDS OR EQUAL).
 12. INSTALL SHALLOW TYPE MANHOLE WITH 2X1" GRATED COVER PER DETAIL.
 13. INSTALL 6" (T) CONCRETE PAVEMENT PER DETAIL.
 14. INSTALL ASPHALT PAVEMENT PER DETAIL.
 15. INSTALL 8"(W) X 6" (H) CONCRETE CURB WITH HANDRAIL PER DETAIL.
 16. INSTALL 6"(H) X 5"(W) X 10"(D) MIN REINFORCED CONCRETE STAIRS PER SHEET SF#.
 17. INSTALL TRASH CONTAINER WITH PITCH IN LID (MODEL #TF-1025) AND SNUFFER ATTACHMENT (MODEL #TF-2090) AS MANUFACTURED BY WASAU OR APPROVED EQUAL. MATERIALS SHALL BE TAN EXPOSED AGGREGATE WITH BROWN CONTAINER LID.
 18. INSTALL TRASH CONTAINER WITH PITCH IN LID (MODEL #TF-1025) AS MANUFACTURED BY WASAU OR APPROVED EQUAL. MATERIALS SHALL BE TAN EXPOSED AGGREGATE WITH BLUE CONTAINER LID. CONTAINER SHALL HAVE BLUE AND WHITE CAST EMBLEM FOR RECYCLE (#TF-1235) ABOVE BLUE AND WHITE CHASIN ARROWS (#TF-1235) ON 2 OPPOSITE SIDES OF EACH CAN.
 19. INSTALL 5 LOOP BIKE RACK (LANDSCAPEFORMS 35 COLLECTION OR APPROVED EQUAL).
 20. INSTALL 96" LONG PRECAST CONCRETE BENCH WITH DOUGLAS FIR WOOD SLATS (QUICK CRETE Q-LAG OR EQUAL).
 21. INSTALL 4'X8" BYPASS SSFM VALVE VAULT PER DETAIL.
 22. INSTALL 2" TALL REINFORCED CONCRETE WALL PER DETAIL.
 23. INSTALL BEACH ACCESS SAFETY SIGNS PER DETAIL.
 24. INSTALL 72" LONG PRECAST CONCRETE BENCH WITH DOUGLAS FIR WOOD SLATS (QUICK CRETE Q-LAG OR EQUAL).
 25. INSTALL RELOCATED NATURAL GAS METER PER SOCAL GAS REQUIREMENTS.
 26. INSTALL MTS AND EMERGENCY GENERATOR SHUTOFF ENCLOSURE (WITH GENERATOR QUICK CONNECT). SEE ELECTRICAL PLANS.
 27. INSTALL RELOCATED SCE SERVICE ENTRANCE ENCLOSURE PER SCE REQUIREMENTS.
 28. EXIST. SCE 30KVA PAD MOUNTED TRANSFORMER P.I.P.
 29. INSTALL ANITA STREET LOGO PER DETAIL.
 30. INSTALL 6"(H) CURB PER DETAIL.
 31. INSTALL SINGLE OCCUPANT STYLE CAISSON SUPPORTED LIFE GUARD TOWER. SEE STRUCTURAL PLANS FOR FOUNDATION DETAILS.

SITE PLAN
SCALE: 1" = 5'-0"

LEGENDS:

- WM VALVE METER
- VC VALVE CAN
- 5' CONTOUR LINE
- 1' CONTOUR LINE
- SD STORM DRAIN MANHOLE
- SSM SANITARY SEWER MANHOLE
- SSFM SEWAGE FORCE MAIN
- SD STORM DRAIN
- SS GRAVITY SEWER LINE
- FP FLOW PATH

POINT TABLE

PNT. #	NORTHING	EASTING	DESCRIPTION
1	2141605.70	6096006.47	SE ANITA ST DAYLIGHT LINE
2	2141615.89	6096000.20	NW ANITA ST DAYLIGHT LINE
3	2141618.24	6095984.18	TOP OF WALL
4	2141605.27	6095962.74	TOP OF RETAINING WALL NW CORNER OF STAIRS
5	2141600.66	6095966.00	TOP OF RETAINING WALL SE CORNER OF STAIRS
6	2141584.75	6095975.78	TOP OF RETAINING WALL
7	2141583.14	6095975.94	NE CORNER OF ELECTRICAL BUILDING
8	2141581.85	6095970.59	NW CORNER OF ELECTRICAL BUILDING
9	2141559.81	6095981.58	SE CORNER OF ELECTRICAL BUILDING
10	2141558.52	6095976.23	SW CORNER OF ELECTRICAL BUILDING
11	2141541.45	6095940.60	CENTER OF LIFE GUARD TOWER

NO.	DATE	REVISIONS	ENGR.	APPROV.	DATE	PLAN PREPARED BY:
7						
6						
5						
4						
3						
2						
1						

PACE
Advanced Water Engineering
17520 Newhope Street, Suite 200 | Fountain Valley, CA 92708
P: (714) 461-7200 | www.pacewater.com

ROBERT RUSSELL MURPHY R.C.E. No. C83207 EXP. DATE 03/31/21

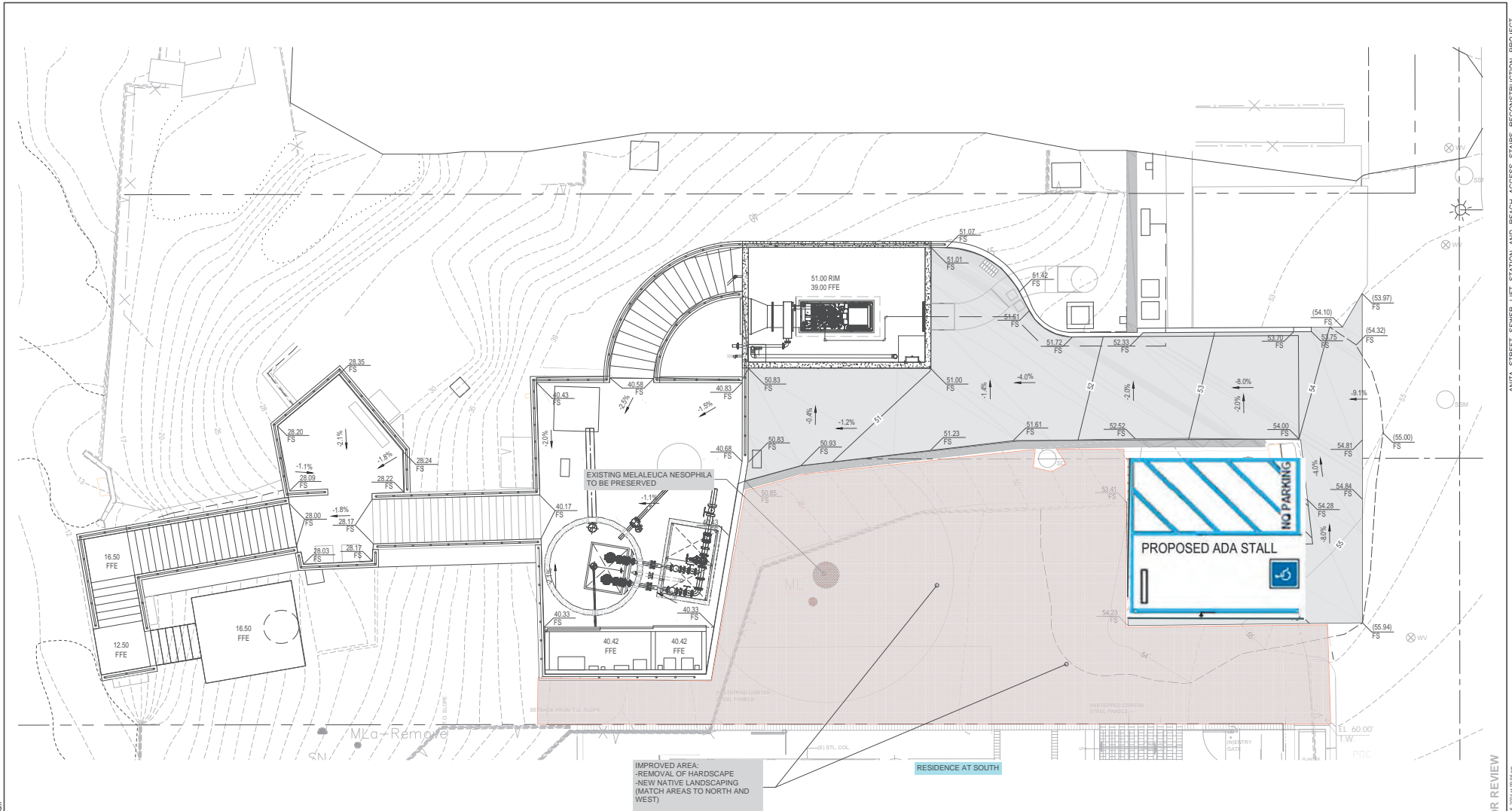
DRAWN BY: MARK S.	DATE: JUNE 2022	CITY OF LAGUNA BEACH WATER QUALITY DEPARTMENT
DESIGNED BY: ROBERT M./THOMAS M.	DATE: JUNE 2022	RECOMMENDED BY: SENIOR PROJECT MANAGER HANNAH JOHNSON
CHECKED BY: ROBERT M.	DATE: JUNE 2022	APPROVED BY: DIRECTOR OF WATER QUALITY DAVID SHISSLER

ANITA STREET SEWER LIFT STATION AND BEACH ACCESS STAIRS RECONSTRUCTION PROJECT
SITE PLAN
CITY OF LAGUNA BEACH WATER QUALITY DEPARTMENT

PROJECT NO. CIP NO. 18-106
SHEET
C1.0
--- of ##

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Know what's below.
Call before you dig.

30% - ISSUED FOR REVIEW
W:\8888\Engineering\8888-C1\Anita\8888-C1-C1.dwg - Tab: C1.0 Bp: mshane on Aug. 11, 2022 at 10:08 am



<p>Know what's below. Call before you dig.</p>			<p>5 2.5 0 5 10</p> <p>SCALE: 1" = 5'</p>			<p>NO. DATE REVISIONS ENGR. APPROV. DATE</p>			<p>PLAN PREPARED BY:</p> <p>17520 Newhope Street, Suite 200 Fountain Valley, CA 92708 P: (714) 481-7300 www.pacewater.com</p> <p>ROBERT RUSSELL MURPHY R.C.E. No. CR007 EXP. DATE 03/31/21</p>			<p>DRAWN BY: MARK S. DATE: JUNE 2022</p> <p>DESIGNED BY: ROBERT M./THOMAS M. DATE: JUNE 2022</p> <p>CHECKED BY: ROBERT M. DATE: JUNE 2022</p>			<p>CITY OF LAGUNA BEACH WATER QUALITY DEPARTMENT</p> <p>RECOMMENDED BY: SENIOR PROJECT MANAGER JULIES ESCALONA</p> <p>APPROVED BY: ASSISTANT DIRECTOR OF PUBLIC WORKS TOM PEREZ</p>			<p>ANITA STREET SEWER LIFT STATION AND BEACH ACCESS STAIRS RECONSTRUCTION PROJECT</p> <p>SITE GRADING PLAN</p> <p>CITY OF LAGUNA BEACH WATER QUALITY DEPARTMENT</p>			<p>PROJECT NO. CIP NO. 18-106</p> <p>SHEET C3.0</p> <p>--- OF ---</p>		
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California Coastal Commission
A-5-LGB-22-0060

ANITA STREET SEWER LIFT STATION AND BEACH ACCESS STAIRS RECONSTRUCTION PROJECT
75% - ISSUED FOR REVIEW



Rendering of street-end upper landing. Note the level pavement (ADA Compliant) and retaining wall to the left.



Rendering of proposed beach access stairway.

EXHIBIT 3 — City Response to Substantial Issue Finding



October 26, 2023

Zach Rehm
District Supervisor
California Coastal Commission
301 E. Ocean Blvd, Suite 300
Long Beach, CA 90802

Subject: Coastal Commission Appeal A-5-LGB-22-0060, City of Laguna Beach Anita Street Beach Access

Dear Mr. Rehm,

The City is providing a response to the Coastal Commission's finding of substantial issue with Appeal No. A-5-LGB-22-0060 related to the Anita Street Beach Access project within the City of Laguna Beach. This letter provides responses to comments made at the April 12, 2023 hearing date. The City asks the Coastal Commission staff to consider the information provided and to schedule the project for a hearing at the earliest possible date.

Issue #1: Provide a description of how the project has been evaluated by an ADA compliance staff person or consultant.

Response: The City contracted with CASp Experts LLC to thoroughly analyze the project site and determine if there are any additional ADA improvements that can be done to maximize the accessibility of the site. The term "CASp" stands for Certified Access Specialist. A CASp is an expert in the field of construction-related accessibility. Certified Access Specialists are certified by the California Division of State Architect and qualified to inspect buildings and sites for compliance with applicable state and federal construction-related accessibility standards. Based on the findings of CASp Experts LLC, the project as submitted by the City meets the requirements of the ADA. Their findings will be discussed in detail below.

Issue #2: Review revocable encroachment permit (REP) area to maximize ADA accessibility.

Response: The design of the City's beach access and sewer lift station project is not negatively impacted by the approved use of the REP area (Design Review 22-0120). The City's proposed project provides improvements required to provide and maintain enhanced public access to the coast including maximizing ADA accessibility to the extent possible.

The proposed project provides three levels with large viewing areas. The top area is ADA accessible from Gaviota Drive. The REP area does not provide an opportunity to improve access to the lower viewing areas for various reasons discussed below.

The beach access area and the REP area are at different elevations separated by an existing wood retaining wall and a landscaped slope as shown in Exhibit 1. To eliminate the grade difference and maximize usable space, a large cast-in-place concrete or masonry block retaining wall would be required between Anita Street right of way and the property at 1007 Gaviota. In doing so, the site grading would result in the loss of at least four (4) existing mature trees. This type of development would be destructive to the slope stability and would conflict with the goal of preserving and maintaining the existing trees.



Exhibit 1: View of REP area and beach access from Gaviota Drive

In addition, the effort to combine the two areas would not improve ADA accessibility to the beach or provide enhanced viewing platforms because the REP area is directly behind the proposed electrical shed and lift station as shown in Exhibit 2 below and the location and elevation of these improvements are fixed by the existing sewer.



Exhibit 2: REP Area behind electrical building and lift station.



Exhibit 3: Proposed beach access improvements and adjacent REP area.

CASp Experts LLC examined the site in depth to determine if it could be used to provide additional space for ramping, installation of an elevator or funicular, or any other ADA improvement; however, given the location and elevation of the REP in comparison to the beach access there would be no benefit of incorporating the REP (Exhibit 3) into the proposed beach access design. The REP was also examined to provide an additional accessible viewing platform. This was deemed to be infeasible because the existing site has a mature melaleuca tree with low branches that would not allow provide the required 84" vertical clearance for an accessible path to be constructed out to the bluff edge. The tree is shown in Exhibit 4 below. The design review approved landscape plan will protect existing trees in place, remove the existing hardscape, and replace it with groundcovers and shrubs suited for the coastal environment.



Exhibit 4: Existing melaleuca tree within revocable encroachment permit area.

As further shown in Exhibit 3, the proposed REP area is predominantly a landscape buffer between the beach access and 1007 Gaviota. The area will not provide a significant benefit to the beach access design or improve ADA accessibility; however, the design review approved improvements will provide a significantly improved public experience at the Anita Street Beach Access. Therefore, it is recommended that the proposed design of the beach access and sewer lift station remain separate from the REP.

Issue #3: Review potential to add an ADA parking space at REP area or north of street end.

Response: CASp Experts LLC has reviewed the REP and determined that it is feasible to construct an ADA compliant parking space with a van loading zone within the REP as shown in Exhibit 5. A detailed analysis is included as Attachment 1. The ADA parking space would replace the design review approved public amenities such as the loading zone, bench seating, water station, bike racks, and surfboard racks. The approved design (Design Review 22-0120) shown in Exhibit 6 was reviewed and approved by the City of Laguna Beach Design Review Board (DRB) on April 28, 2022. Any significant change to the plan would require an additional review and approval from the DRB.

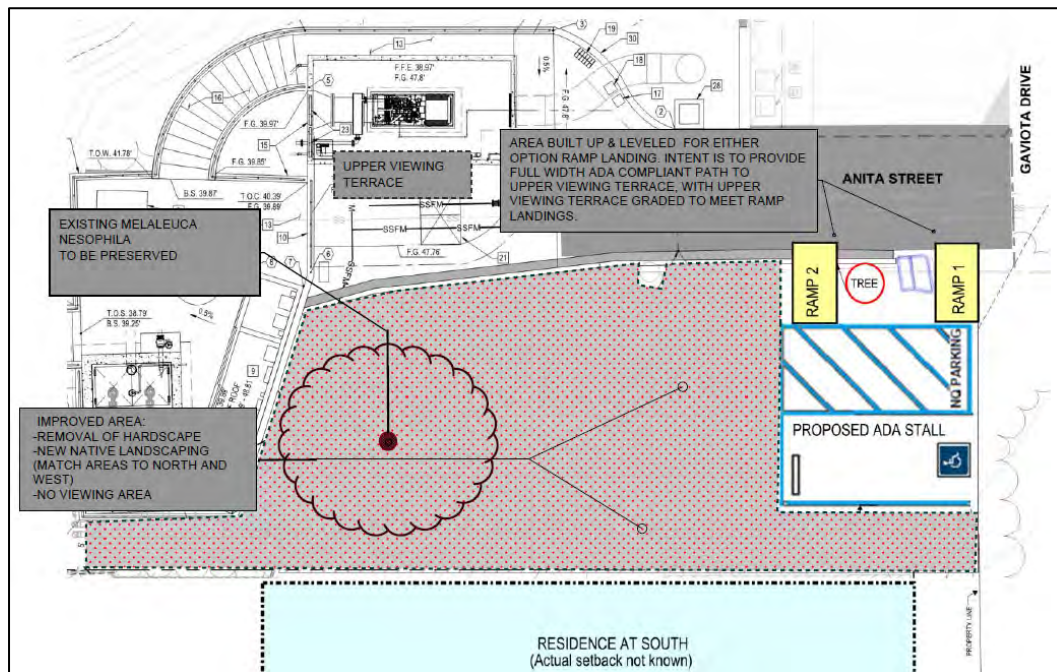


Exhibit 5: Conceptual layout of ADA accessible parking within the Revocable Encroachment Permit area.

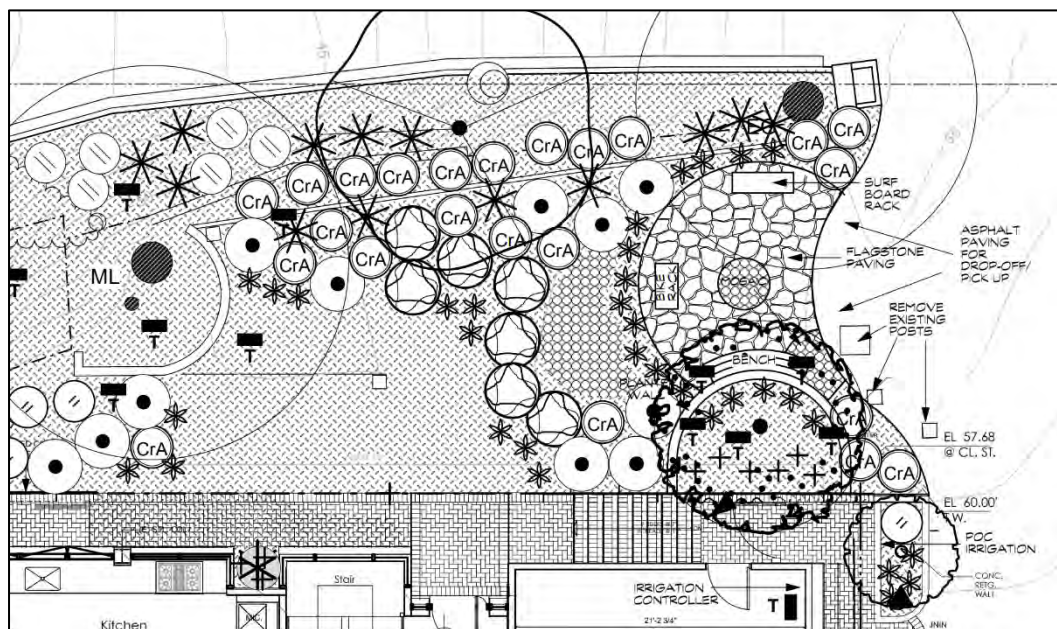


Exhibit 6: Design Review 22-0120 approved improvements for the Revocable Encroachment Area.

The City was provided a Disabled Parking Space Evaluation for the proposed 1007 Gaviota Drive Remodel Project prepared by Linscott, Law & Greenspan, Engineers (Attachment 2), on behalf of the owner of 1007 Gaviota Drive which concluded that the proposed drop-off would be superior to an ADA accessible parking space for the following reasons:

- Based on the roadway grades along Gaviota Drive and Anita Street in the project site and the absence of ADA access to the beach (stairs only), there is no logical or safe reason for a disabled person(s) to park along Gaviota Drive in this area.
- The proposed passenger loading turnout area improvement does not preclude a disabled person(s) from using the loading area and it is much more likely that should a disabled person wish to access this area, they would be accompanied by a non-disabled person driving a vehicle that could access the passenger loading area and then park in the area like every other visitor to the area.

The proposed drop-off area has been identified as an improvement to the overall area because the off-street drop-off area will greatly reduce conflicts between vehicles, pedestrians, and other modes of transportation along Gaviota Drive and Anita Street. The condition of approval for the REP requires the property owner of 1007 Gaviota Drive to construct all of the REP improvements prior to receiving a certificate of occupancy for their adjacent residence, and also requires the property owner to maintain the improvements in perpetuity unless the REP is revoked by the City.

Issue #4: Analyze feasibility to provide a ramp, elevator, or funicular to provide ADA access to beach.

Response: The design of the beach access and sewer lift station project is not impacted by the REP area. The City's design provides ADA improvements to the extent feasible as required to enhance public access to the coast. CASp Experts provided a detailed analysis to provide a ramp from Gaviota to beach level (Attachment 3) and an additional analysis to determine if the City could feasibly construct an elevator or funicular to beach level (Attachment 4). Based on the analysis, the City has determined that the proposed design meets the requirements of the ADA by providing accessible features to the extent feasible at the site. Further discussion is provided below.

Pedestrian Ramp

An ADA-compliant ramp must include a level landing every 30' of run and at each change in direction. In addition, the ramp cannot exceed a slope of 8.33%. CASp Experts LLC looked at eliminating stairs between the top landing and middle landing and between the middle landing and the beach. The areas are depicted in Exhibit 7.

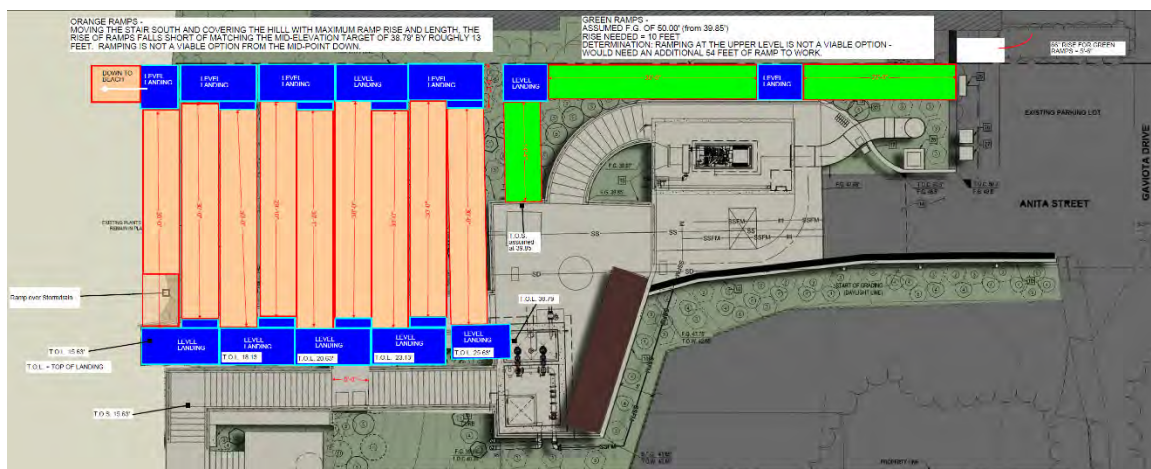
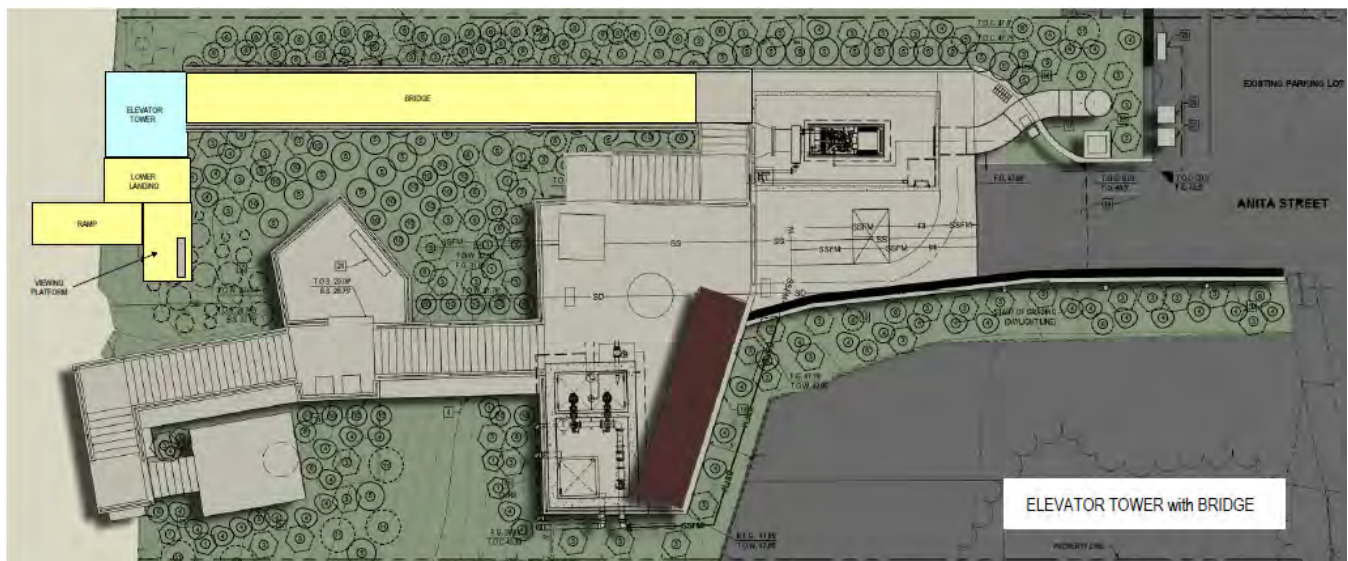


Exhibit 7: Stair analysis.

Top Landing to Middle Landing – The grade difference between the top landing and the middle landing is 105 inches. This requires a ramp of 105' with at least four (4) level landings. The space will only accommodate a ramp length of approximately 66'.

Based on the discussion above, it is not feasible to eliminate stairs to construct an ADA compliant ramp from the top landing to the middle landing or to beach level because there is not enough space available within the right-of-way and because the existing and proposed sewer facilities create physical barriers and design constraints.

The construction of an elevator at the site would require a bridge from the top landing to the elevator location near beach level. There would need to be a level landing at the elevator exit and then a ramp to beach level as depicted in Exhibit 8. Maintenance and operation of an elevator in this location would be very expensive because elevators have several moving parts and mechanical components that would be affected by the salt air and the sand making continuous service of the elevator unreliable.



In addition to the disproportionate cost to construct and maintain an elevator at the site, the construction of an elevator at the site would create a major visual impact and would require significant disruption to the coastal bluff. Exhibit 9 provides an example of how the elevator and bridge would need to be constructed at the site.

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structural improvement. The rocky landing would not provide a safe or ADA compliant destination for a person with disabilities.



Exhibit 9: Sample outdoor elevator at a slope.



Exhibit 10: Narrow and rocky beach.



Exhibit 11: Rocky beach demonstrating unsafe condition for people with disabilities.

California Building Code 11B-202.4 states that when the adjusted construction cost, exceeds the current valuation threshold and the enforcing agency determines the cost of compliance with Section 11B-202.4 is an unreasonable hardship, full compliance with Section 11B-202.4 shall not be required. Furthermore, pursuant to ADA CFR § 36.403(f), alterations made to provide an accessible path of travel to the altered area will be deemed disproportionate to the overall alteration when the cost exceeds 20% of the cost of the alteration to the primary function area. Based on this, CASp Experts LLC has concluded that the

construction of an elevator at the site is not required due to excessive construction costs of said conveyances. In addition, due to the physical limitations of the site and the impacts to surrounding areas, staff has determined that an elevator is not feasible in this location.



Exhibit 12: Anita Street Beach with sand and rock outcropping.

Funicular

The construction of a funicular at the site is not feasible for several of the same reasons as an elevator. In addition, the construction of a funicular would be very disruptive to the site and would likely require an operator to be on site to ensure safe operation. A conceptual layout for a funicular is shown in Exhibit 13. It was mentioned that a similar type of funicular was constructed in Dana Point. It should be noted that the site is much larger than the Anita Street Beach site (Exhibit 14) and in addition to large retaining walls, the funicular requires towers (Exhibit 15) at both the top and bottom of the slope. These types of structures will not fit within the tight constraints of the project site.

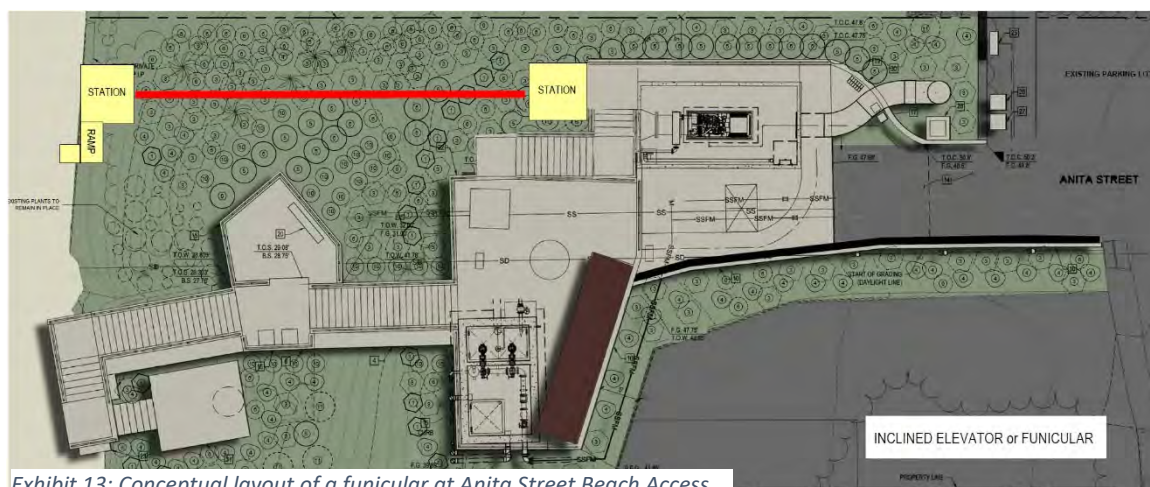


Exhibit 13: Conceptual layout of a funicular at Anita Street Beach Access

Therefore, the construction of a funicular at the Anita Street Beach Access is neither required to meet ADA or feasible.

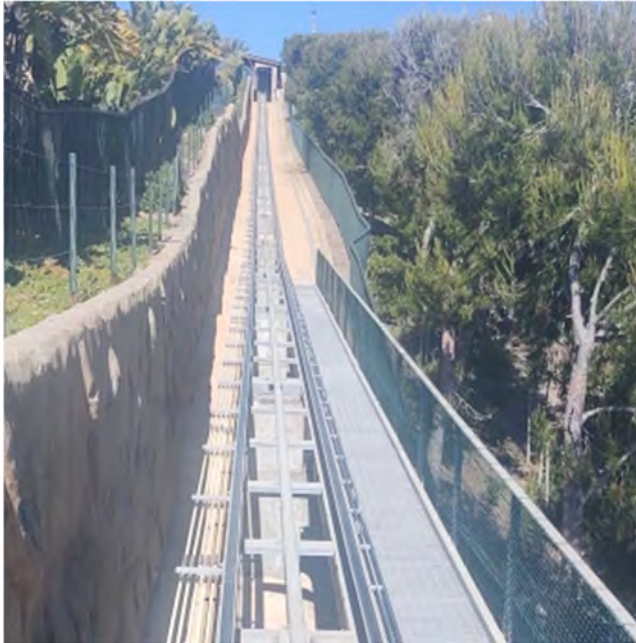


Exhibit 14: Strands Beach funicular (Dana Point)



Exhibit 15: Strands Beach funicular tower (Dana Point)

Issue #5: Coastal Commissioners felt the site would not easily be identified by the public to be a public beach access.

Response: The Anita Street Beach Access has been in place over 70 years. It is known to locals and visitors alike to be a public beach access. In order to ensure future residents and visitors are aware that the beach access provides public beach access, the City proposes to install the typical beach access signage found at all City maintained beach accesses (Exhibit 16). The signs will be placed at the top landing (Exhibit 17) and will also provide information to promote the continued health and well being of



Exhibit 16: Existing Anita Street Beach Access signage.



Exhibit 17: Rendering of proposed top landing with signage and other public amenities.

the coastal wildlife and marine environment. The proposed design does not include any type of gates, fences, or other physical barriers that would cause visitor to feel as though the beach access was for private use only.

Issue #6: Coastal Commissioners questioned the installation of the permanent lifeguard tower.

Response: The proposed permanent lifeguard tower is designed to pull the tower completely off of the sandy beach. This maximizes the amount of unobstructed beach available to the public. In addition, the placement of a tower on a caisson allows the City to provide year-round lifeguard at this very popular beach. Due to the rockiness of the beach when sand levels are low, the tower cannot be safely founded on the rocks. Recently the City lost a tower in a similar situation (Exhibit 19). City staff has worked with neighbors immediately adjacent to the proposed tower location, and they have been receptive to placing the permanent tower in the proposed location. Staff finds that the proposed permanent tower is a betterment for public safety and for the elimination of barriers on the sandy beach.



Exhibit 18: Existing lifeguard tower on the sand.



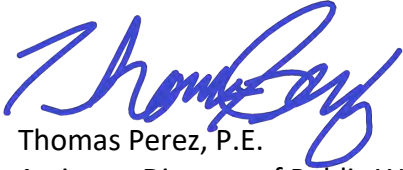
Exhibit 19: Damaged tower at Oak Street.

Based on the discussion above, the City feels that the project as approved by the City of Laguna Beach Planning Commission, meets the requirements of the ADA and the requirements of a Coastal Development Permit (CDP). The proposed REP is not a “giveaway” of public land because the area will be improved to provide public amenities consistent with and complementary to the proposed beach access. The approval of the improvements within the REP area would result in a significant improvement to public amenities over what exists today. As such, the City has determined that the proposed improvements in the approved REP do not negatively affect or limit the design of the beach access improvements, but rather that they are complementary to the City’s proposed beach access improvements and increase the amount of public amenities available in the area.

The City of Laguna Beach respectfully requests that the Coastal Commission staff support the project as presented and to place it on the earliest Coastal Commission agenda for consideration.

If you have any questions, please do not hesitate to contact me at 949-464-6688, or at tperez@lagunabeachcity.net.

Sincerely,



Thomas Perez, P.E.

Assistant Director of Public Works

Attachments:

1. ADA Parking Analysis
2. Disabled Parking Space Evaluation
3. ADA Ramp Analysis
4. Elevator or Funicular Analysis

EXHIBIT 4 — Existing Site Photos



Rocky shoreline at bottom of existing stairway.



Image of steep and narrow site with existing infrastructure.

